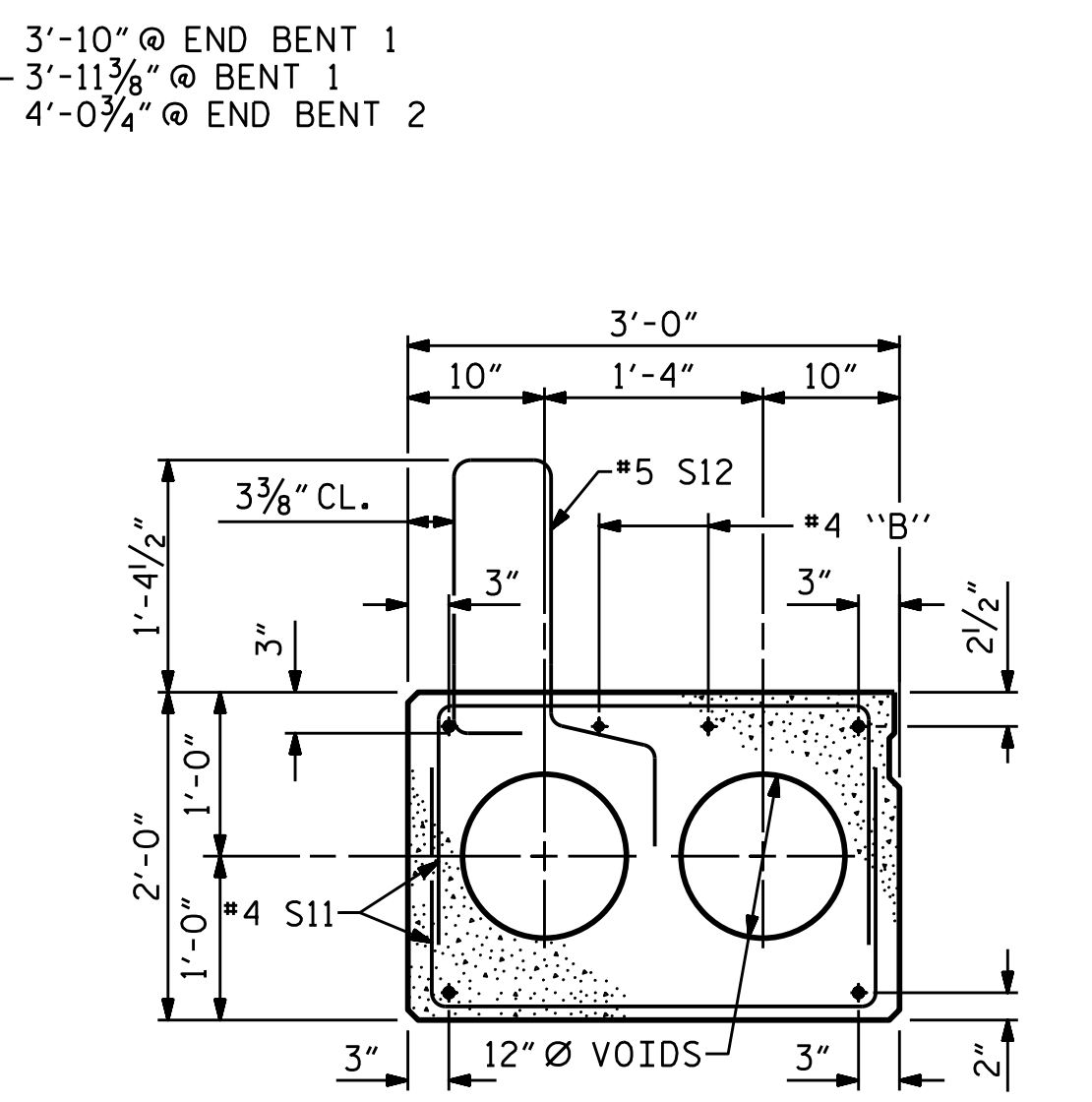
