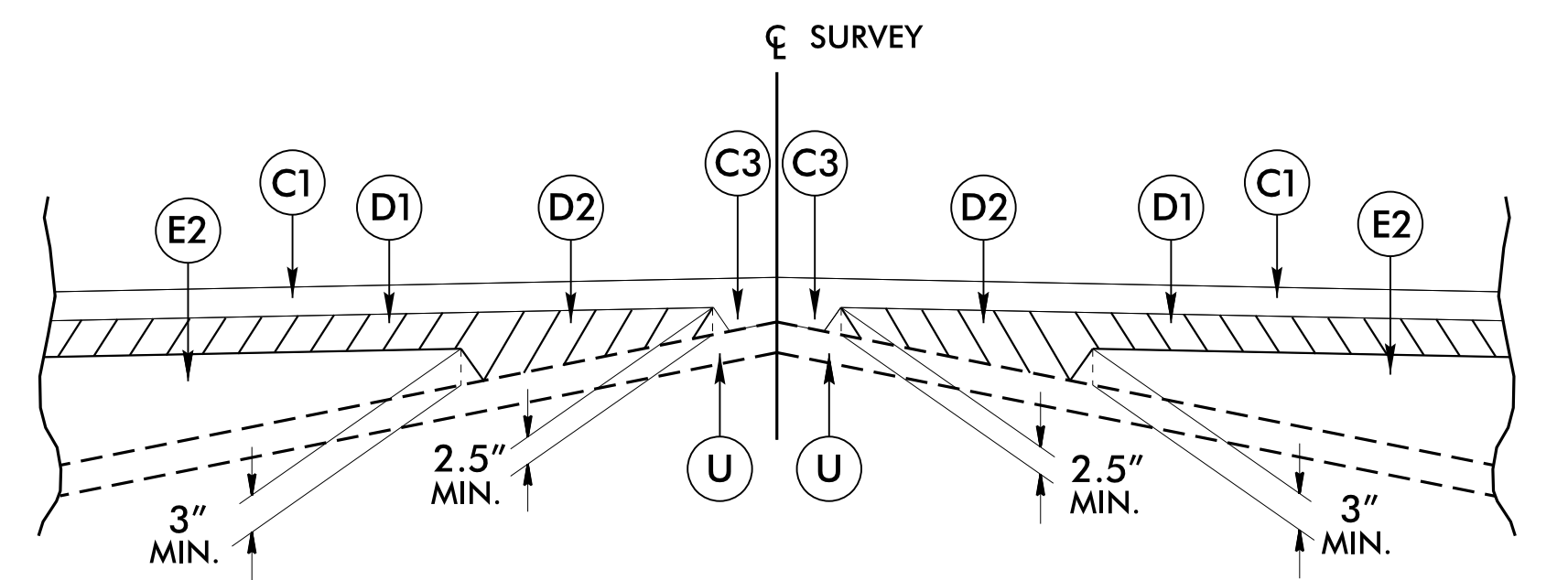
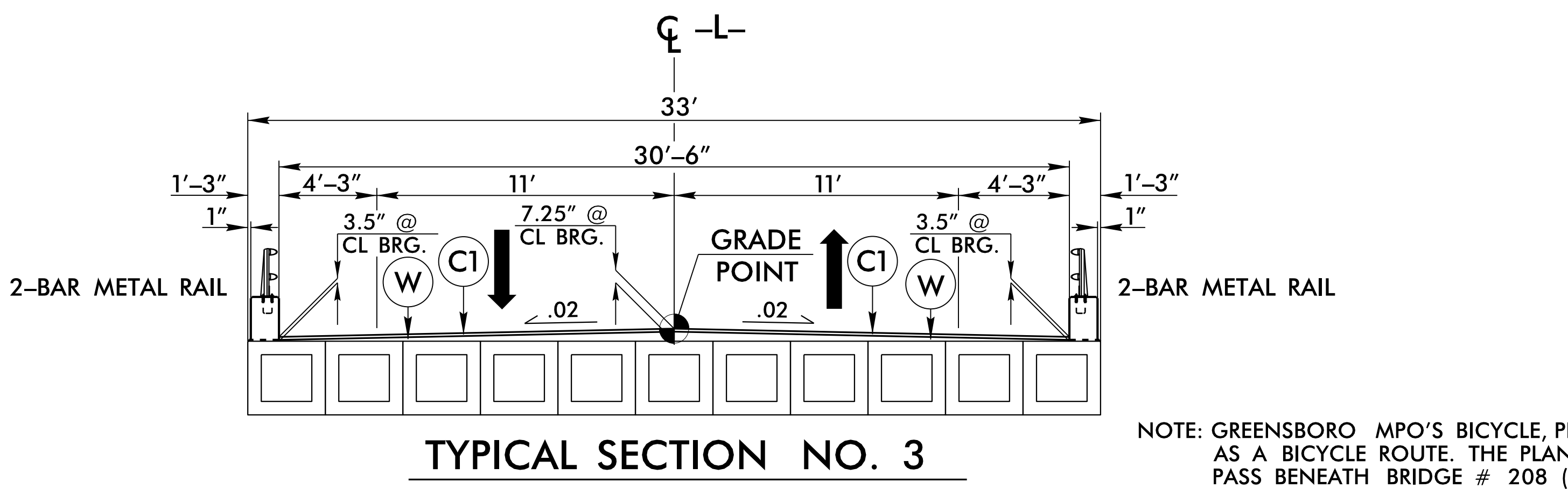
