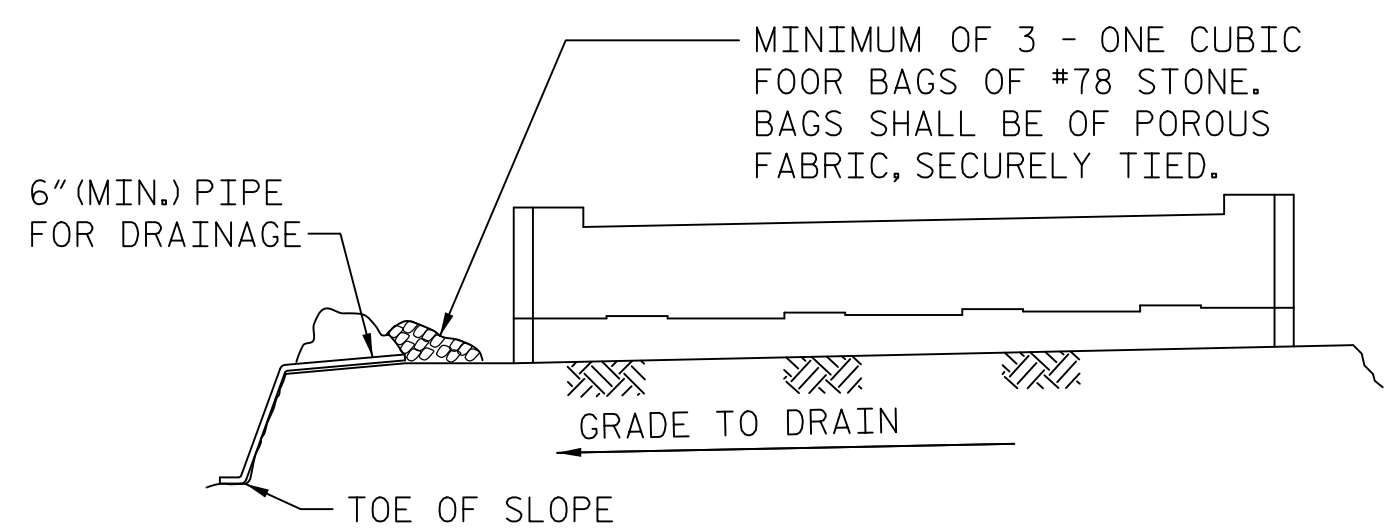


SECTION "A-A"