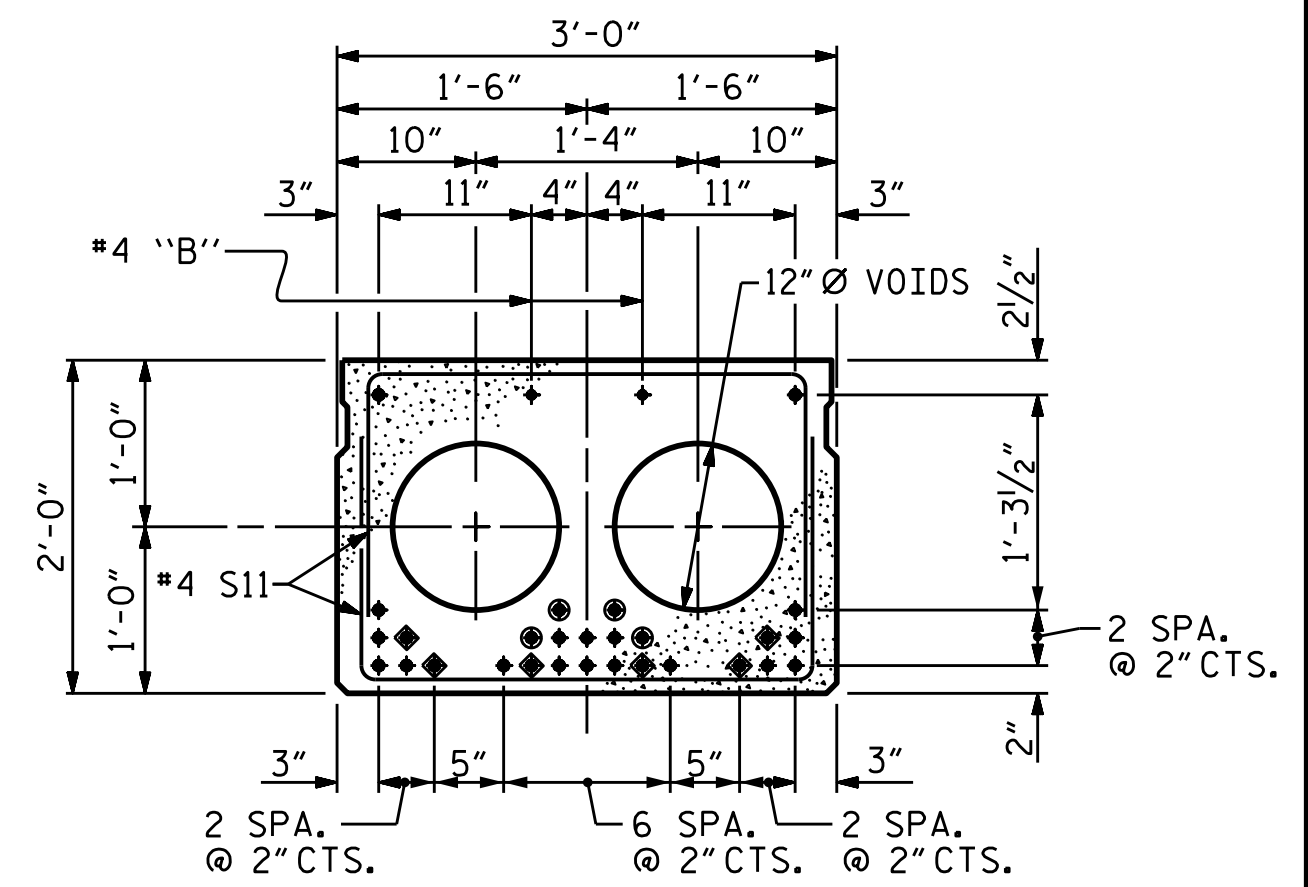


