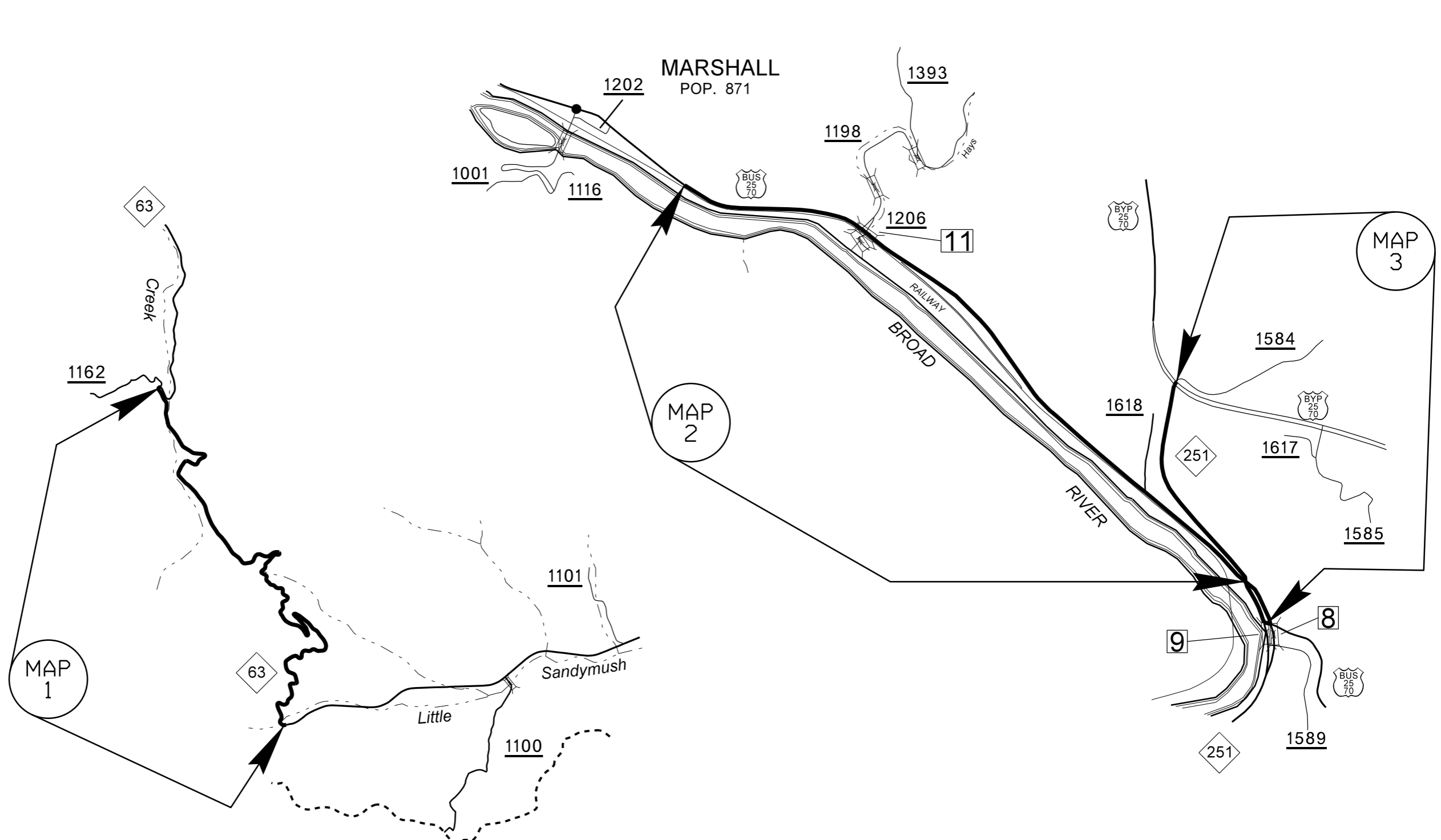


**This electronic collection of documents is provided  
for the convenience of the user  
and is Not a Certified Document –**

**The documents contained herein were originally issued  
and sealed by the individuals whose names and license  
numbers appear on each page, on the dates appearing  
with their signature on that page.**

**This file or an individual page  
shall not be considered a certified document.**

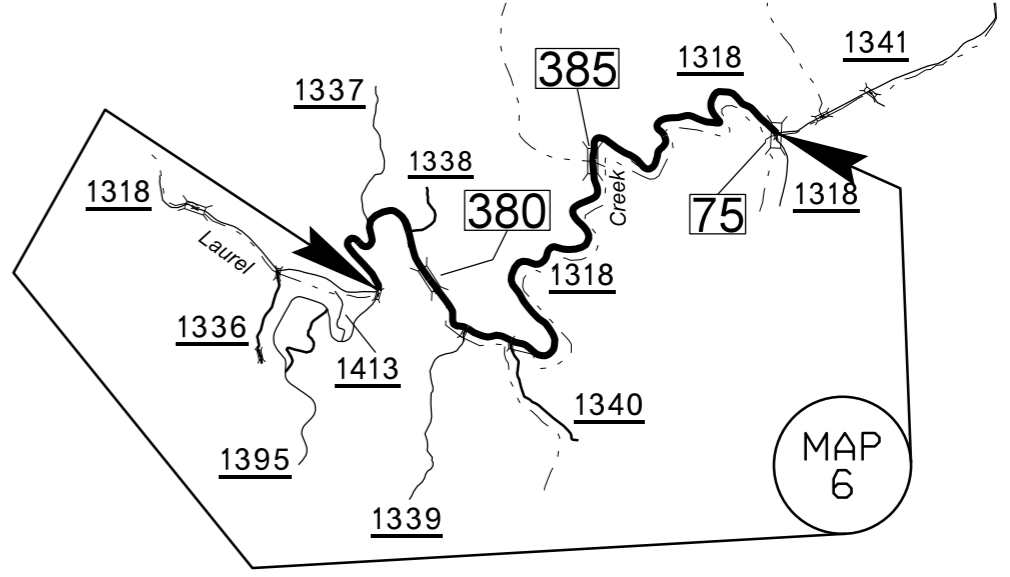
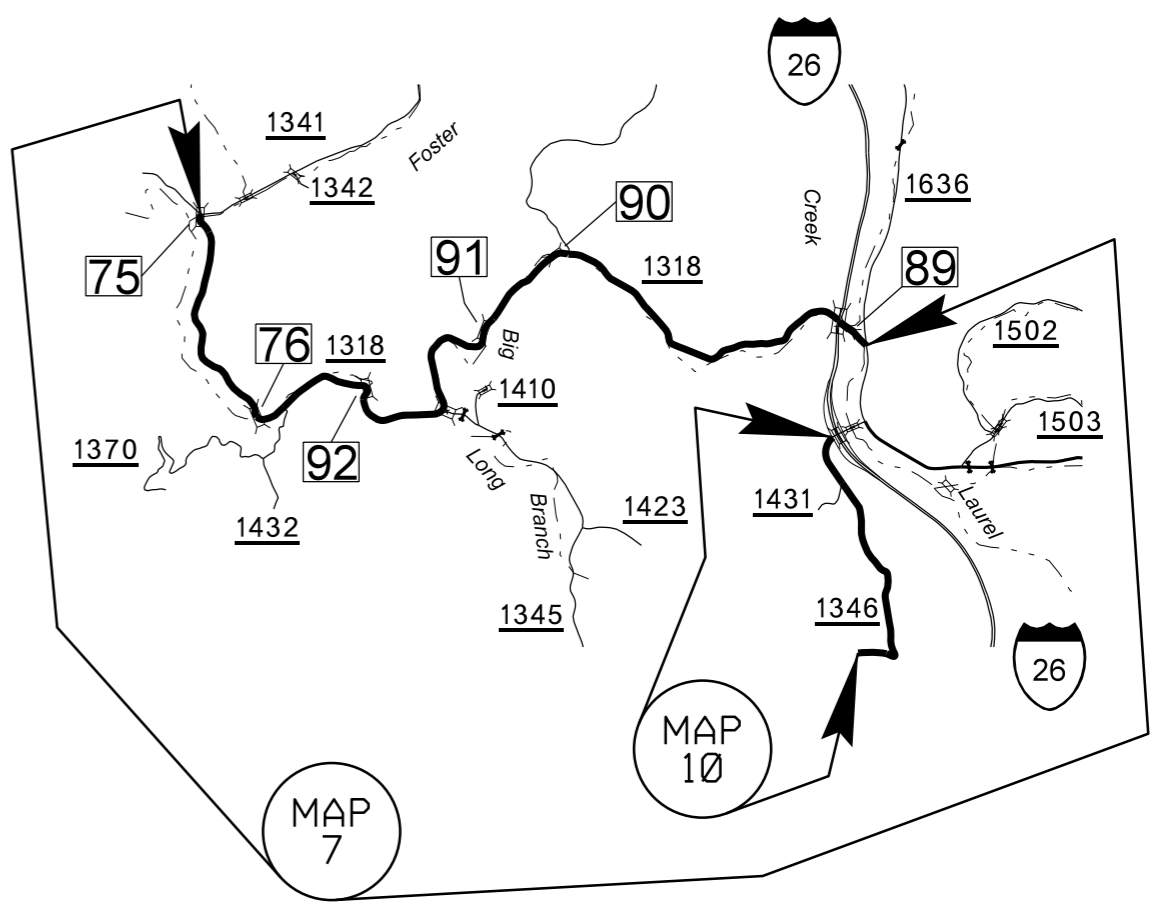
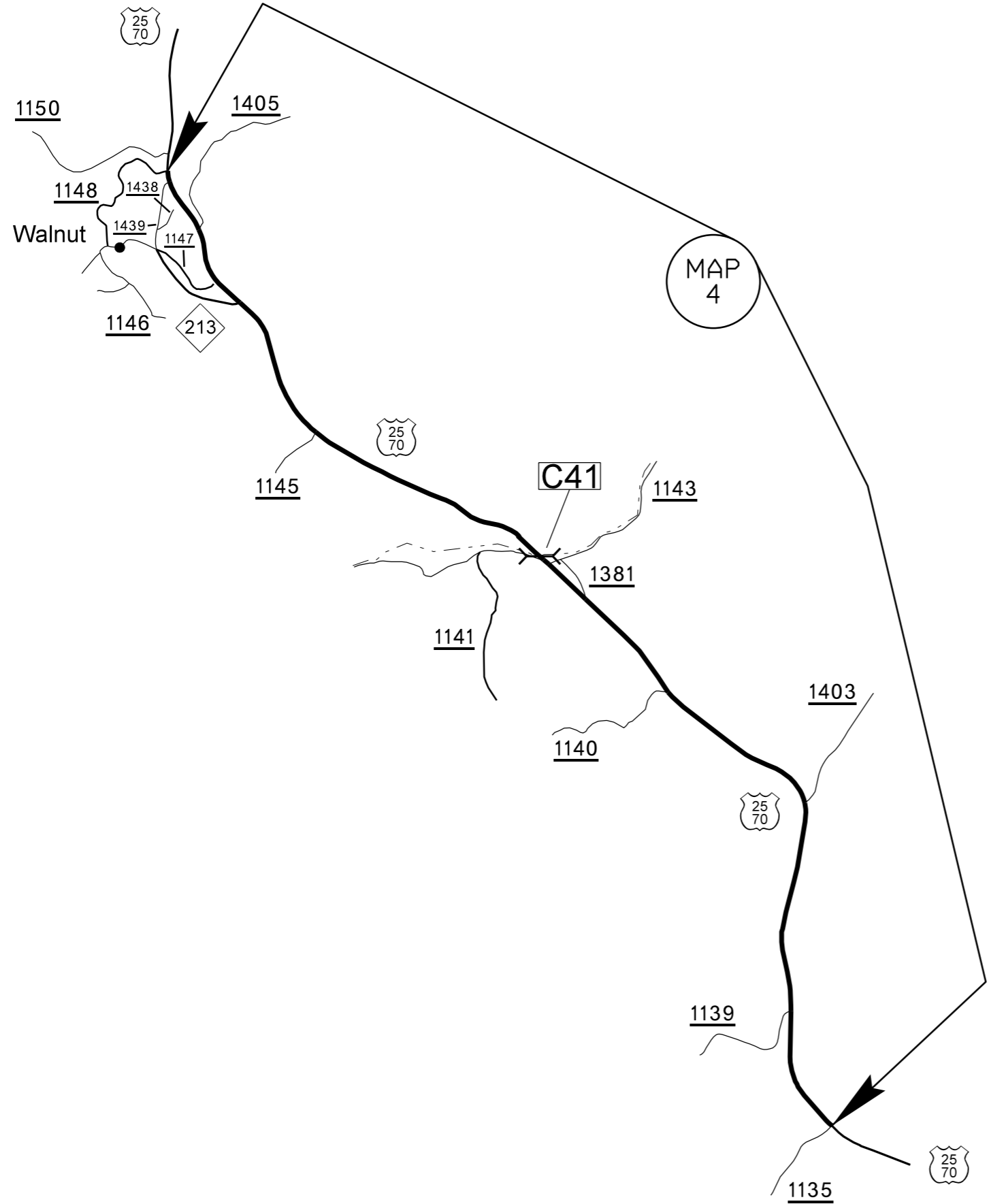
PROJECT NO.	SHEET NO.	TOTAL SHEETS
2017CPT.13.03.10571, 2017CPT.13.03.20571, 2017CPT.13.03.20572	1	