* ELEVATIONS BETWEEN BRIDGE SEAT BUILDUPS ARE SHOWN AT THIS POINT



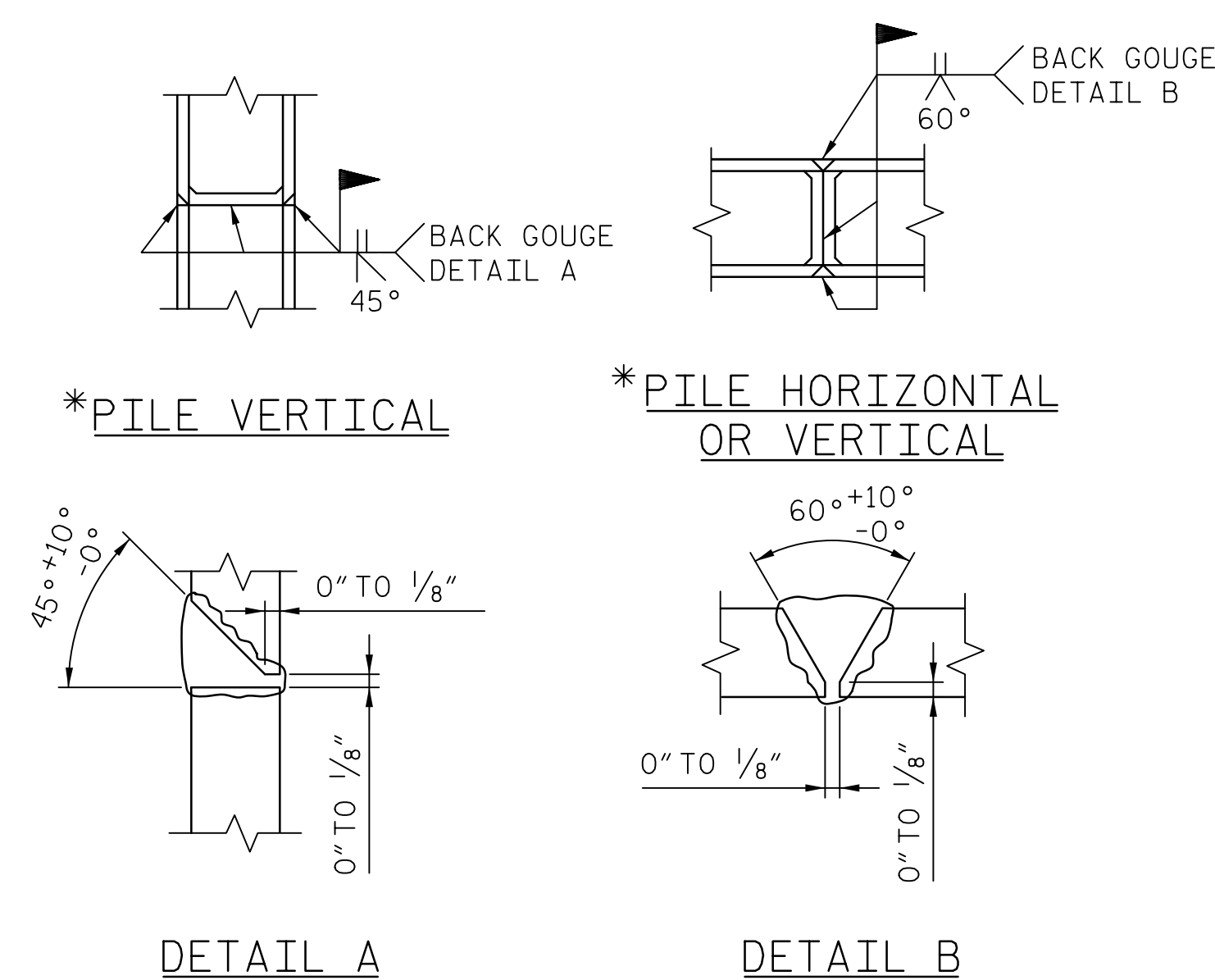
BAGGED STONE AND PIPE SHALL BE PLACED IMMEDIATELY AFTER COMPLETION OF END BENT EXCAVATION. PIPE MAY BE EITHER CONCRETE, CORRUGATED STEEL, CORRUGATED ALUMINUM ALLOY, OR CORRUGATED PLASTIC. PERFORATED PIPE WILL NOT BE ALLOWED.

BAGGED STONE SHALL REMAIN IN PLACE UNTIL THE ENGINEER DIRECTS THAT IT BE REMOVED. THE CONTRACTOR SHALL REMOVE AND DISPOSE OF SILT ACCUMULATIONS AT BAGGED STONE WHEN SO DIRECTED BY THE ENGINEER. BAGS SHALL BE REMOVED AND REPLACED WHENEVER THE ENGINEER DETERMINES THAT THEY HAVE DETERIORATED AND LOST THEIR EFFECTIVENESS.

NO SEPARATE PAYMENT WILL BE MADE FOR THIS WORK AND THE ENTIRE COST OF THIS WORK SHALL BE INCLUDED IN THE UNIT CONTRACT PRICE FOR THE SEVERAL PAY ITEMS.