HALF SECTION AT INTERMEDIATE DIAPHRAGMS **TYPICAL SECTION** HALF SECTION THROUGH VOIDS

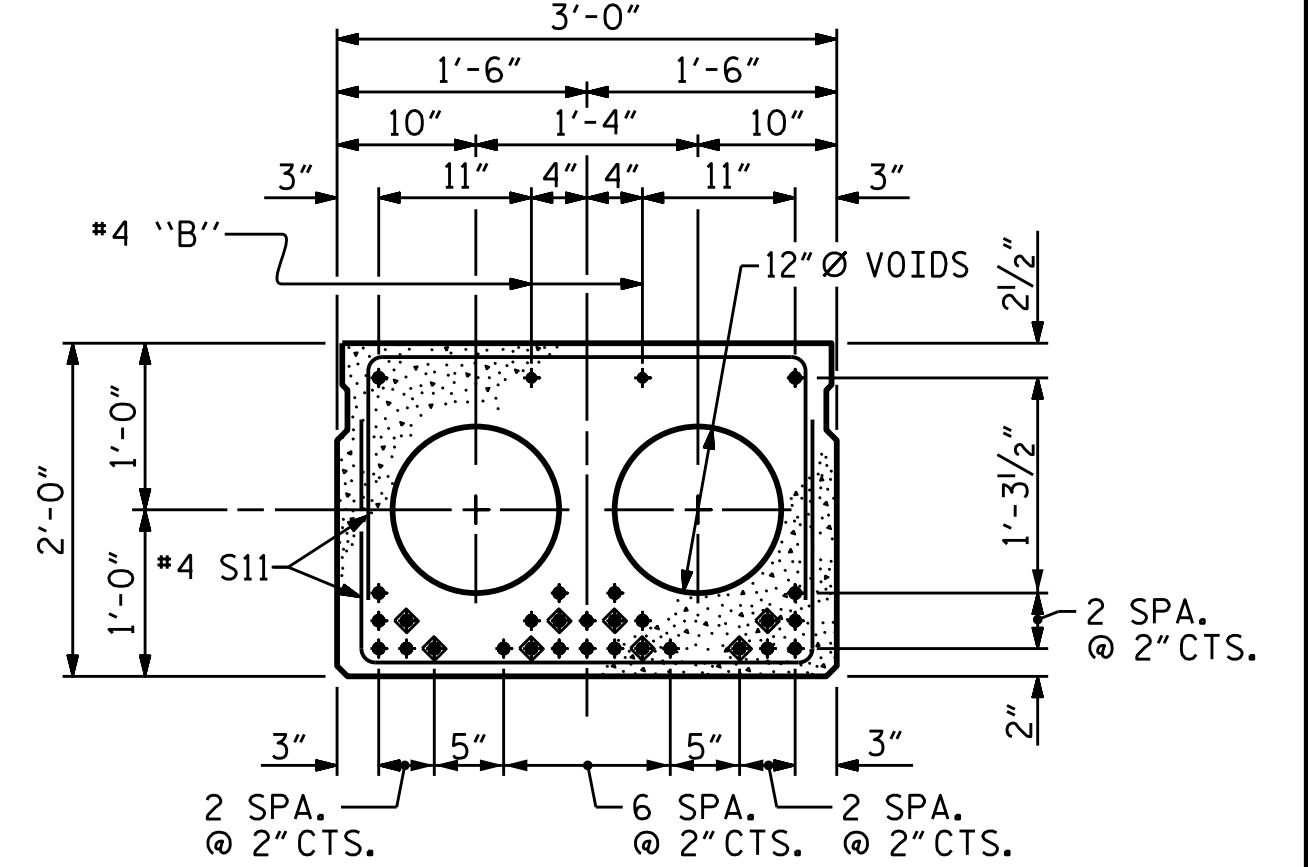
* - THE MAXIMUM BARRIER RAIL HEIGHT AND ASPHALT THICKNESS IS SHOWN. THE HEIGHT OF THE BARRIER RAIL AND ASPHALT THICKNESS VARIES WHILE THE TOP OF THE BARRIER RAIL FOLLOWS THE PROFILE OF THE GUTTERLINE. FOR RAIL HEIGHT DETAILS AND ASPHALT THICKNESS, SEE THE "VERTICAL CONCRETE BARRIER RAIL SECTION" DETAIL.



EXTERIOR SLAB SECTION
(FOR PRESTRESSED STRAND LAYOUT, SEE INTERIOR SLAB SECTION.)



INTERIOR SLAB SECTION (65' UNIT)
(24 STRANDS REQUIRED)



INTERIOR SLAB SECTION (70' UNIT)
(28 STRANDS REQUIRED)

0.6" Ø LOW RELAXATION STRAND LAYOUT

- ◆ BOND SHALL BE BROKEN ON THESE STRANDS FOR A DISTANCE OF 12'-0" FROM END OF CORED SLAB UNIT. SEE STANDARD SPECIFICATIONS, ARTICLE 1078-7.
- OPTIONAL FULL LENGTH DEBONDED STRANDS. THESE STRANDS ARE NOT REQUIRED. IF THE FABRICATOR CHOOSES TO INCLUDE THESE STRANDS IN THE CORED SLAB UNIT, THE STRANDS SHALL BE DEBONDED FOR THE FULL LENGTH OF THE UNIT AT NO ADDITIONAL COST. SEE STANDARD SPECIFICATIONS, ARTICLE 1078-7.

