



PAT McCRORY
Governor

NICHOLAS J. TENNYSON
Secretary

April 11, 2016

Addendum No. 1

RE: Contract # C203849

WBS # 2016CPT.09.18.10291.1, Etc.

STATE FUNDED

Davidson County

1 Section Of US-64, 2 Section of NC-8, And 8 Sections of Secondary Roads

April 19, 2016 Letting

To Whom It May Concern:

Reference is made to the plans and proposal form furnished to you on this project.

The following revisions have been made to the plans (Sketch Maps):

The Project WBS numbers have been changed on all sheets. Please discard the Sketch Map sheets you previously received (entire package) and replace with the revised set. Only the WBS numbers were changed.

The following revisions have been made to the proposal:

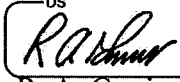
Page No.	Revisions
Proposal Cover	Note added that reads "Includes Addendum No. 1 Dated 04-11-16", also the WBS numbers were revised
R-8 and R-9	Replaced the project special provision entitled "Non-Tracking Hot Applied Tack Coat (Entire Project)" with the project special provision entitled "Non-Tracking Tack Coat"

The WBS number was revised in the header on all pages of the proposal. Please discard the proposal we originally sent you and use the revised proposal we are providing by way of this addendum. Other than the changes listed in the above table, only the WBS number was revised on the other pages.



The contract will be prepared accordingly.

Sincerely,


R. A. Garris, PE
Contract Officer

RAG/jag

cc: Mr. Ron Hancock, PE
Mr. Pat Ivey, PE
Mr. Rodger Rochelle, PE
Mr. R.E. Davenport, PE
Mr. Ken Kennedy, PE
Ms. Lori Strickland
Project File (2)

Mr. Ray Arnold, PE
Ms. Theresa Canales, PE
Ms. Marsha Sample
Mr. Mike Gwyn
Ms. Jaci Kincaid
Ms. Penny Higgins

STATE OF NORTH CAROLINA
DEPARTMENT OF TRANSPORTATION
RALEIGH, N.C.

PROPOSAL

INCLUDES ADDENDUM No. 1 DATED 04-11-16

DATE AND TIME OF BID OPENING: **APRIL 19, 2016 AT 2:00 PM**

CONTRACT ID C203849
WBS 2016CPT.09.18.10291.1, 2016CPT.09.19.20291.1

FEDERAL-AID NO. STATE FUNDED
COUNTY DAVIDSON
T.I.P. NO.
MILES 18.729
ROUTE NO. US 64
LOCATION 1 SECTION OF US-64, 2 SECTIONS OF NC-8, AND 8 SECTIONS OF
SECONDARY ROADS.
TYPE OF WORK MILLING, RESURFACING, AND SHOULDER RECONSTRUCTION.

NOTICE:

ALL BIDDERS SHALL COMPLY WITH ALL APPLICABLE LAWS REGULATING THE PRACTICE OF GENERAL CONTRACTING AS CONTAINED IN CHAPTER 87 OF THE GENERAL STATUTES OF NORTH CAROLINA WHICH REQUIRES THE BIDDER TO BE LICENSED BY THE N.C. LICENSING BOARD FOR CONTRACTORS WHEN BIDDING ON ANY NON-FEDERAL AID PROJECT WHERE THE BID IS \$30,000 OR MORE, EXCEPT FOR CERTAIN SPECIALTY WORK AS DETERMINED BY THE LICENSING BOARD. BIDDERS SHALL ALSO COMPLY WITH ALL OTHER APPLICABLE LAWS REGULATING THE PRACTICES OF ELECTRICAL, PLUMBING, HEATING AND AIR CONDITIONING AND REFRIGERATION CONTRACTING AS CONTAINED IN CHAPTER 87 OF THE GENERAL STATUTES OF NORTH CAROLINA. NOTWITHSTANDING THESE LIMITATIONS ON BIDDING, THE BIDDER WHO IS AWARDED ANY FEDERAL - AID FUNDED PROJECT SHALL COMPLY WITH CHAPTER 87 OF THE GENERAL STATUTES OF NORTH CAROLINA FOR LICENSING REQUIREMENTS WITHIN 60 CALENDAR DAYS OF BID OPENING.

BIDS WILL BE RECEIVED AS SHOWN BELOW:

THIS IS A ROADWAY PROPOSAL

5% BID BOND OR BID DEPOSIT REQUIRED

NON-TRACKING TACK COAT:

(1-2-14)(Rev. 4-19-16)

605

SPI 6-13A

Revise the *2012 Standard Specifications* as follows:

Page 6-4, SECTION 605 ASPHALT TACK COAT, line 29, after Section 605, add the following:

**SECTION 606
NON-TRACKING ASPHALT TACK COAT**

606-1 DESCRIPTION

Refer to Article 605-1.

Non-tracking tack coat will be required. Notify the Engineer prior to using non-tracking tack coat.

606-2 MATERIALS

Use only products from the “Non-tracking Asphalt Tack Coat” materials on the NCDOT Approved Product List maintained by the Materials and Tests Unit located at the following website:

<https://connect.ncdot.gov/resources/Materials/MaterialsResources/Approved%20Non-Tracking%20Tack%20Coat%20Products%20for%20NC.pdf>

Use only an approved Hot Applied Tack for the open-graded asphalt friction course (OGAFC), if applicable.

Do not dilute or mix the non-tracking tack coat with water, solvents, or other materials before application.

606-3 WEATHER LIMITATIONS

Refer to Article 605-3 and Article 605-5 for OGAFC requirements.

606-4 SURFACE PREPARATION

Refer to Article 605-4.

606-5 ACCEPTANCE OF ASPHALT MATERIALS

Refer to Article 605-5.

606-6 APPLICATION EQUIPMENT

Refer to Article 605-6.

606-7 APPLICATION RATES AND TEMPERATURES

Target application rate for EMULSIFIED ASPHALT non-tracking tack shall be in accordance with Table 605-1, Application Rates for Tack Coat, contained elsewhere in this document, and if necessary adjust based on existing pavement conditions, milled surface, and manufacturer's recommendations and approved by the Engineer. Application temperatures shall be in accordance with manufacturer's recommendations. Non-tracking asphalt tack coat shall not be overheated.

Target application rate for HOT APPLIED TACK shall be 0.10 gal/sy and adjusted based on existing pavement conditions, milled surface, and manufacturer's recommendations and approved by the Engineer. Application temperatures shall be in accordance with manufacturer's recommendations. Non-tracking asphalt tack coat shall not be overheated.

606-8 APPLICATION OF TACK COAT

Refer to Article 605-8 except on **Page 6-4, Article 605-8**, line 6, replace second paragraph with the following:

If public traffic is being maintained, cover the non-tracking tack coat in the same day's operation or provide continual lane closures until the tack is covered. Due to non-tracking tack requiring heat to activate the adhesive properties, the application of temporary granular material will not be allowed. If extended lane closures are required, applicable liquidated damages will apply. To facilitate safe traffic conditions or for other reasons the Engineer may limit the application of non-tracking tack in advance of the paving operation.

606-9 MEASUREMENT AND PAYMENT

There will be no direct payment for the non-tracking tack coat and work covered by this section.

Page 6-26, Article 610-8, line 30, replace "Section 605" with "Sections 605 or 606".

Page 6-42, Article 650-5, line 32, replace "Section 605" with "Sections 605 or 606".

Page 6-44, Article 652-1, line 11, replace "Section 605" with "Sections 605 or 606".

PRICE ADJUSTMENT - ASPHALT BINDER FOR PLANT MIX:

(11-21-00)

