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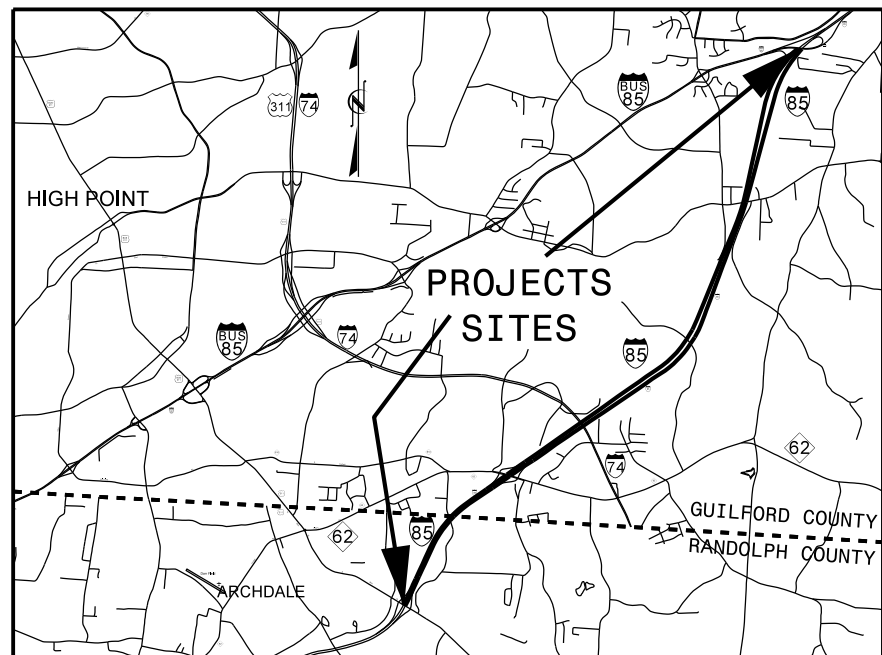
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05-APR-2016 11:26
 S:\Contracts\Resurfacing\Projects\Division 7\I5738 I5821\I5738_I-5789 Title.dgn
 05/28/99

CONTRACT: C203843 **TIP PROJECT: I-5821, I-5738, I-5789**

VICINITY



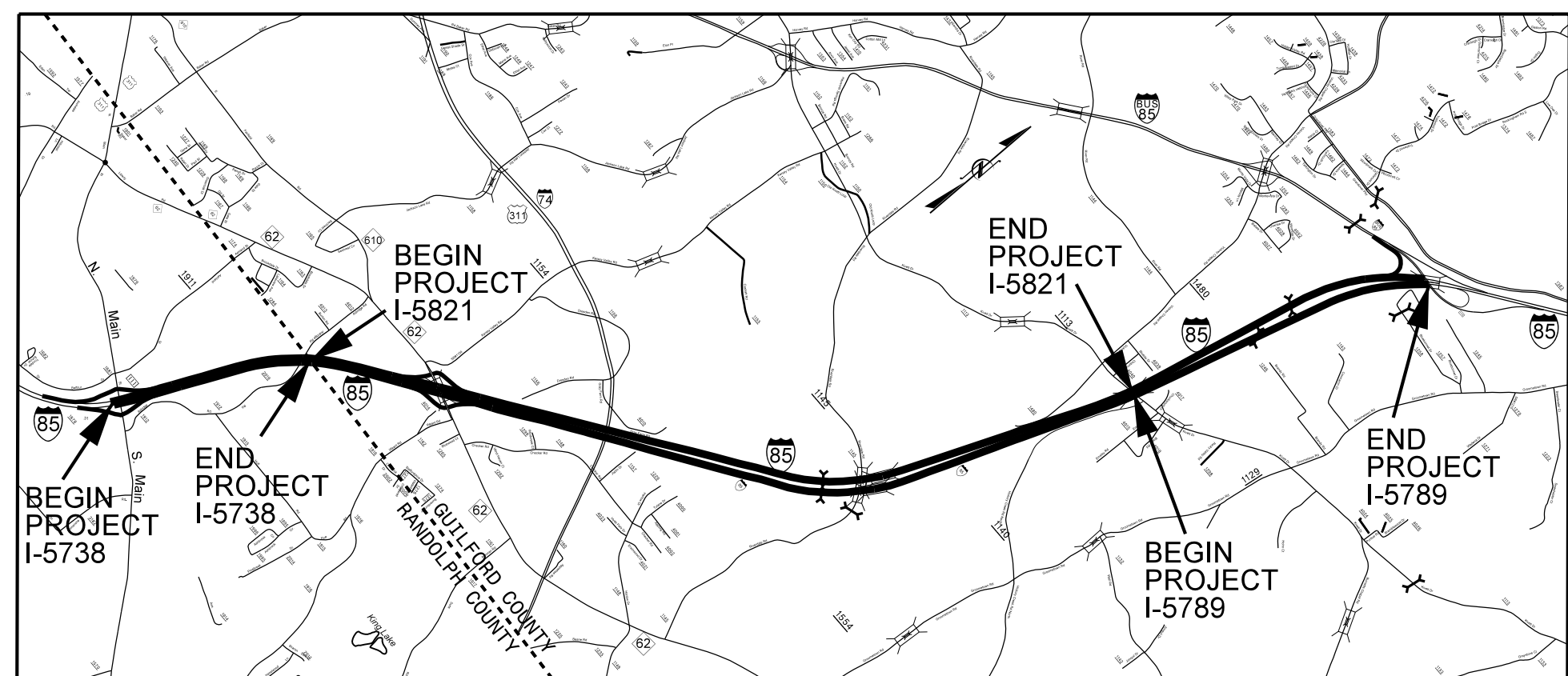
STATE OF NORTH CAROLINA
 DIVISION OF HIGHWAYS

GUILFORD AND RANDOLPH COUNTIES

LOCATION: I-85 FROM 0.05 MILE SOUTH OF MAIN STREET IN ARCHDALE TO I-85 BUSINESS IN HIGH POINT

TYPE OF WORK: PAVEMENT AND BRIDGE REHABILITATION

| STATE | STATE PROJECT REFERENCE NO. | SHEET NO. | TOTAL SHEETS |
|-----------------|-----------------------------|-----------------|--------------|
| N.C. | I-5821, I-5738, I-5789 | 1 | |
| STATE PROJ. NO. | F.A. PROJ. NO. | DESCRIPTION | |
| 50464.1.1 | NHPIM-0085(5)116 | I-5821 (P.E.) | |
| 50464.3.1 | NHPIM-0085(5)116 | I-5821 (CONST.) | |
| 52020.1.1 | NHPP-0085(12) | I-5738 (P.E.) | |
| 52020.3.1 | NHPP-0085(12) | I-5738 (CONST.) | |
| 53029.1.1 | NHPIM-0085(3)117 | I-5789 (P.E.) | |
| 53029.3.1 | NHPIM-0085(3)117 | I-5789 (CONST.) | |

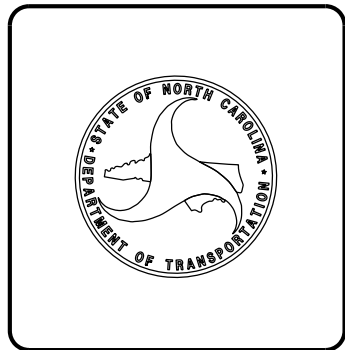


GRAPHIC SCALES
 NOT TO SCALE

DESIGN DATA
 NA

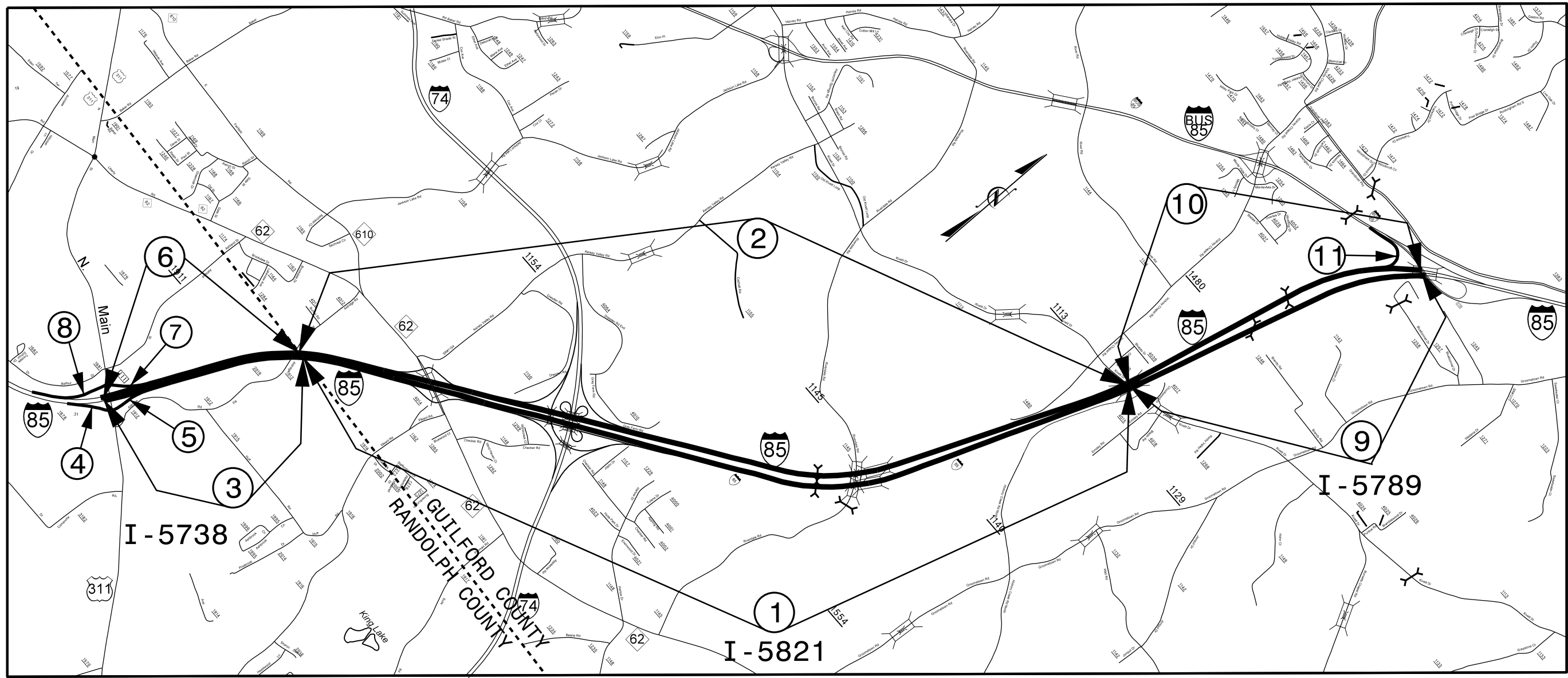
PROJECT LENGTH
 I-5821 = 4.49 miles
 I-5738 = 1.08 miles
 I-5789 = 1.70 miles
 Total Project Length = 7.27