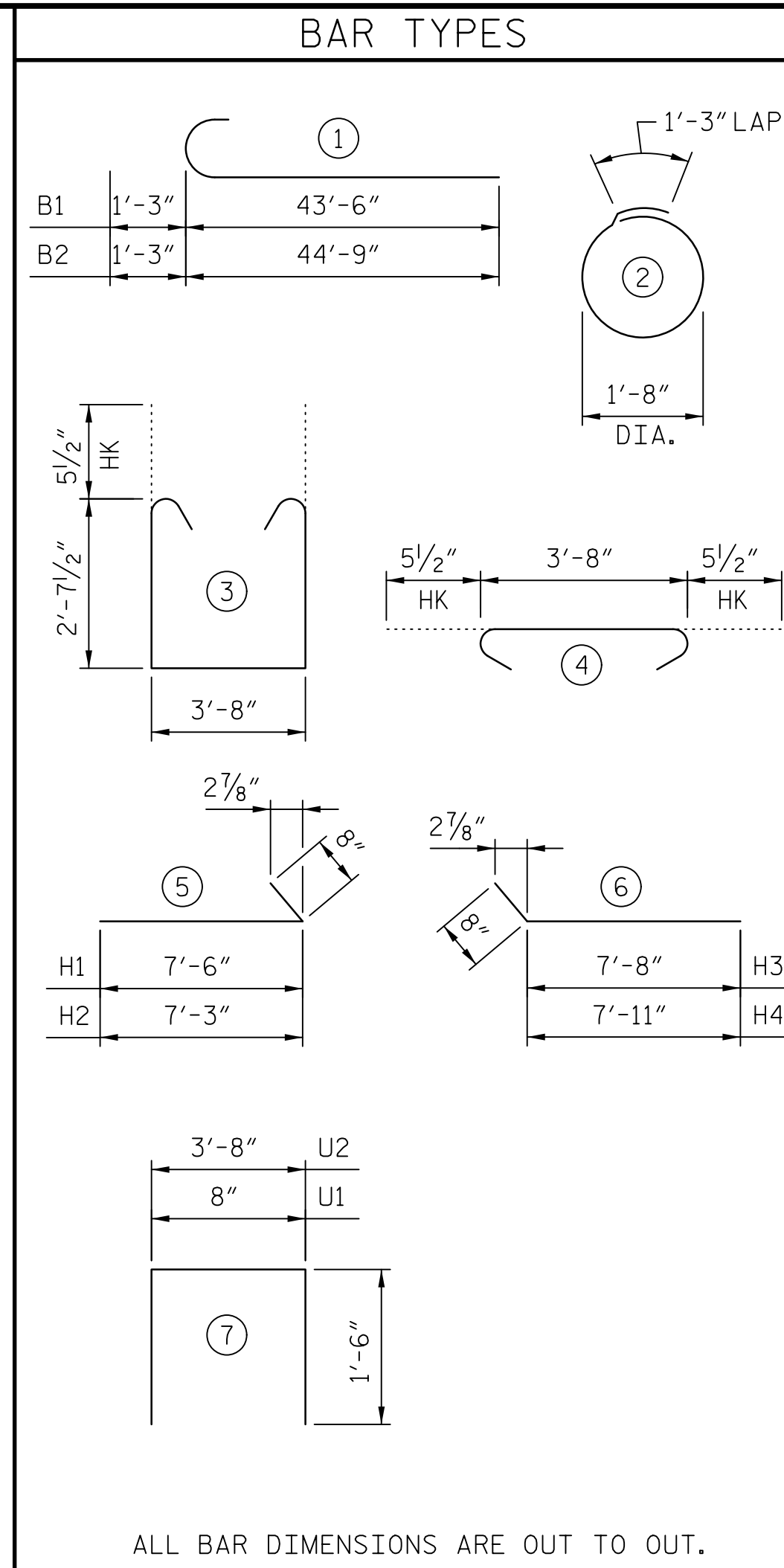
TEMPORARY DRAINAGE AT END BENT

DRAWN BY : MTB DATE : 11/2015
 CHECKED BY : JCM DATE : 11/2015



* POSITION OF PILE DURING WELDING.

PILE SPLICE DETAILS



ALL BAR DIMENSIONS ARE OUT TO OUT.