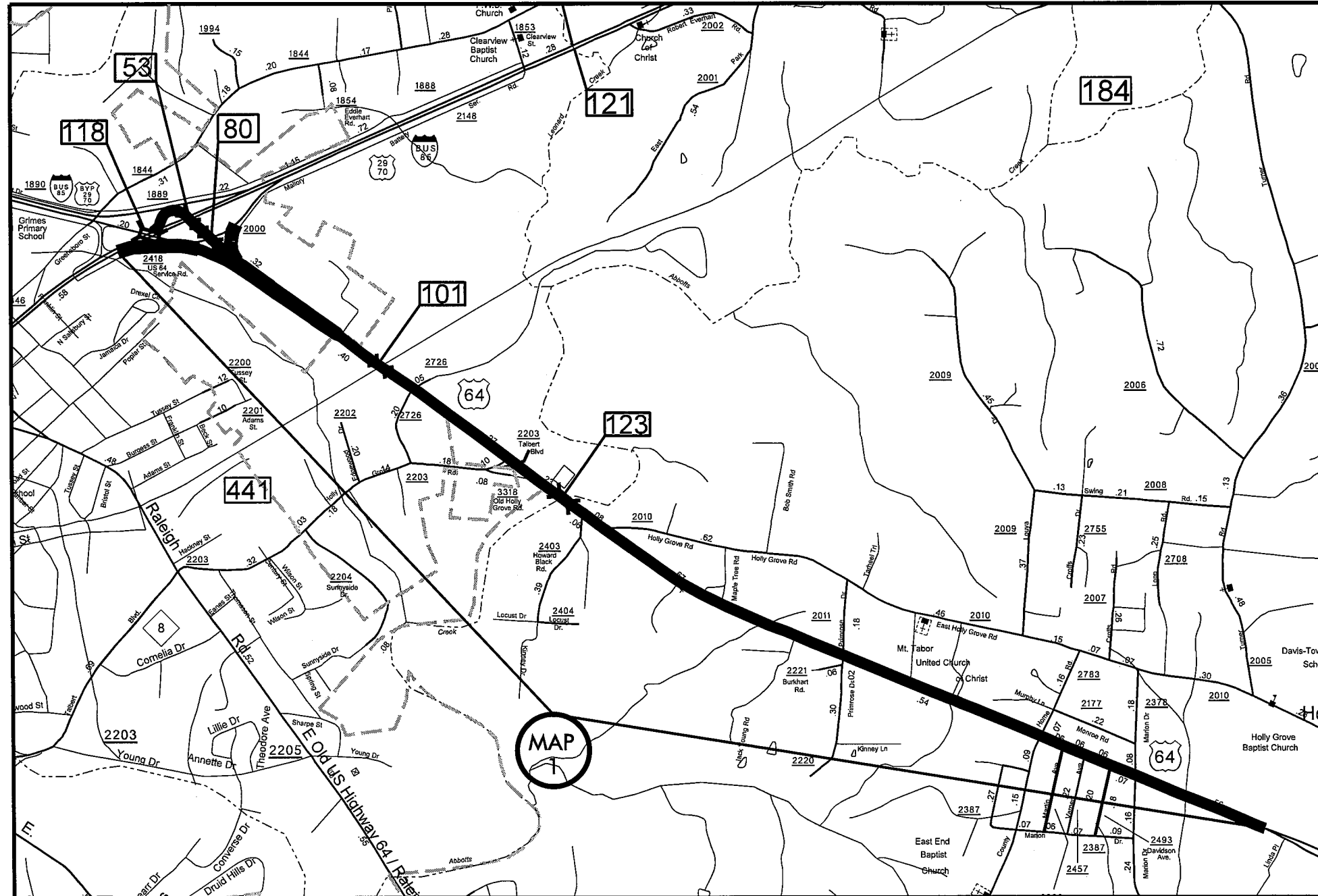
620

SP6 R25

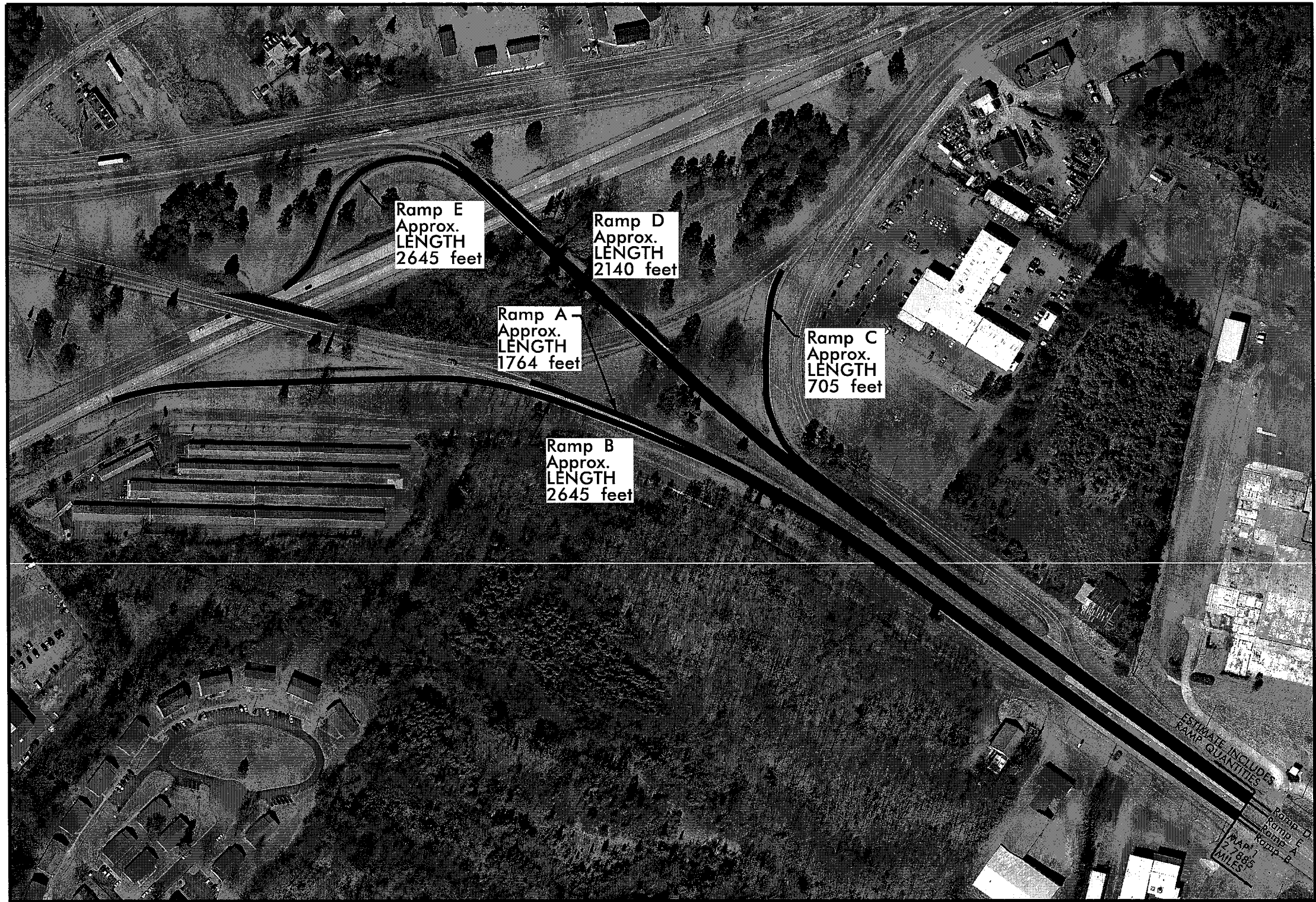
Price adjustments for asphalt binder for plant mix will be made in accordance with Section 620 of the *2012 Standard Specifications*.

The base price index for asphalt binder for plant mix is \$ **350.36** per ton.

This base price index represents an average of F.O.B. selling prices of asphalt binder at supplier's terminals on **March 1, 2016**.



MAP 1
 US 64
 Mill 1½" Depth full width of pavement,
 edge of pavement to edge of pavement.
 SEE SHEET NO. 2 FOR RAMP BEGINNING
 AND END POINTS.