Prepared in the Office of:
DIVISION OF HIGHWAYS
 1584 Yanceyville St. Greensboro, NC, 27405
 2012 STANDARD SPECIFICATIONS
C. B. SMITHERMAN, P.E.
 PROJECT ENGINEER
CLINTON A. WILES
 ENGINEERING TECHNICIAN
 LETTING DATE:
 June 21, 2016

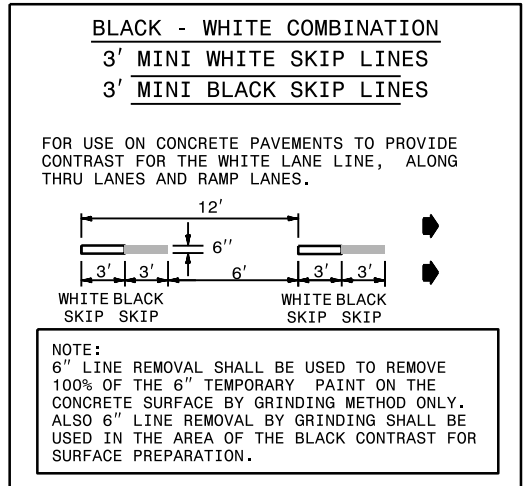
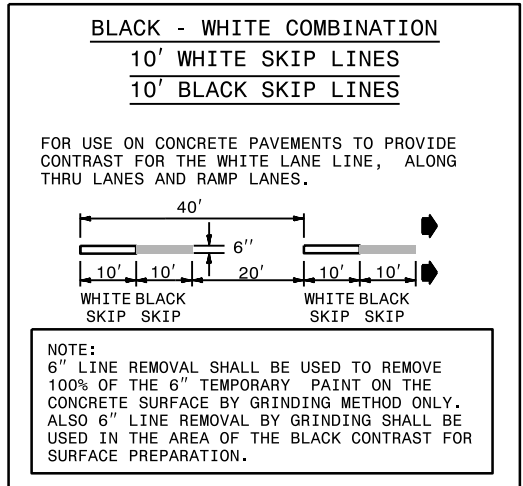


| STATE | PROJECT NO. | SHEET NO. | TOTAL SHEETS |
|-------|------------------------|-----------|--------------|
| N.C. | I-5821, I-5738, I-5789 | 2 | |

GUILFORD AND RANDOLPH COUNTIES

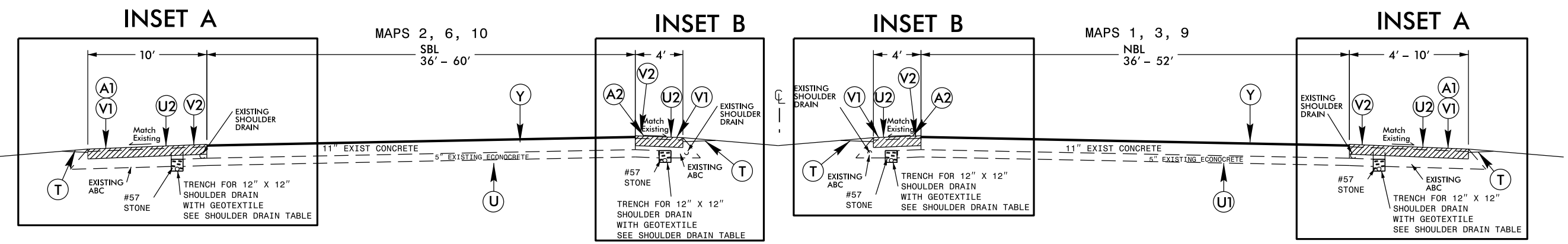


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 S:\Contracts\Resurfacing Projects\Division 7\I5738 I5821 I5789 Guilford Randolph June 2016\I-5821_I-5738_I-5789_vicinity.dgn
 Supporter AT CSN-2016-02



- NOTES:
1. SHOULDER DRAIN SHALL BE INSTALLED PRIOR TO CONSTRUCTING THE PROPOSED 4' PCCP SHOULDER OR 10' RCC SHOULDER.
 2. CONCRETE REPAIRS SHALL BE COMPLETED ADJACENT TO THE SHOULDER PRIOR TO CONSTRUCTING THE PROPOSED 4' PCCP SHOULDER OR 10' RCC SHOULDER.
 3. CONCRETE REPAIRS & PROPOSED SHOULDERS SHALL BE COMPLETE PRIOR TO DIAMOND GRINDING.
 4. LONGITUDINAL JOINTS IN EXISTING CONCRETE PAVEMENT SHALL BE SAWED & SEALED IN ACCORDANCE WITH SECTION 700 OF THE STANDARD SPECIFICATIONS. PLEASE NOTE, LONGITUDINAL JOINTS IN OLD PAVEMENT WERE NOT SAWED & SEALED.
 5. TIE BARS SHALL BE INSTALLED IN LOCATIONS WHERE PCCP SHOULDER IS PLACED IN ACCORDANCE WITH SECTION 700 OF THE STANDARD SPECIFICATIONS.

6. TIE BARS SHALL NOT BE INSTALLED AT LOCATIONS WHERE ROLLER COMPACTED CONCRETE SHOULDER IS PLACED.
7. TRANSVERSE JOINTS IN ROLLER COMPACTED CONCRETE SHOULDER SHALL MATCH TRANSVERSE JOINTS IN EXISTING CONCRETE PAVEMENT. IF TRANSVERSE JOINTS ARE MORE THAN 20' APART, PLACE AN ADDITIONAL TRANSVERSE JOINT MIDWAY OF SLAB.
8. TRANSVERSE JOINTS IN PORTLAND CEMENT CONCRETE PAVED SHOULDER SHALL MATCH TRANSVERSE JOINTS IN EXISTING CONCRETE PAVEMENT. TWO ADDITIONAL JOINTS SHALL BE SPACED EQUALLY BETWEEN THE EXISTING JOINTS.
9. PRIOR TO PLACING FINAL PAVEMENT MARKING MATERIAL ON CONCRETE SURFACES THAT ARE DIAMOND GROUND, THE CONTRACTOR SHALL USE AN ACCEPTABLE METHOD TO GRIND RIDGES SMOOTH ONLY WHERE PAVEMENT MARKINGS WILL BE INSTALLED.



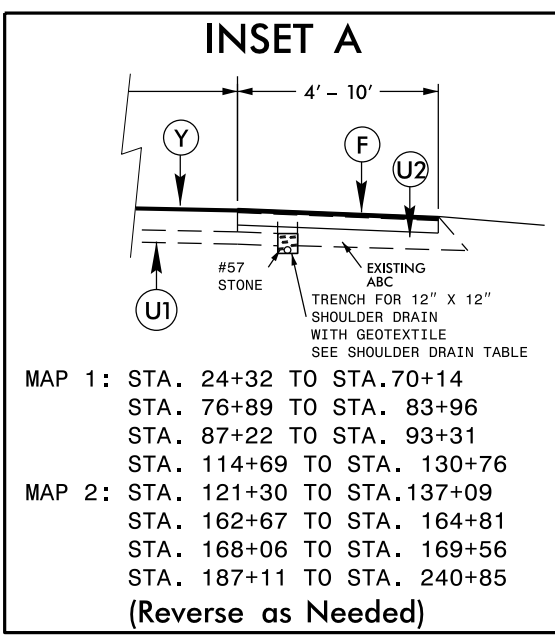
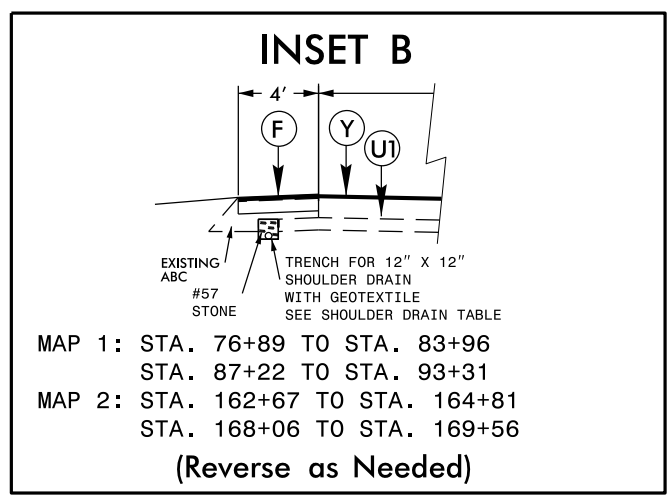
TYPICAL SECTION NO. 1

TO BE USED ON MAPS 2, 6, 10

MAP 2: STA. 7+81 TO STA. 10+00 (Bridge)
STA. 10+00 TO STA. 79+54
STA. 79+54 TO STA. 83+19 (Bridge)
STA. 83+19 TO STA. 164+81
STA. 164+81 TO STA. 168+06 (Bridge)
STA. 168+06 TO STA. 242+16

