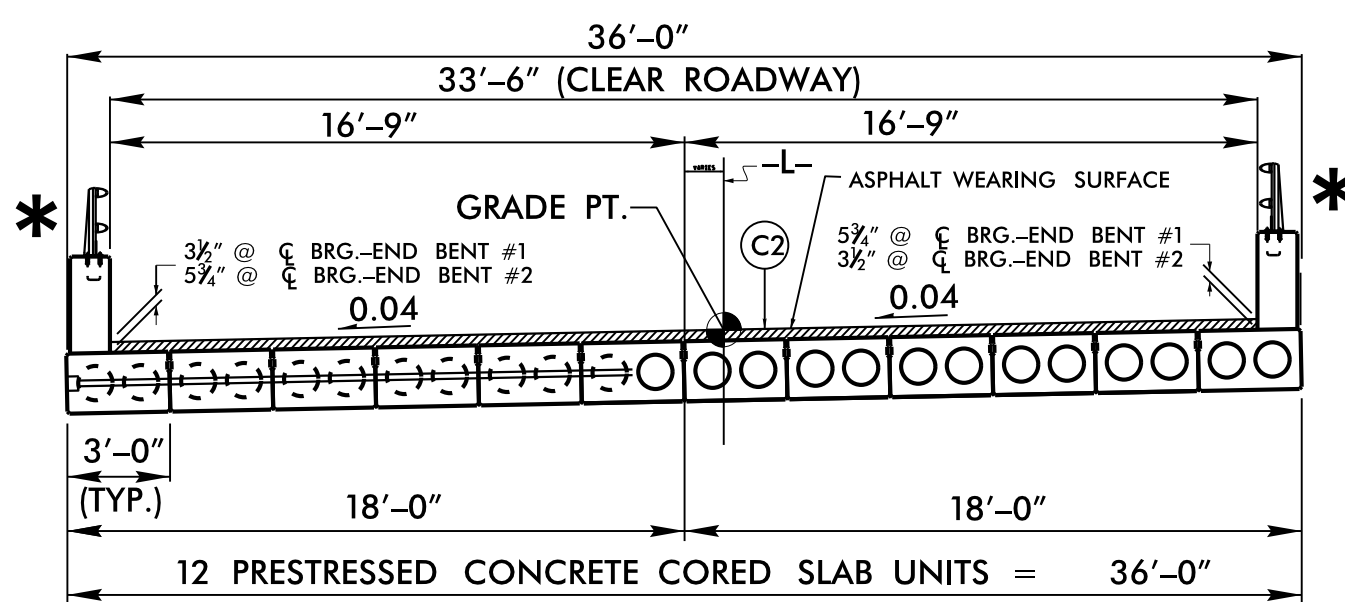
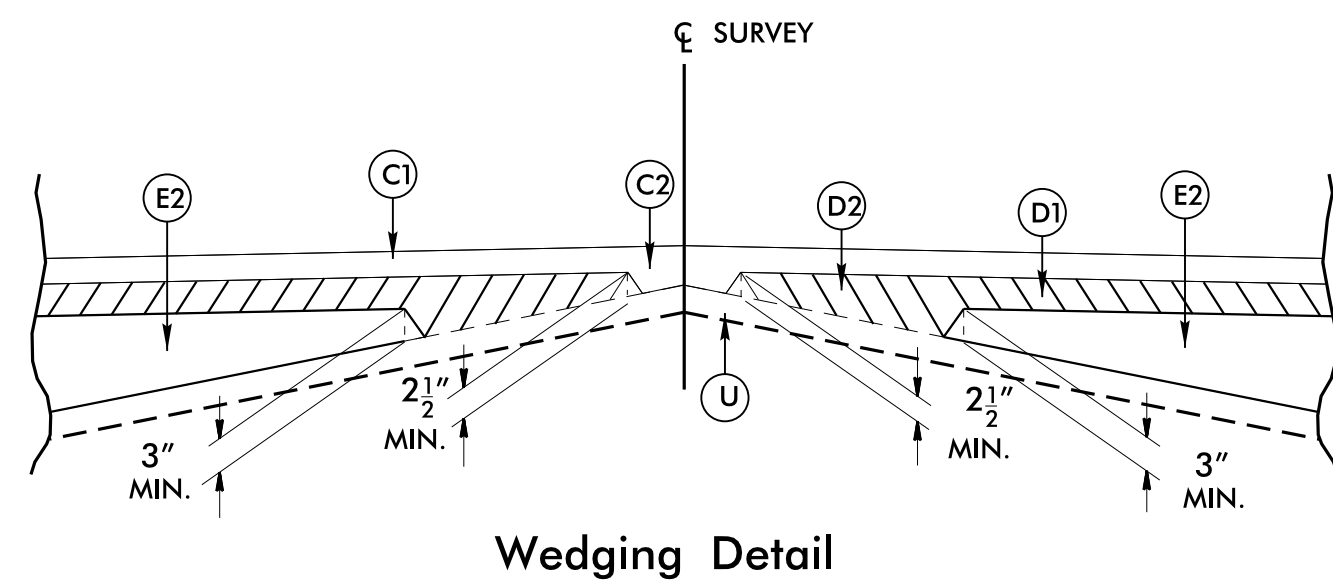


