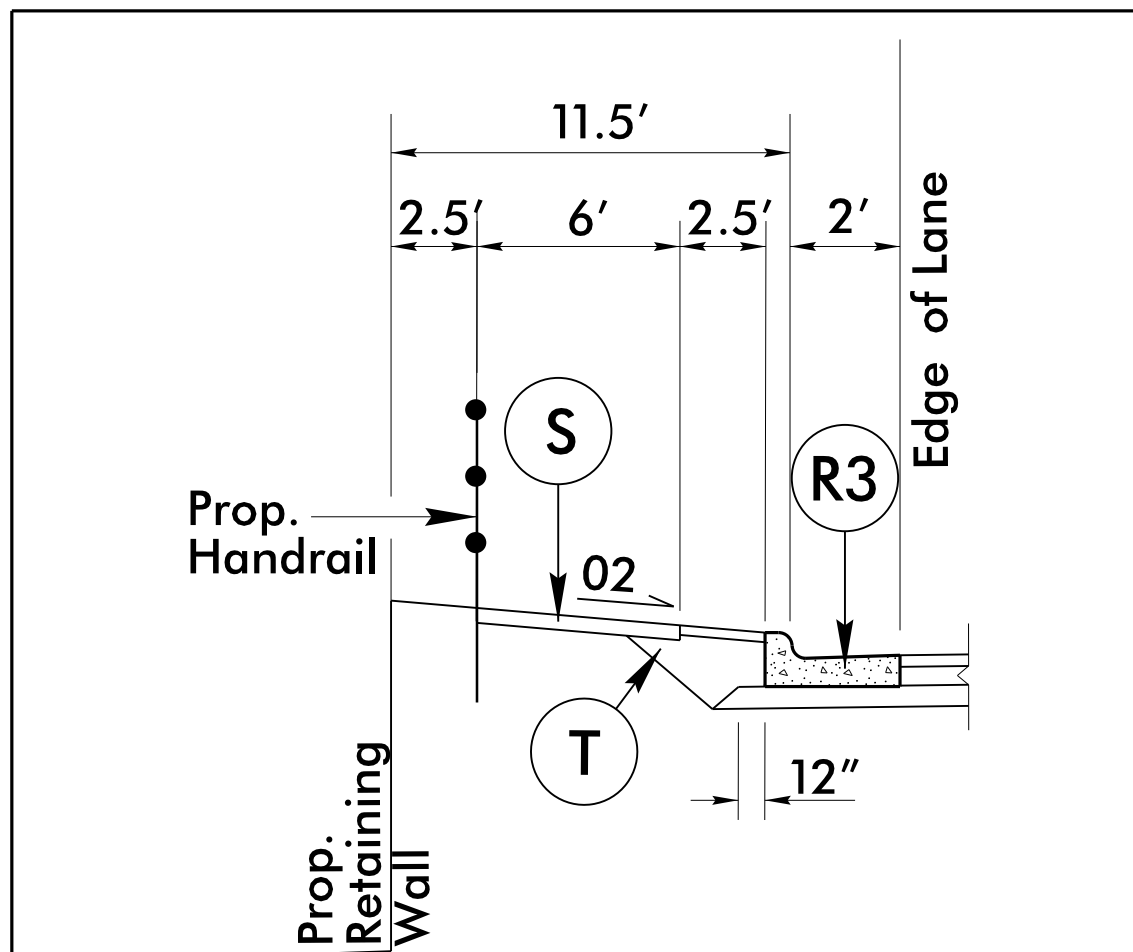
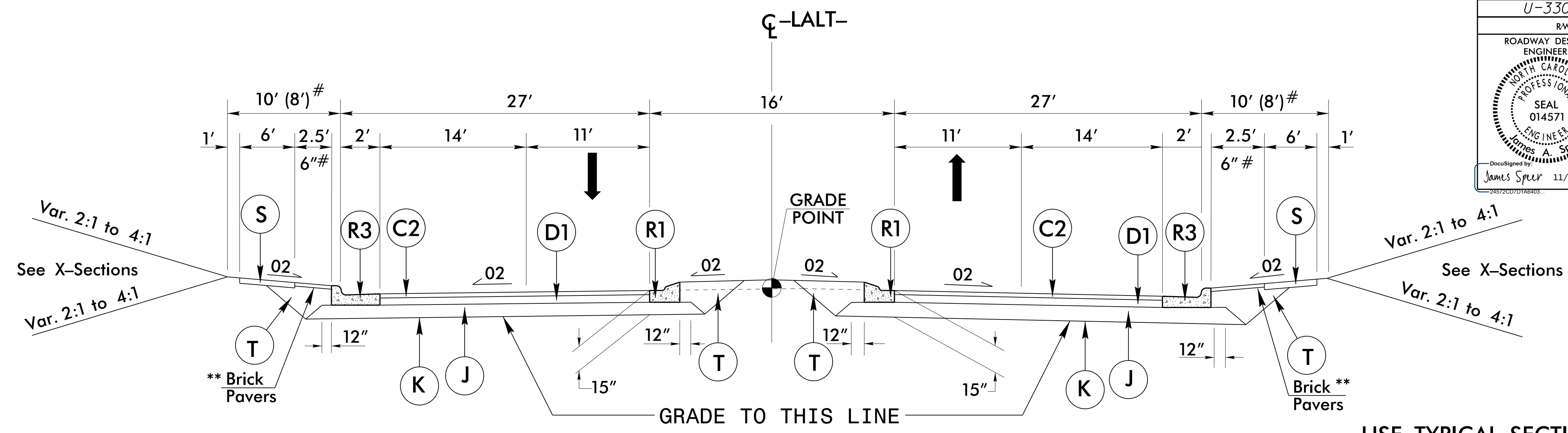