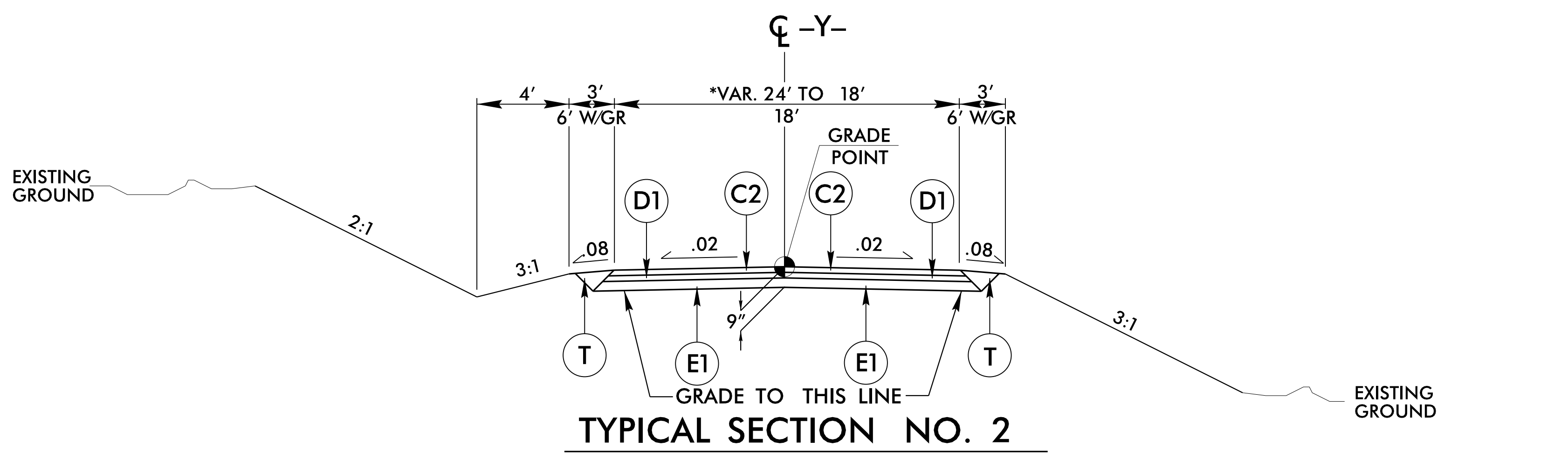
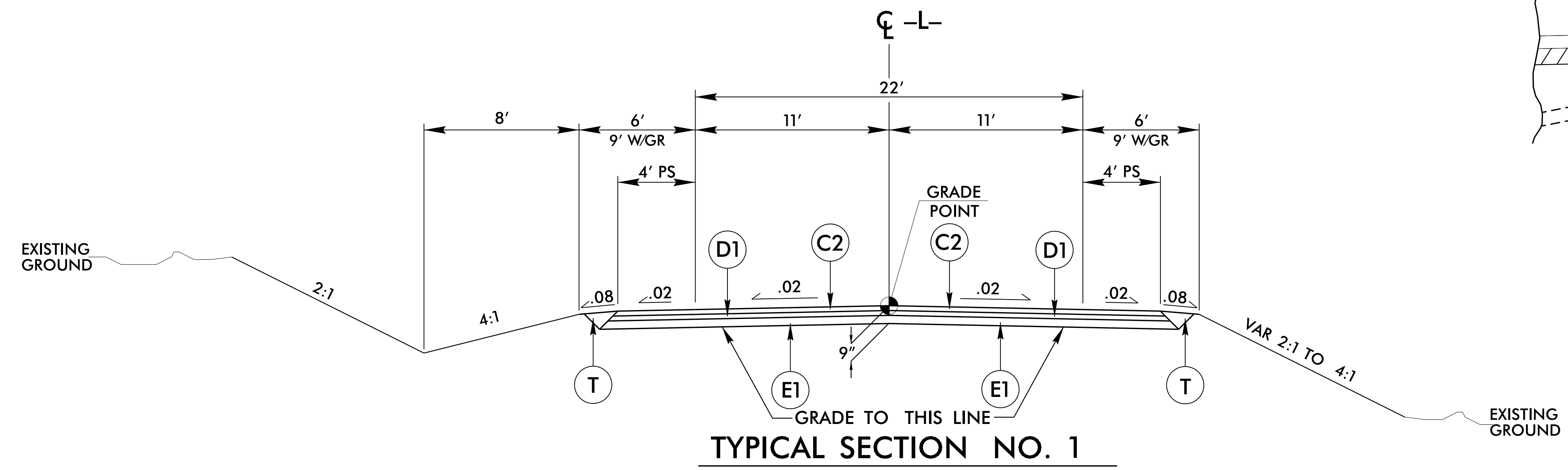