BILL OF MATERIAL

END BENT 2

BAR	NO.	SIZE	TYPE	LENGTH	WEIGHT
B1	12	#9	①	44'-9"	1826
B2	12	#9	①	46'-0"	1877
B3	8	#5	STR	41'-10"	349
B4	12	#4	STR	28'-7"	229
B5	21	#4	STR	3'-8"	51
B6	35	#4	STR	3'-2"	74
H1	12	#6	⑤	8'-2"	147
H2	12	#6	⑤	7'-11"	143
H3	12	#6	⑥	8'-4"	150
H4	12	#6	⑥	8'-7"	155
K1	30	#4	STR	28'-7"	573
K2	8	#4	STR	4'-2"	22
S1	84	#5	④	4'-7"	402
S2	84	#5	③	9'-10"	862
S3	34	#4	②	6'-6"	148
U1	72	#4	⑦	3'-8"	176
U2	35	#4	⑦	6'-8"	156
V1	144	#5	STR	7'-1"	1064
V2	26	#5	STR	9'-2"	249
V3	26	#5	STR	9'-0"	244

TOTAL REINFORCING STEEL LBS. 8897

END BENT 2 TOTAL QUANTITIES

CLASS A CONCRETE	
POUR #1 (CAP & LOWER WING)	C.Y. 42.4
POUR 2 (BACKWALL & UPPER WING)	C.Y. 17.2
TOTAL	C.Y. 59.6
HP 12x53 STEEL PILES	NO. 17
	LIN. FT. 1105

NOTES:

- STIRRUPS IN CAP MAY BE SHIFTED AS NECESSARY TO CLEAR ANCHOR BOLTS.
- BACKWALL SHALL BE PLACED BEFORE APPLYING THE EPOXY PROTECTIVE COATING.
- THE TOP SURFACE AREAS OF THE END BENT CAP SHALL BE CURED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS EXCEPT THAT THE MEMBRANE CURING COMPOUND METHOD SHALL NOT BE USED.
- THE TOP SURFACE OF THE CAP EXCEPT THE BRIDGE SEAT BUILDUPS SHALL BE SLOPED TRANSVERSELY FROM THE FILL FACE TO THE BACK FACE AT THE RATE OF 2%.
- FOR OTHER NOTES, SEE "FOUNDATION LAYOUT" SHEET AND "LONG CHORD LAYOUT" SHEET.

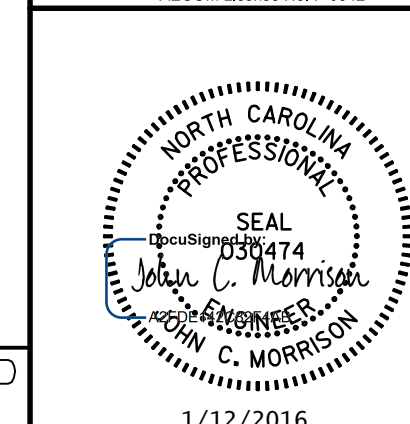
THE CONCRETE IN THE SHADED AREA OF THE WING SHALL BE POURED AFTER THE JOINT BETWEEN THE DECK AND THE APPROACH SLAB HAS BEEN SAWED AND THE BARRIER RAIL (PARAPET AND END POST) ARE CAST IF SLIP FORMING IS USED.

INSTALL THE 4" DIA. DRAIN PIPE THROUGH THE WING WALL AS REQUIRED FOR REINFORCED BRIDGE APPROACH FILLS, SEE THE ROADWAY PLANS. REINFORCING STEEL IN THE WING WALL MAY BE SHIFTED AS NECESSARY TO CLEAR THE DRAIN PIPE.

PROJECT NO. U-2707
 FORSYTH COUNTY

STATION: 17+34.88 -Y-

SHEET 4 OF 4 BRIDGE NO. 109



STATE OF NORTH CAROLINA
 DEPARTMENT OF TRANSPORTATION
 RALEIGH

SUBSTRUCTURE
 END BENT 2
 SECTIONS AND DETAILS

REVISIONS

NO.	BY:	DATE:	NO.	BY:	DATE:

SHEET NO.
 S-81
 TOTAL SHEETS
 86

DOCUMENT NOT CONSIDERED
 FINAL UNLESS ALL
 SIGNATURES COMPLETED

1/12/2016

DATE: 1/12/2016 TIME: 8:55:35 AM USER: jcm.dwg DGN: R:\Bridges\109\109-100\402_DTT_U2707_SML_SBI_EB2_04.dgn