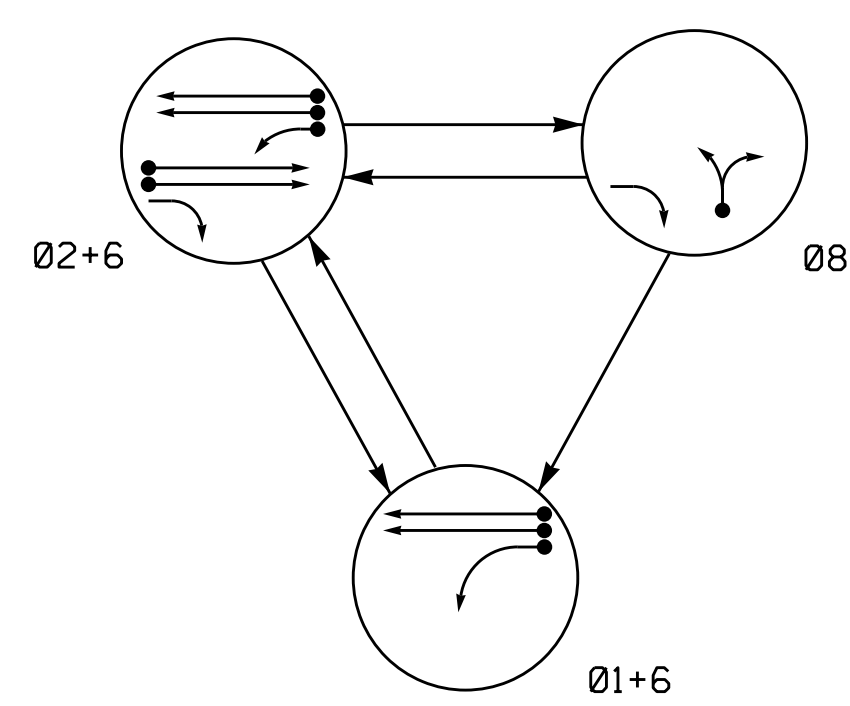
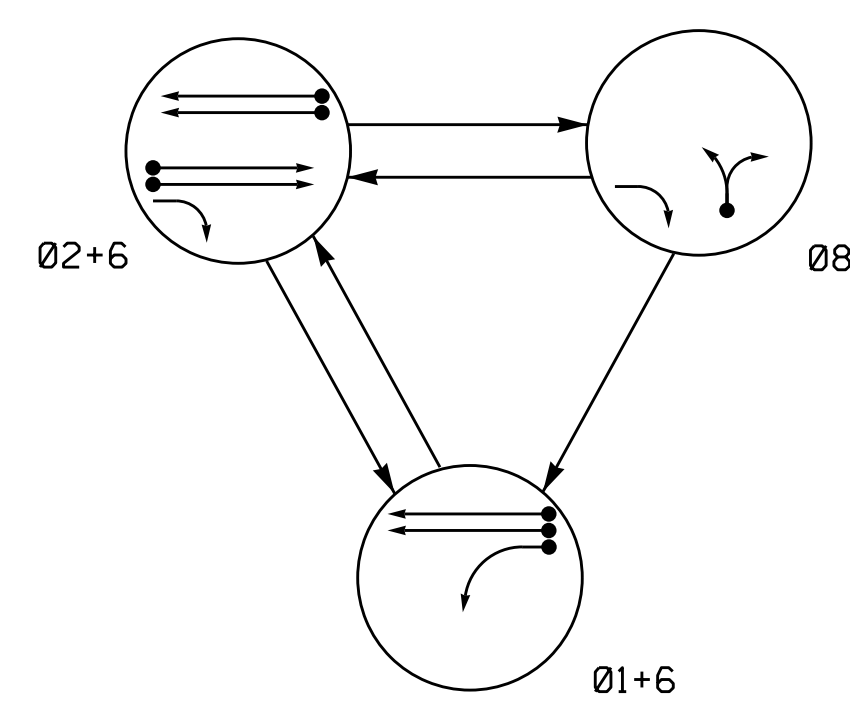