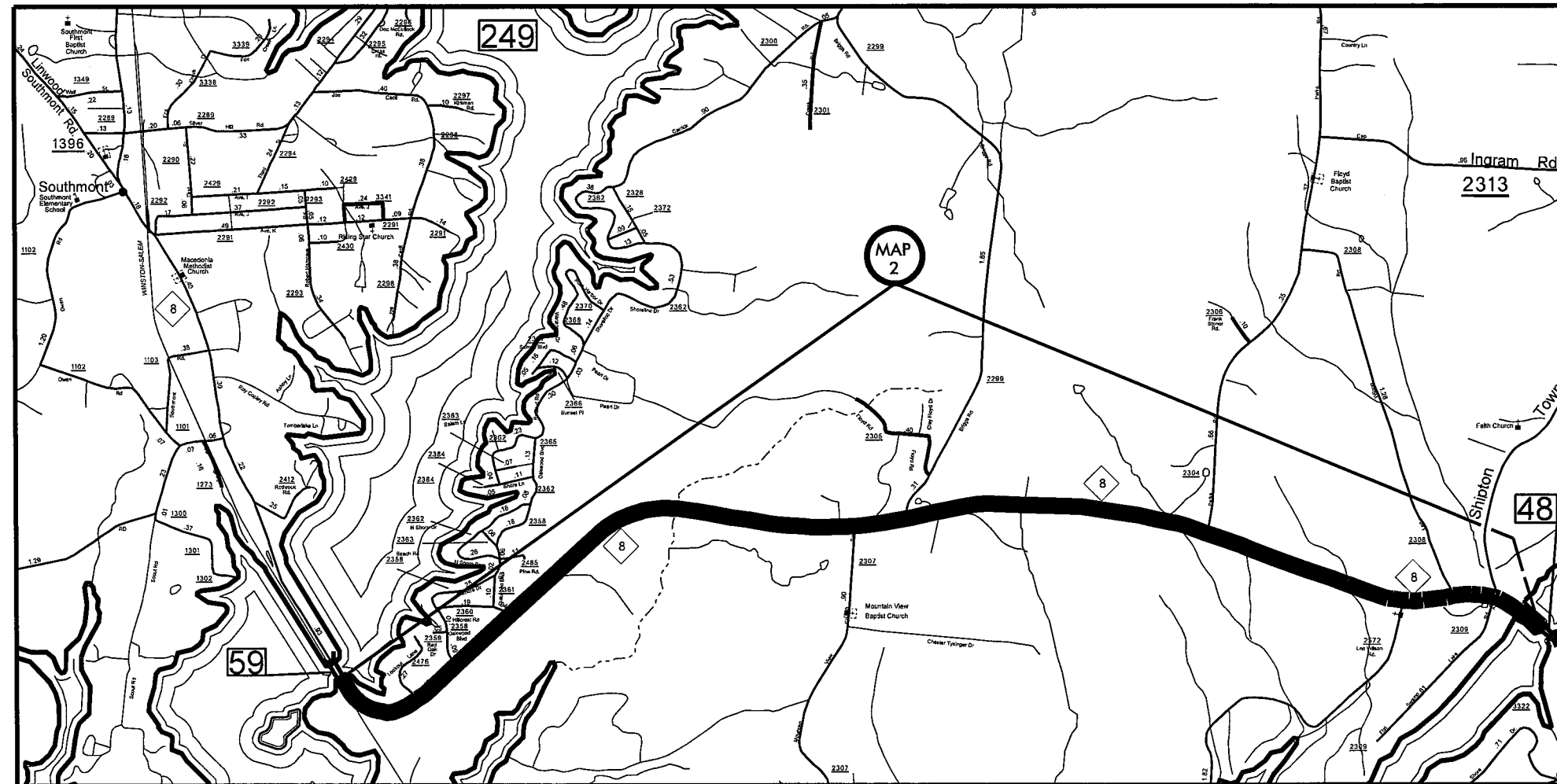


MAP 1 US 64

Mill 1½" Depth full width of pavement,
edge of pavement to edge of pavement.

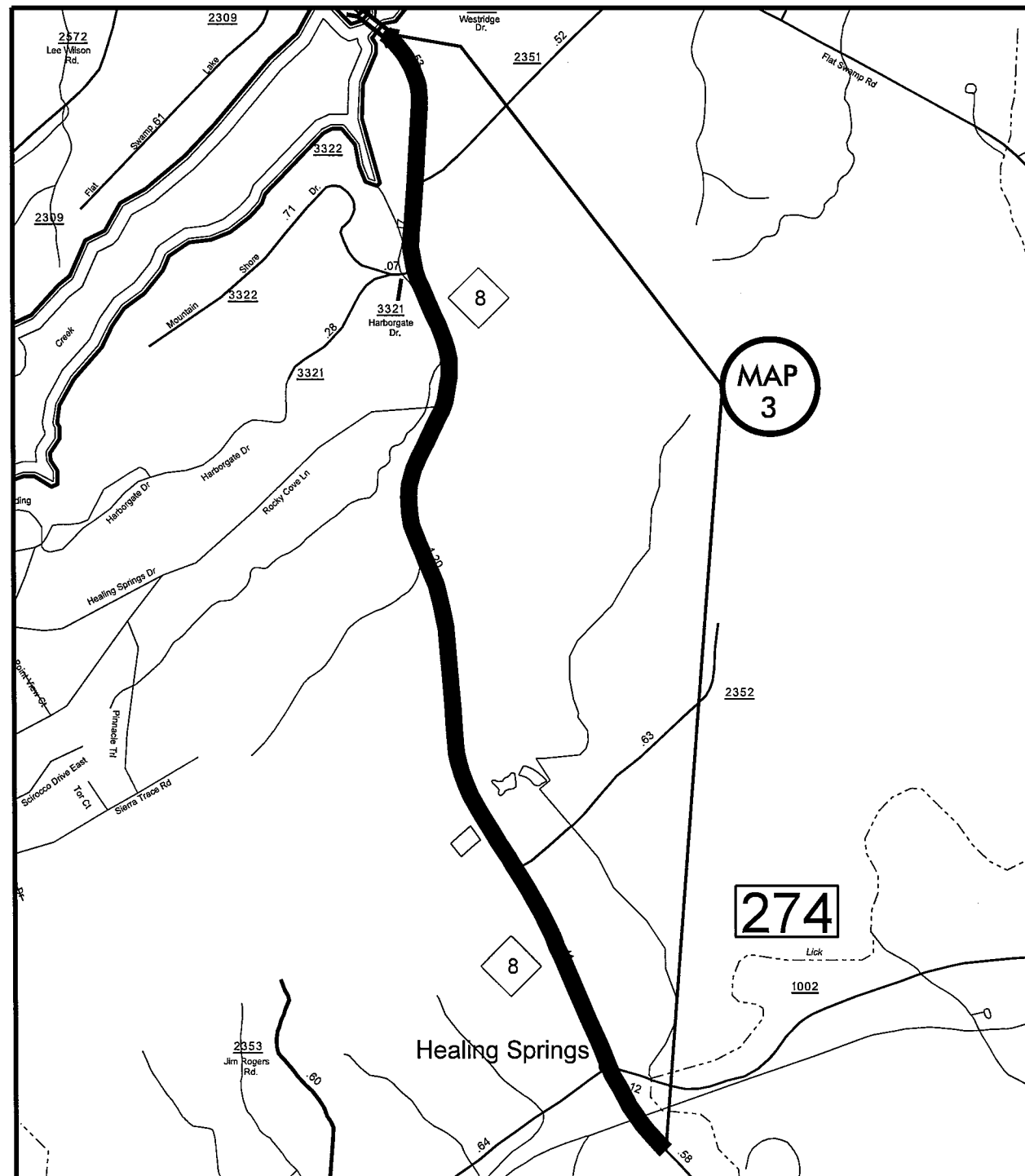
ALL WORK ON THIS MAP TO BE
NIGHT TIME ONLY
7 P.M. TO 6 A.M., Monday-Sunday.

DISTANCE IN ESTIMATE IS TO NOSE
OF ISLAND AS SHOWN,
QUANTITIES INCLUDE PAVING
OF RAMPS AS SHOWN.

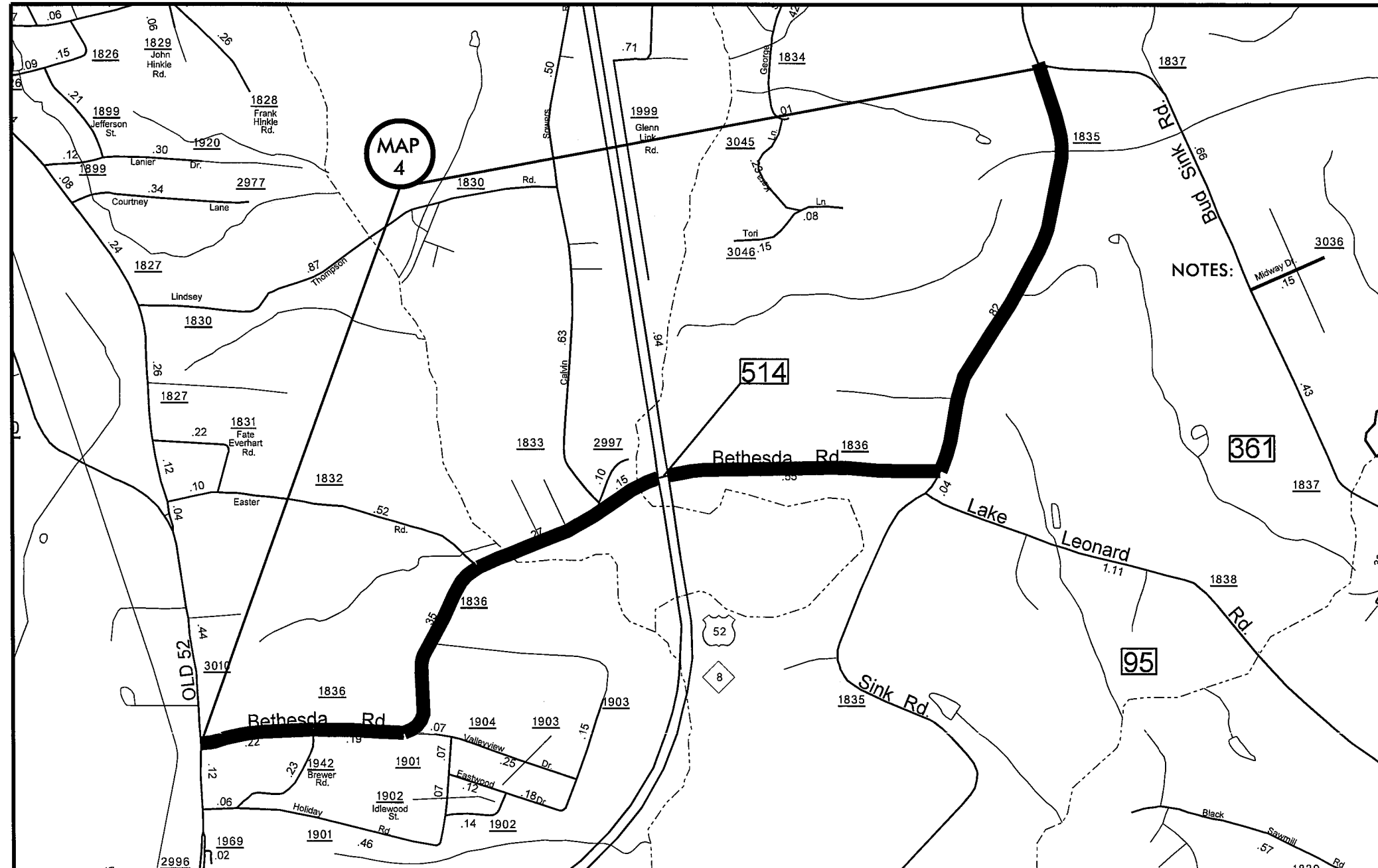


MAP 2
 NC 8
 Mill 1½" depth full width of pavement.
 Pave back with 1½" S9.5B

DAVIDSON COUNTY
 NORTH CAROLINA



MAP 3
 NC 8
 Mill 1½" depth full width of pavement.
 Pave back with 1½" S9.5B.
 Tie in Mill at South end of Map, Map
 ends Approximately 100 feet south of
 drive at Street Address "18846 NC 8".