BUNCOMBE COUNTY

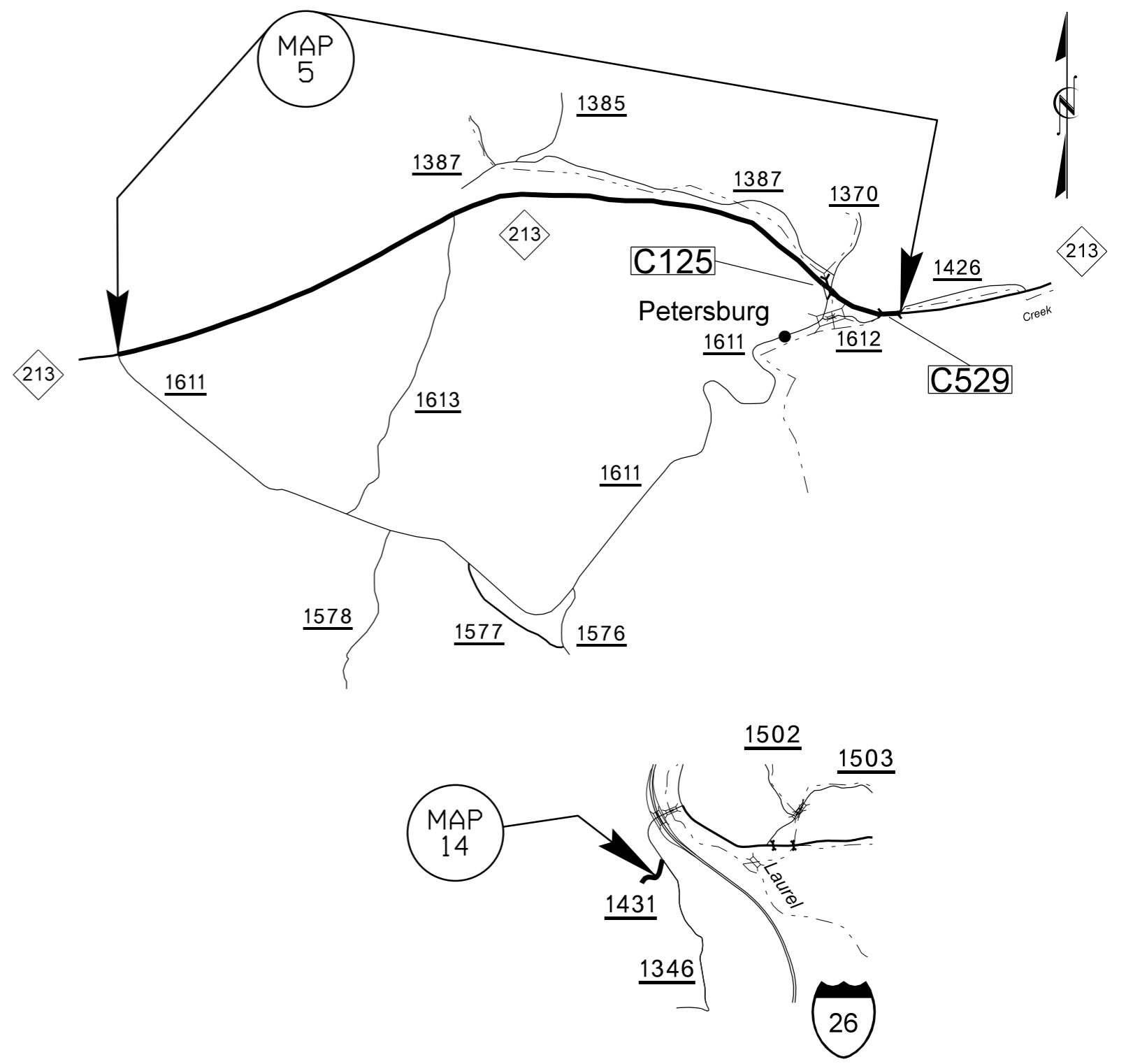
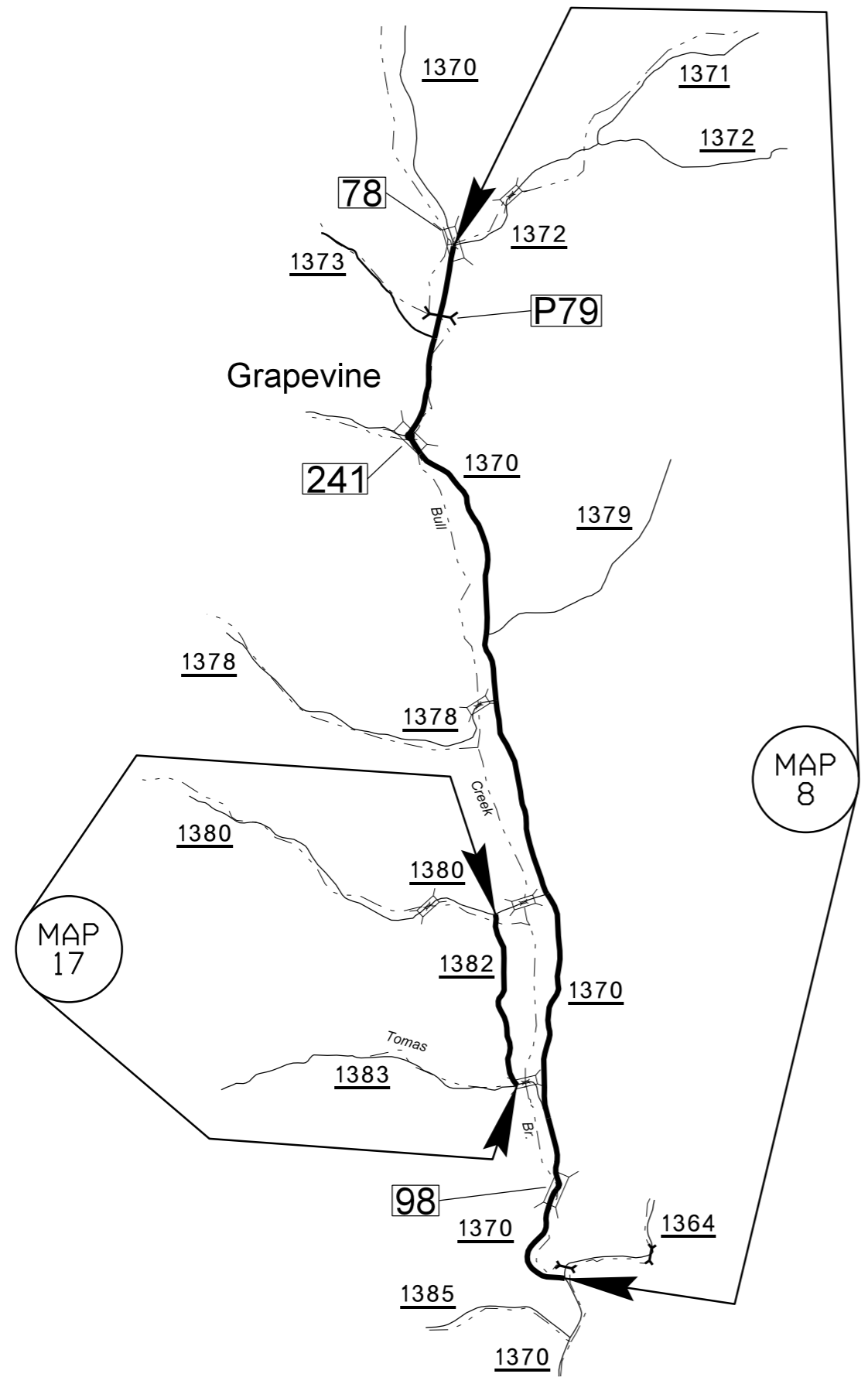
**MADISON COUNTY**

PROJECT NO.	SHEET NO.	TOTAL SHEETS
2017CPT.13.03.10571, 2017CPT.13.03.20571, 2017CPT.13.03.20572	2	



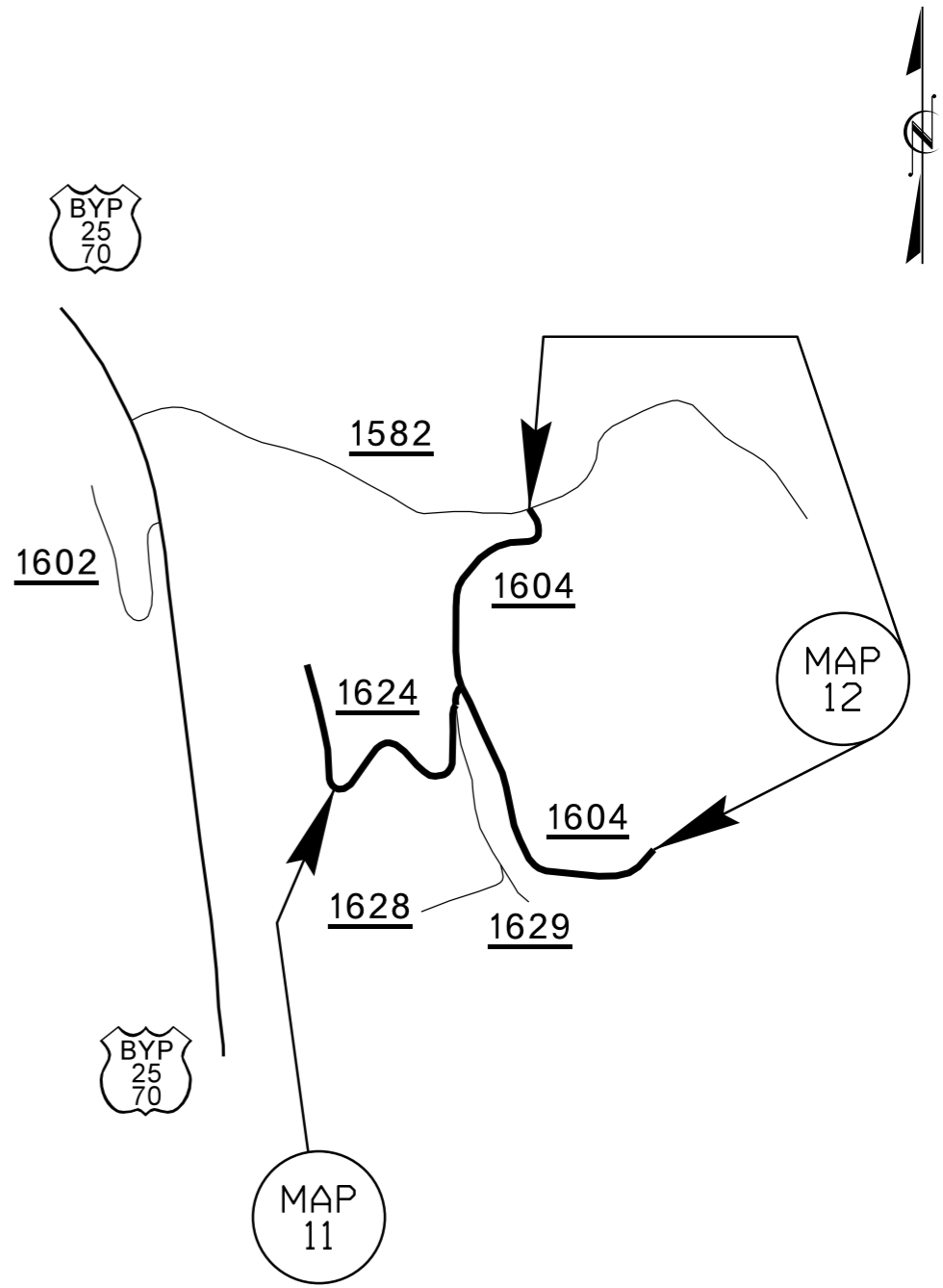
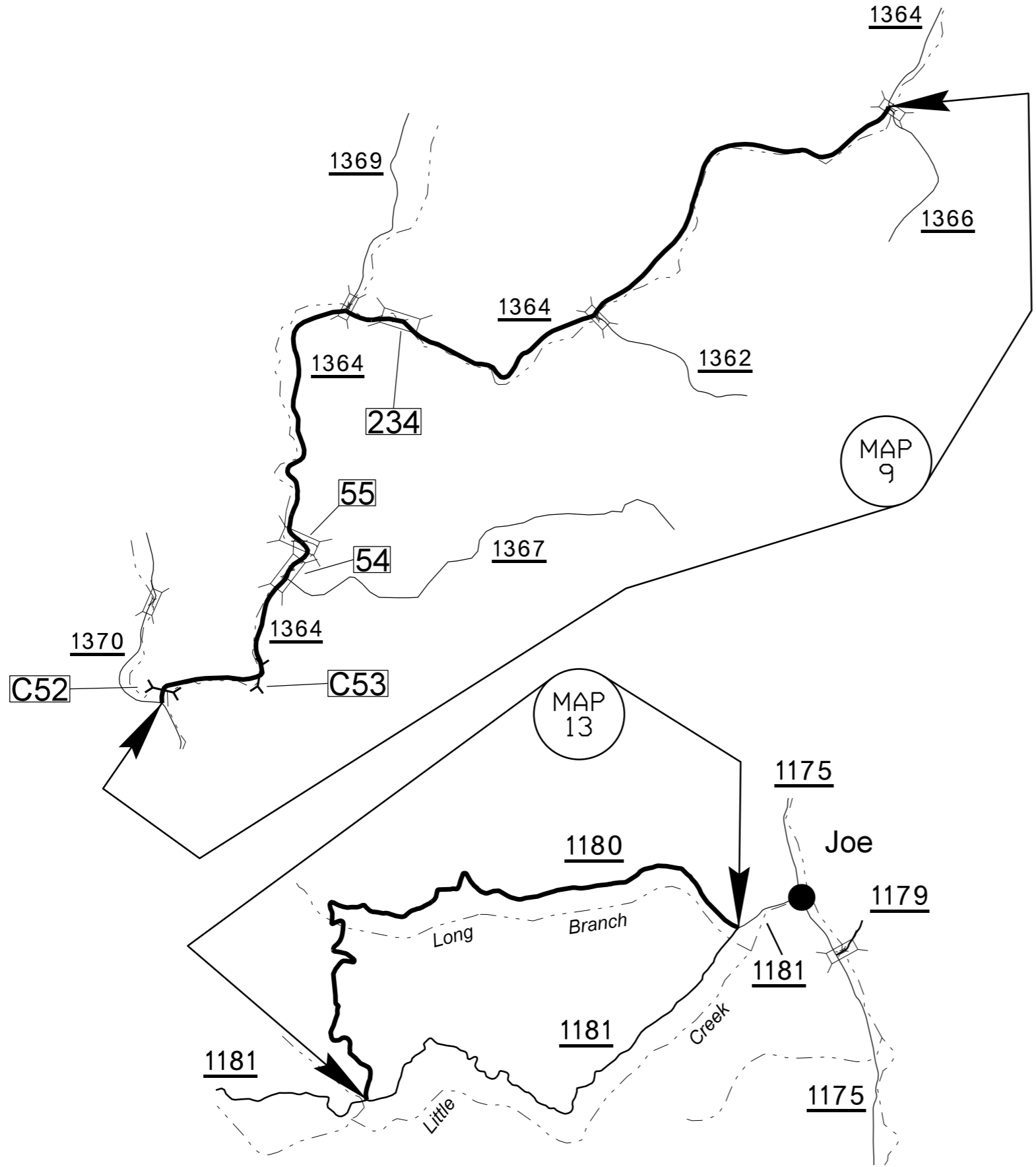
**MADISON COUNTY**