DEFAULT PHASING DIAGRAM



ALTERNATE PHASING DIAGRAM



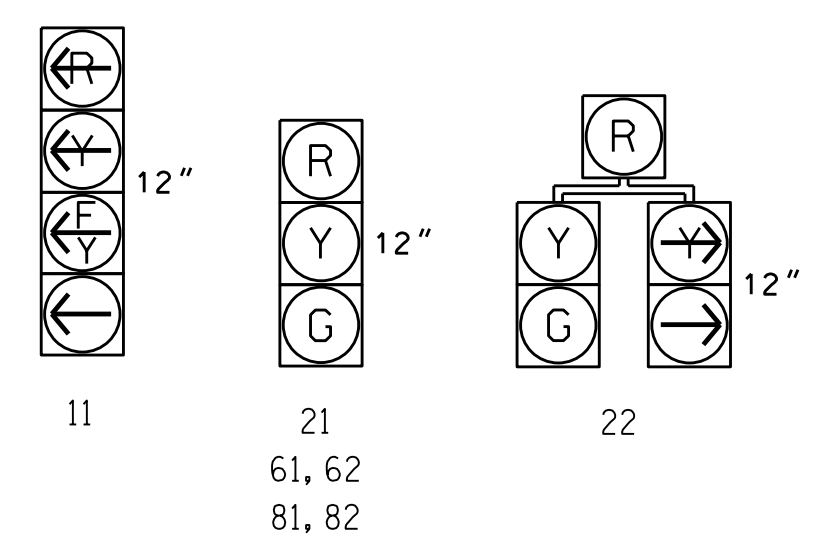
SIGNAL FACE	PHASE			
	01+6	02+6	08	FLASH
11	-	-	-	-
21	R	G	R	Y
22	R	G	R	Y
61,62	G	G	R	Y
81,82	R	R	G	R

SIGNAL FACE	PHASE			
	01+6	02+6	08	FLASH
11	-	-	-	-
21	R	G	R	Y
22	R	G	R	Y
61,62	G	G	R	Y
81,82	R	R	G	R

OASIS 2070 LOOP & DETECTOR INSTALLATION CHART												
INDUCTIVE LOOPS					DETECTOR PROGRAMMING							
LOOP	SIZE (FT)	DISTANCE FROM STOPBAR (FT)	TURNS	NEW LOOP	PHASE	CALLING	EXTENSION	FULL TIME DELAY	STRETCH TIME	DELAY TIME	SYSTEM LOOP	NEW CARD
1A	6X40	0	2-4-2	Y	1	Y	Y	-	-	*20	-	-
2A	6X6	355	4	-	2	Y	Y	-	-	-	-	-
2B	6X6	355	4	-	2	Y	Y	-	-	-	-	-
6A	6X6	355	4	-	6	Y	Y	-	-	-	-	-
6B	6X6	355	4	-	6	Y	Y	-	-	-	-	-
8A	6X40	0	2-4-2	Y	8	Y	Y	-	-	10	-	-
S3	6X6	+160	4	-	-	-	-	-	-	-	Y	-
S4	6X6	+160	4	-	-	-	-	-	-	-	Y	-

SIGNAL FACE I.D.

All Heads L.E.D.