DEBONDING LEGEND

PROJECT NO. B-5171
GRANVILLE COUNTY
 STATION: 15+12.00 -L-
 SHEET 1 OF 5

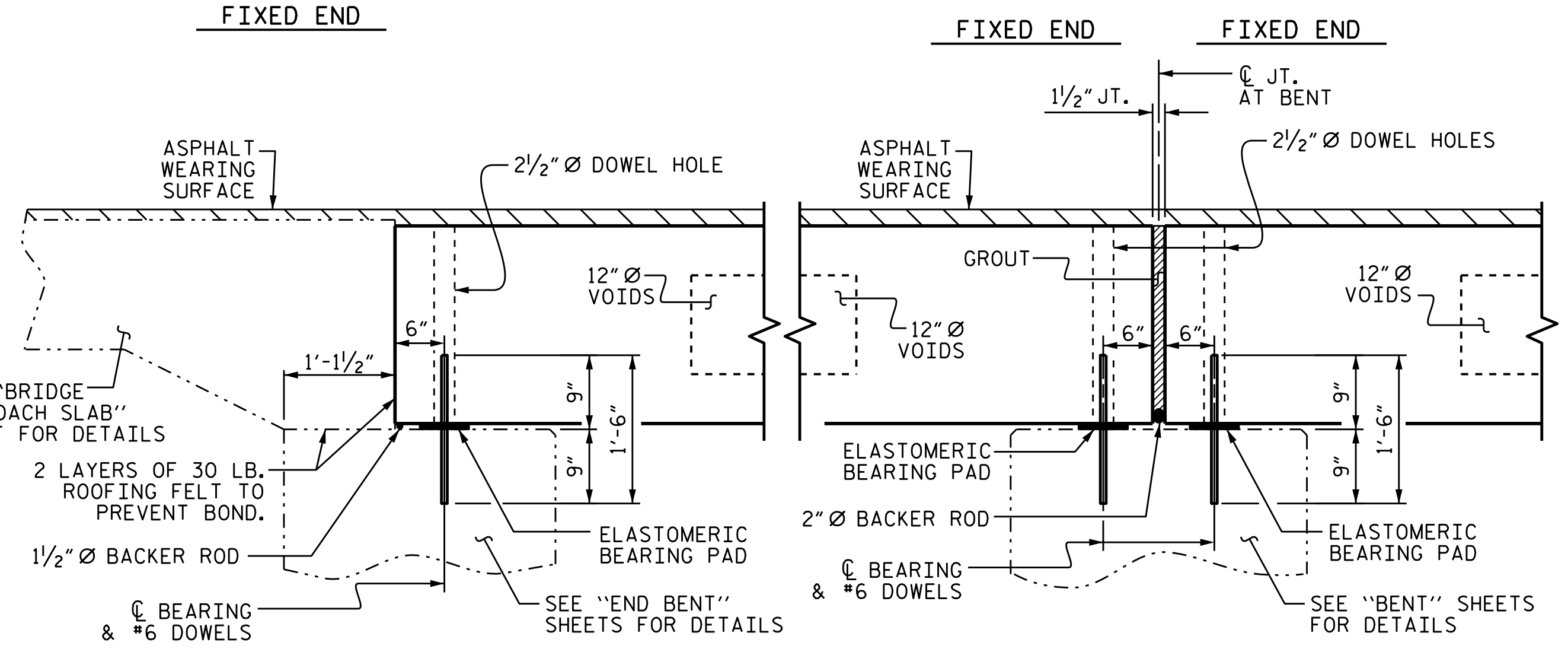
STATE OF NORTH CAROLINA
 DEPARTMENT OF TRANSPORTATION
 RALEIGH
 STANDARD
 3'-0" X 2'-0"
 PRESTRESSED CONCRETE
 CORED SLAB UNIT