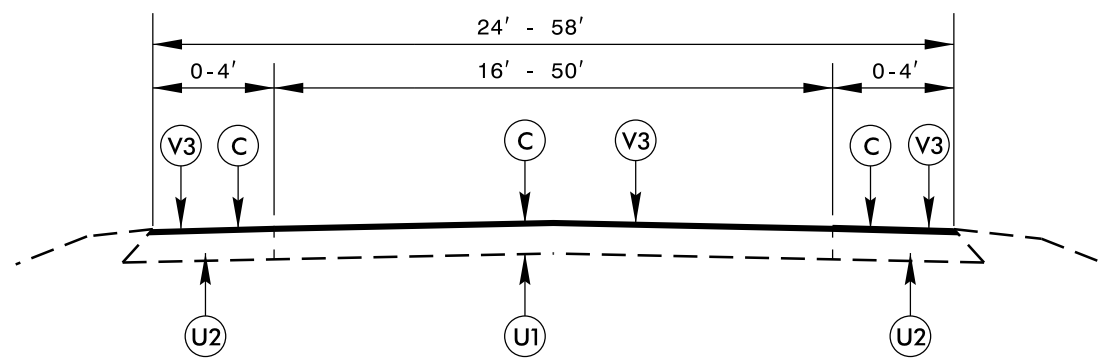
TO BE USED ON MAPS 1, 3, 9

MAP 1: STA. 10+00 TO STA. 83+96
STA. 83+96 TO STA. 87+22 (Bridge)
STA. 87+22 TO STA. 168+11
STA. 168+11 TO STA. 171+11 (Bridge)
STA. 171+11 TO STA. 244+74
STA. 244+74 TO STA. 246+97 (Bridge)

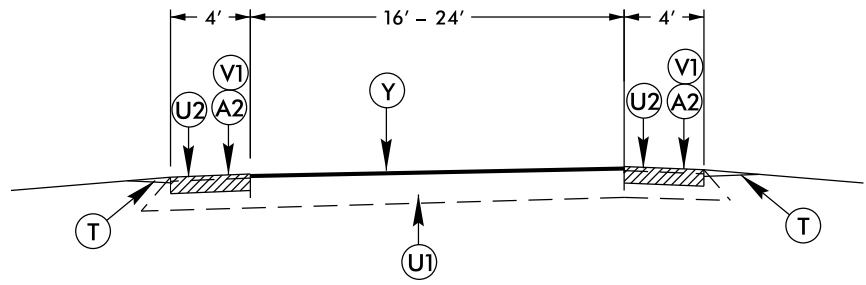


| PAVEMENT SCHEDULE | |
|-------------------|--|
| A1 | 8" ROLLED COMPACTED CONCRETE (RCC). |
| A2 | 8" PORTLAND CEMENT CONCRETE PAVEMENT (PCCP) |
| C | PROP. APPROX. 1 1/2" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD. |
| E | PROP. APPROX. 6 1/2" ASPHALT CONCRETE BASE COURSE, TYPE B25.0B, AT AN AVERAGE RATE OF 741 LBS. PER SQ. YD. IN ONE LIFT |
| F | PROPOSED FOG SEAL TO BE APPLIED TO THE EXISTING ASPHALT SHOULDER |
| R | EXISTING CONCRETE STRUCTURE |
| T | SHOULDER RECONSTRUCTION |
| U1 | EXISTING TRAVELWAY |
| U2 | EXISTING PAVED SHOULDER. |
| V1 | 8" TRENCHING |
| V2 | MILLED RUMBLE STRIP |
| V3 | 1 1/2" MILLING |
| Y | PROPOSED DIAMOND GRINDING |

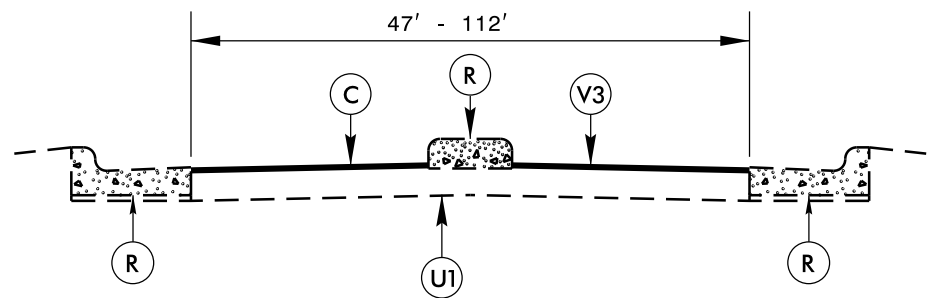
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7:\15738\01-UN-2016-106-06-Gulford Randolph June 2016\1-5821,1-5738,1-5789_ttypicals MOST UP-TO-DATE.dgn
pjw



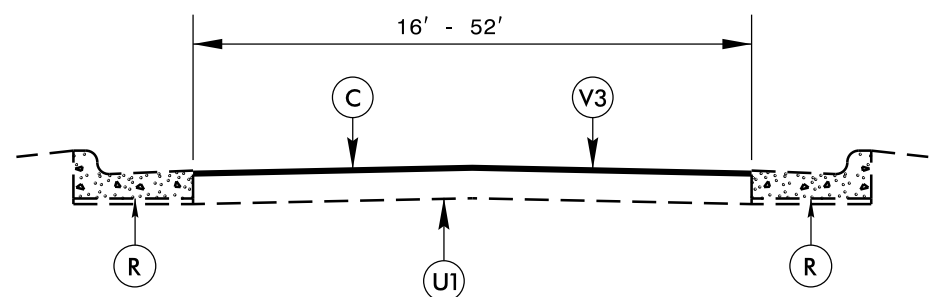
TYPICAL SECTION NO. 2
 TO BE USED ON MAPS: 4, 7, 8
 MAP 4: STA. 10+00 TO STA. 17+37
 MAP 7: STA. 16+27 TO STA. 17+87
 MAP 8: STA. 10+64 TO STA. 20+64



TYPICAL SECTION NO. 5
 TO BE USED ON MAPS 5, 7, 11
 MAP 5: STA. 9+70 TO STA. 18+34
 MAP 7: STA. 10+00 TO STA. 16+27
 MAP 11: STA. 10+00 TO STA. 17+18

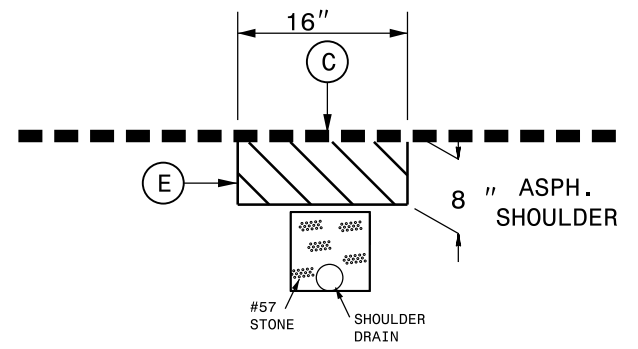


TYPICAL SECTION NO. 3
 TO BE USED ON MAPS 4, 7
 MAP 4: STA. 17+37 TO STA. 17+57
 MAP 7: STA. 17+87 TO STA. 18+37



TYPICAL SECTION NO. 4
 TO BE USED ON MAPS 5, 8
 MAP 5: STA. 9+18 TO STA. 9+70
 MAP 8: STA. 10+00 TO STA. 10+64

**PATCHING DETAIL FOR SHOULDER DRAIN
 (Pavement Repair)**

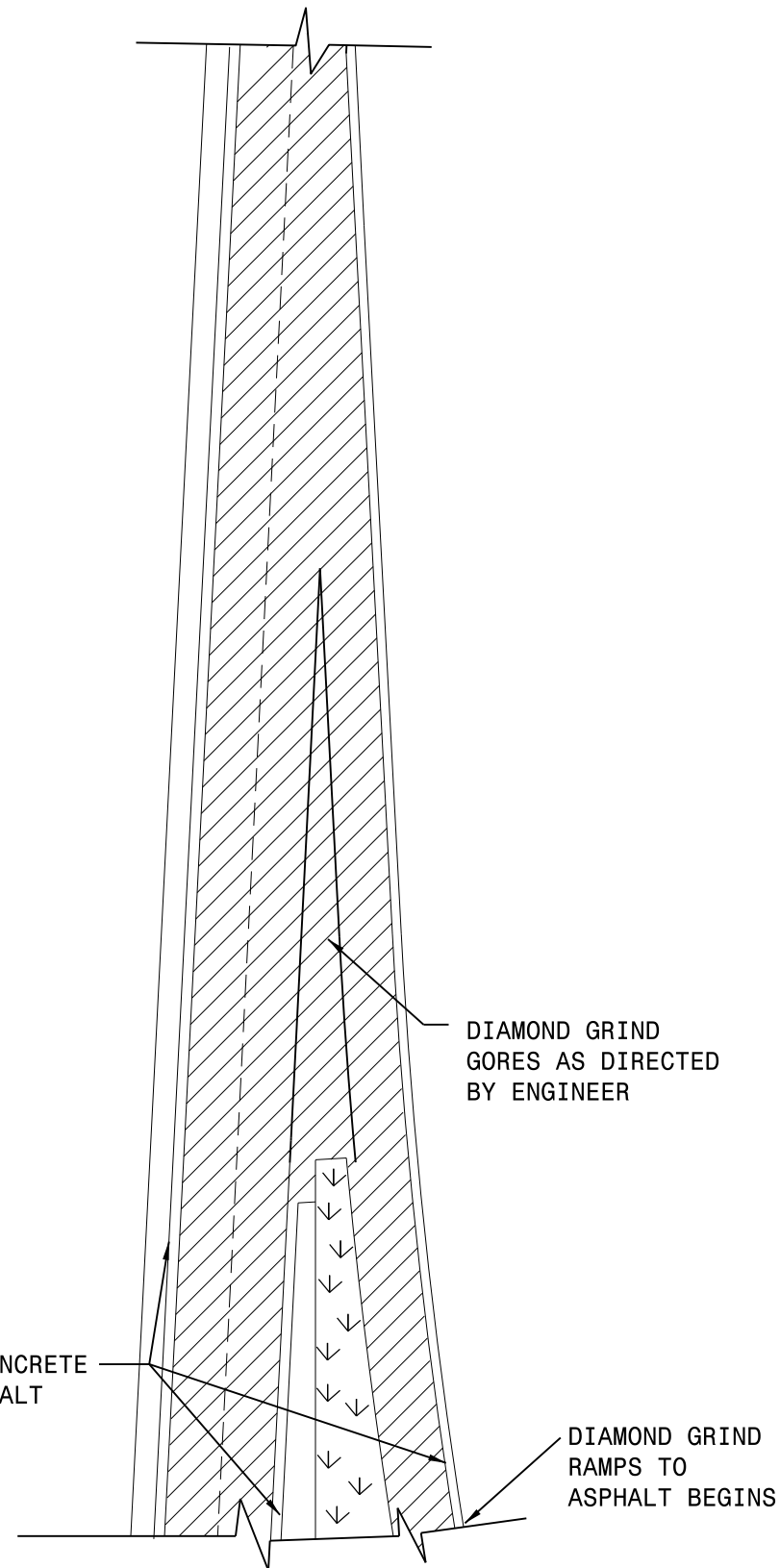


REMOVE EXISTING ASPHALT PAVEMENT 8" IN DEPTH AND FILL WITH 6½" BASE COURSE TYPE B25.0B IN ONE LIFT AND 1½" SURFACE COURSE, TYPE S9.5C AT LOCATIONS AS DIRECTED BY THE ENGINEER.