PROJECT NO.	SHEET NO.	TOTAL SHEETS
2017CPT.13.03.10571, 2017CPT.13.03.20571, 2017CPT.13.03.20572	3	



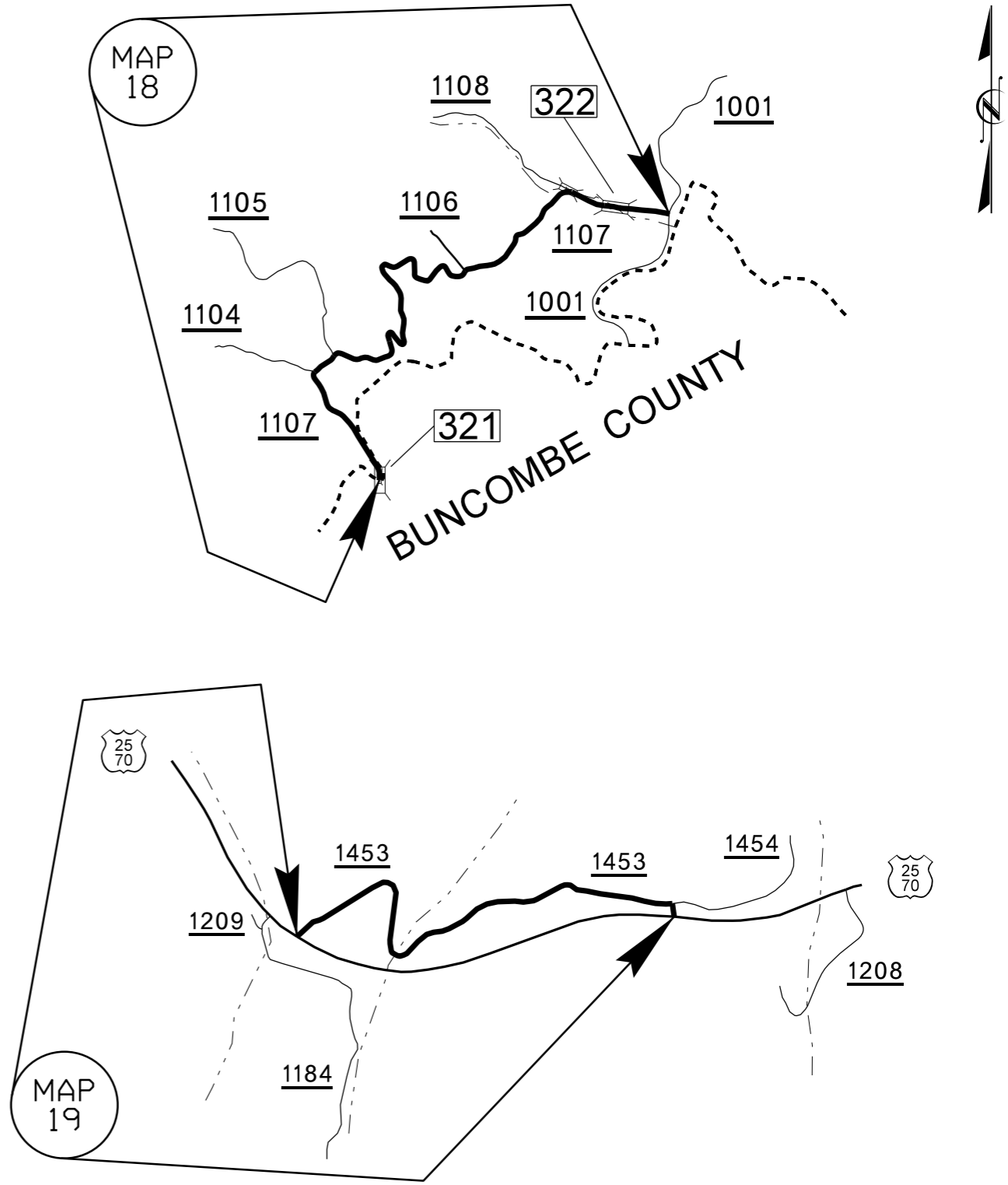
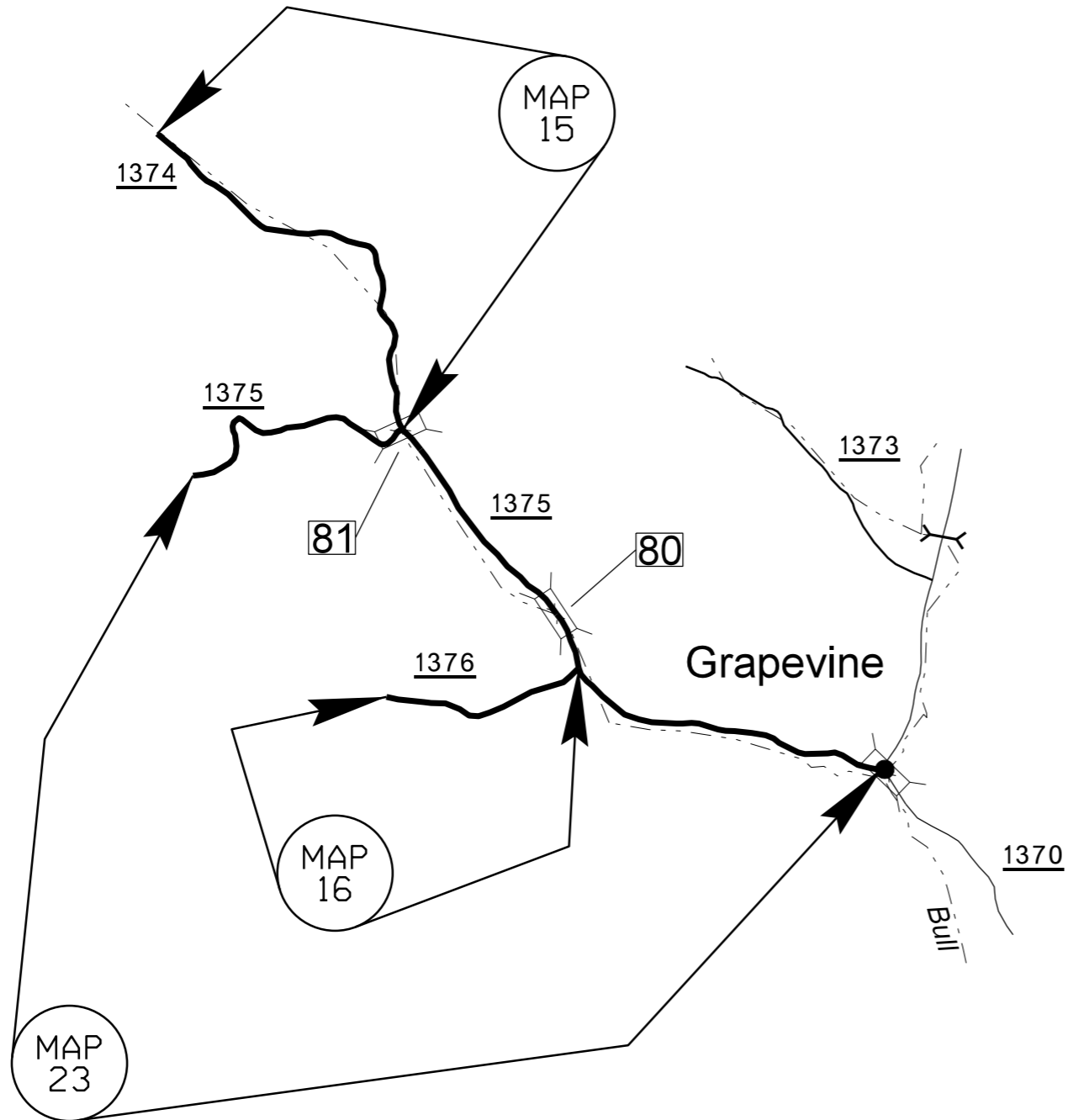
**MADISON COUNTY**