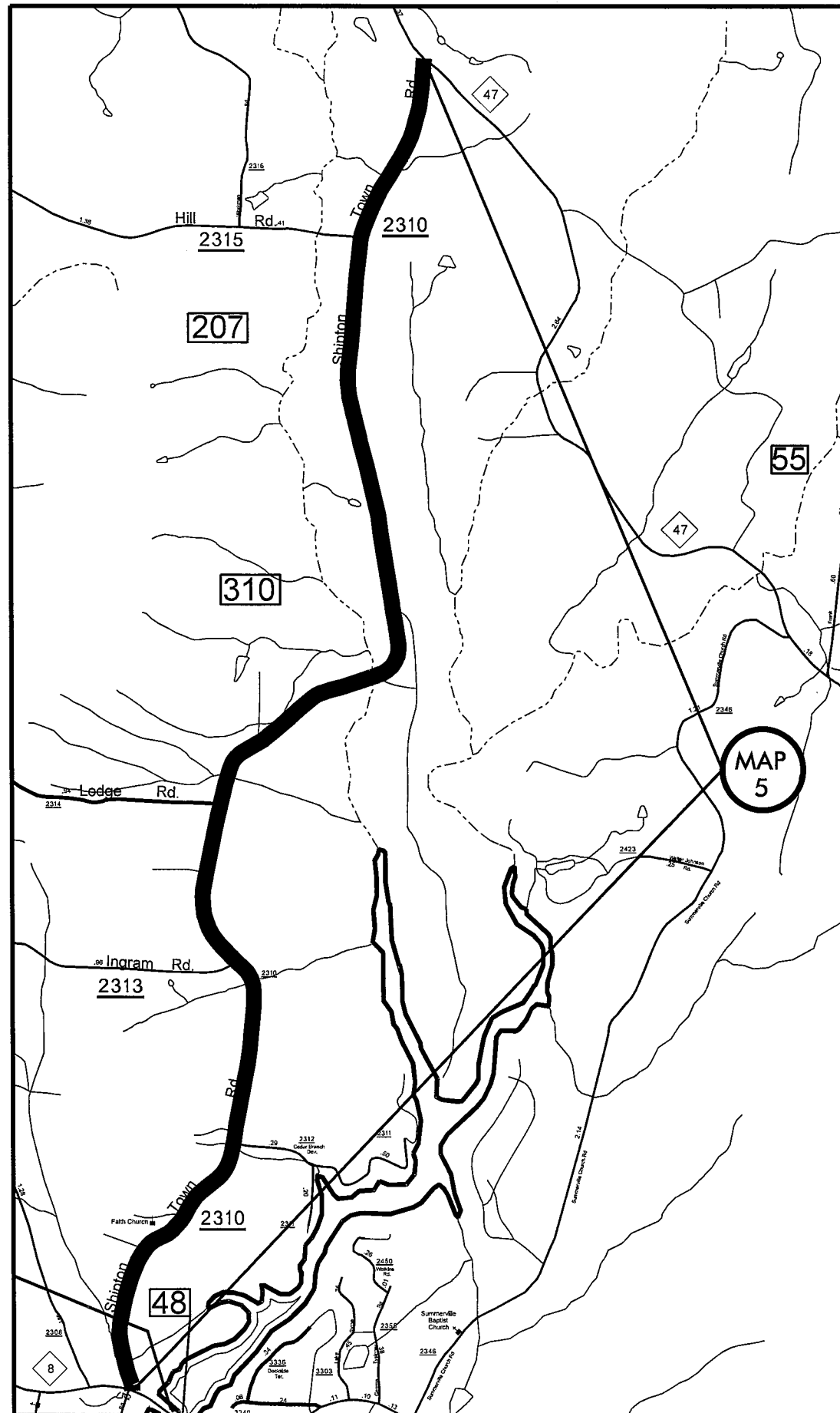


NOTES:

MAP 4
Bethesda Rd. SR 1836

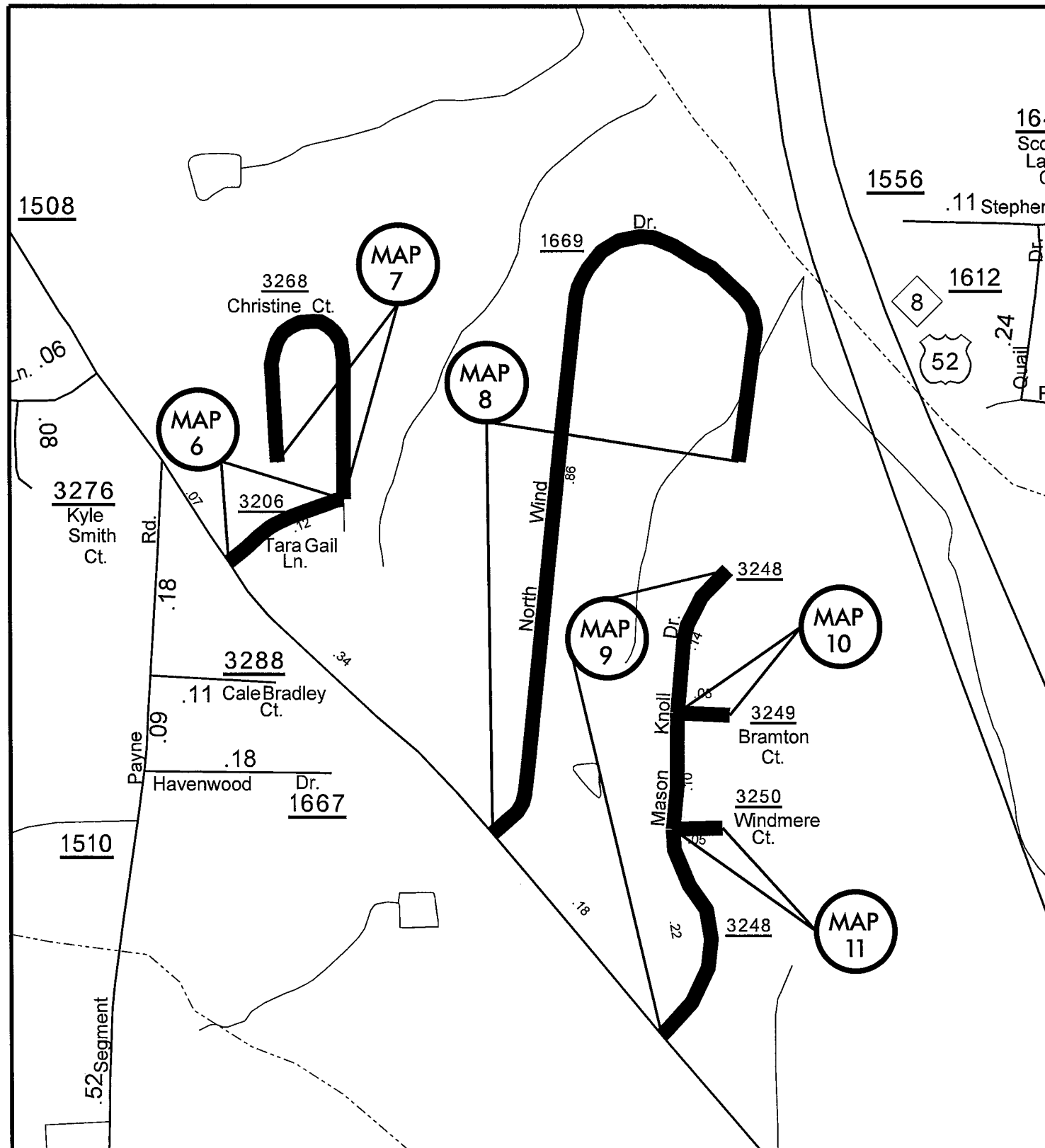
Re-establish crown with 2" in middle
and 1½" at pavement edge.

DAVIDSON COUNTY
NORTH CAROLINA



MAP 5
 Shiptontown Rd. SR 2310
 Tie in Mill at NC 47.
 Tie into new surface at NC 8,
 NO MILLING on NC 8 end
 tie into new surface at NC 8.