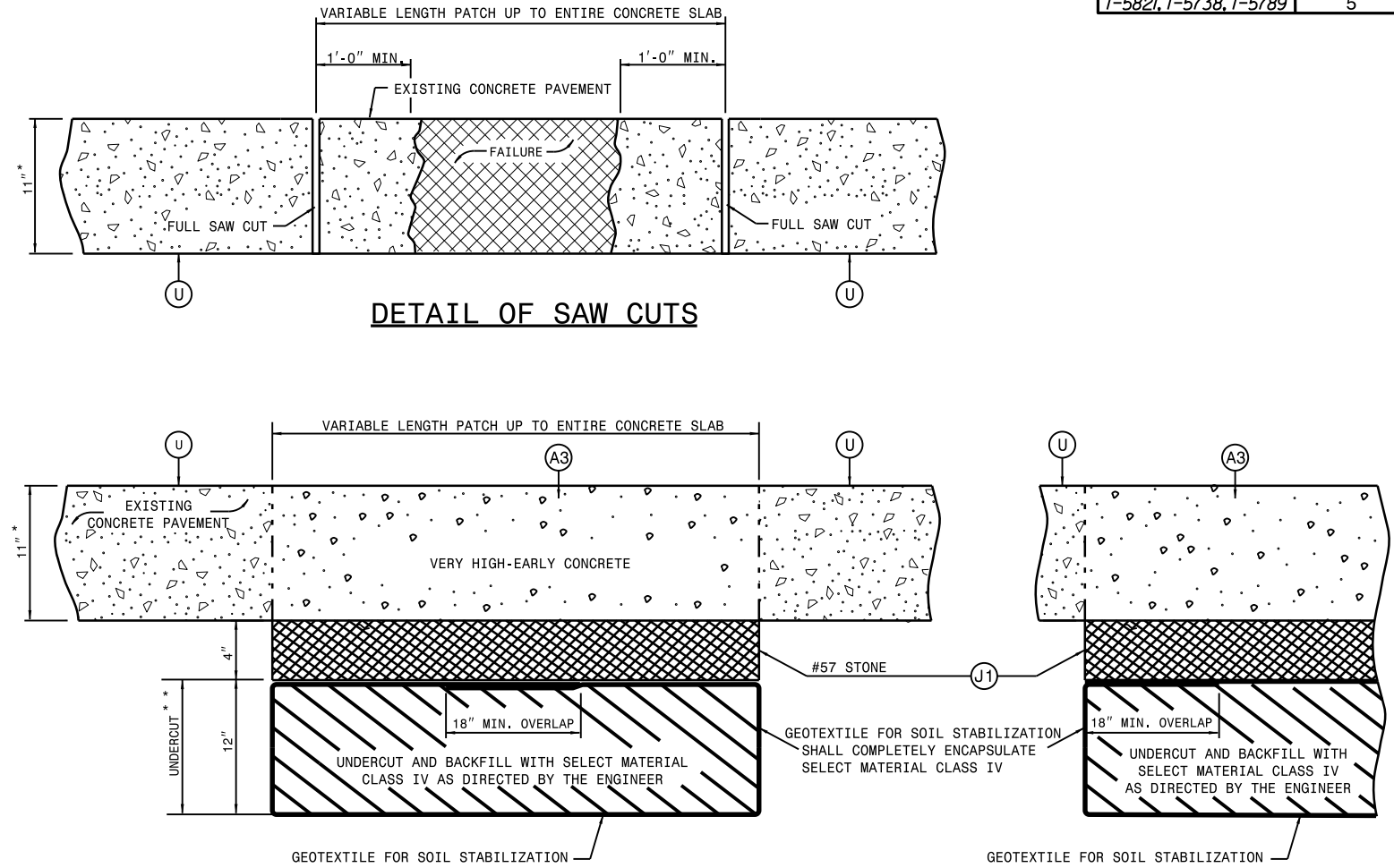
TO BE USED IN CONJUNCTION WITH MAPS 1 AND 2
 (SEE SHOULDER DRAIN SHEET)

| PAVEMENT SCHEDULE | |
|-------------------|---|
| A1 | 8" ROLLED COMPACTED CONCRETE (RCC). |
| A2 | 8" PORTLAND CEMENT CONCRETE PAVEMENT (PCCP) |
| C | PROP. APPROX. 1½" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD. |
| E | PROP. APPROX. 6½" ASPHALT CONCRETE BASE COURSE, TYPE B25.0B, AT AN AVERAGE RATE OF 741 LBS. PER SQ. YD. IN ONE LIFT |
| F | PROPOSED FOG SEAL TO BE APPLIED TO THE EXISTING ASPHALT SHOULDER |
| R | EXISTING CONCRETE STRUCTURE |
| T | SHOULDER RECONSTRUCTION |
| U1 | EXISTING TRAVELWAY |
| U2 | EXISTING PAVED SHOULDER. |
| V1 | 8" TRENCHING |
| V2 | MILLED RUMBLE STRIP |
| V3 | 1½" MILLING |
| Y | PROPOSED DIAMOND GRINDING |

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DETAIL FOR LIMITS OF MILLING AND DIAMOND GRINDING AT GORE AREAS



DETAIL OF CONCRETE PAVEMENT REPAIR

- * DIMENSIONS ARE APPROXIMATE AND SHOULD BE FIELD VERIFIED
- ** UNDERCUT REQUIRED IN AREAS AS DIRECTED BY THE ENGINEER

| PAVEMENT SCHEDULE | |
|-------------------|------------------------------|
| A3 | 11" VERY HIGH-EARLY CONCRETE |
| J1 | #57 STONE |
| U | EXISTING PAVEMENT |

Refer to the North Carolina Department of Transportation "Partial and Full Depth Repair Manual" when Replacing Slabs and when Repairing Concrete Pavement.

DETAIL FOR REPAIR OF CONCRETE PAVEMENT

| | |
|------------------------|-----------|
| PROJECT REFERENCE NO. | SHEET NO. |
| I-5821, I-5738, I-5789 | 6 |

SHOULDER DRAIN SUMMARY

| PROJECT # | MAP # | STATION | STATION | AGGREGATE SHOULDER DRAIN | | | | SHOULDER DRAIN OUTLET LOCATIONS | | | | | Remove & Replace Concrete Apron EA | FLOWABLE FILL CY | ASPHALT PLANT MIX, PAVEMENT REPAIR TON | |
|---------------------|-------|---------|---------|--------------------------|----------|----------------|------------------------|---|----------|----------|-----------------------|-------------|--|------------------------|--|-----------|
| | | | | LOCATION | | SHOULDER DRAIN | 4" SHOULDER DRAIN PIPE | STATION | LOCATION | | LENGTH OF OUTLET PIPE | OUTLET TYPE | | | | |
| | | | | LANE | LT or RT | FT | FT | | LANE | LT or RT | FT | TIE TO DI | | | | PAD EA |
| I-5821 | 1 | 10+00 | 24+32 | NB | RT | 1,432 | 1,432 | | | | | | | | | |
| I-5821 | 1 | | | | | | | 11+20 | NB | RT | 30 | X | | 1 | | |
| I-5821 | 1 | | | | | | | 16+20 | NB | RT | 30 | | 1 | | | |
| I-5821 | 1 | | | | | | | 21+70 | NB | RT | 30 | X | | 1 | | |
| I-5821 | 1 | 24+32 | 55+50 | | | | | NO SHOULDER DRAIN NEEDED IN THIS LOCATION | | | | | | | | |
| I-5821 | 1 | 55+50 | 82+38 | NB | RT | 2,688 | 2,688 | 55+50 to 70+14 in new Asp. Sh. | | | | | | | 94 | |
| I-5821 | 1 | | | | | | | 61+07 | NB | RT | 30 | X | | 1 | | |
| I-5821 | 1 | | | | | | | 66+32 | NB | RT | 30 | X | | 1 | | |
| I-5821 | 1 | | | | | | | 70+09 | NB | RT | 30 | X | | 1 | | |
| I-5821 | 1 | | | | | | | 74+50 | NB | RT | 30 | | 1 | | | |
| I-5821 | 1 | | | | | | | 78+78 | NB | RT | 30 | X | | 1 | | |
| I-5821 | 1 | | | | | | | 82+38 | NB | RT | 30 | X | | 1 | | |
| I-5821 | 1 | 82+38 | 93+31 | | | | | NO SHOULDER DRAIN NEEDED IN THIS LOCATION | | | | | | | | |
| I-5821 | 1 | 93+31 | 114+69 | NB | RT | 2,138 | 2,138 | | | | | | | | | |
| I-5821 | 1 | | | | | | | 97+14 | NB | RT | 30 | X | | 1 | | |
| I-5821 | 1 | | | | | | | 101+86 | NB | RT | 30 | X | | 1 | | |
| I-5821 | 1 | | | | | | | 108+60 | NB | RT | 30 | X | | 1 | | |
| I-5821 | 1 | | | | | | | 114+76 | NB | RT | | X | | 1 | | 2 |
| | | | | | | | | NO OUTLET PIPE NEEDED, DI IN ASPHALT GORE (On-ramp from I-74/NC 62) | | | | | | | | |
| I-5821 | 1 | 114+69 | 130+76 | | | | | NO SHOULDER DRAIN NEEDED IN THIS LOCATION | | | | | | | | |
| I-5821 | 1 | 130+76 | 143+76 | NB | RT | 1,300 | 1,300 | | | | | | | | | |
| I-5821 | 1 | | | | | | | 136+00 | NB | RT | 30 | | 1 | | | |
| I-5821 | 1 | | | | | | | 140+00 | NB | RT | 30 | | 1 | | | |
| I-5821 | 1 | | | | | | | 143+76 | NB | RT | 30 | | 1 | | | |
| I-5821 | 1 | 143+76 | 168+11 | NB | LT | 2,435 | 2,435 | | | | | | | | | |
| I-5821 | 1 | | | | | | | 148+75 | NB | LT | 30 | | 1 | | | |
| I-5821 | 1 | | | | | | | 153+75 | NB | LT | 30 | | 1 | | | |
| I-5821 | 1 | | | | | | | 156+00 | NB | LT | 30 | | 1 | | | |
| I-5821 | 1 | | | | | | | 159+00 | NB | LT | 30 | X | | 1 | | |
| I-5821 | 1 | | | | | | | 162+00 | NB | LT | 30 | | 1 | | | |
| I-5821 | 1 | | | | | | | 165+00 | NB | LT | 30 | | 1 | | | |
| I-5821 | 1 | 168+11 | 171+11 | | | | | BRIDGE OVER DEEP RIVER | | | | | | | | |
| I-5821 | 1 | 171+11 | 179+00 | NB | LT | 789 | 789 | | | | | | | | | |
| I-5821 | 1 | | | | | | | 171+25 | NB | LT | 30 | | 1 | | | |
| I-5821 | 1 | | | | | | | 174+00 | NB | LT | 30 | | 1 | | | |
| I-5821 | 1 | 179+00 | 236+75 | NB | RT | 5,775 | 5,775 | | | | | | | | | |
| I-5821 | 1 | | | | | | | 179+00 | NB | RT | 30 | | 1 | | | |
| I-5821 | 1 | | | | | | | 184+00 | NB | RT | 30 | | 1 | | | |
| I-5821 | 1 | | | | | | | 187+00 | NB | RT | 30 | X | | 1 | | |
| I-5821 | 1 | | | | | | | 194+50 | NB | RT | 30 | X | | 1 | 2 | |
| I-5821 | 1 | | | | | | | 201+50 | NB | RT | 30 | X | | 1 | | |
| I-5821 | 1 | | | | | | | 204+75 | NB | RT | 30 | X | | 1 | 2 | |
| I-5821 | 1 | | | | | | | 210+25 | NB | RT | 30 | X | | 1 | | |
| I-5821 | 1 | | | | | | | 215+25 | NB | RT | 30 | | 1 | | | |
| I-5821 | 1 | | | | | | | 220+25 | NB | RT | 30 | | 1 | | | |
| I-5821 | 1 | | | | | | | 225+25 | NB | RT | 30 | | 1 | | | |
| I-5821 | 1 | | | | | | | 230+25 | NB | RT | 30 | | 1 | | | |
| I-5821 | 1 | | | | | | | 236+75 | NB | RT | 30 | | 1 | | | |
| I-5821 | 1 | 236+75 | 244+75 | NB | LT | 800 | 800 | | | | | | | | | |
| I-5821 | 1 | | | | | | | 240+75 | NB | LT | 30 | | 1 | | | |
| I-5821 | 1 | | | | | | | 244+75 | NB | LT | 30 | | 1 | | | |
| MAP 1 TOTALS | | | | | | 17,357 | 17,357 | | | | 1,110 | | 21 | 17 | 4 | 96 |