PROJECT NO.	SHEET NO.	TOTAL SHEETS
2017CPT.13.03.10571, 2017CPT.13.03.20571, 2017CPT.13.03.20572	4	



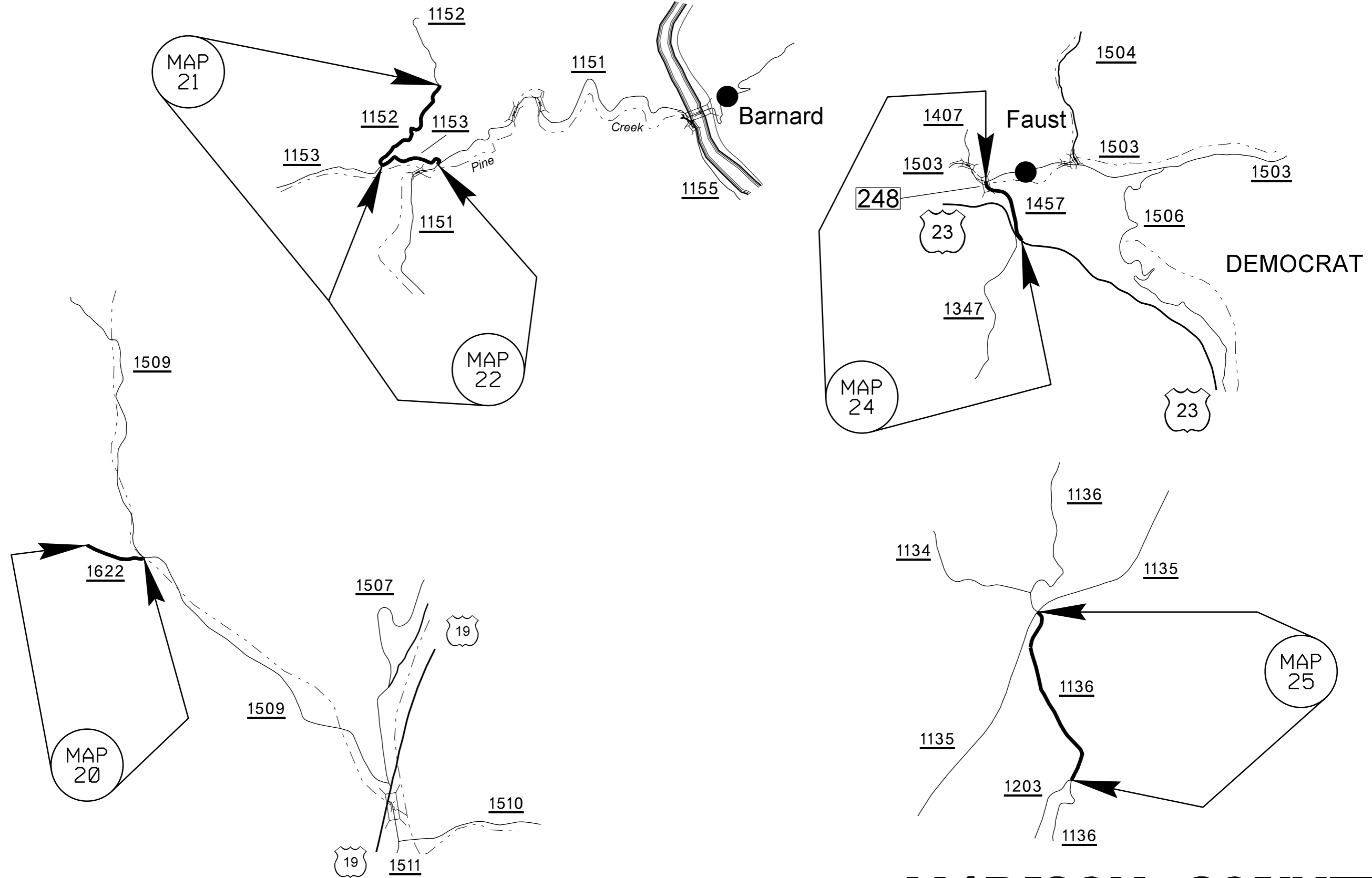
**MADISON COUNTY**

PROJECT NO.	SHEET NO.	TOTAL SHEETS
2017CPT.13.03.10571, 2017CPT.13.03.20571, 2017CPT.13.03.20572	5	



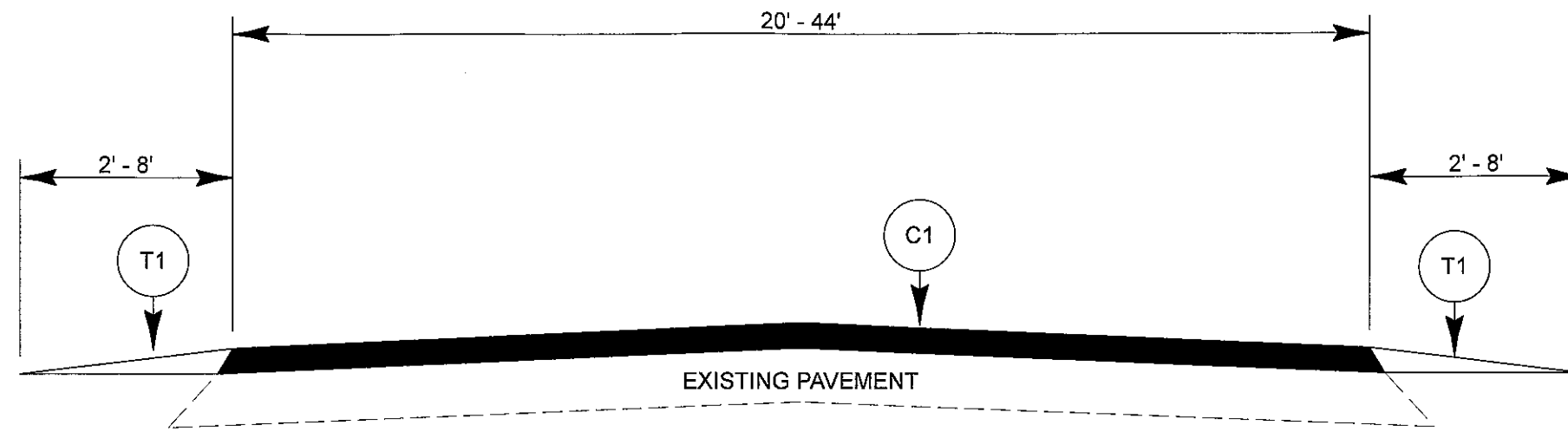
**MADISON COUNTY**

PROJECT NO.	SHEET NO.	TOTAL SHEETS
2017CPT.13.03.10571, 2017CPT.13.03.20571, 2017CPT.13.03.20572	6	

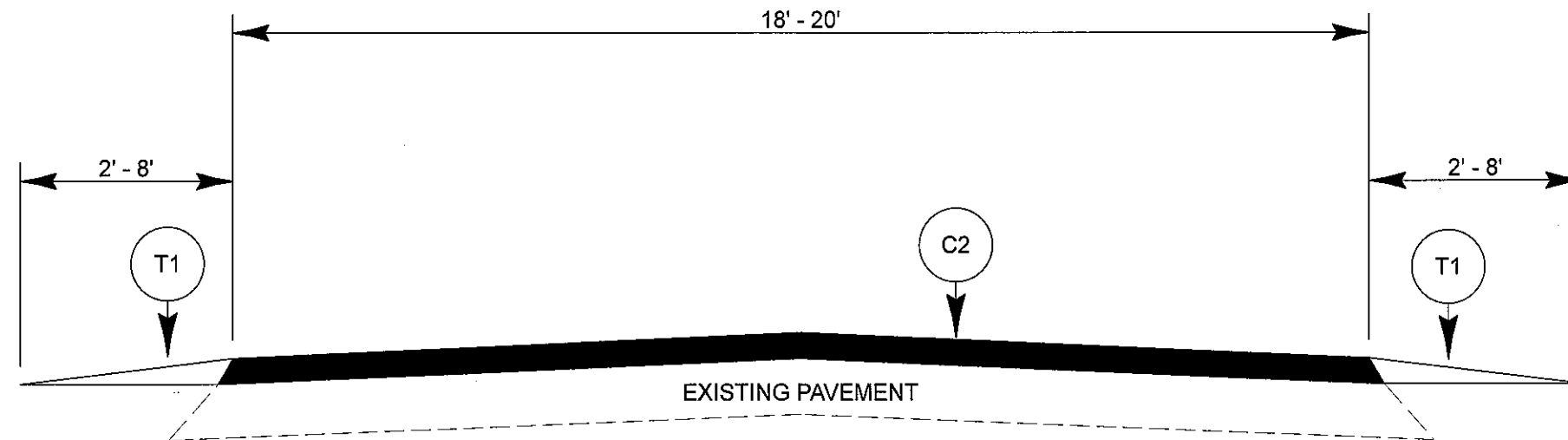


**MADISON COUNTY**

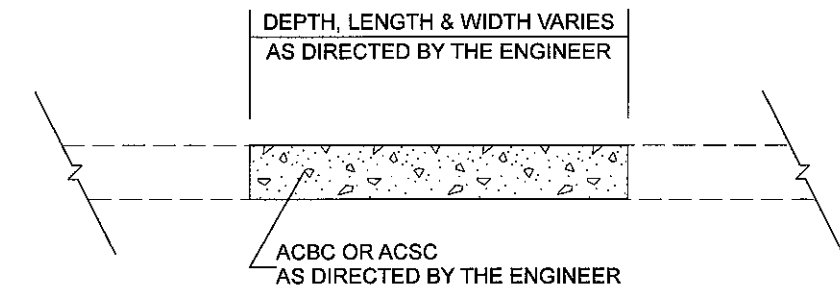
PROJECT NO. 2017CPT.13.03.10571, 2017CPT.13.03.20571, 2017CPT.13.03.20572,	SHEET NO. 7	TOTAL SHEETS
---	----------------	--------------



TYPICAL SECTION NO. 1



TYPICAL SECTION NO. 2

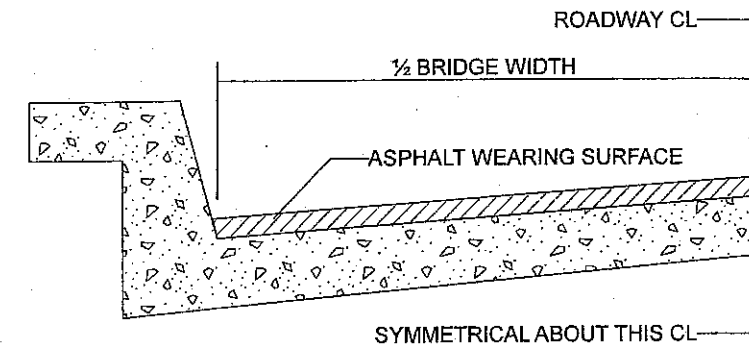
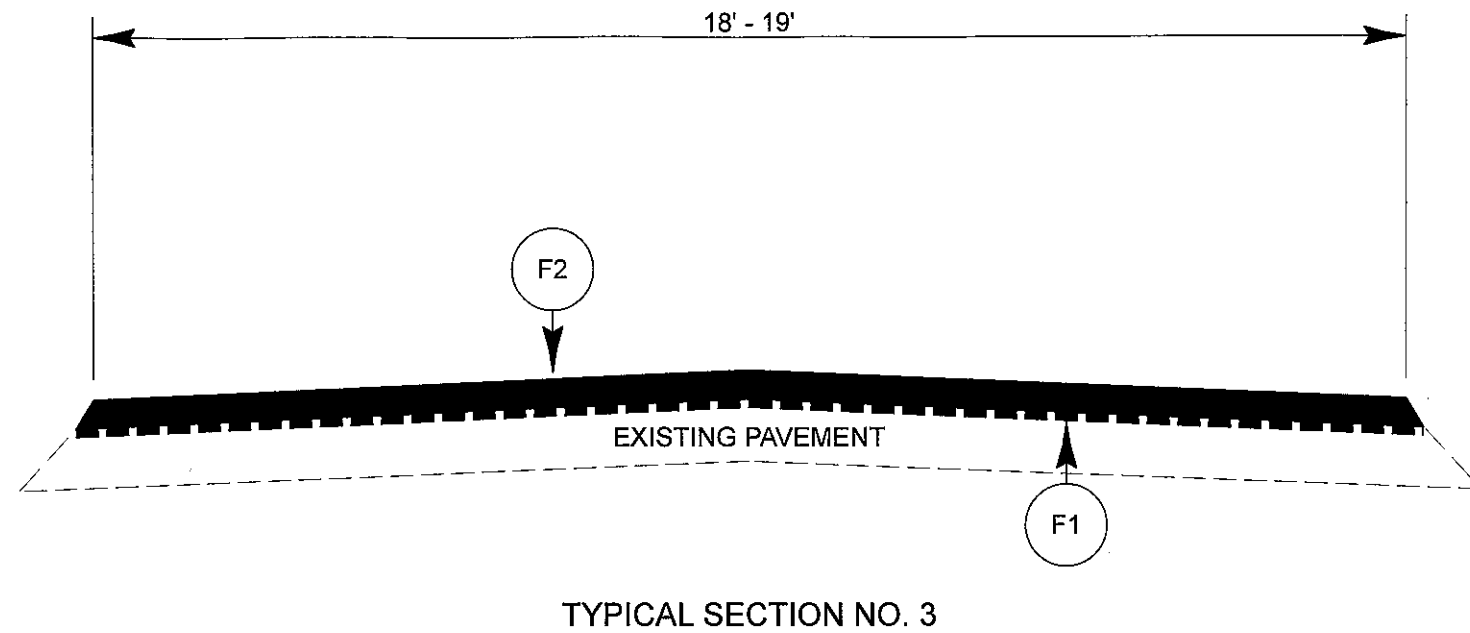


PATCHING EXISTING PAVEMENT

PAVEMENT SCHEDULE	
C1	PROP. APPROX. 1-1/2" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YARD
C2	PROP. APPROX. 1-1/2" ASPHALT CONCRETE SURFACE COURSE, TYPE SF9.5A, AT AN AVERAGE RATE OF 165 LBS. PER SQ. YARD
F1	ASPHALT SURFACE TREATMENT, DOUBLE SEAL
F2	ASPHALT SURFACE TREATMENT, FOG SEAL
T1	SHOULDER RECONSTRUCTION
V1	MILLING ASPHALT PAVEMENT, 0 TO 1-1/2" DEPTH
V2	INCIDENTAL MILLING