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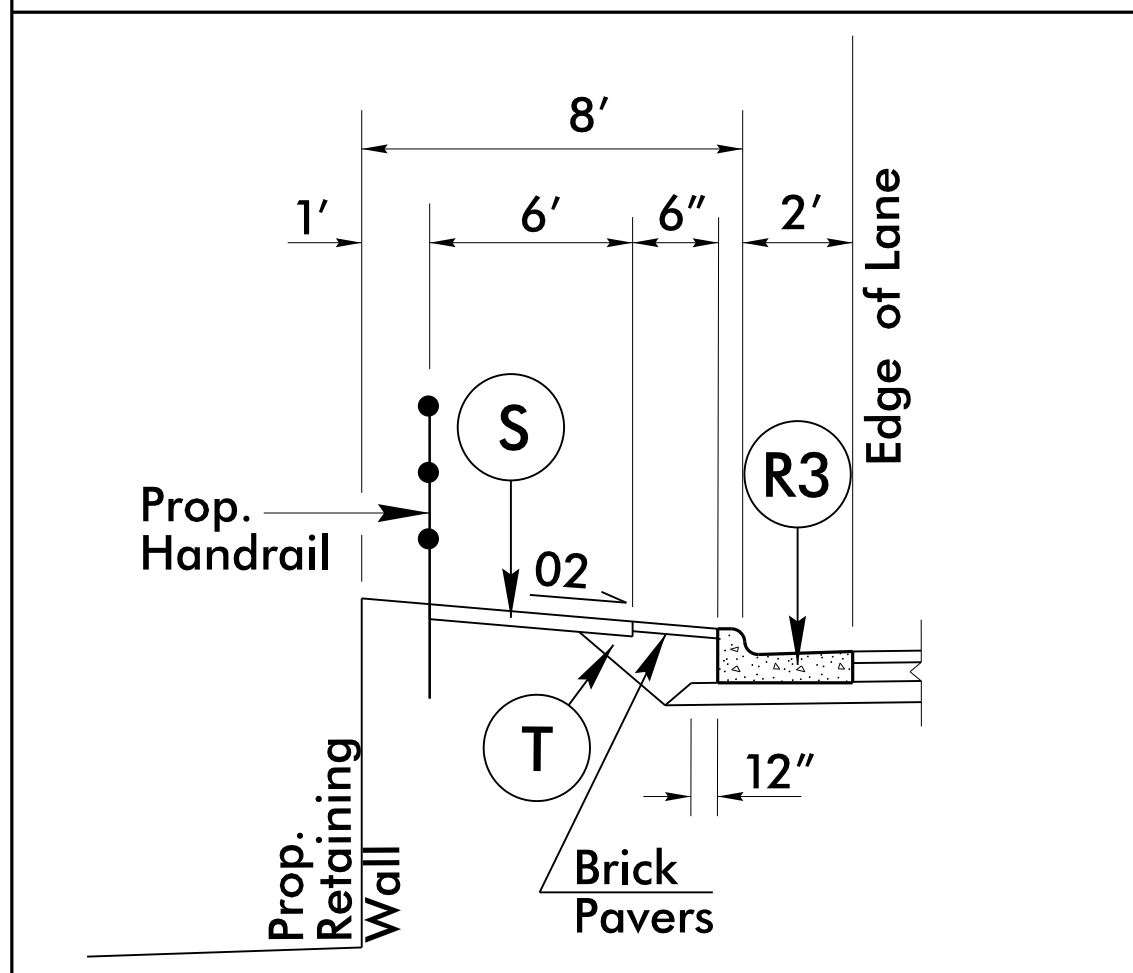
PROJECT REFERENCE NO. U-3308	SHEET NO. 2A-3
RW SHEET NO.	
ROADWAY DESIGN ENGINEER SEAL 014571 James Speer 11/23/2015	PAVEMENT DESIGN ENGINEER SEAL 031484 Vladimir G. Mitchev 11/19/2015