| | |
|------------------------|-----------|
| PROJECT REFERENCE NO. | SHEET NO. |
| I-5821, I-5738, I-5789 | 7 |

SHOULDER DRAIN SUMMARY

| PROJECT # | MAP # | AGGREGATE SHOULDER DRAIN | | | | | | SHOULDER DRAIN OUTLET LOCATIONS | | | | | Remove & Replace Concrete Apron EA | FLOWABLE FILL CY | ASPHALT PLANT MIX, PAVEMENT REPAIR TON | |
|---------------|-------|--------------------------|---------|----------|----------|----------------------|------------------------------|---|----------|----------|-----------------------------|-------------|--|------------------------|--|-----------|
| | | STATION | STATION | LOCATION | | SHOULDER DRAIN FT | 4" SHOULDER DRAIN PIPE FT | STATION | LOCATION | | LENGTH OF OUTLET PIPE FT | OUTLET TYPE | | | | |
| | | | | LANE | LT or RT | | | | LANE | LT or RT | | TIE TO DI | | | | PAD EA |
| I-5821 | 2 | 10+00 | 79+54 | NB | RT | 6,954 | 6,954 | | | | | | | | | |
| I-5821 | 2 | | | | | | | 10+00 | SB | RT | 30 | | 1 | | | |
| I-5821 | 2 | | | | | | | 14+00 | SB | RT | 30 | | 1 | | | |
| I-5821 | 2 | | | | | | | 21+00 | SB | RT | 30 | | 1 | | | |
| I-5821 | 2 | | | | | | | 26+00 | SB | RT | 30 | | 1 | | | |
| I-5821 | 2 | | | | | | | 31+00 | SB | RT | 30 | | 1 | | | |
| I-5821 | 2 | | | | | | | 36+00 | SB | RT | 30 | | 1 | | | |
| I-5821 | 2 | | | | | | | 41+00 | SB | RT | 30 | | 1 | | | |
| I-5821 | 2 | | | | | | | 46+00 | SB | RT | 30 | | 1 | | | |
| I-5821 | 2 | | | | | | | 51+00 | SB | RT | 30 | | 1 | | | |
| I-5821 | 2 | | | | | | | 56+00 | SB | RT | 30 | | 1 | | | |
| I-5821 | 2 | | | | | | | 60+25 | SB | RT | 30 | X | | 1 | | |
| I-5821 | 2 | | | | | | | 65+25 | SB | RT | 30 | | 1 | | | |
| I-5821 | 2 | | | | | | | 70+50 | SB | RT | 30 | X | | 1 | | |
| I-5821 | 2 | | | | | | | 75+00 | SB | RT | 30 | | 1 | | | |
| I-5821 | 2 | | | | | | | 79+25 | SB | RT | 30 | | 1 | | | |
| I-5821 | 2 | 79+54 | 83+19 | | | | | BRIDGE OVER DEEP RIVER | | | | | | | | |
| I-5821 | 2 | 83+19 | 121+30 | SB | RT | 3,811 | 3,811 | | | | | | | | | |
| I-5821 | 2 | | | | | | | 88+00 | SB | RT | 30 | | 1 | | | |
| I-5821 | 2 | | | | | | | 93+00 | SB | RT | 30 | | 1 | | | |
| I-5821 | 2 | | | | | | | 97+00 | SB | RT | 30 | | 1 | | | |
| I-5821 | 2 | | | | | | | 100+00 | SB | RT | 30 | | 1 | | | |
| I-5821 | 2 | | | | | | | 105+00 | SB | RT | 30 | | 1 | | | |
| I-5821 | 2 | | | | | | | 110+00 | SB | RT | 30 | | 1 | | | |
| I-5821 | 2 | | | | | | | 115+25 | SB | RT | 30 | X | | 1 | | |
| I-5821 | 2 | | | | | | | 120+50 | SB | RT | 30 | X | | 1 | | |
| I-5821 | 2 | 121+30 | 136+15 | | | | | NO SHOULDER DRAIN NEEDED IN THIS LOCATION | | | | | | | | |
| I-5821 | 2 | 136+15 | 162+67 | SB | RT | 2,652 | 2,652 | | | | | | | | | |
| I-5821 | 2 | | | | | | | 136+15 | SB | RT | | | | | | 11 |
| I-5821 | 2 | | | | | | | 140+00 | SB | RT | 30 | X | | 1 | | |
| I-5821 | 2 | | | | | | | 143+50 | SB | LT | | | | 1 | | |
| I-5821 | 2 | | | | | | | 145+00 | SB | RT | 30 | | 1 | | | |
| I-5821 | 2 | | | | | | | 150+00 | SB | RT | 30 | | 1 | | | |
| I-5821 | 2 | | | | | | | 150+00 | SB | LT | | | | 1 | 2 | |
| I-5821 | 2 | | | | | | | 154+75 | SB | RT | 30 | X | | 1 | | |
| I-5821 | 2 | 162+67 | 169+56 | | | | | NO SHOULDER DRAIN NEEDED IN THIS LOCATION | | | | | | | | |
| I-5821 | 2 | 169+56 | 200+76 | SB | RT | 3,120 | 3,120 | 187+11 to 200+76 in new Asp. Sh. | | | | | | | 96 | |
| I-5821 | 2 | | | | | | | 169+50 | SB | RT | 30 | | 1 | | | |
| I-5821 | 2 | | | | | | | 175+00 | SB | RT | 30 | X | | 1 | | |
| I-5821 | 2 | | | | | | | 178+00 | SB | RT | 30 | X | | 1 | | |
| I-5821 | 2 | | | | | | | 185+00 | SB | RT | 30 | X | | 1 | | |
| I-5821 | 2 | | | | | | | 190+23 | SB | RT | | | | | | |
| I-5821 | 2 | 200+76 | 233+25 | | | | | NO SHOULDER DRAIN NEEDED IN THIS LOCATION | | | | | | | | |
| I-5821 | 2 | 233+25 | 242+16 | SB | LT | 891 | 891 | | | | | | | | | |
| I-5821 | 2 | | | | | | | 237+25 | SB | LT | 30 | X | | 1 | | |
| I-5821 | 2 | | | | | | | 240+75 | SB | LT | 30 | X | | 1 | | |
| MAP 2 TOTALS | | | | | | 17,428 | 17,428 | | | | 990 | | 22 | 13 | 2 | 107 |
| I-5821 TOTALS | | | | | | 34,785 | 34,785 | | | | 2,100 | | 43 | 30 | 6 | 203 |

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| PROJECT REFERENCE NO. | SHEET NO. |
| I-5821, I-5738, I-5789 | 8 |