PHASING DIAGRAM DETECTION LEGEND

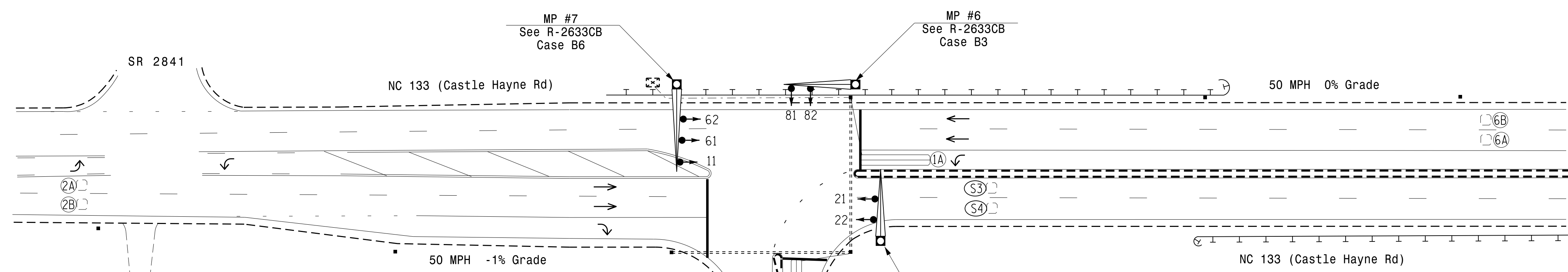
- DETECTED MOVEMENT
- UNDETECTED MOVEMENT (OVERLAP)
- UNSIGNALIZED MOVEMENT
- PEDESTRIAN MOVEMENT

* Disable delay during alternate phasing operation
 ** Disable phase 6 calling during alternate phasing operation

3 Phase Fully Actuated NC 133 (Castle Hayne Rd) CLS

NOTES

- Refer to "Roadway Standard Drawings NCDOT" dated January 2012 and "Standard Specifications for Roads and Structures" dated January 2012.
- Do not program signal for late night flashing operation unless otherwise directed by the Engineer.
- Phase 1 may be lagged.
- Set all detector units to presence mode.
- The Division Traffic Engineer will determine the hours of use for each phasing plan.
- Maximum times shown in timing chart are for free-run operation only. Coordinated signal system timing values supersede these values.
- Closed loop system data: Controller Asset #0929.



FEATURE	PHASE			
	1	2	6	8
Min Green 1 *	7	14	14	7
Extension 1 *	2.0	6.0	6.0	2.0
Max Green 1 *	25	120	120	20
Yellow Clearance	3.0	4.8	4.8	3.0
Red Clearance	2.6	1.0	1.0	2.3
Walk 1 *	-	-	-	-
Don't Walk 1	-	-	-	-
Seconds Per Actuation *	-	1.5	1.5	-
Max Variable Initial *	-	40	40	-
Time Before Reduction *	-	15	15	-
Time To Reduce *	-	30	30	-
Minimum Gap	-	3.1	3.1	-
Recall Mode	-	MIN RECALL	MIN RECALL	-
Vehicle Call Memory	-	YELLOW	YELLOW	-
Dual Entry	-	-	-	-
Simultaneous Gap	ON	ON	ON	ON

* These values may be field adjusted. Do not adjust Min Green and Extension times for phases 2 and 6 lower than what is shown. Min Green for all other phases should not be lower than 4 seconds.

PROPOSED	LEGEND	EXISTING
○	Traffic Signal Head	●
○	Modified Signal Head	N/A
○	Sign	○
○	Pedestrian Signal Head With Push Button & Sign	○
○	Signal Pole with Guy	○
○	Signal Pole with Sidewalk Guy	○
○	Metal Pole with Mastarm	○
○	Inductive Loop Detector	○
○	Controller & Cabinet Junction Box	○
○	2-in Underground Conduit	○
N/A	Right of Way	○
○	Directional Arrow	○
N/A	Guard Rail	○
○	2-2" Rigid Metal Conduit	○

Signal Upgrade

Prepared In the Offices of:

750 N. Greenfield Pkwy, Garner, NC 27529

NC 133 (Castle Hayne Road) at I-140-US 17 EB Ramps

Division 3 New Hanover County, Wilmington

PLAN DATE: January 2016 REVIEWED BY: PLA, PE

PREPARED BY: EM Minshew REVIEWED BY:

REVISIONS: INIT. DATE

SCALE: 0 40 1"=40'

DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED

SEAL: PROFESSIONAL ENGINEER, STATE OF NORTH CAROLINA, SEAL 023489, ENGINEER, ALEXANDER

DocuSigned by: P. Alexander 2/16/16

SIG. INVENTORY NO. 03-0929

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 emminshew