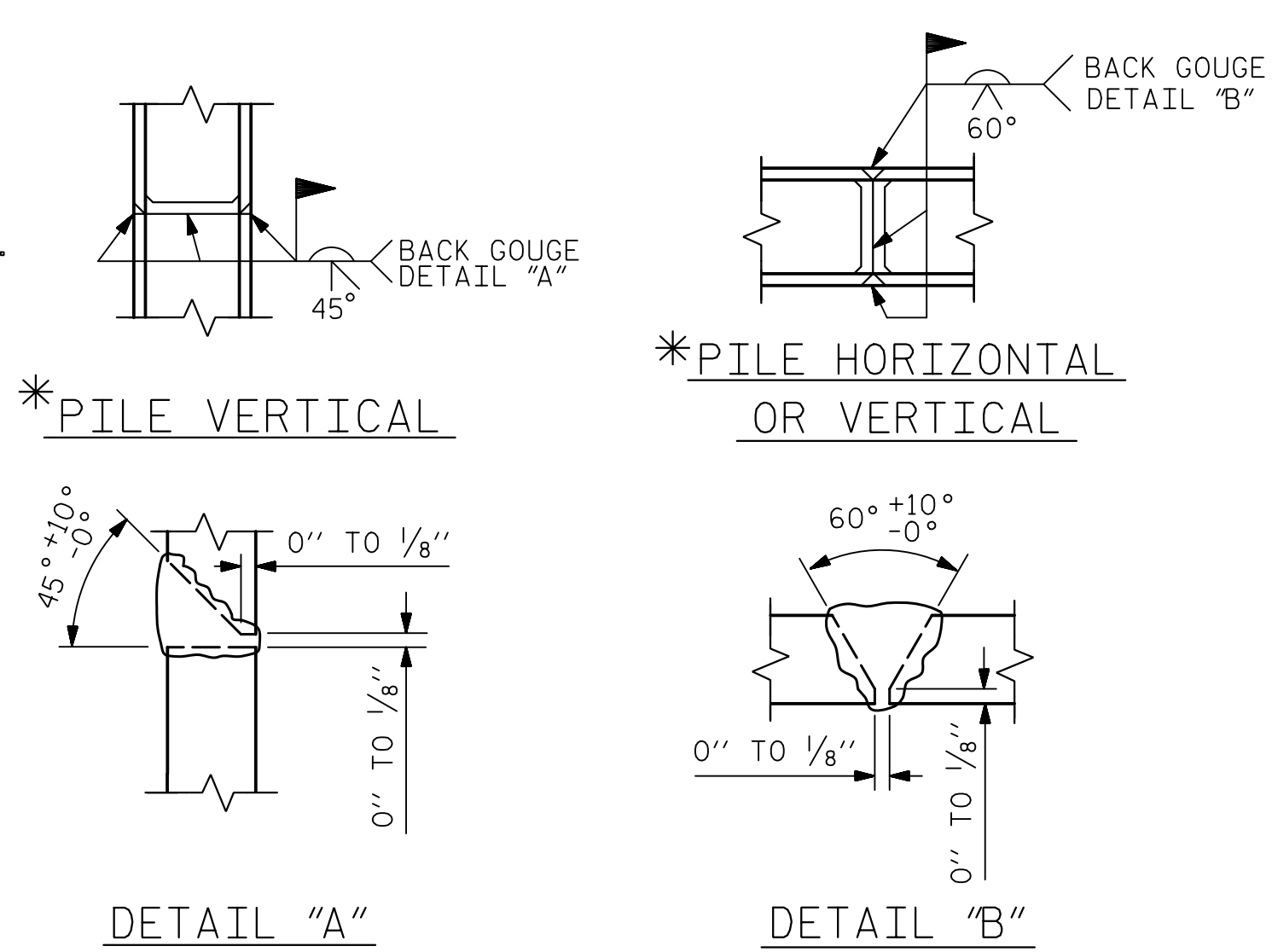


BLOCKOUT IN WINGWALL



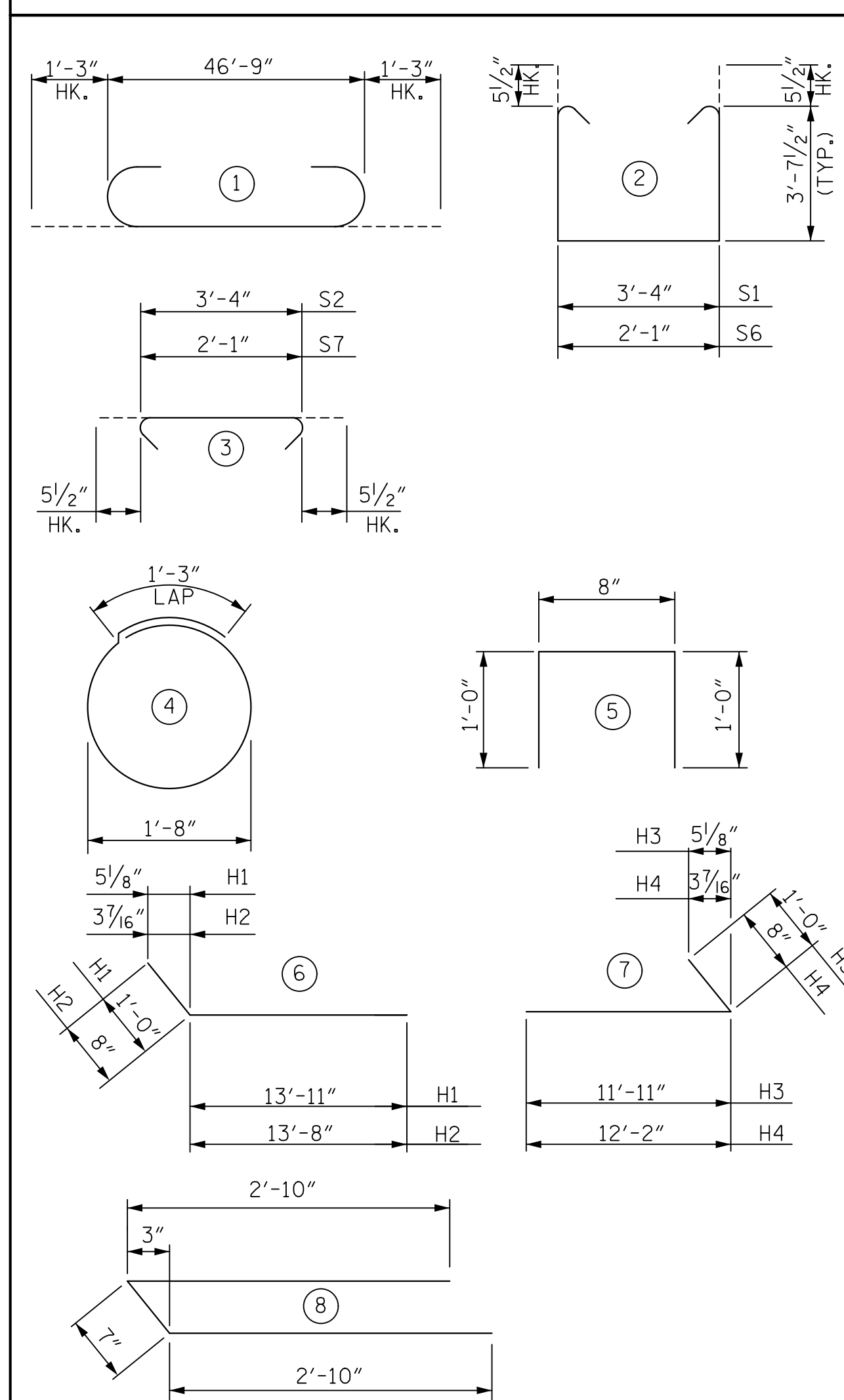
* POSITION OF PILE DURING WELDING.

PILE SPLICE DETAILS

NOTES:

- STIRRUPS IN CAP MAY BE SHIFTED AS NECESSARY TO CLEAR ANCHOR BOLTS.
- BACKWALL SHALL BE PLACED BEFORE APPLYING THE EPOXY PROTECTIVE COATING.
- THE TOP SURFACE AREAS OF THE END BENT CAPS SHALL BE CURED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS EXCEPT THE MEMBRANE CURING COMPOUND METHOD SHALL NOT BE USED.
- THE TOP SURFACE OF THE END BENT CAP EXCEPT THE BRIDGE SEAT BUILDUPS SHALL BE SLOPED TRANSVERSELY FROM THE FILL FACE TO THE BACK FACE AT THE RATE OF 2%.
- THE CONTRACTOR SHALL PROVIDE FOR INSTALLATION OF THE 4" Ø DRAIN PIPE THROUGH THE WINGWALL AS REQUIRED FOR REINFORCED BRIDGE APPROACH FILLS, SEE ROADWAY PLANS. REINFORCING STEEL IN THE WINGWALL MAY BE SHIFTED AS NECESSARY TO CLEAR THE DRAIN PIPE.