SHOULDER DRAIN SUMMARY

| PROJECT # | MAP # | AGGREGATE SHOULDER DRAIN | | | | | | SHOULDER DRAIN OUTLET LOCATIONS | | | | | | Remove & Replace Concrete Apron EA | FLOWABLE FILL CY | ASPHALT PLANT MIX, PAVEMENT REPAIR TON |
|----------------------|-------|--------------------------|---------|----------|----------|----------------------|------------------------------|--|----------|----------|-----------------------------|-------------|-----------|--|------------------------|--|
| | | STATION | STATION | LOCATION | | SHOULDER DRAIN FT | 4" SHOULDER DRAIN PIPE FT | STATION | LOCATION | | LENGTH OF OUTLET PIPE FT | OUTLET TYPE | | | | |
| | | | | LANE | LT or RT | | | | LANE | LT or RT | | TIE TO DI | PAD EA | | | |
| I-5738 | 3 | 10+00 | 15+00 | NB | LT | 500 | 500 | | | | | | | | | |
| I-5738 | 3 | | | | | | | 09+00 | NB | LT | 100 | | 1 | | | |
| I-5738 | 3 | | | | | | | 13+20 | NB | LT | 15 | X | | 1 | | |
| I-5738 | 3 | 15+00 | 65+13 | NB | RT | 5,013 | 5,013 | | | | | | | | | |
| I-5738 | 3 | | | | | | | 15+00 | NB | RT | 30 | | 1 | | | |
| I-5738 | 3 | | | | | | | 17+40 | NB | RT | 30 | X | | 1 | | |
| I-5738 | 3 | | | | | | | 26+80 | NB | RT | 30 | X | | 1 | | |
| I-5738 | 3 | | | | | | | 32+30 | NB | RT | 30 | X | | 1 | | |
| I-5738 | 3 | | | | | | | 37+30 | NB | RT | 30 | | 1 | | | |
| I-5738 | 3 | | | | | | | 42+30 | NB | RT | 30 | | 1 | | | |
| I-5738 | 3 | | | | | | | 47+30 | NB | RT | 30 | | 1 | | | |
| I-5738 | 3 | | | | | | | 53+60 | NB | RT | 30 | | 1 | | | |
| I-5738 | 3 | | | | | | | 55+60 | NB | RT | 30 | | 1 | | | |
| I-5738 | 3 | | | | | | | 60+60 | NB | RT | 30 | X | | 1 | | |
| MAP 3 TOTALS | | | | | | 5,513 | 5,513 | | | | 415 | | 7 | 5 | 0 | 0 |
| I-5738 | 6 | 10+00 | 28+50 | SB | LT | 1,850 | 1,850 | | | | | | | | | |
| I-5738 | 6 | | | | | | | 12+00 | SB | LT | 30 | X | | 1 | | |
| I-5738 | 6 | | | | | | | 15+50 | SB | LT | 30 | X | | 1 | | |
| I-5738 | 6 | | | | | | | 18+50 | SB | LT | 30 | X | | 1 | | |
| I-5738 | 6 | | | | | | | 22+50 | SB | LT | 30 | X | | 1 | | |
| I-5738 | 6 | | | | | | | 25+50 | SB | LT | 30 | | 1 | | | |
| I-5738 | 6 | 28+50 | 31+50 | | | | | NO SHOULDER DRAIN NEEDED IN THIS LOCATION | | | | | | | | |
| I-5738 | 6 | 31+50 | 52+00 | SB | RT | 2,050 | 2,050 | | | | | | | | | |
| I-5738 | 6 | | | | | | | 31+50 | SB | RT | 30 | | 1 | | | |
| I-5738 | 6 | | | | | | | 36+50 | SB | RT | 30 | | 1 | | | |
| I-5738 | 6 | | | | | | | 40+50 | SB | RT | 30 | | 1 | | | |
| I-5738 | 6 | | | | | | | 44+50 | SB | RT | 30 | X | | 1 | | |
| I-5738 | 6 | | | | | | | 49+50 | SB | RT | 30 | X | | 1 | | |
| I-5738 | 6 | 52+00 | 55+50 | | | | | NO SHOULDER DRAIN NEEDED IN THIS LOCATION | | | | | | | | |
| I-5738 | 6 | 55+00 | 67+13 | SB | RT | 1,213 | 1,213 | | | | | | | | | |
| I-5738 | 6 | | | | | | | 59+50 | SB | RT | 30 | | 1 | | | |
| I-5738 | 6 | | | | | | | 62+50 | SB | RT | 30 | X | | 1 | | |
| I-5738 | 6 | | | | | | | 67+50 | SB | RT | 100 | | 1 | | | |
| MAP 6 TOTALS | | | | | | 5,113 | 5,113 | | | | 460 | | 6 | 7 | 0 | 0 |
| I-5738 TOTALS | | | | | | 10,626 | 10,626 | | | | 875 | | 13 | 12 | 0 | 0 |

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|------------------------|-----------|
| PROJECT REFERENCE NO. | SHEET NO. |
| I-5821, I-5738, I-5789 | 9 |

SHOULDER DRAIN SUMMARY

| PROJECT # | MAP # | AGGREGATE SHOULDER DRAIN | | | | | | SHOULDER DRAIN OUTLET LOCATIONS | | | | | | Remove & Replace Concrete Apron EA | FLOWABLE FILL CY | ASPHALT PLANT MIX, PAVEMENT REPAIR TON |
|----------------------|-------|--------------------------|---------|------------------------|----------|----------------------|------------------------------|---------------------------------|----------|----------|-----------------------------|-------------|-----------|--|------------------------|--|
| | | STATION | STATION | LOCATION | | SHOULDER DRAIN FT | 4" SHOULDER DRAIN PIPE FT | STATION | LOCATION | | LENGTH OF OUTLET PIPE FT | OUTLET TYPE | | | | |
| | | | | LANE | LT or RT | | | | LANE | LT or RT | | TIE TO DI | PAD EA | | | |
| I-5789 | 9 | 10+00 | 13+00 | NB | LT | 300 | 300 | | | | | | | | | |
| I-5789 | 9 | | | | | | | 13+00 | NB | LT | 30 | | 1 | | | |
| I-5789 | 9 | 13+00 | 95+00 | NB | RT | 8,200 | 8,200 | | | | | | | | | |
| I-5789 | 9 | | | | | | | 18+00 | NB | RT | 30 | | 1 | | | |
| I-5789 | 9 | | | | | | | 23+00 | NB | RT | 30 | | 1 | | | |
| I-5789 | 9 | | | | | | | 28+00 | NB | RT | 30 | | 1 | | | |
| I-5789 | 9 | | | | | | | 32+00 | NB | RT | 30 | | 1 | | | |
| I-5789 | 9 | | | | | | | 37+00 | NB | RT | 30 | | 1 | | | |
| I-5789 | 9 | | | | LOW PT. | | | 42+00 | NB | RT | 30 | | 1 | | | |
| I-5789 | 9 | | | | | | | 46+00 | NB | RT | 30 | | 1 | | | |
| I-5789 | 9 | | | | | | | 49+50 | NB | RT | 30 | | 1 | | | |
| I-5789 | 9 | | | | | | | 53+50 | NB | RT | 30 | | 1 | | | |
| I-5789 | 9 | | | | | | | 57+50 | NB | RT | 30 | X | | 1 | | |
| I-5789 | 9 | | | | | | | 62+00 | NB | RT | 30 | X | | 1 | | |
| I-5789 | 9 | | | | | | | 65+50 | NB | RT | 30 | | 1 | | | |
| I-5789 | 9 | | | | | | | 69+00 | NB | RT | 30 | X | | 1 | | |
| I-5789 | 9 | | | | | | | 74+00 | NB | RT | 30 | X | | 1 | | |
| I-5789 | 9 | | | | | | | 79+00 | NB | RT | 30 | | 1 | | | |
| I-5789 | 9 | | | | HP 89+00 | | | 84+00 | NB | RT | 30 | | 1 | | | |
| I-5789 | 9 | | | | | | | 92+50 | NB | RT | 30 | | 1 | | | |
| I-5789 | 9 | | | | | | | 95+00 | NB | RT | 30 | | 1 | | | |
| MAP 9 TOTALS | | | | | | 8,500 | 8,500 | | | | 570 | | 15 | 4 | 0 | 0 |
| I-5789 | 10 | 09+80 | 47+00 | SB | LT | 3,720 | 3,720 | | | | | | | | | |
| I-5789 | 10 | | | | | | | 09+80 | SB | LT | 30 | X | | 1 | | |
| I-5789 | 10 | | | 13+75 - 14+75 HIGH PT. | | | | 20+00 | SB | LT | 30 | X | | 1 | | |
| I-5789 | 10 | | | | | | | 25+00 | SB | LT | 30 | X | | 1 | | |
| I-5789 | 10 | | | | | | | 31+25 | SB | LT | 30 | X | | 1 | | |
| I-5789 | 10 | | | | | | | 35+25 | SB | LT | 30 | | 1 | | | |
| I-5789 | 10 | | | | | | | 39+00 | SB | LT | 30 | X | | 1 | | |
| I-5789 | 10 | | | | | | | 41+00 | SB | LT | 30 | X | | 1 | | |
| I-5789 | 10 | | | | | | | 47+00 | SB | LT | 30 | | 1 | | | |
| I-5789 | 10 | 47+00 | 96+87 | SB | RT | 4,987 | 4,987 | | | | | | | | | |
| I-5789 | 10 | | | | | | | 49+75 | SB | RT | 30 | | 1 | | | |
| I-5789 | 10 | | | | LOW PT. | | | 53+75 | SB | RT | 30 | | 1 | | | |
| I-5789 | 10 | | | | | | | 58+75 | SB | RT | 30 | | 1 | | | |
| I-5789 | 10 | | | | | | | 63+75 | SB | RT | 30 | | 1 | | | |
| I-5789 | 10 | | | | | | | 69+00 | SB | RT | 30 | X | | 1 | | |
| I-5789 | 10 | | | | | | | 74+00 | SB | RT | 30 | | 1 | | | |
| I-5789 | 10 | | | | | | | 79+00 | SB | RT | 30 | | 1 | | | |
| I-5789 | 10 | | | | | | | 84+00 | SB | RT | 30 | | 1 | | | |
| I-5789 | 10 | | | | | | | 88+00 | SB | RT | 30 | | 1 | | | |
| I-5789 | 10 | | | | | | | 92+00 | SB | RT | 30 | | 1 | | | |
| MAP 10 TOTALS | | | | | | 8,707 | 8,707 | | | | 540 | | 11 | 7 | 0 | 0 |
| I-5789 TOTALS | | | | | | 17,207 | 17,207 | | | | 1,110 | | 26 | 11 | 0 | 0 |
| PROJECT GRAND TOTALS | | | | | | 62,618 | 62,618 | | | | 4,085 | | 82 | 53 | 6 | 203 |