PROJECT NO.	SHEET NO.	TOTAL SHEETS
2017CPT.13.03.10571, 2017CPT.13.03.20571, 2017CPT.13.03.20572,	8	



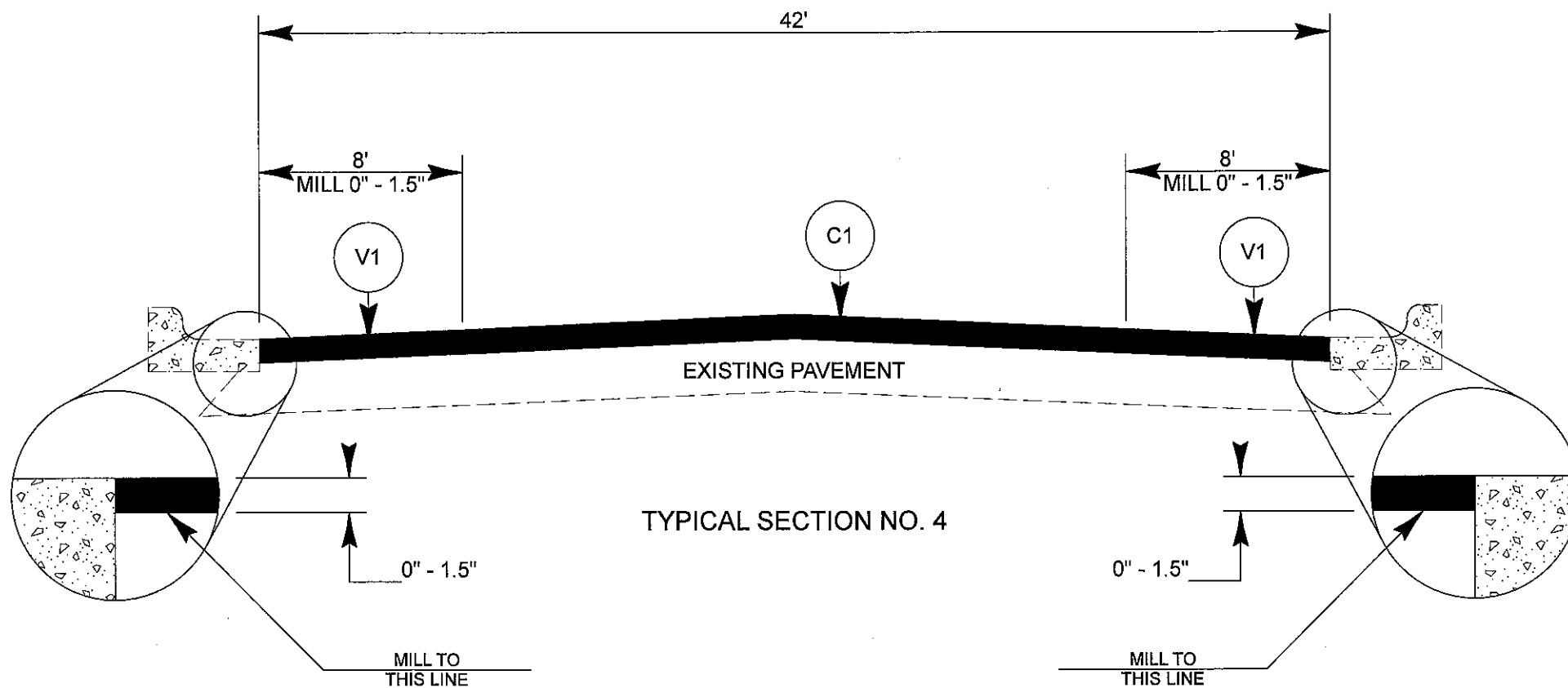
**BRIDGE HALF TYPICAL SECTION**

FOR BRIDGES WITH FLOOR DRAINS, CARE SHALL BE EXERCISED IN PLACING THE WEARING SURFACE AROUND FLOOR DRAINS SO AS NOT TO HINDER EFFECTIVE DRAINAGE. ALL DRAINS SHALL BE LEFT OPEN

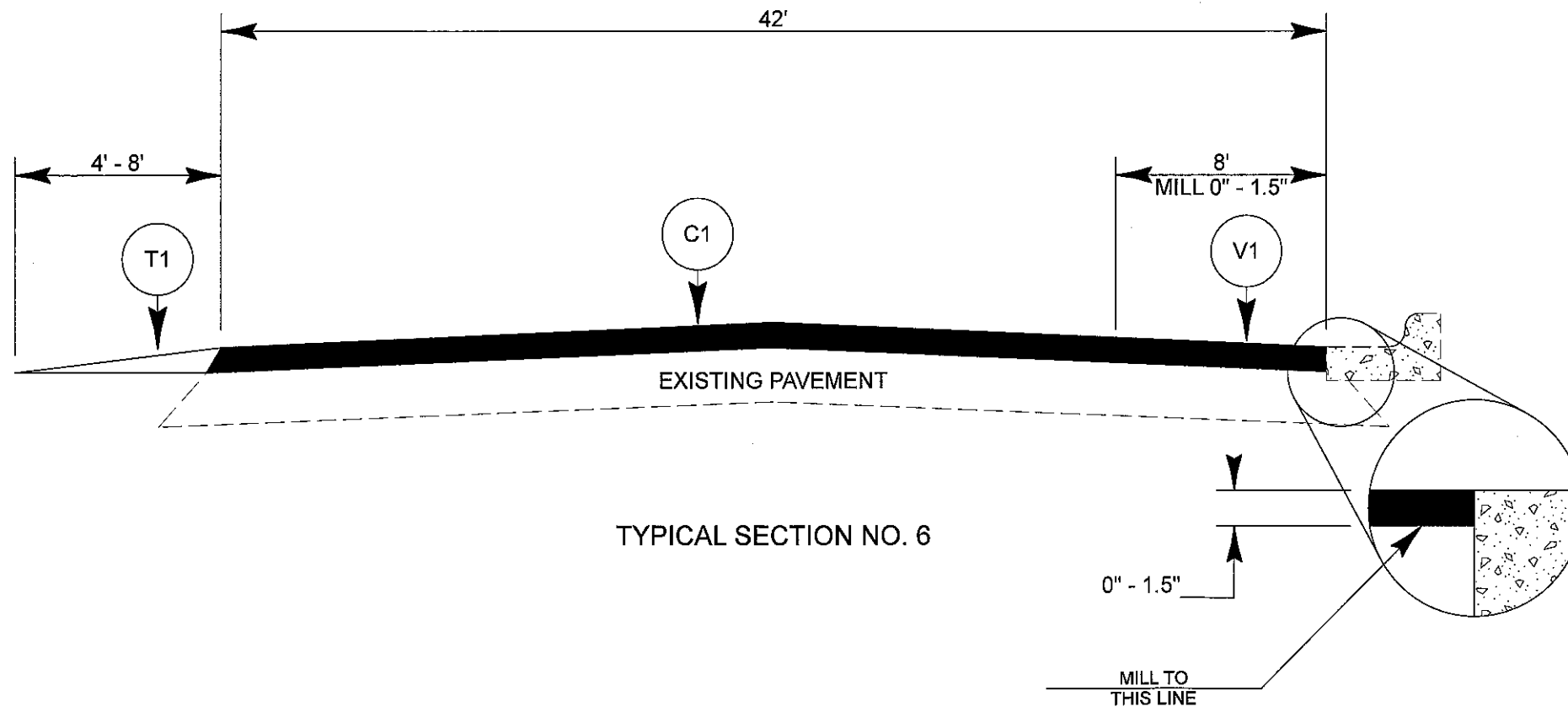
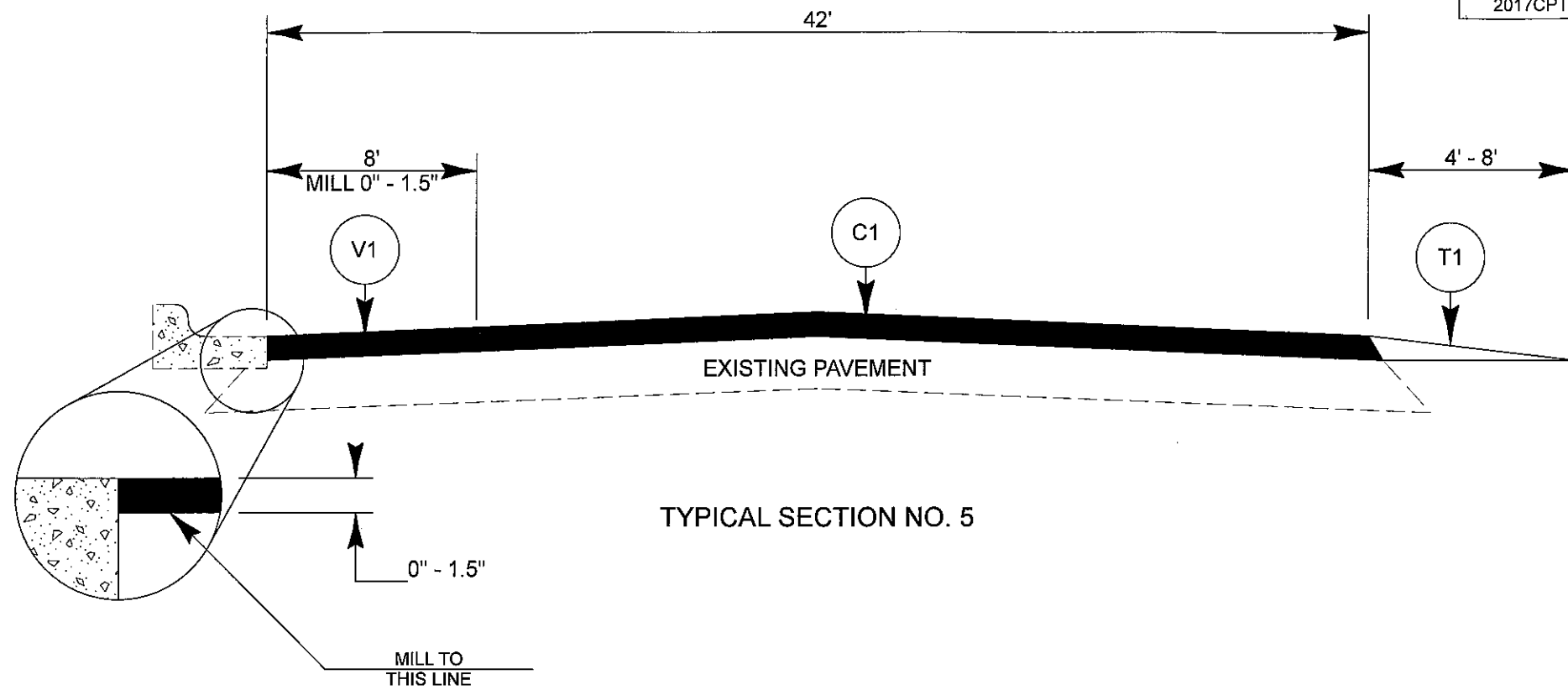
THE PROPOSED WEARING SURFACE SHALL VARY IN THICKNESS AS NECESSARY TO PROVIDE A SMOOTH RIDING SURFACE. THE MINIMUM THICKNESS SHOULD DEPEND ON PAVEMENT TYPE AS FOLLOWS: S4.75A 1/2", SF9.5A 1.0", S9.5X 1.5", S12.5X 2.0", ULTRATHIN HOT MIX ASPHALT-TYPE A 1/2", ULTRATHIN HOT MIX ASPHALT-TYPE B 5/8", ULTRATHIN HOT MIX ASPHALT-TYPE C 1/2". THE MAXIMUM THICKNESS SHOULD DEPEND ON PAVEMENT TYPE AS FOLLOWS: S4.75A 1.0", SF9.5A 1.5", S9.5X 2.0", S12.5X 2.0", ULTRATHIN HOT MIX ASPHALT-TYPE A 1/2", ULTRATHIN HOT MIX ASPHALT-TYPE B 5/8", ULTRATHIN HOT MIX ASPHALT-TYPE C 1/2".

**NOTES**

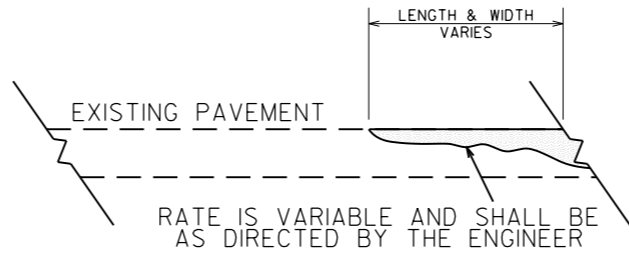
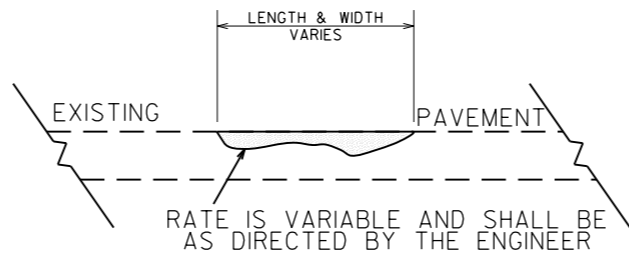
ALL UNPAVED ROADS TO BE RESURFACED 50' FROM EDGE OF PAVEMENT OF MAIN PROJECT.  
 ALL PAVED S.R. ROADS TO BE RESURFACED TO THE ENDS OF THE RADII, OR AS DIRECTED BY THE ENGINEER.  
 EDGES, PAVEMENT WIDENING, INTERSECTIONS AND BRIDGE FLARES ARE INCLUDED IN THE TABLE OF QUANTITIES.  
 SHOULDERS AND DITCHES ARE TO BE CONSTRUCTED BY OTHERS UNLESS OTHERWISE INDICATED.  
 BRIDGES ARE TO BE RESURFACED AT LOCATIONS AND TO DEPTH AS DIRECTED BY THE ENGINEER.



