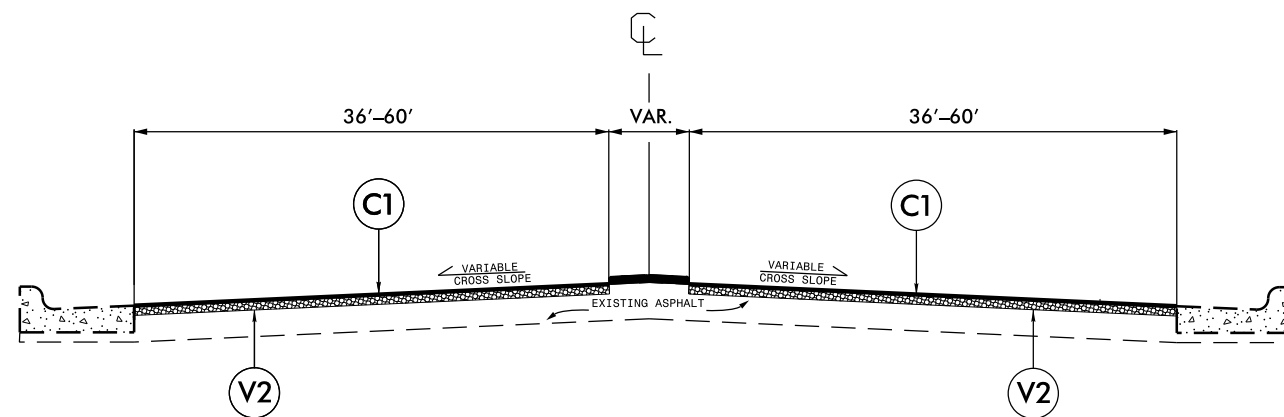


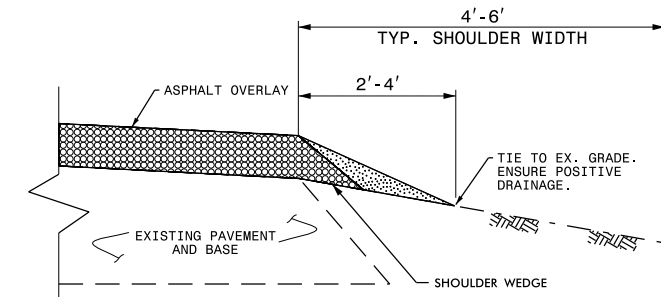
TYPICAL SECTION NO. 3

MAP 6: FROM BEGIN C&G EBL & WBL TO END C&G EBL & WBL.



NOTE:

1. PLACE ASPHALT SURFACE COURSE AT FULL WIDTH OF EXISTING ASPHALT PAVEMENT, AS DIRECTED BY THE ENGINEER.



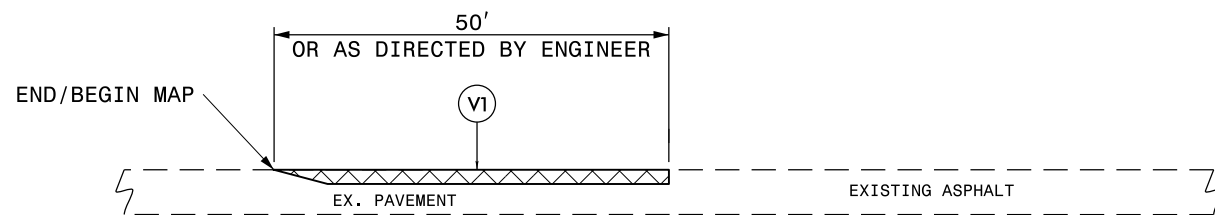
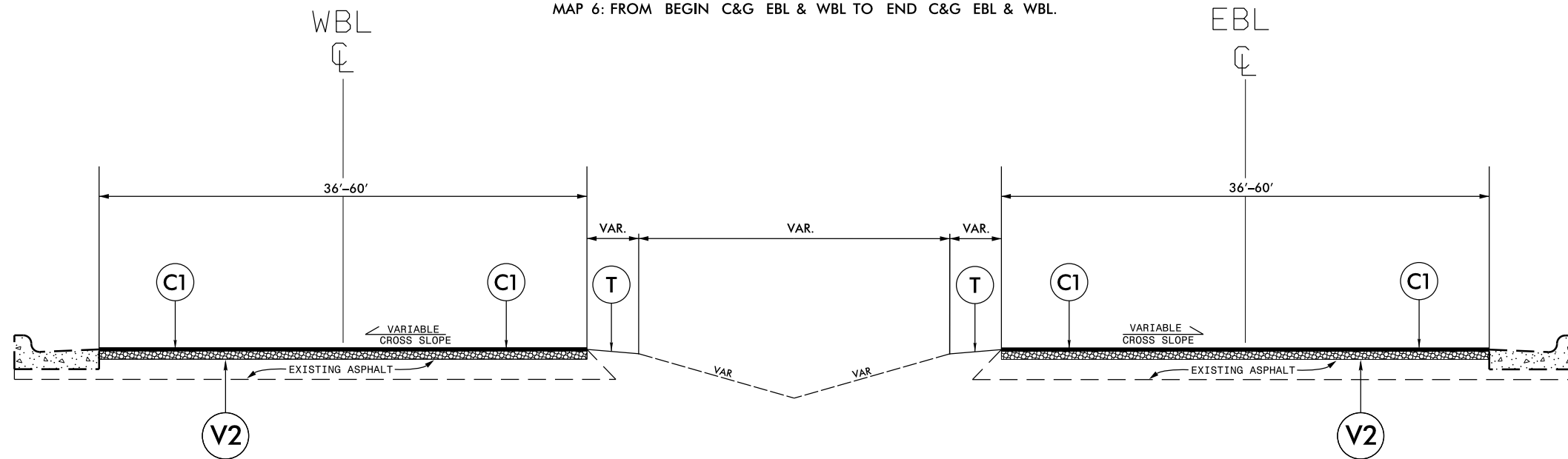
SHOULDER RECONSTRUCTION DETAIL

NOTE:

1. SHOULDERS SHALL BE RECONSTRUCTED AS SHOWN IN STD. DWG. NO. 560.01 & 560.02, WITH A MINIMUM SLOPE OF 1" PER FOOT TO ENSURE POSITIVE DRAINAGE AWAY FROM THE ROADWAY.
2. A VEGETATIVE BUFFER SHALL BE MAINTAINED BETWEEN THE DISTURBED AREA ALONG THE EDGE OF PAVEMENT AND THE DITCH SHOULDER POINT TO MINIMIZE EROSION. PULLING DITCHES OR CUTTING SHOULDERS TO GENERATE BORROW MATERIAL WILL NOT BE ALLOWED.
3. REQUIRED BORROW MATERIAL MAY BE OBTAINED FROM NCDOT STOCKPILES. ANY EXCESS MATERIAL SHALL BE DISPOSED OF BY THE CONTRACTOR IN AN APPROVED DISPOSAL SITE.

TYPICAL SECTION NO. 4

MAP 6: FROM BEGIN C&G EBL & WBL TO END C&G EBL & WBL.

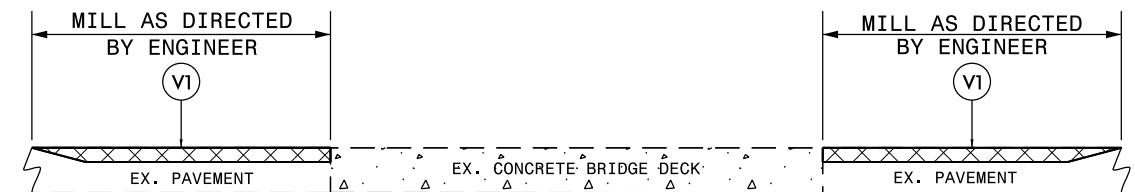


DETAIL 1

MAIN LINE MILLING

NOTE:

1. INCLUDES INCIDENTAL MILLING AT THE ENDS OF MAIN LINE SECTIONS, OR AS DIRECTED BY THE ENGINEER.
2. PAVE TO THE END OF THE MILLED SURFACE TO CREATE A SMOOTH TRANSITION.



DETAIL 2

BRIDGE MILLING

NOTE:

1. MILLING SHALL BE PERFORMED AT BRIDGE APPROACHES AS DIRECTED BY THE ENGINEER, IN ACCORDANCE WITH THIS DETAIL.