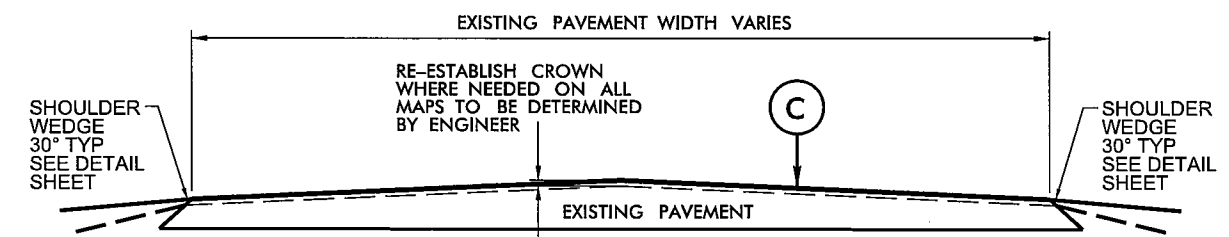
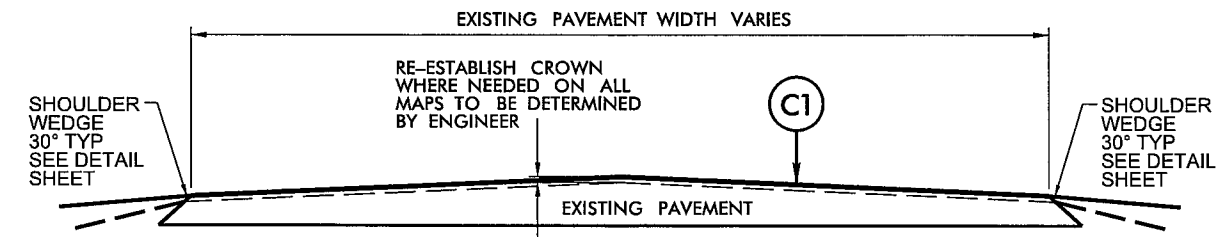
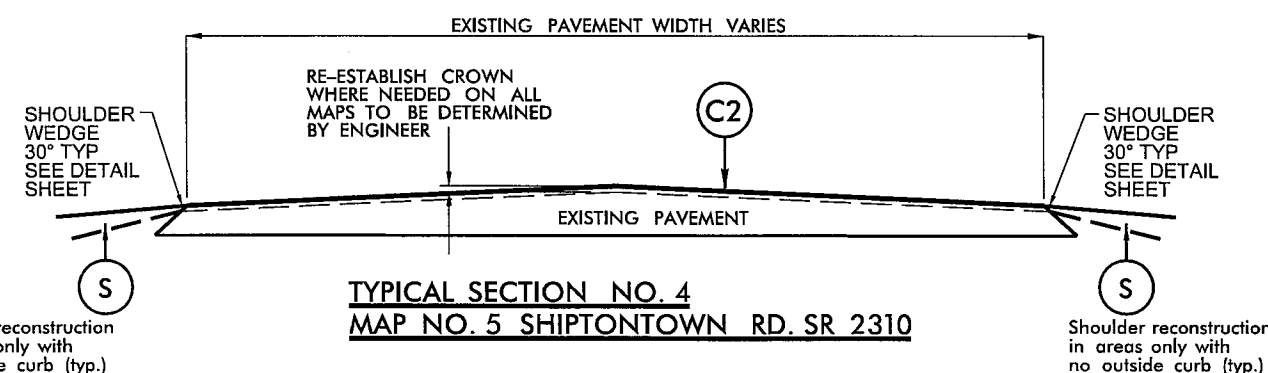
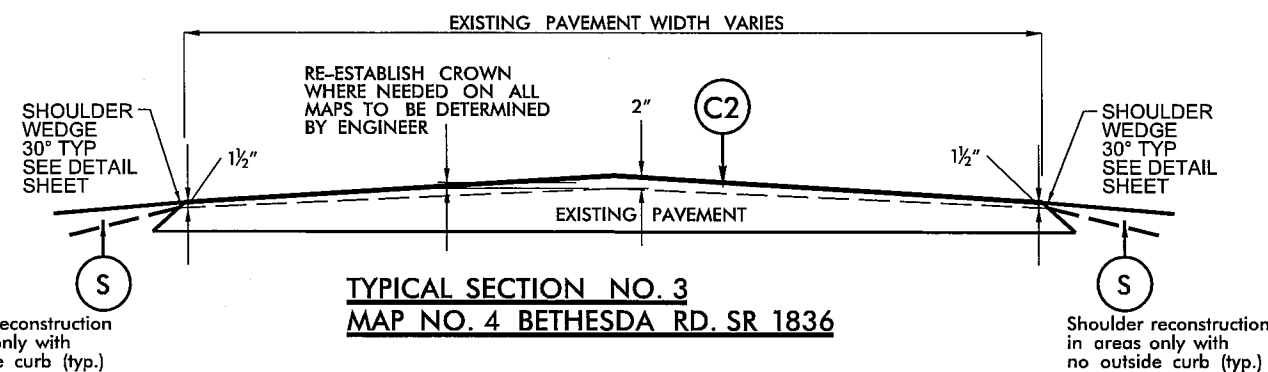
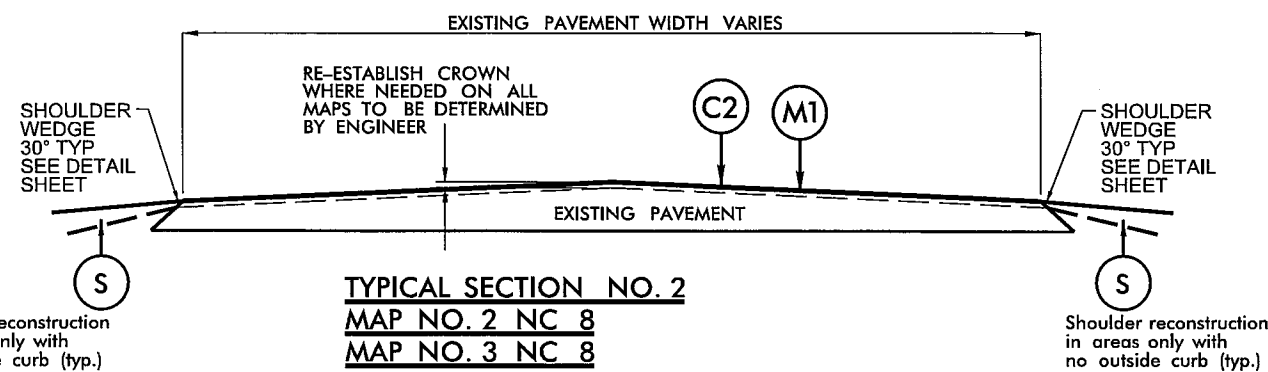
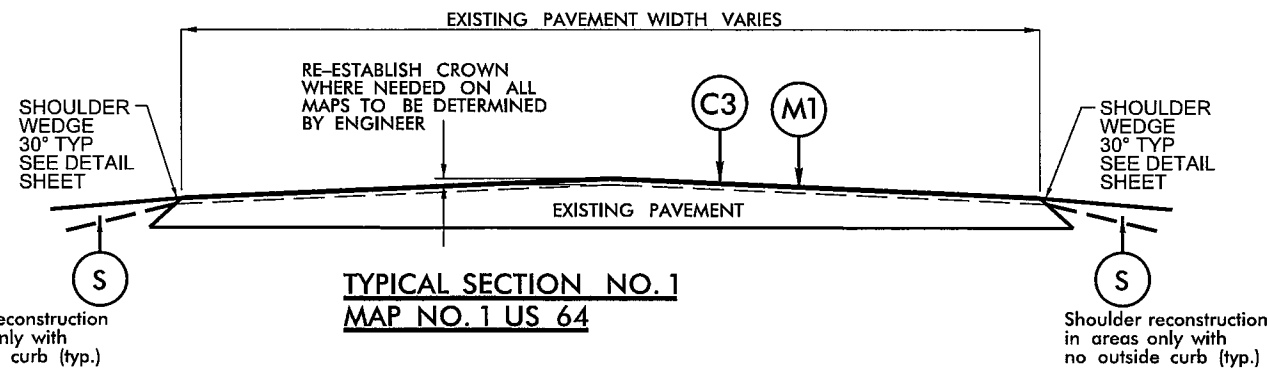
DAVIDSON COUNTY
 NORTH CAROLINA



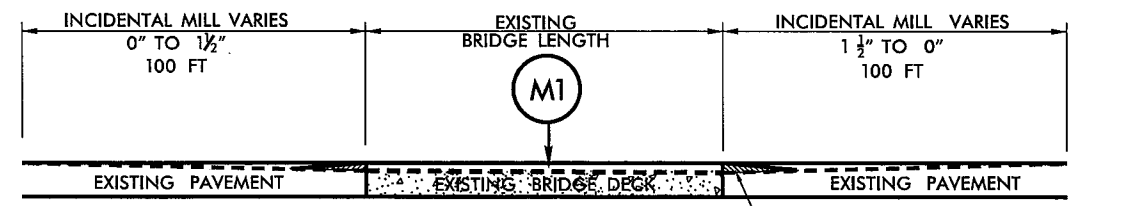
MAP 6 Tara Gail SR 3206
 MAP 7 Christine Ct. SR 3268
 MAP 8 North Wind Dr. SR 1669
 PAVE WITH 1½" SF9.5A
 NO PAVEMENT MARKINGS REQUIRED.