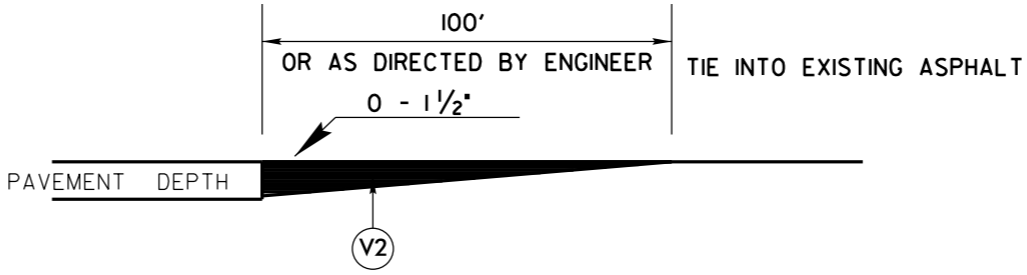
PROJECT NO.	SHEET NO.	TOTAL SHEETS
2017CPT.13.03.10571, 2017CPT.13.03.20571, 2017CPT.13.03.20572,	9	



PROJECT NO.	SHEET NO.	TOTAL SHEETS
2017CPT.13.03.10571, 2017CPT.13.03.20571, 2017CPT.13.03.20572	10	



**DETAIL SHOWING  
METHOD OF WEDGING**

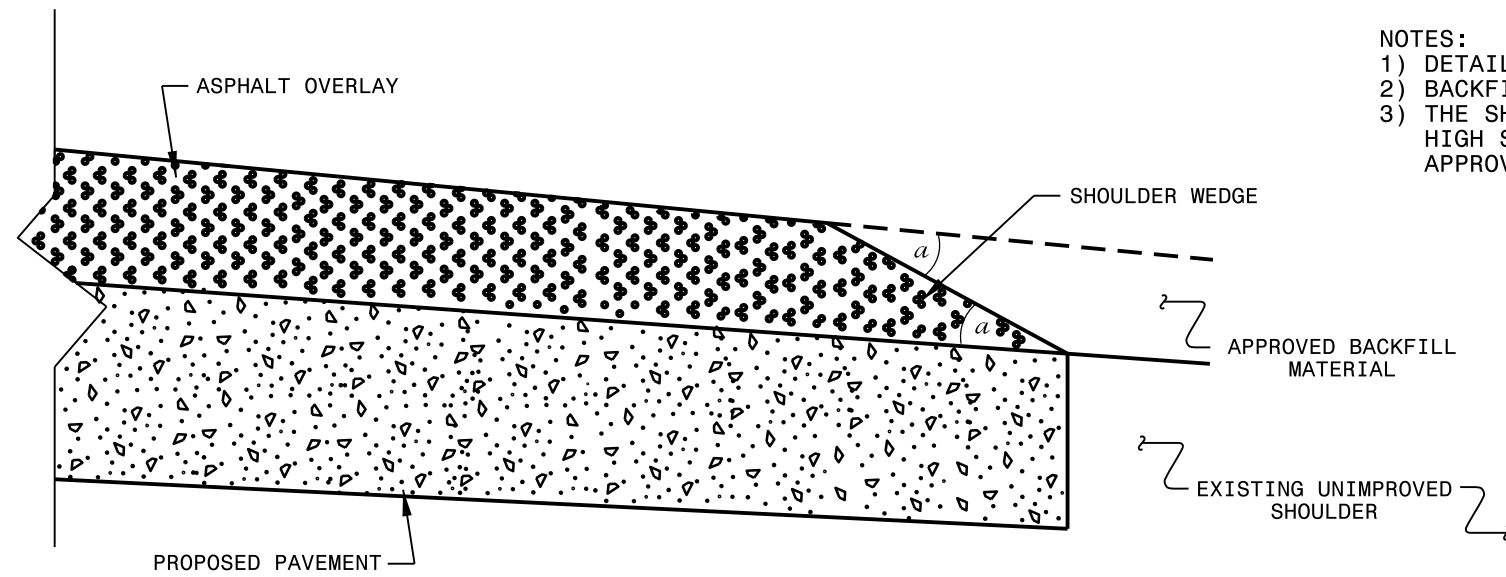


**DETAIL TO TIE INTO EXIST PAVEMENT**

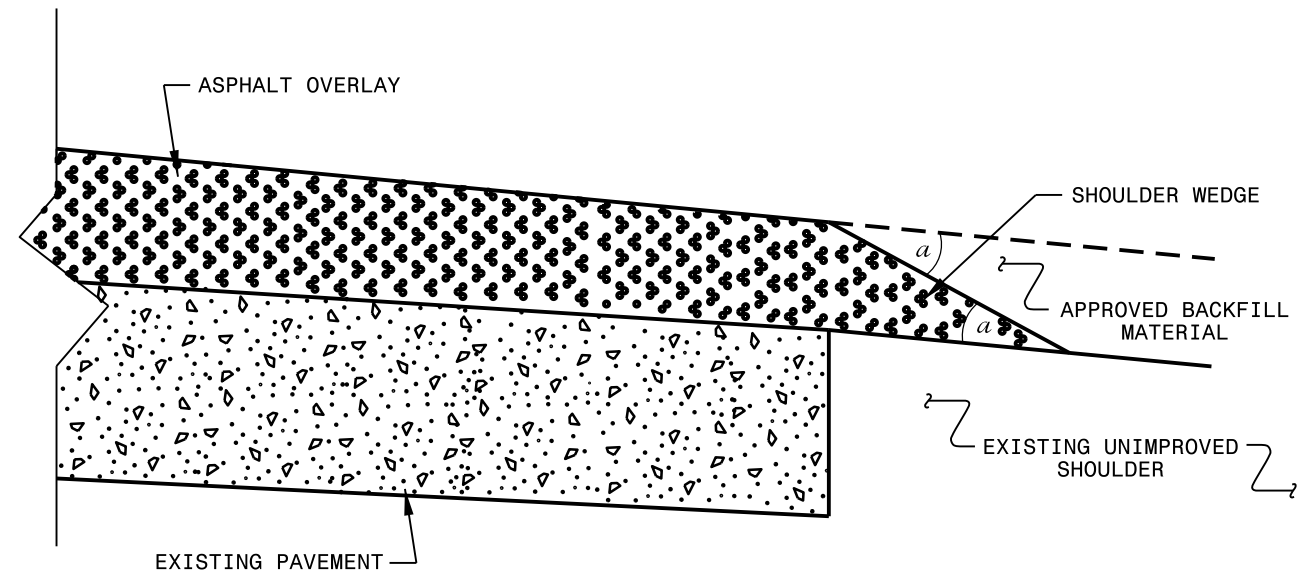
**THE CONTRACTOR'S ATTENTION IS DIRECTED TO THE FACT THAT HE WILL BE REQUIRED TO MILL THE EXISTING ASPHALT PAVEMENT TO ENSURE A PROPER TIE-IN WITH THE EXISTING SURFACE AT THE BEGINNING, END AND Y LINES OF EACH MAP TO BE RESURFACED WITH ASPHALT CONC SURFACE COURSE, TYPE S9.5B. THIS WILL BE PAID FOR AS INCIDENTAL MILLING.**

**NOTES:**

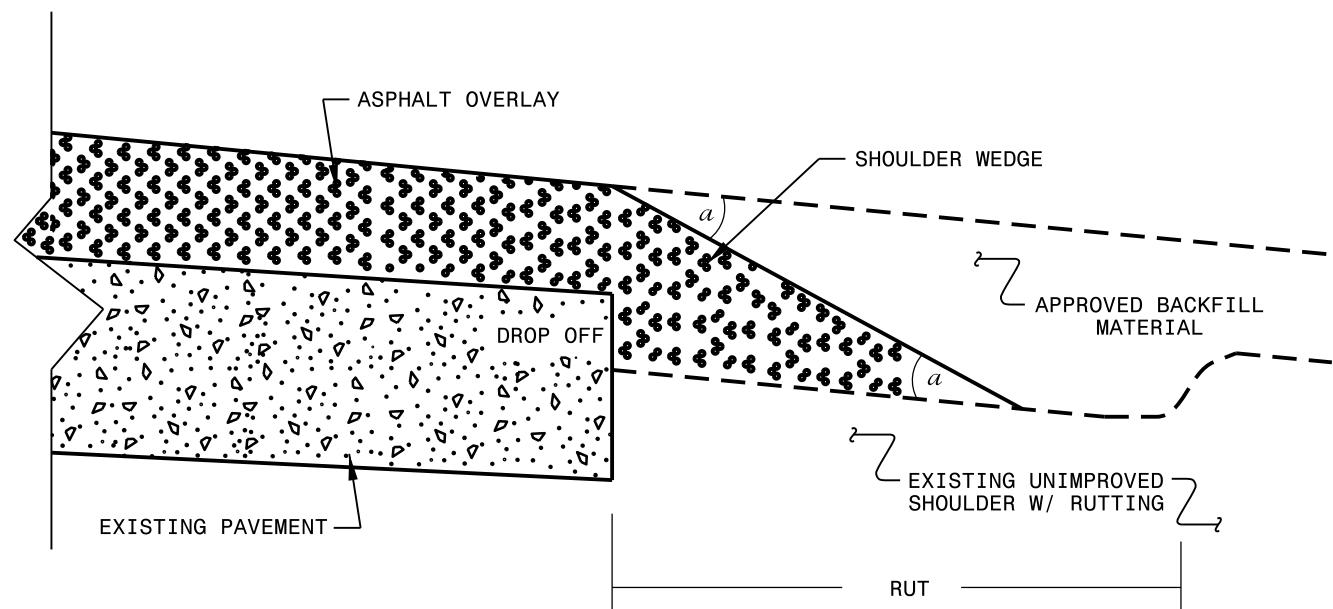
- 1) DETAIL DOES NOT APPLY TO OGAFB AND ULTRA-THIN BONDED WEARING COURSE.
- 2) BACKFILL SHOULDER WITH APPROVED MATERIAL.
- 3) THE SHOULDER WEDGE DEVICE MAY BE DISENGAGED AT PAVED DRIVEWAYS, SIDE STREETS, HIGH SHOULDERS, AND OTHER LOCATIONS NOT FEASIBLE TO CONSTRUCT AS APPROVED BY THE ENGINEER.



**SHOULDER WEDGE DETAIL**  
(Resurfacing Projects w/ Widening or  
with Existing Paved Shoulder having no dropoffs)



**SHOULDER WEDGE DETAIL**  
(Resurfacing Projects w/ NO Widening)



**SHOULDER WEDGE DETAIL**  
(Resurfacing Adjacent to  
Rutted Shoulder)

- SHOULDER WEDGE ANGLE = 30°

<b>CONTRACT STANDARDS AND DEVELOPMENT UNIT</b>			
Office 919-707-6950		FAX 919-250-4119	
<b>SHOULDER WEDGE DETAILS</b>			
ORIGINAL BY: T.SPELL	DATE: 7-19-11		
MODIFIED BY:	DATE: 2/2/16		
CHECKED BY:	DATE:		
FILE SPEC.: susr/details/stand/shoulderwedgedetail.dgn			

Q:\JUN-2016 1207  
 S:\Projects\Resurfacing\Projects\Shoulder Wedge Details\Revised Shoulder Wedge Detail.dgn  
 \$\$\$USERNAME\$\$\$