DocuSigned by:
 7245838050P40E
 3/22/2016

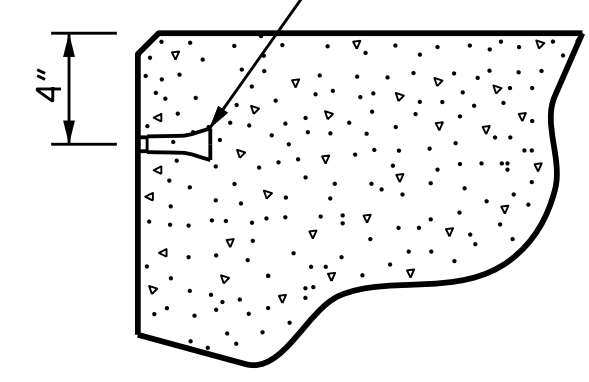
REVISIONS						SHEET NO.	
NO.	BY:	DATE:	NO.	BY:	DATE:	S-5	
1			3			TOTAL SHEETS 18	
2			4				

DOCUMENT NOT CONSIDERED FINAL
 UNLESS ALL SIGNATURES COMPLETED

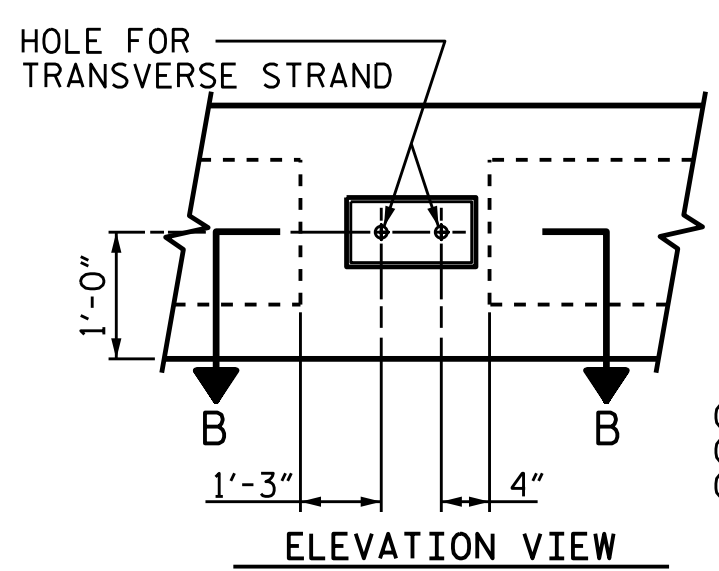


SECTION AT END BENT **SECTION AT BENT**

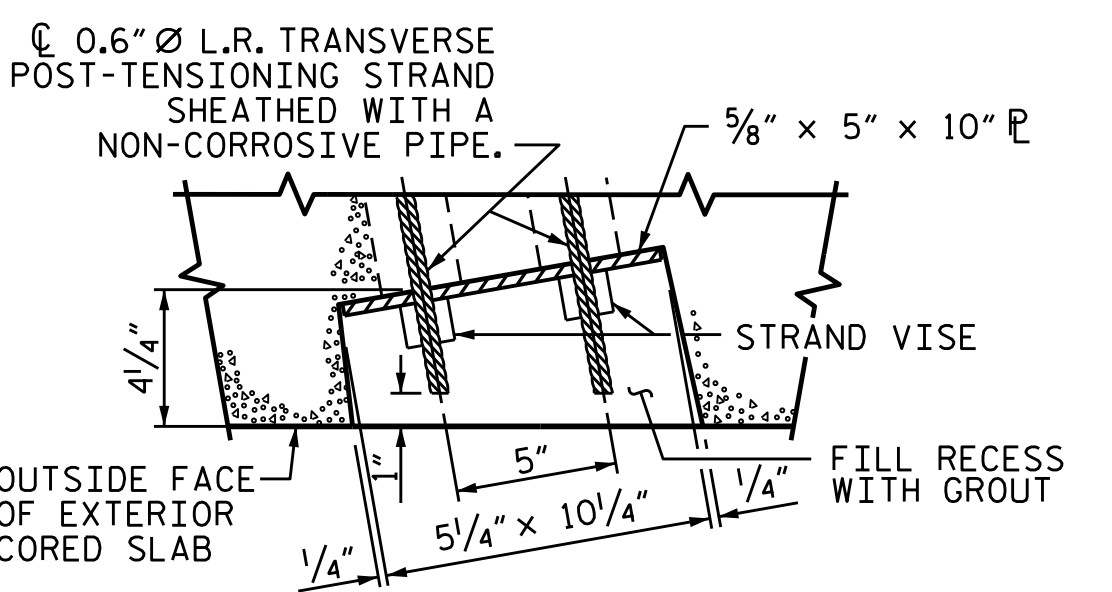
PERMITTED THREADED INSERT CAST IN OUTSIDE FACE OF EXTERIOR UNIT AND RECESSED 3/8" SIZE TO BE DETERMINED BY CONTRACTOR.