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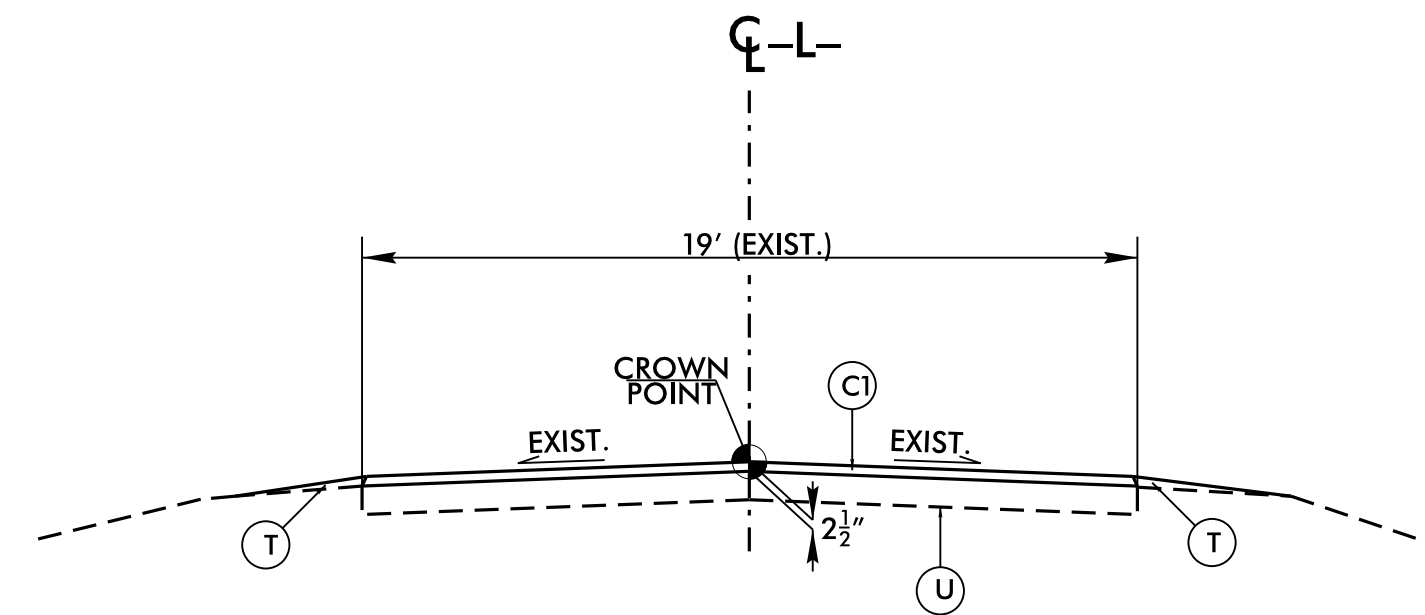
PAVEMENT SCHEDULE FINAL PAVEMENT DESIGN	
C1	PROP. APPROX. 2 1/2" ASPHALT CONCRETE SURFACE COURSE, TYPE SF9.5A, AT AN AVERAGE RATE OF 137.5 LBS. PER SQ. YD. IN EACH OF TWO LAYERS
C2	PROP. VAR. DEPTH ASPHALT CONCRETE SURFACE COURSE, TYPE SF9.5A, AT AN AVERAGE RATE OF 110 LBS. PER SQ. YD. PER 1" DEPTH. TO BE PLACED IN LAYERS NOT TO EXCEED 1.5" IN DEPTH.
D1	PROP. APPROX. 2 1/2" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE I19.0B, AT AN AVERAGE RATE OF 285 LBS. PER SQ. YD.
D2	PROP. VAR. DEPTH ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE I19.0B, AT AN AVERAGE RATE OF 114 LBS. PER SQ. YD. PER 1" DEPTH. TO BE PLACED IN LAYERS NOT LESS THAN 2 1/2" IN DEPTH OR GREATER THAN 4" IN DEPTH.
E1	PROP. APPROX. 4" ASPHALT CONCRETE BASE COURSE, TYPE B25.0B, AT AN AVERAGE RATE OF 456 LBS. PER SQ. YD.
E2	PROP. VAR. DEPTH ASPHALT CONCRETE BASE COURSE, TYPE B25.0B, AT AN AVERAGE RATE OF 114 LBS. PER SQ. YD. PER 1" DEPTH. TO BE PLACED IN LAYERS NOT LESS THAN 3" IN DEPTH OR GREATER THAN 5 1/2" IN DEPTH.
J	PROP. 6" AGGREGATE BASE COURSE.
R	SHOULDER BERM GUTTER.
T	EARTH MATERIAL.
U	EXISTING PAVEMENT.
W	WEDGING (SEE DETAIL)