MAP 9 Mason Knoll SR 3248
 MAP 10 Bramton Ct. SR 3249
 MAP 11 Windemere Ct. SR 3250
 PAVE WITH 1" S4.75A
 NO PAVEMENT MARKINGS REQUIRED.

- All Maps
- NO MILLING
 - NO PAVEMENT MARKINGS
 - NO SHOULDER RECONSTRUCTION
 - PAVE ONLY



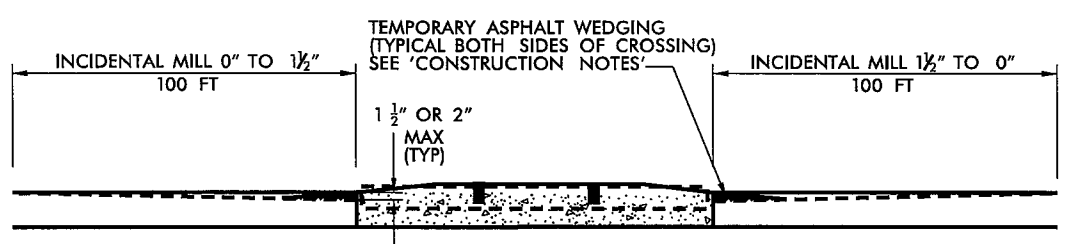
PAVEMENT SCHEDULE	
C	PROP. APPROX. 1" ASPHALT CONCRETE SURFACE COURSE, TYPE S 4.75A, TO BE APPLIED AT AN AVERAGE RATE OF 100 LBS PER SQ. YD.
C1	PROP. APPROX. 1 1/4" ASPHALT CONCRETE SURFACE COURSE, TYPE SF9.5A, TO BE APPLIED AT AN AVERAGE RATE OF 165 LBS PER SQ. YD.
C2	PROP. APPROX. 1 3/4" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, TO BE APPLIED AT AN AVERAGE RATE OF 196 LBS PER SQ. YD.
C3	PROP. APPROX. 1 1/2" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, TO BE APPLIED AT AN AVERAGE RATE OF 168 LBS PER SQ. YD.
M1	MILL ASPHALT PAVEMENT, 1 1/2" DEPTH
S	SHOULDER RECONSTRUCTION (SEE DETAIL)
U	EXISTING PAVEMENT



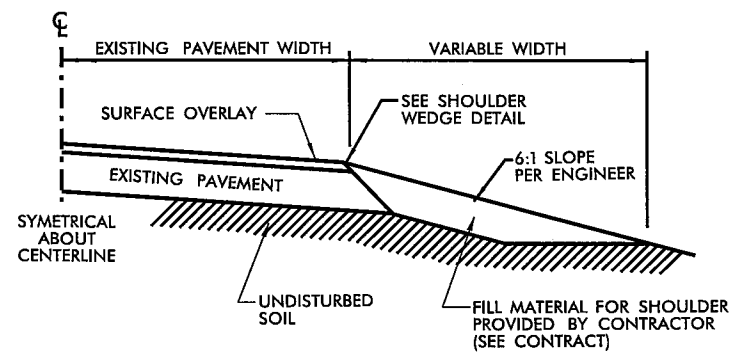
INCIDENTAL MILLING-BRIDGE DECK AND APPROACHES
(SEE BRIDGE DATA SHEET)



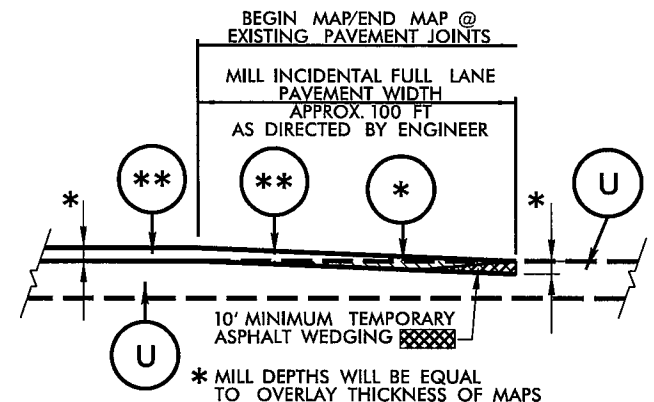
INCIDENTAL MILLING-BRIDGE APPROACHES
(SEE BRIDGE DATA SHEET)



INCIDENTAL MILLING-RAILROAD CROSSING APPROACHES



SHOULDER RECONSTRUCTION

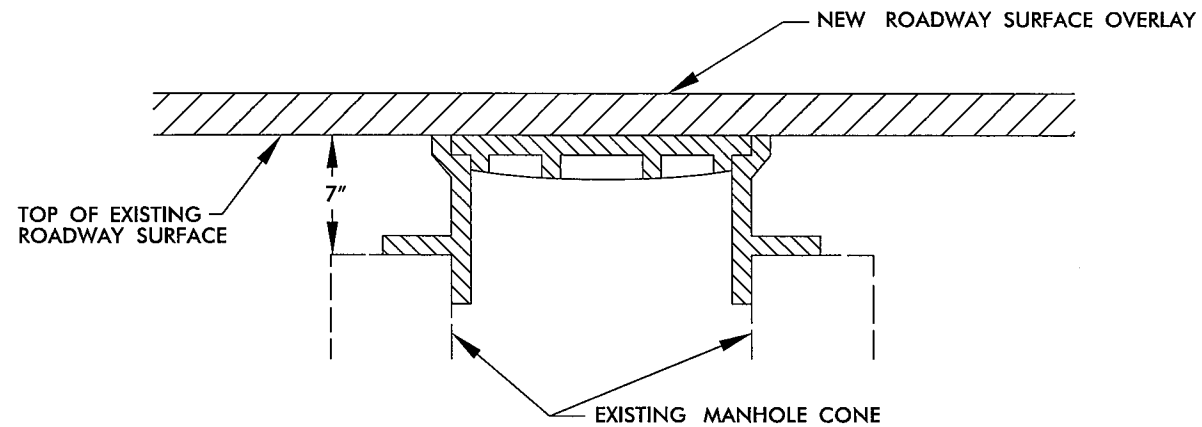


* MILL DEPTHS WILL BE EQUAL TO OVERLAY THICKNESS OF MAPS SEE TYPICALS AND BRIDGE DATA SHEETS
** SEE TYPICALS FOR MIX TYPE
INCIDENTAL MILLING AT TIE-IN DETAIL

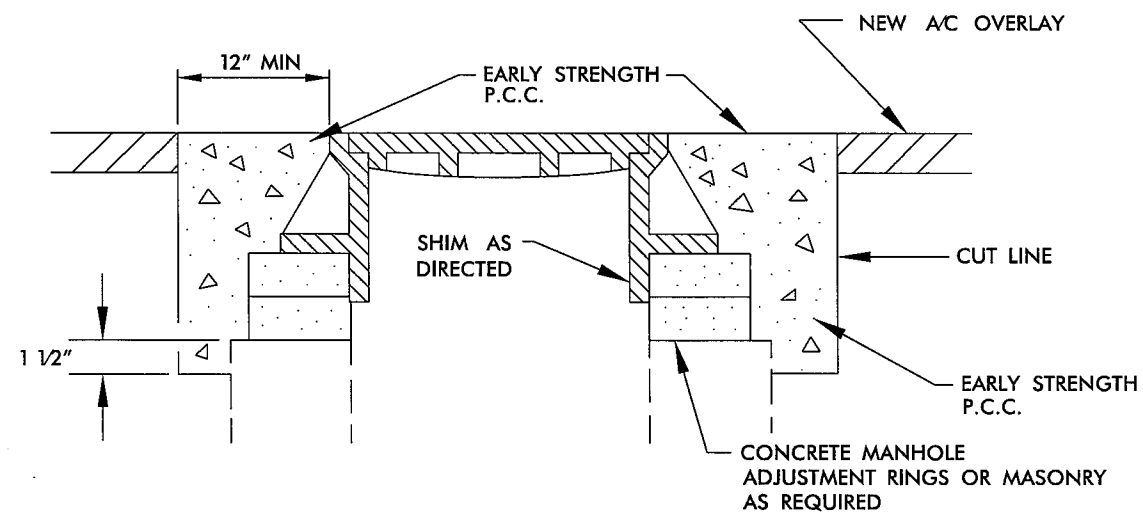
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M1	MILL ASPHALT PAVEMENT, 1 1/2" DEPTH
S	SHOULDER RECONSTRUCTION (SEE DETAIL)
U	EXISTING PAVEMENT

CONSTRUCTION NOTES:

1. ALL QUANTITIES ARE "ESTIMATED" AS INDICATED IN THE "SUMMARY OF QUANTITIES".
2. CONSTRUCTION SHALL PROGRESS IN PHASES, IN THE ORDER INDICATED BELOW:
 - PHASE 1 - MILLING AND PATCHING (WHEN REQUIRED)
 - PHASE 2 - SURFACE OVERLAY
 - PHASE 3 - SHOULDER DROP-OFF REPAIR (AS NEEDED AND DIRECTED BY ENGINEER)
 - PHASE 4 - UTILITY ADJUSTMENTS (MANHOLE RING/COVER, VALVE/METER BOX RING/COVER, CATCH BASIN GRATE/COVER, DROP INLET GRATE/COVER, ETC.) WHEN REQUIRED.
3. BRIDGES THAT HAVE FLOOR DRAINS, SHALL HAVE ALL FLOOR DRAINS LEFT OPEN. EXTRA CARE SHALL BE EXERCISED IN MILLING (IF REQUIRED) AND IN PLACING THE WEARING SURFACE AROUND FLOOR DRAINS SO AS NOT TO HINDER EFFECTIVE DRAINAGE.
4. TEMPORARY ASPHALT WEDGING SHALL BE PLACED ON THE SAME DAY THAT BRIDGE AND/OR RAILROAD APPROACHES ARE MILLED (AND IF APPROACHES ARE MILLED PRIOR TO BRIDGE DECK).
5. FOR TWO-LANE ROADWAYS - IT SHALL BE UNDERSTOOD THAT TYPICALLY ON A ROADWAY MEASURING 20 FEET OR LESS IN WIDTH, THE CENTER OF THE WHITE EDGELINE SHALL BE LOCATED SIX INCHES FROM THE EDGE OF PAVEMENT ON EITHER SIDE OF THE ROADWAY; ON A ROADWAY MEASURING 22 FEET IN WIDTH, TRAVEL LANES SHALL MEASURE 10 FEET AND THE WHITE EDGELINE SHALL BE LOCATED ONE FOOT FROM THE EDGE OF PAVEMENT ON EITHER SIDE; ON A ROADWAY MEASURING 24 FEET IN WIDTH, TRAVEL LANES SHALL MEASURE 11 FEET AND THE WHITE EDGELINE SHALL BE LOCATED ONE FOOT FROM THE EDGE OF PAVEMENT ON EITHER SIDE; ON A ROADWAY MEASURING 26 FEET OR MORE IN WIDTH, TRAVEL LANES SHALL MEASURE 12 FEET AND THE WHITE EDGELINE SHALL BE LOCATED NO LESS THAN ONE FOOT FROM THE EDGE OF PAVEMENT ON EITHER SIDE. THIS SHALL BE STANDARD PRACTICE UNLESS OTHERWISE DIRECTED BY THE ENGINEER.
6. PAPER JOINTS ARE TO BE PLACED BETWEEN DAYS OF PAVING OPERATIONS AS SPECIFIED IN THE STANDARD SPECIFICATIONS SECTION 610-11.
7. ALL MILLED AREAS WILL BE PAVED WITHIN 72 HOURS UNLESS APPROVED BY THE ENGINEER.
9. REPLACE ANY PORTION OF STOP BARS AND OTHER PAVEMENT MARKINGS AT ANY INTERSECTION INCLUDING Y-LINES NOT ACTUALLY BEING PAVED OVER, THAT ARE OBLITERATED BY THE PAVING OPERATION EITHER BY HAULING WHEEL TRACKS OR TACK TRUCK BY THE END OF EACH RESURFACING OPERATION



STEP 1

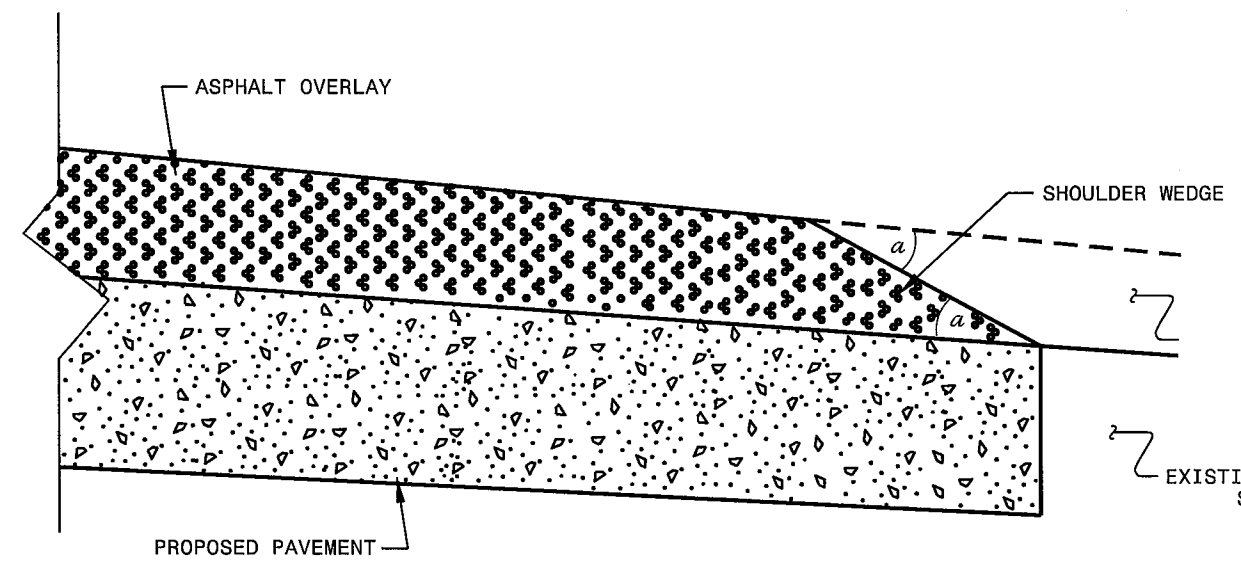


STEPS 2,3, & 4

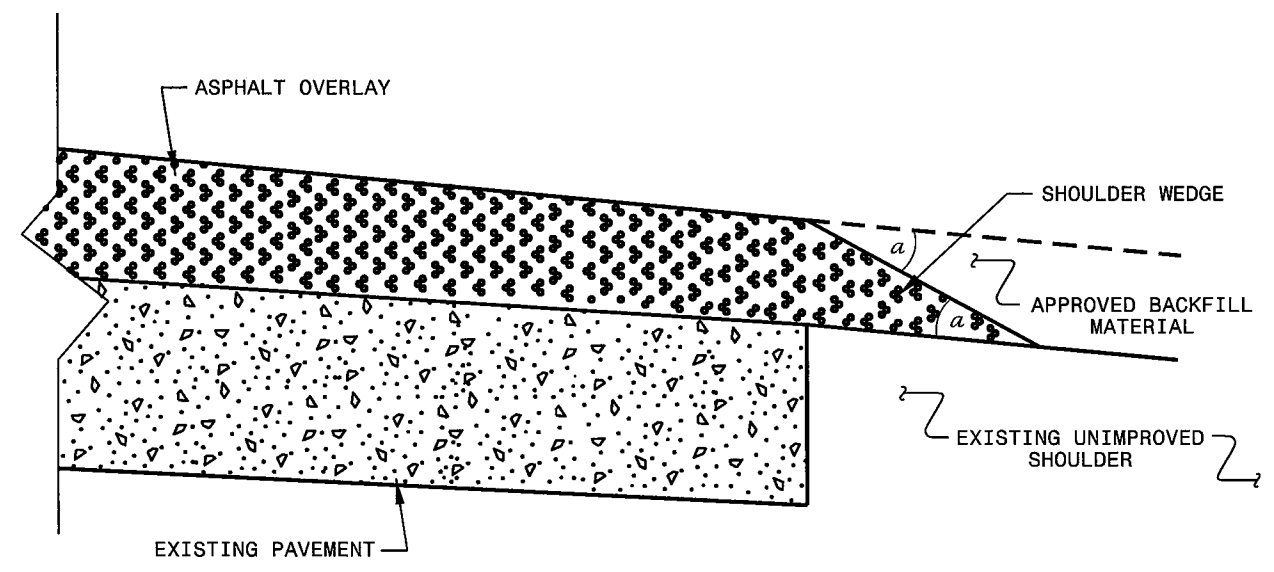
- STEP 1 COVER EXISTING MANHOLE WITH APPROVED MATERIAL AND CONSTRUCT OVERLAY ACROSS TOP OF MANHOLE
- STEP 2 SAW CUT EXCAVATION AROUND MANHOLE 12" MIN. FROM MANHOLE FRAME.
- STEP 3 RAISE MANHOLE FRAME RINGS TO FINISH PAVEMENT PROFILE AND CROSS SLOPE.
- STEP 4 BACKFILL WITH EARLY STRENGTH P.C.C. TO DEPTHS AS DIRECTED.