THREADED INSERT DETAIL

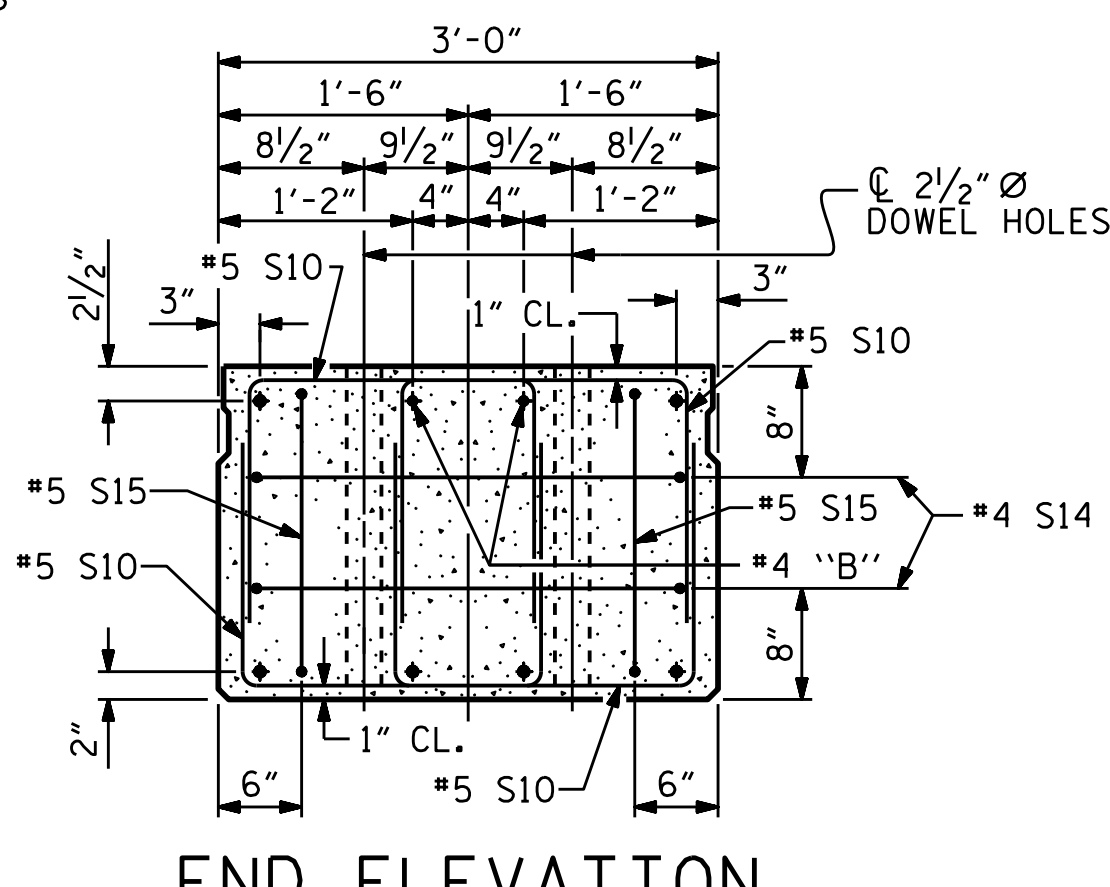


ELEVATION VIEW



SECTION B-B

GRAUTED RECESS AT END OF POST-TENSIONED STRAND OF CORED SLABS



END ELEVATION

SHOWING PLACEMENT OF DOUBLE STIRRUPS AND LOCATION OF DOWEL HOLES. (STRAND LAYOUT NOT SHOWN.) INTERIOR SLAB UNIT SHOWN-EXTERIOR SLAB UNIT SIMILAR EXCEPT SHEAR KEY LOCATION.

ASSEMBLED BY : A. SORSENGINH	DATE : 1/2015
CHECKED BY : JP ADAMS	DATE : 11/2015
DRAWN BY : MAA	6/10
CHECKED BY : MKT	7/10
REV. 12/11	MAA/AAC
REV. 8/14	MAA/TMG