NOTE: PAVEMENT EDGE SLOPES ARE 1:1 UNLESS SHOWN OTHERWISE.



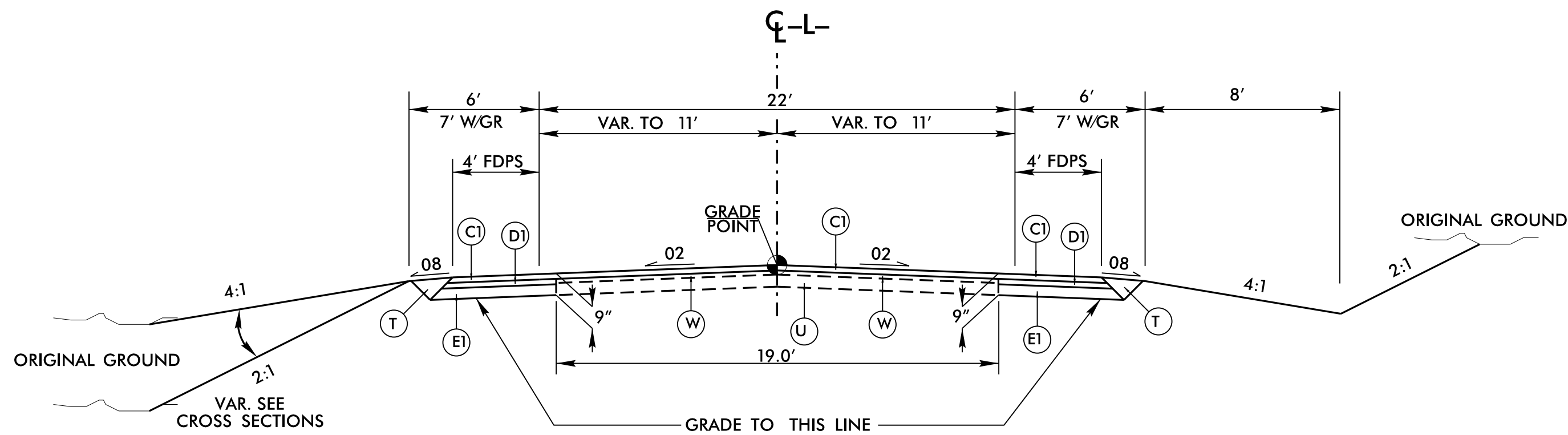
-L- STA. 13+84.17 (BEGIN BRIDGE) TO STA. 14+41.83 (END BRIDGE)