C1	1.5" TYPE S9.5B
C2	3" TYPE S9.5B
C3	VARIABLE DEPTH S9.5B
D1	4" TYPE I19.0B
D2	VARIABLE DEPTH TYPE I19.0B
E1	3" TYPE B25.0B
E2	4" TYPE B25.0B
E3	5" TYPE B25.0B
E4	VAR. DEPTH TYPE B25.0B
J	8" AGGREGATE BASE COURSE
K	SUBGRADE STABILIZATION
L	CLASS IV SUBGRADE STABILIZATION
N1	GEOTEXTILE FOR SOIL STABILIZATION
N2	GEOTEXTILE FOR PAVEMENT STABILIZATION
R1	PROP. 1'6" CONC. CURB AND GUTTER
R2	EXIST. 2'6" CONC. CURB AND GUTTER
R3	PROP. 2'6" CONC. CURB AND GUTTER
R4	PROP. CONCRETE ISLAND
S	PROPOSED CONCRETE SIDEWALK
T	EARTH MATERIAL
U	EXISTING PAVEMENT
V1	PROP. 1.5" ASPHALT MILLING
V2	PROP. 3" ASPHALT MILLING
V3	PROP. ASPHALT MILLING VARIABLE
W	ASPHALT WEDGING (SEE DETAIL)

NOTE: ALL SLOPES ARE 1:1 UNLESS OTHERWISE NOTED.



Use With Typical Sections 3A & #4  
Sta. 49+72.00 to 52+50.00 Rt. -LALT-  
Sta. 57+70.00 to 59+60.00 Rt. -LALT-