REMOVAL OF EXISTING SHOULDER DRAIN OUTLET PIPE & CONCRETE PADS FOR SHOULDER DRAIN PIPE OUTLET

EXISTING SHOULDER DRAIN OUTLET LOCATIONS

| PROJECT # | MAP # | STATION | LOCATION | | REMOVAL OF EXISTING SHOULDER DRAIN OUTLET PIPE | REMOVAL OF EXISTING CONCRETE PADS FOR SHOULDER DRAIN PIPE OUTLETS | REMARKS |
|---------------|-------|---------|----------|----------|--|---|---------------------------------|
| | | | LANE | LT or RT | FT | EA | |
| I-5821 | 1 | 11+00 | NB | RT | 30 | | OUTLET PIPE TIED TO DI |
| I-5821 | 1 | 15+25 | NB | RT | 30 | 1 | |
| I-5821 | 1 | 20+25 | NB | RT | 30 | 1 | |
| I-5821 | 1 | 134+65 | NB | RT | 30 | 1 | |
| I-5821 | 1 | 139+65 | NB | RT | 30 | 1 | |
| I-5821 | 1 | 143+00 | NB | RT | 30 | 1 | |
| I-5821 | 1 | 147+65 | NB | LT | 30 | 1 | |
| I-5821 | 1 | 151+50 | NB | LT | 30 | 1 | |
| I-5821 | 1 | 155+35 | NB | LT | 30 | 1 | |
| I-5821 | 1 | 161+35 | NB | LT | 30 | 1 | |
| I-5821 | 1 | 164+35 | NB | LT | 30 | 1 | |
| I-5821 | 1 | 174+00 | NB | LT | 30 | 1 | |
| I-5821 | 1 | 176+35 | NB | LT | 30 | 1 | |
| I-5821 | 1 | 179+35 | NB | RT | 30 | 1 | |
| I-5821 | 1 | 184+35 | NB | RT | 30 | | NO EXISTING CONCRETE OUTLET PAD |
| I-5821 | 1 | 189+50 | NB | RT | 30 | 1 | |
| I-5821 | 1 | 194+50 | NB | RT | 30 | 1 | |
| I-5821 | 1 | 199+50 | NB | RT | 30 | 1 | OUTLET NOT FOUND |
| I-5821 | 1 | 205+35 | NB | RT | 30 | | OUTLET PIPE TIED TO DI |
| I-5821 | 1 | 211+35 | NB | RT | 30 | | OUTLET PIPE TIED TO DI |
| I-5821 | 1 | 215+35 | NB | RT | 30 | 1 | |
| I-5821 | 1 | 219+35 | NB | RT | 30 | 1 | |
| I-5821 | 1 | 223+35 | NB | RT | 30 | | NO EXISTING CONCRETE OUTLET PAD |
| I-5821 | 1 | 227+50 | NB | RT | 30 | 1 | OUTLET NOT FOUND |
| I-5821 | 1 | 231+50 | NB | RT | 30 | 1 | |
| I-5821 | 1 | 237+00 | NB | RT | 30 | 1 | OUTLET NOT FOUND |
| I-5821 | 1 | 240+00 | NB | LT | 30 | 1 | OUTLET NOT FOUND |
| I-5821 | 1 | 244+25 | NB | LT | 30 | 1 | OUTLET NOT FOUND |
| MAP 1 TOTALS | | | | | 840 | 23 | |
| I-5821 | 2 | 10+00 | SB | RT | 30 | | NO EXISTING CONCRETE OUTLET PAD |
| I-5821 | 2 | 13+70 | SB | RT | 30 | | NO EXISTING CONCRETE OUTLET PAD |
| I-5821 | 2 | 20+95 | SB | RT | 30 | 1 | |
| I-5821 | 2 | 24+90 | SB | RT | 30 | 1 | |
| I-5821 | 2 | 28+85 | SB | RT | 30 | 1 | |
| I-5821 | 2 | 32+90 | SB | RT | 30 | 1 | |
| I-5821 | 2 | 33+70 | SB | RT | 30 | 1 | |
| I-5821 | 2 | 36+90 | SB | RT | 30 | 1 | |
| I-5821 | 2 | 40+85 | SB | RT | 30 | 1 | |
| I-5821 | 2 | 45+90 | SB | RT | 30 | 1 | |
| I-5821 | 2 | 50+90 | SB | RT | 30 | 1 | |
| I-5821 | 2 | 55+90 | SB | RT | 30 | 1 | |
| I-5821 | 2 | 60+90 | SB | RT | 30 | 1 | |
| I-5821 | 2 | 65+90 | SB | RT | 30 | 1 | |
| I-5821 | 2 | 70+90 | SB | RT | 30 | 1 | |
| I-5821 | 2 | 75+90 | SB | RT | 30 | | NO EXISTING CONCRETE OUTLET PAD |
| I-5821 | 2 | 79+25 | SB | RT | 30 | | NO EXISTING CONCRETE OUTLET PAD |
| I-5821 | 2 | 85+20 | SB | RT | 30 | 1 | |
| I-5821 | 2 | 89+20 | SB | RT | 30 | 1 | |
| I-5821 | 2 | 92+40 | SB | RT | 30 | 1 | |
| I-5821 | 2 | 96+40 | SB | RT | 30 | 1 | OUTLET NOT FOUND |
| I-5821 | 2 | 99+90 | SB | RT | 30 | 1 | OUTLET NOT FOUND |
| I-5821 | 2 | 102+90 | SB | RT | 30 | 1 | OUTLET NOT FOUND |
| I-5821 | 2 | 107+90 | SB | RT | 30 | 1 | OUTLET NOT FOUND |
| I-5821 | 2 | 112+55 | SB | RT | 30 | 1 | |
| I-5821 | 2 | 117+55 | SB | RT | 30 | 1 | |
| I-5821 | 2 | 149+55 | SB | RT | 30 | | OUTLET PIPE TIED TO DI |
| I-5821 | 2 | 235+90 | SB | LT | 30 | 1 | |
| I-5821 | 2 | 240+60 | SB | LT | 30 | | OUTLET PIPE TIED TO DI |
| MAP 2 TOTALS | | | | | 870 | 23 | |
| I-5821 TOTALS | | | | | 1,710 | 46 | |

REMOVAL OF EXISTING SHOULDER DRAIN OUTLET PIPE & CONCRETE PADS FOR SHOULDER DRAIN PIPE OUTLET

EXISTING SHOULDER DRAIN OUTLET LOCATIONS

| PROJECT # | MAP # | STATION | LOCATION | | REMOVAL OF EXISTING SHOULDER DRAIN OUTLET PIPE | REMOVAL OF EXISTING CONCRETE PADS FOR SHOULDER DRAIN PIPE OUTLETS | REMARKS |
|----------------------|-------|---------|----------|----------|--|---|---------------------------------|
| | | | LANE | LT or RT | FT | EA | |
| I-5738 | 3 | 10+00 | NB | LT | 30 | 1 | OUTLET NOT FOUND |
| I-5738 | 3 | 13+35 | NB | LT | 30 | | OUTLET PIPE TIED TO DI |
| I-5738 | 3 | 15+00 | NB | RT | 30 | 1 | OUTLET NOT FOUND |
| I-5738 | 3 | 17+53 | NB | RT | 30 | | OUTLET PIPE TIED TO DI |
| I-5738 | 3 | 26+72 | NB | RT | 30 | | OUTLET PIPE TIED TO DI |
| I-5738 | 3 | 27+77 | NB | RT | 30 | 1 | |
| I-5738 | 3 | 32+60 | NB | RT | 30 | 1 | |
| I-5738 | 3 | 36+75 | NB | RT | 30 | 1 | OUTLET NOT FOUND |
| I-5738 | 3 | 41+75 | NB | RT | 30 | 1 | |
| I-5738 | 3 | 46+80 | NB | RT | 30 | 1 | |
| I-5738 | 3 | 53+70 | NB | RT | 30 | 1 | |
| I-5738 | 3 | 57+45 | NB | RT | 30 | 1 | OUTLET NOT FOUND |
| I-5738 | 3 | 62+45 | NB | RT | 30 | 1 | OUTLET NOT FOUND |
| MAP 3 TOTALS | | | | | 390 | 10 | |
| I-5738 | 6 | 14+14 | SB | LT | 30 | 1 | OUTLET NOT FOUND |
| I-5738 | 6 | 18+49 | SB | LT | 30 | | OUTLET PIPE TIED TO DI |
| I-5738 | 6 | 22+57 | SB | LT | 30 | | OUTLET PIPE TIED TO DI |
| I-5738 | 6 | 25+49 | SB | LT | 30 | | OUTLET PIPE TIED TO DI |
| I-5738 | 6 | 31+59 | SB | RT | 30 | | NO EXISTING CONCRETE OUTLET PAD |
| I-5738 | 6 | 35+59 | SB | RT | 30 | 1 | OUTLET NOT FOUND |
| I-5738 | 6 | 39+59 | SB | RT | 30 | 1 | OUTLET NOT FOUND |
| I-5738 | 6 | 44+47 | SB | RT | 30 | | OUTLET PIPE TIED TO DI |
| I-5738 | 6 | 49+50 | SB | RT | 30 | | OUTLET PIPE TIED TO DI |
| I-5738 | 6 | 57+25 | SB | RT | 30 | 1 | OUTLET NOT FOUND |
| I-5738 | 6 | 59+86 | SB | RT | 30 | 1 | |
| I-5738 | 6 | 62+40 | SB | RT | 30 | | OUTLET PIPE TIED TO DI |
| I-5738 | 6 | 66+75 | SB | RT | 30 | 1 | OUTLET NOT FOUND |
| MAP 6 TOTALS | | | | | 390 | 6 | |
| I-5738 TOTALS | | | | | 780 | 16 | |
| I-5789 | 9 | 12+95 | NB | LT | 30 | 1 | |
| I-5789 | 9 | 18+25 | NB | RT | 30 | 1 | |
| I-5789 | 9 | 23+24 | NB | RT | 30 | 1 | |
| I-5789 | 9 | 28+22 | NB | RT | 30 | | NO EXISTING CONCRETE OUTLET PAD |
| I-5789 | 9 | 33+21 | NB | RT | 30 | | NO EXISTING CONCRETE OUTLET PAD |
| I-5789 | 9 | 38+15 | NB | RT | 30 | 1 | |
| I-5789 | 9 | 41+90 | NB | RT | 30 | | NO EXISTING CONCRETE OUTLET PAD |
| I-5789 | 9 | 45+85 | NB | RT | 30 | 1 | |
| I-5789 | 9 | 50+80 | NB | RT | 30 | 1 | |
| I-5789 | 9 | 55+80 | NB | RT | 30 | 1 | |
| I-5789 | 9 | 60+80 | NB | RT | 30 | 1 | |
| I-5789 | 9 | 65+75 | NB | RT | 30 | 1 | |
| I-5789 | 9 | 70+70 | NB | RT | 30 | 1 | |
| I-5789 | 9 | 75+65 | NB | RT | 30 | 1 | |
| I-5789 | 9 | 80+65 | NB | RT | 30 | 1 | |
| I-5789 | 9 | 84+65 | NB | RT | 30 | 1 | |
| I-5789 | 9 | 91+97 | NB | RT | 30 | 1 | |
| I-5789 | 9 | 94+50 | NB | RT | 30 | 1 | |
| MAP 9 TOTALS | | | | | 540 | 15 | |
| I-5789 | 10 | 10+00 | SB | LT | 30 | | OUTLET PIPE TIED TO DI |
| I-5789 | 10 | 17+90 | SB | LT | 30 | 1 | |
| I-5789 | 10 | 21+85 | SB | LT | 30 | 1 | |
| I-5789 | 10 | 26+85 | SB | LT | 30 | 1 | |
| I-5789 | 10 | 31+95 | SB | LT | 30 | 1 | |
| I-5789 | 10 | 36+75 | SB | LT | 30 | 1 | |
| I-5789 | 10 | 41+70 | SB | LT | 30 | 1 | |
| I-5789 | 10 | 46+60 | SB | LT | 30 | | NO EXISTING CONCRETE OUTLET PAD |
| I-5789 | 10 | 49+70 | SB | RT | 30 | 1 | |
| I-5789 | 10 | 53+30 | SB | RT | 30 | 1 | |
| I-5789 | 10 | 56+45 | SB | RT | 30 | 1 | |
| I-5789 | 10 | 60+40 | SB | RT | 30 | 1 | |
| I-5789 | 10 | 65+40 | SB | RT | 30 | 1 | |
| I-5789 | 10 | 70+35 | SB | RT | 30 | 1 | |
| I-5789 | 10 | 75+40 | SB | RT | 30 | 1 | |
| I-5789 | 10 | 80+30 | SB | RT | 30 | | NO EXISTING CONCRETE OUTLET PAD |
| I-5789 | 10 | 85+35 | SB | RT | 30 | | NO EXISTING CONCRETE OUTLET PAD |
| I-5789 | 10 | 90+45 | SB | RT | 30 | | NO EXISTING CONCRETE OUTLET PAD |
| MAP 10 TOTALS | | | | | 540 | 13 | |
| I-5789 TOTALS | | | | | 1,080 | 28 | |
| PROJECT GRAND TOTALS | | | | | 3,570 | 90 | |