* BICYCLE SAFE RAILS REQUIRED



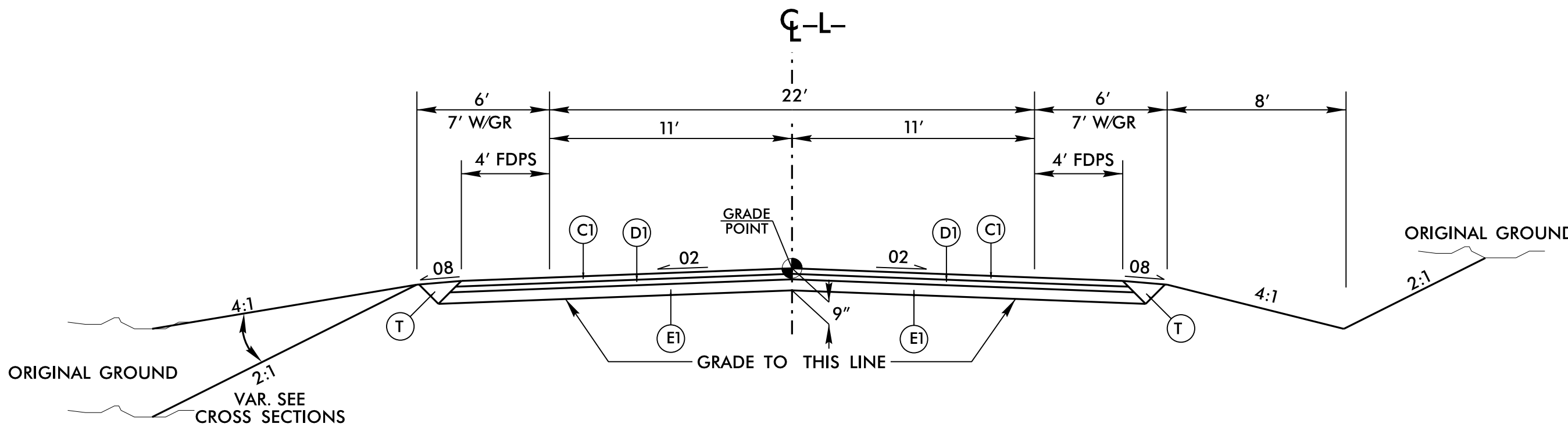
TYPICAL SECTION NO. 1

USE TYPICAL SECTION NO. 1 AS FOLLOWS:
-L- STA. 11+00.00 TO STA. 11+35.34
-L- STA. 17+46.14 TO STA. 17+60.43



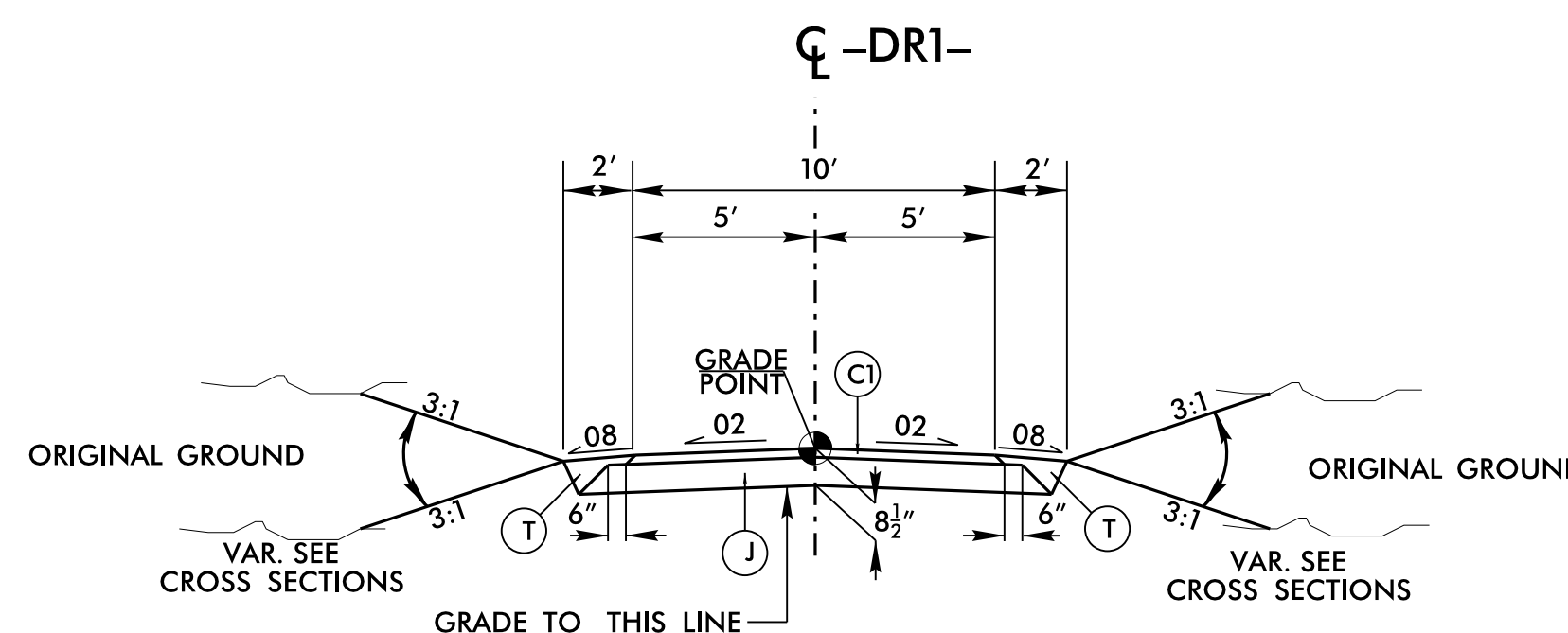
TYPICAL SECTION NO. 2

USE TYPICAL SECTION NO. 2 AS FOLLOWS:
-L- STA. 11+35.34 TO STA. 13+00.00
-L- STA. 16+00.00 TO STA. 17+46.14



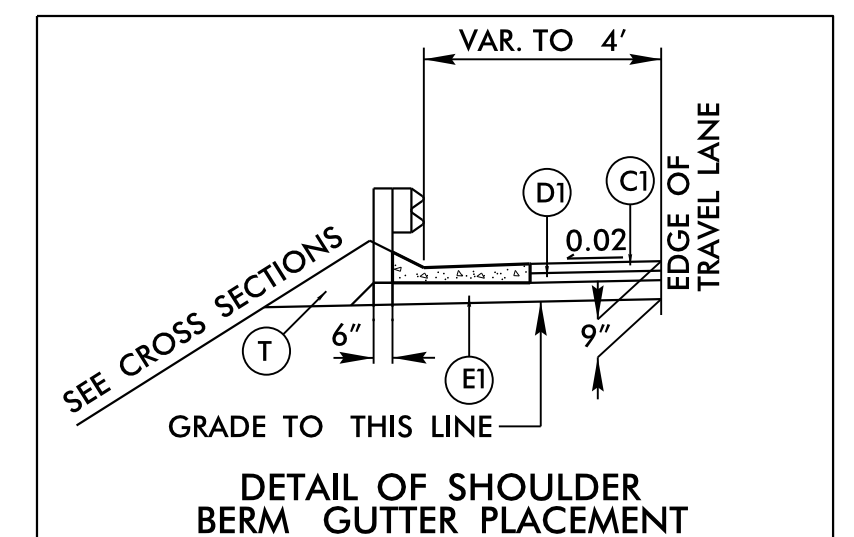
TYPICAL SECTION NO. 3

USE TYPICAL SECTION NO. 3 AS FOLLOWS:
-L- STA. 13+00.00 TO STA. 13+84.17 (BEGIN BRIDGE)
-L- STA. 14+41.83 (END BRIDGE) TO STA. 16+00.00



TYPICAL SECTION NO. 4

USE TYPICAL SECTION NO. 4 AS FOLLOWS:
-DRI- STA. 10+15.00 TO STA. 11+15.70



USE IN CONJUNCTION WITH TYPICAL SECTIONS 2 AND 3
-L- LT STA. 13+35.48 TO 13+81.60
-L- LT STA. 14+63.87 TO 16+98.00

PROJECT REFERENCE NO. B-5410	SHEET NO. 2A-1
ROADWAY DESIGN ENGINEER SEAL 033296 Steven D. Kendall 3/23/2016	PAVEMENT DESIGN ENGINEER SEAL 022896 Clark S. Morrison 3/23/2016
DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED	

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