PROJECT NO.	SHEET NO.	TOTAL NO.
2017CPT.13.03.10571, 2017CPT.13.03.20571, 2017CPT.13.03.20572	12	

### SUMMARY OF QUANTITIES

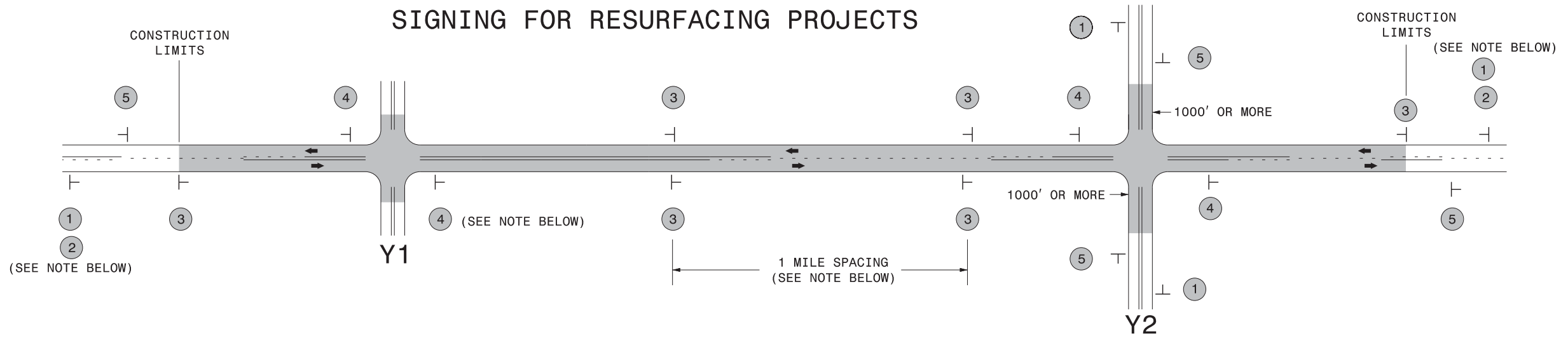
PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE TYPE	FINAL SURFACE TESTING REQUIRED	WARM MIX ASPHALT REQUIRED	LENGTH MI	WIDTH FT	INCIDENTAL STONE BASE TON	SHOULDER RECONSTRUCTION SMI	MILLING ASPHALT PAVEMENT, 0" TO 1-1/2" DEPTH SY	INCIDENTAL MILLING SY	ASPHALT CONC SURFACE COURSE, TYPE S9.5B TON	ASPHALT CONC SURFACE COURSE, TYPE SF9.5A TON	ASPHALT BINDER FOR PLANT MIX TON	PATCHING EXISTING PAVEMENT TON	ASPHALT SURFACE TREATMENT, DOUBLE SEAL SY	ASPHALT SURFACE TREATMENT, FOG SEAL SY	EMULSION FOR ASPHALT SURFACE TREATMENT GAL		
2017CPT.13.03.10571	Madison	1	NC 63	FROM 1.5 MILES WEST OF SR 1100 TO SR 1162 (MP 4.12 - MP 8.22)	1	2	2WU	NO	NO	4.1	20	205	8.20		500	4,473		268	55					
		2	US 25/70 BUS	FROM NC 251 TO PVMT CHANGE AT MARSHALL (MP 1.47 - MP 3.55)	1	2	2WU	NO	NO	2.08	20	104	4.16		500	2,269		136	25					
		3	NC 251	FROM BRIDGE NO.'S 8 & 9 TO US 25/70 (MP 4.10 - MP 4.91)	1	2	2WU	NO	NO	0.81	44	41	1.62		1,655	1,937		116	25					
		4	US 25/70	FROM SR 1135 TO SR 1439 (MP 7.34 - MP 10.76)	1	2	2WU	NO	NO	3.42	30	171	6.84		950	5,585		335	25					
		5	NC 213	FROM SR 1611 TO SR 1426 (MP 7.92 - MP 9.92)	1,4,5,6	3	MU	NO	NO	2	42	100	3.23		5,000	1,500	4,567		274	20				
<b>TOTAL FOR PROJ NO. 2017CPT.13.03.10571</b>										<b>12.41</b>		<b>621</b>	<b>24.05</b>		<b>5,105</b>	<b>18,831</b>		<b>1,129</b>	<b>150</b>					
2017CPT.13.03.20571	Madison	6	SR 1318	FROM SR 1395 TO SR 1341 (MP 7.94 - MP 12.80)	2	2	2WU	NO	NO	4.86	18	243	9.72				4,690	314	50					
		7	SR 1318	FROM SR 1341 TO SR 1636 (MP 12.80 - MP 18.20)	2	2	2WU	NO	NO	5.4	20	270	10.80				5,786	388	40					
		8	SR 1370	FROM SR 1364 TO SR 1372 (MP 1.28 - MP 4.45)	2	2	2WU	NO	NO	3.17	18	159	6.34				3,059	205	40					
		9	SR 1364	FROM SR 1370 TO SR 1366 (MP 0.00 - MP 3.10)	2	2	2WU	NO	NO	3.1	18	155	6.20				2,991	200	30					
		10	SR 1346	FROM I-26 TO EOM (MP 0.00 - MP 1.30)	2	2	2WU	NO	NO	1.3	18	65	2.60				1,254	84	20					
		11	SR 1624	FROM SR 1604 TO EOM (MP 0.00 - MP 0.33)	2	2	2WU	NO	NO	0.33	20	17	0.66				354	24	10					
		12	SR 1604	FROM SR 1582 TO EOM (MP 0.00 - MP 0.45)	2	2	2WU	NO	NO	0.45	20	23	0.90				482	32	20					
		13	SR 1180	FROM SR 1181 TO SR 1181 (MP 0.00 - MP 3.23)	2	2	2WU	NO	NO	3.23	20	162	6.46					3,461	232	20				
		14	SR 1431	FROM SR 1346 TO EOP (MP 0.00 - MP 0.21)	3	2	2WU	NO	NO	0.21	19									5	2,341	2,341	1,290	
		<b>TOTAL FOR PROJ NO. 2017CPT.13.03.20571</b>										<b>22.05</b>		<b>1,094</b>	<b>43.68</b>				<b>22,077</b>	<b>1,479</b>	<b>235</b>	<b>2,341</b>	<b>2,341</b>	<b>1,290</b>
		2017CPT.13.03.20572	Madison	15	SR 1374	FROM SR 1375 TO EOP (MP 0.00 - MP 0.60)	3	2	2WU	NO	NO	0.6	18								10	6,336	6,336	3,485
				16	SR 1376	FROM SR 1375 TO EOP (MP 0.00 - MP 0.20)	3	2	2WU	NO	NO	0.2	18								6	2,112	2,112	1,162
				17	SR 1382	FROM SR 1383 TO SR 1380 (MP 0.00 - MP 0.51)	3	2	2WU	NO	NO	0.51	18								10	5,386	5,386	2,963
				18	SR 1107	FROM SR 1001 TO BUNCOMBE COUNTY LINE (MP 0.00 - MP 2.51)	3	2	2WU	NO	NO	2.51	18									35	26,506	26,506
19	SR 1453			FROM US 25/70 TO US 25/70 (MP 0.00 - MP 0.78)	3	2	2WU	NO	NO	0.78	20									8	9,152	9,152	5,034	
20	SR 1622			FROM SR 1509 TO EOM (MP 0.00 - MP 0.20)	3	2	2WU	NO	NO	0.2	18									6	2,112	2,112	1,162	
21	SR 1152			FROM SR 1153 TO EOP (MP 0.00 - MP 0.61)	3	2	2WU	NO	NO	0.61	18									6	6,442	6,442	3,543	
22	SR 1153			FROM SR 1151 TO SR 1152 (MP 0.00 - MP 0.37)	3	2	2WU	NO	NO	0.37	18									1	3,907	3,907	2,149	
23	SR 1375			FROM SR 1370 TO EOP (MP 0.00 - MP 1.85)	3	2	2WU	NO	NO	1.85	18									30	19,536	19,536	10,745	
24	SR 1457			FROM US 23A TO SR 1503 (MP 0.00 - MP 0.40)	3	2	2WU	NO	NO	0.4	18									12	4,224	4,224	2,325	
25	SR 1136			SR 1135 TO SR 1203 (MP 1.10 - MP1.55)	3	2	2WU	NO	NO	0.45	19									25	5,016	5,016	2,760	
<b>TOTAL FOR PROJ NO. 2017CPT.13.03.20572</b>										<b>8.48</b>								<b>149</b>	<b>90,729</b>	<b>90,729</b>	<b>49,907</b>			
<b>GRAND TOTAL</b>										<b>42.94</b>		<b>1,715</b>	<b>67.73</b>		<b>5,105</b>	<b>18,831</b>	<b>22,077</b>	<b>2,608</b>	<b>534</b>	<b>93,070</b>	<b>93,070</b>	<b>51,197</b>		

PROJECT NO.	SHEET NO.	TOTAL NO.
2017CPT.13.03.10571, 2017CPT.13.03.20571, 2017CPT.13.03.20572	13	

## THERMOPLASTIC AND PAINT QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE TYPE	LENGTH	WIDTH	4413000000-E	4457000000-N	4710000000-E	4721000000-E	4725000000-E				4810000000-E		4905000000-N				
										WORK ZONE ADVANCE/GENERAL WARNING SIGNING SF	TEMPORARY TRAFFIC CONTROL LS	THERMOPLASTIC PAVEMENT MARKING LINES (24", 120 MILS) WHITE LF	THERMOPLASTIC PAVEMENT MARKING CHARACTER (120 MILS) SCHOOL EA	THERMOPLASTIC PAVEMENT MARKING SYMBOL (90 MILS) LT ARROW EA	THERMOPLASTIC PAVEMENT MARKING SYMBOL (90 MILS) MERGE ARROW EA	THERMOPLASTIC PAVEMENT MARKING SYMBOL (90 MILS) STR ARROW EA	THERMOPLASTIC PAVEMENT MARKING SYMBOL (90 MILS) RT ARROW EA	PAINT PAVEMENT MARKING LINES (4") WHITE LF	PAINT PAVEMENT MARKING LINES (4") YELLOW LF	SNOWPLOWABLE PAVEMENT MARKERS EA				
2017CPT.13.03.10571	Madison	1	NC 63	FROM 1.5 MILES WEST OF SR 1100 TO SR 1162 (MP 4.12 - MP 8.22)	1	2	2WU	4.1	20	1,386	*							86,592	86,592	360				
		2	US 25/70 BUS	FROM NC 251 TO PVMT CHANGE AT MARSHALL (MP 1.47 - MP 3.55)	1	2	2WU	2.08	20										43,930	43,930	140			
		3	NC 251	FROM BRIDGE NO.'S 8 & 9 TO US 25/70 (MP 4.10 - MP 4.91)	1	2	2WU	0.81	44					3	5	5			20,000	20,000	60			
		4	US 25/70	FROM SR 1135 TO SR 1439 (MP 7.34 - MP 10.76)	1	2	2WU	3.42	30			48	12	11					72,230	72,230	230			
		5	NC 213	FROM SR 1611 TO SR 1426 (MP 7.92 - MP 9.92)	1,4,5,6	2	MU	2	42					4	10	4	2		47,520	42,240	264			
<b>TOTAL FOR PROJ NO. 2017CPT.13.03.10571</b>										<b>1,386</b>		<b>48</b>	<b>12</b>	<b>18</b>	<b>15</b>	<b>9</b>	<b>2</b>	<b>270,272</b>	<b>264,992</b>	<b>1,054</b>				
														<b>44</b>			<b>535,264</b>							
2017CPT.13.03.20571	Madison	6	SR 1318	FROM SR 1395 TO SR 1341 (MP 7.94 - MP 12.80)	2	2	2WU	4.86	18	2,414	*							102,643	102,643					
		7	SR 1318	FROM SR 1341 TO SR 1636 (MP 12.80 - MP 18.20)	2	2	2WU	5.4	20											114,048	114,048			
		8	SR 1370	FROM SR 1364 TO SR 1372 (MP 1.28 - MP 4.45)	2	2	2WU	3.17	18											66,950	66,950			
		9	SR 1364	FROM SR 1370 TO SR 1366 (MP 0.00 - MP 3.10)	2	2	2WU	3.1	18												65,472	65,472		
		10	SR 1346	FROM I-26 TO EOM (MP 0.00 - MP 1.30)	2	2	2WU	1.3	18												27,456	27,456		
		11	SR 1624	FROM SR 1604 TO EOM (MP 0.00 - MP 0.33)	2	2	2WU	0.33	20												6,970	6,970		
		12	SR 1604	FROM SR 1582 TO EOM (MP 0.00 - MP 0.45)	2	2	2WU	0.45	20												9,504	9,504		
		13	SR 1180	FROM SR 1181 TO SR 1181 (MP 0.00 - MP 3.23)	2	2	2WU	3.23	20												68,218	68,218		
		14	SR 1431	FROM SR 1346 TO EOP (MP 0.00 - MP 0.21)	3	2	2WU	0.21	19												4,435	4,435		
		<b>TOTAL FOR PROJ NO. 2017CPT.13.03.20571</b>										<b>2,414</b>							<b>465,696</b>	<b>465,696</b>				
																	<b>931,392</b>							
2017CPT.13.03.20572	Madison	15	SR 1374	FROM SR 1375 TO EOP (MP 0.00 - MP 0.60)	3	2	2WU	0.6	18	1,166	*							12,672	12,672					
		16	SR 1376	FROM SR 1375 TO EOP (MP 0.00 - MP 0.20)	3	2	2WU	0.2	18															
		17	SR 1382	FROM SR 1383 TO SR 1380 (MP 0.00 - MP 0.51)	3	2	2WU	0.51	18												10,771	10,771		
		18	SR 1107	FROM SR 1001 TO BUNCOMBE COUNTY LINE (MP 0.00 - MP 2.51)	3	2	2WU	2.51	18												53,011	53,011		
		19	SR 1453	FROM US 25/70 TO US 25/70 (MP 0.00 - MP 0.78)	3	2	2WU	0.78	20												16,474	16,474		
		20	SR 1622	FROM SR 1509 TO EOM (MP 0.00 - MP 0.20)	3	2	2WU	0.2	18															
		21	SR 1152	FROM SR 1153 TO EOP (MP 0.00 - MP 0.61)	3	2	2WU	0.61	18															
		22	SR 1153	FROM SR 1151 TO SR 1152 (MP 0.00 - MP 0.37)	3	2	2WU	0.37	18															
		23	SR 1375	FROM SR 1370 TO EOP (MP 0.00 - MP 1.85)	3	2	2WU	1.85	18													39,072	39,072	
		24	SR 1457	FROM US 23A TO SR 1503 (MP 0.00 - MP 0.40)	3	2	2WU	0.4	18													16,896	16,896	
		25	SR 1136	SR 1135 TO SR 1203 (MP 1.10 - MP 1.55)	3	2	2WU	0.45	19													9,504	9,504	
<b>TOTAL FOR PROJ NO. 2017CPT.13.03.20572</b>										<b>1,166</b>							<b>158,400</b>	<b>158,400</b>						
																	<b>316,800</b>							
<b>GRAND TOTAL</b>									<b>42.94</b>	<b>4,966</b>	<b>1</b>	<b>48</b>	<b>12</b>	<b>18</b>	<b>15</b>	<b>9</b>	<b>2</b>	<b>894,368</b>	<b>889,088</b>	<b>1,054</b>				
														<b>44</b>			<b>1,783,456</b>							