| | | |
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| PROJECT NO. | SHEET NO. | TOTAL NO. |
| I-5821, I-5738, I-5789 | 16 | |

THERMOPLASTIC AND PAINT QUANTITIES

| PROJECT NO | COUNTY | MAP NO | ROUTE | DESCRIPTION | TYP NO | LANES | LANE TYPE | LENGTH | WIDTH | 4697000000-E | 4710000000-E | 4725000000-E | | | 4800000000-N | 4805000000-N | | | | | 4815000000-E | | 4820000000-E | 4825000000-E | | | |
|----------------------------------|----------|--------|---------|--|--------|-------|-----------|--------------|-------|-------------------------------|--------------------------------|----------------------------|----------------------------|-------------------------|---|---|---|---|---|----------------------|-----------------------|----------------------|-----------------------|--------------|--|--|--|
| | | | | | | | | | | 8" X 120 M WHITE THERMO LF | 24" X 120 M WHITE THERMO LF | THERMO LT ARROW 90 M EA | THERMO RT ARROW 90 M EA | THERMO RAMP ARROW EA | COLD APPLIED PLASTIC PAVEMENT MARKING CHARACTER, "ONLY" TYPE II EA | COLD APPLIED PLASTIC PAVEMENT MARKING SYMBOL, MERGE LT, TYPE II EA | COLD APPLIED PLASTIC PAVEMENT MARKING SYMBOL, RT ARROW, TYPE II EA | COLD APPLIED PLASTIC PAVEMENT MARKING SYMBOL, RAMP ARROW, TYPE II EA | COLD APPLIED PLASTIC PAVEMENT MARKING SYMBOL, LT ARROW, TYPE II EA | 6" WHITE PAINT LF | 6" YELLOW PAINT LF | 8" WHITE PAINT LF | 12" WHITE PAINT LF | | | | |
| 50464.3.1 | Guilford | 1 | I-85 NB | FROM RANDOLPH COUNTY LINE TO KIVETT DR. | 1 | 3 | MD | 0.271 | 36 | | | | | | 8 | 3 | 4 | | | | 72,832 | 47,394 | | 7,084 | | | |
| | | | | | 1 | 3 | MD | 0.261 | 42 | | | | | | | | | | | | | | | | | | |
| | | | | | 1 | 4 | MD | 0.041 | 48 | | | | | | | | | | | | | | | | | | |
| | | | | | 1 | 4 | MD | 0.236 | 48 | | | | | | | | | | | | | | | | | | |
| | | | | | 1 | 4 | MD | 0.052 | 48 | | | | | | | | | | | | | | | | | | |
| | | | | | 1 | 3 | MD | 0.277 | 36 | | | | | | | | | | | | | | | | | | |
| | | | | | 1 | 3 | MD | 0.128 | 36 | | | | | | | | | | | | | | | | | | |
| | | | | | 1 | 3 | MD | 0.115 | 36 | | | | | | | | | | | | | | | | | | |
| | | | | | 1 | 3 | MD | 0.019 | 36 | | | | | | | | | | | | | | | | | | |
| | | | | | 1 | 3 | MD | 0.062 | 36 | | | | | | | | | | | | | | | | | | |
| | | | | | 1 | 3 | MD | 0.028 | 36 | | | | | | | | | | | | | | | | | | |
| | | | | | 1 | 3 | MD | 0.087 | 36 | | | | | | | | | | | | | | | | | | |
| | | | | | 1 | 3 | MD | 0.405 | 36 | | | | | | | | | | | | | | | | | | |
| | | | | | 1 | 4 | MD | 0.073 | 48 | | | | | | | | | | | | | | | | | | |
| | | | | | 1 | 4 | MD | 0.232 | 48 | | | | | | | | | | | | | | | | | | |
| | 1 | 3 | MD | 0.707 | 36 | | | | | | | | | | | | | | | | | | | | | | |
| | 1 | 3 | MD | 0.057 | 36 | | | | | | | | | | | | | | | | | | | | | | |
| | 1 | 3 | MD | 1.395 | 36 | | | | | | | | | | | | | | | | | | | | | | |
| | 1 | 3 | MD | 0.042 | 36 | | | | | | | | | | | | | | | | | | | | | | |
| TOTAL FOR MAP NO. 1 | | | | | | | | 4.488 | | | | | | 8 | 3 | 4 | | | | 72,832 | 47,394 | | 7,084 | | | | |
| 50464.3.1 | Guilford | 2 | I-85 SB | FROM KIVETT DR. TO RANDOLPH COUNTY LINE (BRIDGE OVER KIVETT DR.) | 1 | 3 | MD | 0.041 | 36 | | | | | | | | | | | | 73,102 | 46,870 | | 4,300 | | | |
| | | | | | 1 | 3 | MD | 1.317 | 36 | | | | | | | | | | | | | | | | | | |
| | | | | | 1 | 3 | MD | 0.069 | 36 | | | | | | | | | | | | | | | | | | |
| | | | | | 1 | 3 | MD | 0.059 | 36 | | | | | | | | | | | | | | | | | | |
| | | | | | 1 | 3 | MD | 0.009 | 36 | | | | | | | | | | | | | | | | | | |
| | | | | | 1 | 3 | MD | 0.018 | 36 | | | | | | | | | | | | | | | | | | |
| | | | | | 1 | 3 | MD | 0.015 | 36 | | | | | | | | | | | | | | | | | | |
| | | | | | 1 | 3 | MD | 0.62 | 36 | | | | | | | | | | | | | | | | | | |
| | | | | | 1 | 4 | MD | 0.299 | 48 | | | | | | | | | | | | | | | | | | |
| | | | | | 1 | 3 | MD | 0.484 | 36 | | | | | | | | | | | | | | | | | | |
| | | | | | 1 | 3 | MD | 0.041 | 36 | | | | | | | | | | | | | | | | | | |
| | | | | | 1 | 3 | MD | 0.062 | 36 | | | | | | | | | | | | | | | | | | |
| | | | | | 1 | 3 | MD | 0.028 | 36 | | | | | | | | | | | | | | | | | | |
| | | | | | 1 | 3 | MD | 0.332 | 36 | | | | | | | | | | | | | | | | | | |
| | | | | | 1 | 3 | MD | 0.259 | 36 | | | | | | | | | | | | | | | | | | |
| | | | | | 1 | 3 | MD | 0.069 | 48 | | | | | | | | | | | | | | | | | | |
| | | | | | 1 | 4 | MD | 0.014 | 48 | | | | | | | | | | | | | | | | | | |
| | | | | | 1 | 4 | MD | 0.034 | 48 | | | | | | | | | | | | | | | | | | |
| | | | | | 1 | 4 | MD | 0.013 | 48 | | | | | | | | | | | | | | | | | | |
| | | | | | 1 | 4 | MD | 0.143 | 48 | | | | | | | | | | | | | | | | | | |
| | 1 | 4 | MD | 0.033 | 48 | | | | | | | | | | | | | | | | | | | | | | |
| | 1 | 5 | MD | 0.114 | 60 | | | | | | | | | | | | | | | | | | | | | | |
| | 1 | 4 | MD | 0.055 | 54 | | | | | | | | | | | | | | | | | | | | | | |
| | 1 | 3 | MD | 0.285 | 42 | | | | | | | | | | | | | | | | | | | | | | |
| | 1 | 3 | MD | 0.025 | 36 | | | | | | | | | | | | | | | | | | | | | | |
| TOTAL FOR MAP NO. 2 | | | | | | | | 4.438 | | | | | | 8 | 8 | 4 | | | | 73,102 | 46,870 | | 4,300 | | | | |
| TOTAL FOR PROJ NO. I-5821 | | | | | | | | 8.926 | | | | | | 8 | 11 | 4 | | | | 145,934 | 94,264 | | 11,384 | | | | |
| | | | | | | | | | | | | | | | 15 | | | | | 240,198 | | | | | | | |