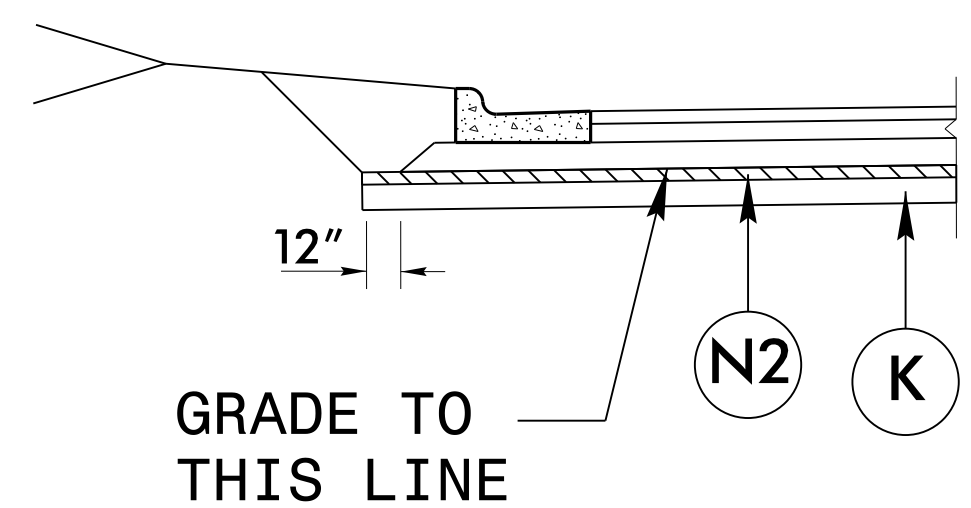


Use With Typical Sections 3A & #4  
Sta. 37+32.00 to 40+30.00 Lt. -LALT-  
Sta. 46+73.00 to 48+71.00 Rt. -LALT-

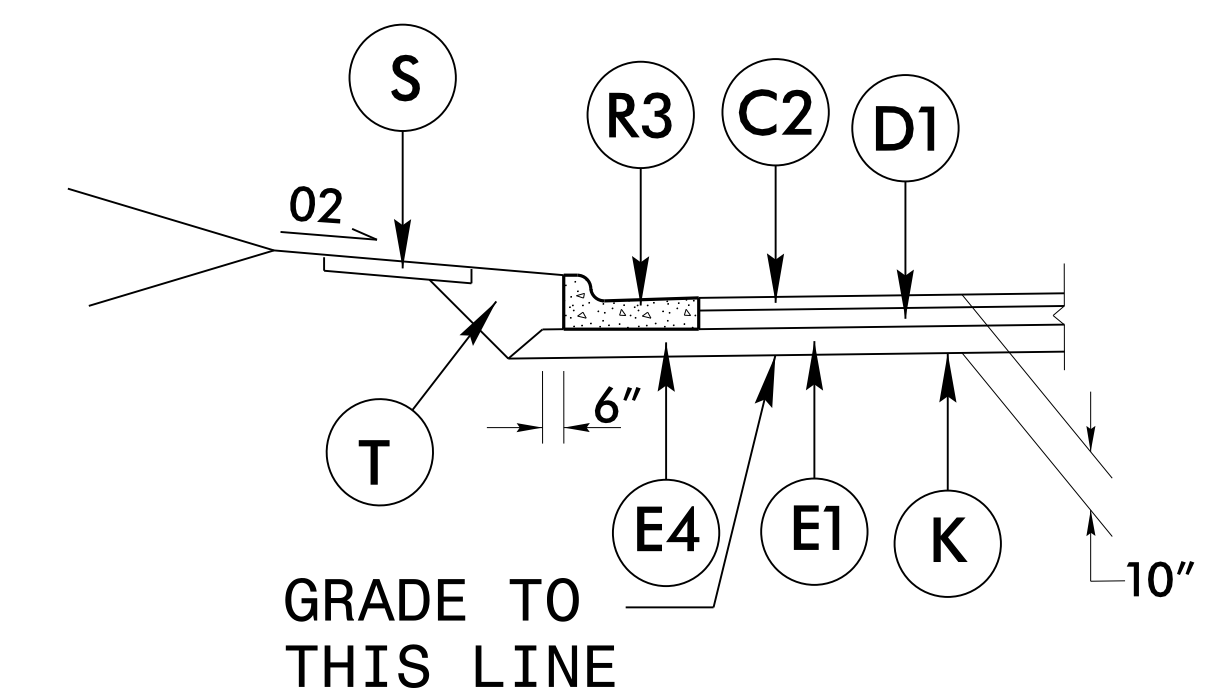
### TYPICAL SECTION NO. 4

USE TYPICAL SECTION NO. 4  
-LALT- Sta. 20+90.00 TO 22+37.00  
-LALT- Sta. 25+72.00 TO 29+76.00  
-LALT- Sta. 38+67.00 TO 41+24.64  
#-LALT- Sta. 41+24.64 TO 46+76.00  
-LALT- Sta. 50+83.00 TO 55+22.00  
-LALT- Sta. 59+12.00 TO 61+67.00