# SIGNING FOR RESURFACING PROJECTS



LEGEND	
T	STATIONARY SIGN
←	DIRECTION OF TRAFFIC FLOW

## MAINLINE (-L-) SIGNING

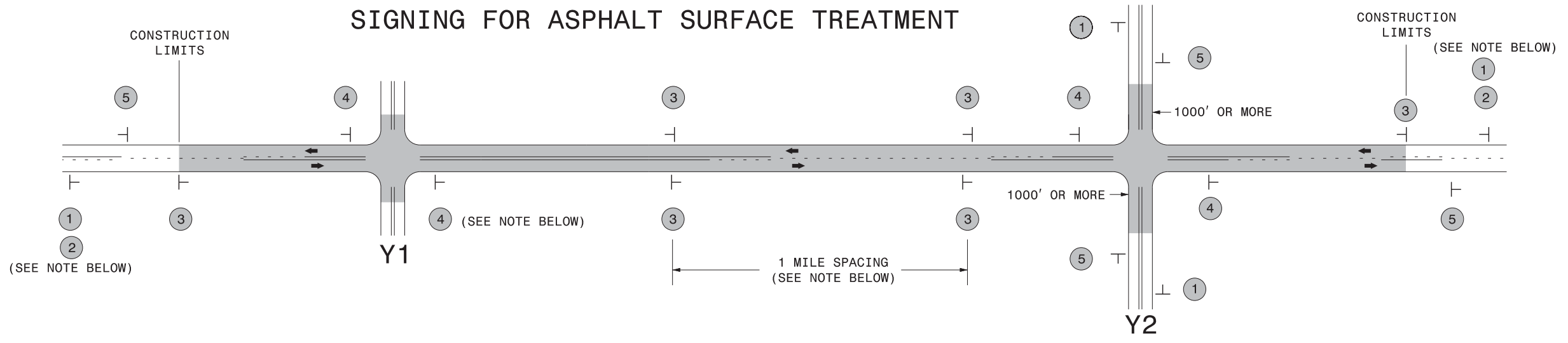
## -Y- LINE SIGNING

SIGNING NOTES AND PLACEMENT PER DIRECTION	1	2	3	4	5	
						<p>NO REQUIRED STATIONARY SIGNING FOR THE FOLLOWING -Y- LINE CONDITIONS:</p> <ol style="list-style-type: none"> <li>LESS THAN 1000' OF RESURFACING ALONG -Y- LINE</li> <li>SUBDIVISION ROADS</li> <li>DEAD END ROADS</li> </ol> <p>WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, ADVANCE WARNING PORTABLE SIGNS SHALL BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.</p> <div style="display: flex; justify-content: space-around;"> <div style="text-align: center;">   <small>W20-1 48" X 48"</small> </div> <div style="text-align: center;">   <small>W20-7 A 48" X 48"</small> </div> </div> <p>PLACED 500' IN ADVANCE OF FLAGGER. PLACED 250' IN ADVANCE OF FLAGGER.</p>
	PLACE 1000' PRIOR TO BEGINNING OF CONSTRUCTION LIMITS. ONLY USED ON -Y- LINES IF RESURFACING LIMITS EXTEND 1000' ALONG -Y- LINE.	#2 SIGN ONLY USED WHEN RESURFACING LIMITS ARE 2 OR MORE MILES IN LENGTH. ROUND UP TO NEXT WHOLE NUMBER. (NO FRACTIONAL OR DECIMAL NUMBERS)	PLACE INITIALLY AT THE CONSTRUCTION LIMITS AND SPACED 1 MILE APART THEREAFTER. IF NO -Y- LINES EXIST, PLACE 2ND SET 1/2 MILE FROM THE CONSTRUCTION LIMITS AND THEN SPACE 1 MILE THEREAFTER.	THESE ARE FOR -Y- LINES THAT ARE "THROUGH" ROADWAYS. DEAD END AND SUBDIVISION ROADS ARE NOT "THROUGH" ROADWAYS. INSTALL 500' +/- FROM EACH -Y- LINE APPROACH AS SHOWN ABOVE. FOR MULTIPLE -Y- LINES THAT ARE SEPARATED BY 0.25 MILES OR LESS, TREAT AS A SINGLE UNIT AND INSTALL WITHIN 500' OF EACH APPROACH. A MAXIMUM OF 2 SIGN SETS PER MILE. DO NOT INSTALL WHEN -Y- LINES ARE WITHIN 0.5 MILES FROM "END ROAD WORK" SIGN.	PLACE 500' FOLLOWING THE END OF CONSTRUCTION LIMITS.	

3/19/2015 C:\Users\rmgarrett\Downloads\Resurfacing\_AdvWarn\_2Ln (2).dgn User:rmgarrett

**RESURFACING  
ADVANCE WARNING SIGNS  
FOR  
RURAL AND SUBURBAN  
2 LANE ROADWAYS**

## SIGNING FOR ASPHALT SURFACE TREATMENT



LEGEND	
	STATIONARY SIGN
←	DIRECTION OF TRAFFIC FLOW

### MAINLINE (-L-) SIGNING

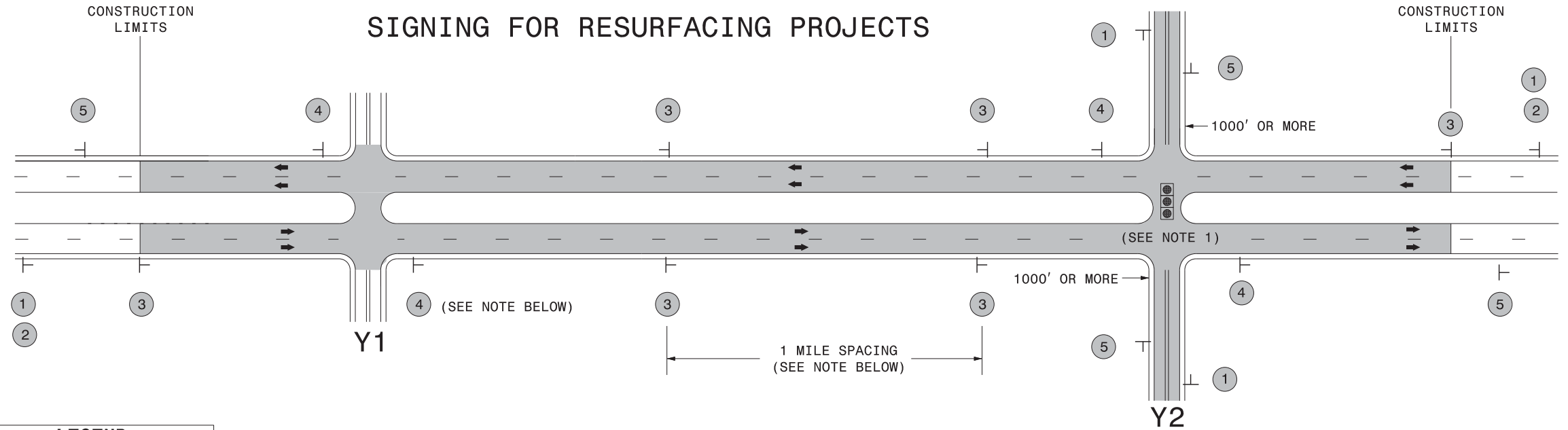
### -Y- LINE SIGNING

SIGNING NOTES AND PLACEMENT PER DIRECTION		
1 2	<p>PLACE 1000' PRIOR TO BEGINNING OF CONSTRUCTION LIMITS. ONLY USED ON -Y- LINES IF RESURFACING LIMITS EXTEND 1000' ALONG -Y- LINE.</p> <p>#2 SIGN ONLY USED WHEN RESURFACING LIMITS ARE 2 OR MORE MILES IN LENGTH. ROUND UP TO NEXT WHOLE NUMBER. (NO FRACTIONAL OR DECIMAL NUMBERS)</p>	<p><b>NO REQUIRED STATIONARY SIGNING FOR THE FOLLOWING -Y- LINE CONDITIONS:</b></p> <ol style="list-style-type: none"> <li>1) LESS THAN 1000' OF RESURFACING ALONG -Y- LINE</li> <li>2) SUBDIVISION ROADS</li> <li>3) DEAD END ROADS</li> </ol>
3	<p>ALTERNATE THE FOLLOWING TWO SIGNS: STARTING WITH "LOOSE GRAVEL" (W8-7) FOLLOWED BY "UNMARKED PAVEMENT".</p> <p>PLACE INITIALLY AT THE CONSTRUCTION LIMITS AND SPACED 1 MILE APART THEREAFTER. IF NO -Y- LINES EXIST, PLACE 2ND SET 1/2 MILE FROM THE CONSTRUCTION LIMITS AND THEN SPACE 1 MILE THEREAFTER.</p>	<p>WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, ADVANCE WARNING PORTABLE SIGNS SHALL BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.</p> <p>PLACED 500' IN ADVANCE OF FLAGGER. PLACED 250' IN ADVANCE OF FLAGGER.</p>
4	<p>THESE ARE FOR -Y- LINES THAT ARE "THROUGH" ROADWAYS. DEAD END AND SUBDIVISION ROADS ARE NOT "THROUGH" ROADWAYS. INSTALL 500' +/- FROM EACH -Y- LINE APPROACH AS SHOWN ABOVE. FOR MULTIPLE -Y- LINES THAT ARE SEPARATED BY 0.25 MILES OR LESS, TREAT AS A SINGLE UNIT AND INSTALL WITHIN 500' OF EACH APPROACH. A MAXIMUM OF 2 SIGN SETS PER MILE. DO NOT INSTALL WHEN -Y- LINES ARE WITHIN 0.5 MILES FROM "END ROAD WORK" SIGN.</p>	
5	<p>PLACE 500' FOLLOWING THE END OF CONSTRUCTION LIMITS.</p>	

**ADVANCE WARNING SIGNS  
FOR  
ASPHALT SURFACE TREATMENTS  
2 LANE ROADWAYS**

12/22/2014 S:\TMU\WZTC\apps\work\ZoneGeneral\ExternalWebPage\DesRes\Resurfacing\Resurfacing\_AdvWarn\_2Ln - AST.dgn User:rmgarratt





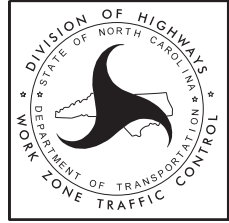
**LEGEND**  
 ┆ STATIONARY SIGN  
 ← DIRECTION OF TRAFFIC FLOW

**MAINLINE (-L-) SIGNING**

**-Y- LINE SIGNING**

SIGNING NOTES AND PLACEMENT PER DIRECTION	 	<p>PLACE 1000' PRIOR TO BEGINNING OF CONSTRUCTION LIMITS. ONLY USED ON -Y- LINES IF RESURFACING LIMITS EXTEND 1000' ALONG -Y- LINE.</p> <p>#2 SIGN ONLY USED WHEN RESURFACING LIMITS ARE 2 OR MORE MILES IN LENGTH. ROUND UP TO NEXT WHOLE NUMBER. (NO FRACTIONAL OR DECIMAL NUMBERS)</p>	<p><b>NO REQUIRED STATIONARY SIGNING FOR THE FOLLOWING -Y- LINE CONDITIONS:</b></p> <ol style="list-style-type: none"> <li>1) LESS THAN 1000' OF RESURFACING ALONG -Y- LINE</li> <li>2) SUBDIVISION ROADS</li> <li>3) DEAD END ROADS</li> </ol> <p>WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, ADVANCE WARNING PORTABLE SIGNS SHALL BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.</p> <div style="display: flex; justify-content: space-around;"> <div style="text-align: center;">   <small>W20-1 48" X 48"</small> </div> <div style="text-align: center;">   <small>W20-7 A 48" X 48"</small> </div> </div> <p>PLACED 500' IN ADVANCE OF FLAGGER. PLACED 250' IN ADVANCE OF FLAGGER.</p> <p><b>NOTES:</b></p> <ol style="list-style-type: none"> <li>1) MAY USE LAW ENFORCEMENT TO CONTROL TRAFFIC AT SIGNALIZED INTERSECTIONS AS DIRECTED BY THE ENGINEER. PROVIDE PORTABLE "ROAD WORK AHEAD" (W20-1) SIGNS 500' IN ADVANCE ALONG BOTH APPROACHES FROM THE SIDE STREETS WHEN PAVING PROCEEDS THROUGH THE INTERSECTION.</li> </ol>
		<p>PLACE INITIALLY AT THE CONSTRUCTION LIMITS AND SPACED 1 MILE APART THEREAFTER. IF NO -Y- LINES EXIST, PLACE 2ND SET 1/2 MILE FROM THE CONSTRUCTION LIMITS AND THEN SPACE 1 MILE THEREAFTER.</p>	
		<p>THESE ARE FOR -Y- LINES THAT ARE "THROUGH" ROADWAYS. DEAD END AND SUBDIVISION ROADS ARE NOT "THROUGH" ROADWAYS. INSTALL 500' +/- FROM EACH -Y- LINE APPROACH AS SHOWN ABOVE. FOR MULTIPLE -Y- LINES THAT ARE SEPARATED BY 0.25 MILES OR LESS, TREAT AS A SINGLE UNIT AND INSTALL WITHIN 500' OF EACH APPROACH. A MAXIMUM OF 2 SIGN SETS PER MILE. DO NOT INSTALL WHEN -Y- LINES ARE WITHIN 0.5 MILES FROM "END ROAD WORK" SIGN.</p>	
		<p>PLACE 500' FOLLOWING THE END OF CONSTRUCTION LIMITS.</p>	

3/23/2015 C:\Users\rmgarrrett\Downloads\Resurfacing\_AdvWarn\_LrSu\_Shldr.dgn User:rmgarrrett



**RESURFACING ADVANCE WARNING SIGNS FOR RURAL AND SUBURBAN MULTI-LANE ROADWAYS W/ SHOULDER SECTIONS**