6/2/99

PAVEMENT SCHEDULE			
C1	PROP. APPROX. 1 1/4" ASPHALT CONCRETE SURFACE COURSE, TYPE SF9.5A, AT AN AVERAGE RATE OF 137.5 LBS. PER SQ. YD.	E1	PROP. APPROX. 4" ASPHALT CONCRETE BASE COURSE, TYPE B25.0B, AT AN AVERAGE RATE OF 456 LBS. PER SQ. YD.
C2	PROP. APPROX. 2 1/2" ASPHALT CONCRETE SURFACE COURSE, TYPE SF9.5A, AT AN AVERAGE RATE OF 137.5 LBS. PER SQ. YD. IN EACH OF TWO LAYERS.	E2	PROP. VAR. DEPTH ASPHALT CONCRETE BASE COURSE, TYPE B25.0B, AT AN AVERAGE RATE OF 114 LBS. PER SQ. YD. PER 1" DEPTH. TO BE PLACED IN LAYERS NOT LESS THAN 3" IN DEPTH OR GREATER THAN 5 1/2" IN DEPTH.
C3	PROP. VAR. DEPTH ASPHALT CONCRETE SURFACE COURSE, TYPE SF9.5A, AT AN AVERAGE RATE OF 110 LBS. PER SQ. YD. PER 1" DEPTH. TO BE PLACED IN LAYERS NOT TO EXCEED 1 1/2" IN DEPTH.	T	EARTH MATERIAL.
D1	PROP. APPROX. 2 1/2" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE I19.0B, AT AN AVERAGE RATE OF 285 LBS. PER SQ. YD.	U	EXISTING PAVEMENT.
D2	PROP. VAR. DEPTH ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE I19.0B, AT AN AVERAGE RATE OF 114 LBS. PER SQ. YD. PER 1" DEPTH. TO BE PLACED IN LAYERS NOT LESS THAN 2 1/2" IN DEPTH OR GREATER THAN 4" IN DEPTH.	W	VARIABLE DEPTH ASPHALT PAVEMENT (SEE STANDARD WEDGING DETAIL).

NOTE: PAVEMENT EDGE SLOPES ARE 1:1 UNLESS SHOWN OTHERWISE.

PROJECT REFERENCE NO. B-4961	SHEET NO. 2A-1
ROADWAY DESIGN ENGINEER DANIEL W. GARDNER, JR. SEAL 033871 3/10/2016	PAVEMENT DESIGN ENGINEER CLARK S. MORRISON SEAL 022896 3/10/2016
<b>DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED</b>	



NOTE: TRANSITION FROM EXISTING TO TYPICAL SECTION NO. 1  
 -L- STA. 14+50.00 TO STA. 15+00.00  
 USE TYPICAL SECTION NO. 1  
 -L- STA. 15+00.00 TO STA. 16+55.88 (BEGIN BRIDGE)  
 -L- STA. 17+58.13 (END BRIDGE) TO STA. 18+50.00  
 NOTE: TRANSITION FROM TYPICAL SECTION NO. 1 TO EXISTING  
 -L- STA. 18+50.00 TO STA. 19+00.00

USE TYPICAL SECTION NO. 2  
 \*-Y- STA. 10+22.51 TO STA. 10+72.51  
 -Y- STA. 10+72.51 TO STA. 11+16.46  
 NOTE: TRANSITION FROM TYPICAL SECTION NO. 2 TO EXISTING  
 -Y- STA. 11+16.46 TO STA. 11+66.46

USE TYPICAL SECTION NO. 3  
 -L- STA. 16+55.88 (BEGIN BRIDGE) TO STA. 17+58.13 (END BRIDGE)

NOTE: GREENSBORO MPO'S BICYCLE, PEDESTRIAN, AND GREENWAY'S MASTER PLAN IDENTIFIES SR 3051 (KNOX ROAD) AS A BICYCLE ROUTE. THE PLAN ALSO IDENTIFIES THE FUTURE SEDALIA GREENWAY (GREENWAY # 80) TO PASS BENEATH BRIDGE # 208 (NORTH SIDE).

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