BAR TYPES



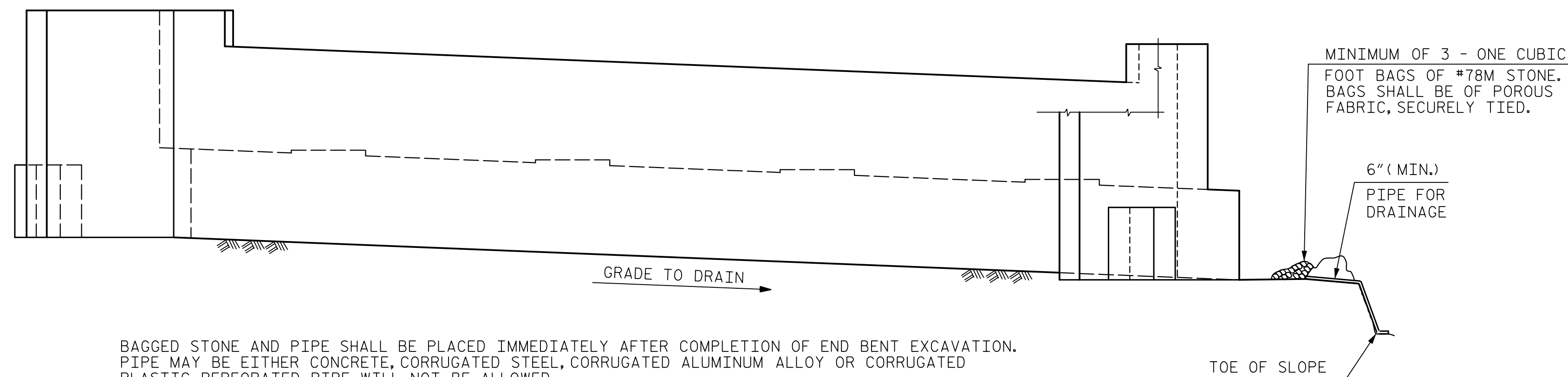
ALL BAR DIMENSIONS ARE OUT TO OUT

BILL OF REINFORCING

END BENT 1					
BAR	NO.	SIZE	TYPE	LENGTH	WEIGHT
B1	10	9		49'-3"	1,675
B2	40	4	STR.	24'-8"	659
B3	12	4	STR.	3'-8"	29
H1	20	6	6	14'-11"	448
H2	11	4	6	14'-4"	105
H3	21	6	7	12'-11"	407
H4	11	4	7	12'-10"	94
K1	20	4	STR.	24'-8"	330
S1	114	5	2	11'-6"	1,367
S2	114	5	3	4'-3"	505
S3	36	4	4	6'-6"	156
S4	40	5	5	2'-8"	111
S5	4	4	8	6'-3"	17
S6	4	5	2	10'-3"	43
S7	4	5	3	3'-0"	13
V1	80	5	STR.	8'-2"	681
V2	34	5	STR.	9'-8"	343
V3	24	5	STR.	10'-0"	250
V4	8	5	STR.	9'-11"	83

QUANTITIES

REINFORCING STEEL	LBS.	7,316
CLASS "A" CONCRETE BREAKDOWN		
POUR 1 - CAP & BOT. OF WINGS	CU. YDS.	32.3
POUR 2 - TOP OF WINGS & BACKWALL	CU. YDS.	14.0
TOTAL	CU. YDS.	46.3
HP 12x53 STEEL PILES	NO.	9
	LIN. FT.	345



BAGGED STONE AND PIPE SHALL BE PLACED IMMEDIATELY AFTER COMPLETION OF END BENT EXCAVATION. PIPE MAY BE EITHER CONCRETE, CORRUGATED STEEL, CORRUGATED ALUMINUM ALLOY OR CORRUGATED PLASTIC. PERFORATED PIPE WILL NOT BE ALLOWED.

BAGGED STONE SHALL REMAIN IN PLACE UNTIL THE ENGINEER DIRECTS THAT IT BE REMOVED. THE CONTRACTOR SHALL REMOVE AND DISPOSE OF SILT ACCUMULATIONS AT BAGGED STONE WHEN SO DIRECTED BY THE ENGINEER. BAGS SHALL BE REMOVED AND REPLACED WHENEVER THE ENGINEER DETERMINES THAT THEY HAVE DETERIORATED AND LOST THEIR EFFECTIVENESS.

NO SEPARATE PAYMENT WILL BE MADE FOR THIS WORK AND THE ENTIRE COST OF THIS WORK SHALL BE INCLUDED IN THE UNIT CONTRACT PRICE FOR THE SEVERAL PAY ITEMS.

TEMPORARY DRAINAGE AT END BENT 1

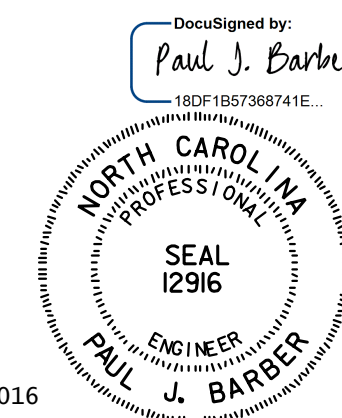
PROJECT NO. B-4811
RUTHERFORD COUNTY
 STATION: POC 15+25.00 -L-

SHEET 3 OF 3

STATE OF NORTH CAROLINA
 DEPARTMENT OF TRANSPORTATION
 RALEIGH

SUBSTRUCTURE

END BENT 1



1/29/2016

1/29/2016



HNTB NORTH CAROLINA, P.C.
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**DOCUMENT NOT CONSIDERED FINAL
 UNLESS ALL SIGNATURES COMPLETED**

DRAWN BY M. WRIGHT DATE 11/14
 CHECKED BY D. HAWKINS DATE 3/15 DWG. NO. 30

REVISIONS						SHEET NO.
NO.	BY	DATE	NO.	BY	DATE	S01-30
1			3			TOTAL SHEETS
2			4			42