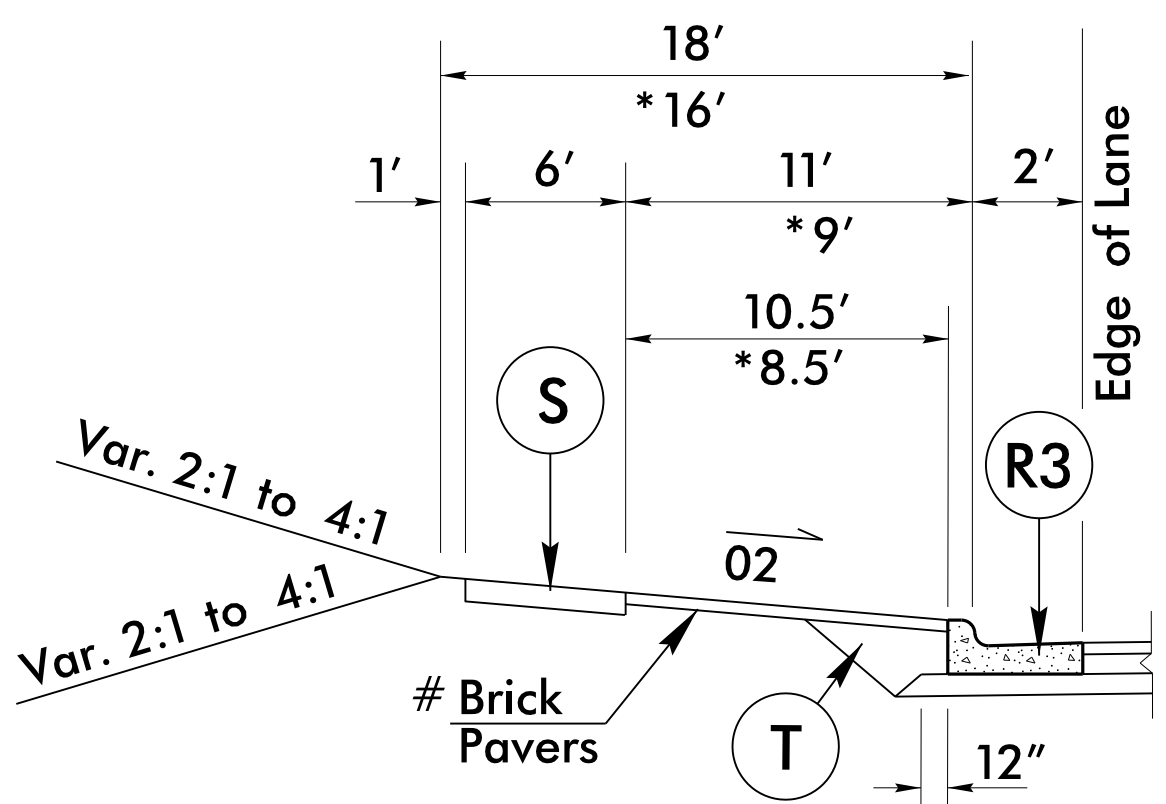
\*\* NOTE: BEGIN BRICK PAVERS NORTH OF THE -LALT- (ALSTON AVE) AND -Y4--Y5- (MAIN ST) INTERSECTION. END BRICK PAVERS SOUTH OF THE -LALT- (ALSTON AVE) AND -Y14--Y15- (TAYLOR ST.)



Geotextile for Pavement Stabilization (Contingencies)



Alternate Mainline Pavement Design



Use With Typical Section #3A & #4

Sta. 31+82.98 to 31+98.22 Rt. -LALT-  
#Sta. 36+84.27 to 37+02.08 Rt. -LALT-  
#\*Sta. 41+03.96 to 41+19.84 Lt. -LALT-  
#\*Sta. 41+10.30 to 41+24.64 Rt. -LALT-  
#\*Sta. 48+36.77 to 48+53.77 Lt. -LALT-  
Sta. 49+03.52 to 49+19.96 Rt. -LALT-  
Sta. 53+73.72 to 53+93.72 Lt. -LALT-  
Sta. 53+74.33 to 53+94.15 Rt. -LALT-  
Sta. 56+72.59 to 56+87.72 Lt. -LALT-  
Sta. 57+51.51 to 57+64.43 Rt. -LALT-  
Sta. 63+30.32 to 63+45.23 Lt. -LALT-

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