MANHOLE ADJUSTMENT DETAIL

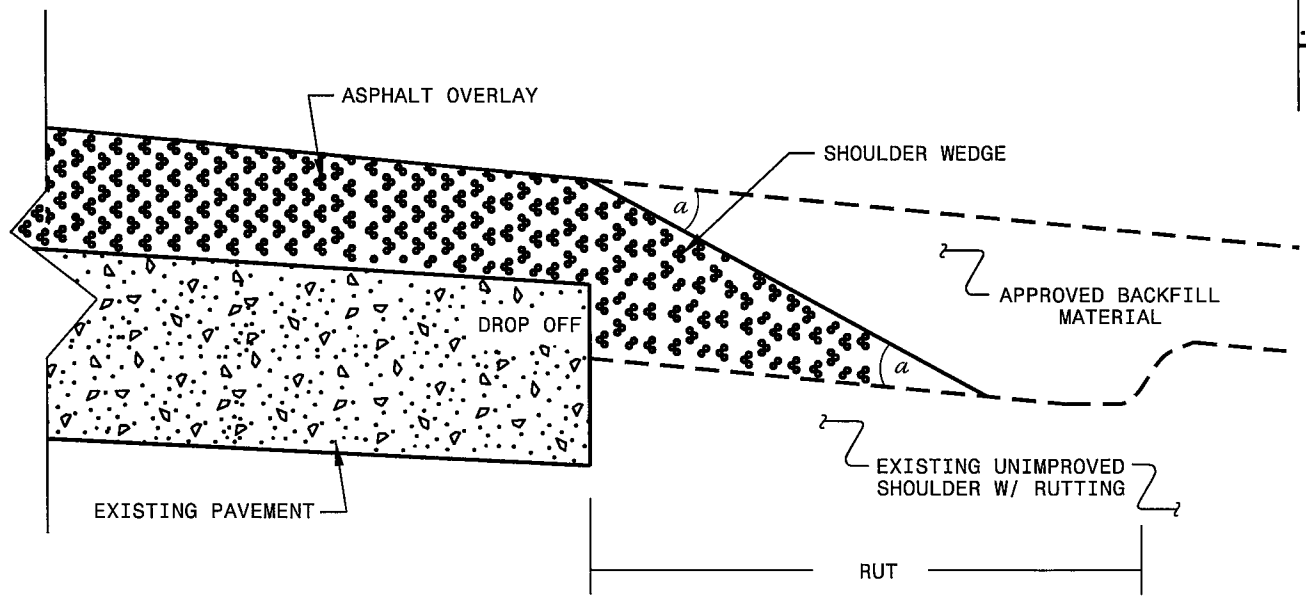
- NOTES:
 1) DETAIL DOES NOT APPLY TO OGAFB AND ULTRA-THIN BONDED WEARING COURSE.
 2) BACKFILL SHOULDER WITH APPROVED MATERIAL.
 3) THE SHOULDER WEDGE DEVICE MAY BE DISENGAGED AT PAVED DRIVEWAYS, SIDE STREETS, HIGH SHOULDERS, AND OTHER LOCATIONS NOT FEASIBLE TO CONSTRUCT AS APPROVED BY THE ENGINEER.



SHOULDER WEDGE DETAIL
 (Resurfacing Projects w/ Widening or
 with Existing Paved Shoulder having no dropoffs)



SHOULDER WEDGE DETAIL
 (Resurfacing Projects w/ NO Widening)



SHOULDER WEDGE DETAIL
 (Resurfacing Adjacent to
 Rutted Shoulder)

- SHOULDER WEDGE ANGLE = 30°

CONTRACT STANDARDS AND DEVELOPMENT UNIT	
Office 919-707-6950 FAX 919-250-4119	
SHOULDER WEDGE DETAILS	
ORIGINAL BY: T.SPELL	DATE: 7-19-11
MODIFIED BY:	DATE: 2/2/16
CHECKED BY:	DATE:
FILE SPEC.: s:\usr\details\stand\shoulderwedge\detail.dgn	

 SYSTEM: 09/18/11
 USER: T.SPELL

Davidson County 2017 Resurfacing Bridges

								PROJECT NO.		SHEET NO.	
								2016CPT.09.18.10291.1 2016CPT.09.19.20291.1		12	
Map No.	Route No.	Route Name	Bridge No.	Feature Intersected	Floor Construction	Clear Roadway Width (Ft)	Horizontal Clearance Under (Ft.)	Vertical Clearance Under	Length (Ft)	Posting	Recommended Treatment, From Bridge Maintenance
1	US 64	US29/70BUS/ I85 NBL	118	US29/70BUS/ I85BUSNBL	6.5 RC, 5" AWS	30	NA	NA	260	NA	INFORMATION ONLY
1	US 64	US 64 WBL	53	SR 3346 MAIN ST.	6.75 RC, 3" AWS	28	NA	NA	164	NA	MILL 1 1/2" AND PAVE Back 1.5"
1	US 64	US 64 WBL	80	US 29/70 BUS/ I-85 BUS NBL	6.75 RC 3 AWS	28	NA	NA	130	NA	MILL 1 1/2" AND PAVE Back 1.5"
1	US 64	US 64	101	SOUTHERN RAILROAD	8.5 RC SLAB	80.1	NA	NA	146	NA	DO NOT MILL OR PAVE
1	US 64	US 64	123	ABBOTTS CREEK	8 1/2 RC SLAB	80.1	NA	NA	112	SV 33 TTST 40	DO NOT MILL OR PAVE
2,3	NC 8	NC 8	59	ABBOTTS CREEK	8 1/8 RC SLAB	40	NA	NA	295	NA	DO NOT MILL OR PAVE
3	NC 8	NC 8	48	HIGH ROCK LAKE	8.5 RC SLAB	40	NA	NA	140	NA	DO NOT MILL OR PAVE
5	SR 1836	BETHESDA RD.	514	US 52 & NC 8	5.25 RC, 3.5 PPC	31	NA	NA	206	NA	DO NOT MILL OR PAVE
6	SR 2310	SHIPTONTOWN RD.	310	FOUR MILE CREEK	PPCCS, 3.25 AWS	24.9	NA	NA	97	NA	MILL 1 1/2" AND PAVE Back 1.5"

PROJECT NO.	SHEET NO.	TOTAL NO.
2016CPT.09.18.10291.1, 2016CPT.09.19.20291.1	13	

SUMMARY OF QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP	LANES	LANE TYPE	FINAL SURFACE TESTING REQUIRED	WARM MIX ASPHALT REQUIRED	NON-TRACKING ASPHALT TACK COAT	LENGTH MI	WIDTH FT	BORROW EXCAVATION CY	INCIDENTAL STONE BASE TONS	SHOULDER RECONSTRUCTION SMI	MILLING ASPHALT PAVEMENT, 1 1/2" DEPTH SY	INCIDENTAL MILLING SY	SURFACE COURSE, S9.5B TONS	SURFACE COURSE, S9.5C TONS	SURFACE COURSE, SF9.5A TONS	SURFACE COURSE, S4.75A TONS	ASPHALT BINDER FOR PLANT MIX TONS	PATCHING EXISTING PAVEMENT TONS	ADJ. OF METER OR VALVE BOX EA	PORTABLE LIGHTING LS	TEMP SILT FENCE LF	WATTLE LF	
2016CPT.09.18.10291.1	Davidson	1	US 64	FROM PAVEMENT JT. AT END OF MEDIAN AT LINDA PLACE TO NOSE OF CONC. MEDIAN NEAR ENT. TO SMART START OF DAVIDSON CO. INCLUDES RAMPS AS SHOWN ON MAPS	1	5	2WU	NO	NO	YES	2.789	71	540		7.44	133,215			12,888			760			1		1,803	181
TOTAL FOR MAP NO. 1											2.789		540		7.44	133,215			12,888			760			1		1,803	181
2016CPT.09.18.10291.1	Davidson	2	NC 8	FROM BRIDGE # 59 TO BRIDGE # 48	2	2	2WU	NO	NO	YES	4.247	22	510	183	8.49	62,002		5,538				332	831	1			1,699	170
TOTAL FOR MAP NO. 2											4.247		510	183	8.49	62,002		5,538				332	831	1			1,699	170
2016CPT.09.18.10291.1	Davidson	3	NC 8	FROM BRIDGE #48 TO APPROX. 100 FEET SOUTH OF DRIVE AT STREET ADDRESS "18846 NC 8"	2	2	2WU	NO	NO	YES	2.3	20	276	207	4.60	28,166		2,737				164	410				920	92
TOTAL FOR MAP NO. 3											2.3		276	207	4.60	28,166		2,737				164	410				920	92
TOTAL FOR PROJ NO. 2016CPT.09.18.10291.1											9.336		1,326	390	20.53	223,383		8,275	12,888			1,256	1,241	1	1	4,422	443	
2016CPT.09.19.20291.1	Davidson	4	SR 1836 BETHESDA RD.	FROM OLD US 52 TO PAVEMENT JT. AT BUDD SINK RD. SR 1837	3	2	2WU	NO	NO	YES	2.483	23	298	180	4.97		1,323	3,800									993	99
TOTAL FOR MAP NO. 4											2.483		298	180	4.97		1,323	3,800									993	99
2016CPT.09.19.20291.1	Davidson	5	SR 2310 SHIPTONTOWN RD.	FROM NC 8 EDGE OF PAVEMENT TO NC 47 EDGE OF PAVEMENT	4	2	2WU	NO	NO	YES	5.027	20	603	204	10.05	268	667	5,733				344					2,011	201
TOTAL FOR MAP NO. 5											5.027		603	204	10.05	268	667	5,733				344					2,011	201
2016CPT.09.19.20291.1	Davidson	6	SR 3206 TARA GAIL LANE	FROM HICKORY TREE RD. SR 1508 TO CHRISTINE CT. SR 3268	5	2	2WU	NO	NO	YES	0.105	18								106		7						
TOTAL FOR MAP NO. 6											0.105									106		7						
2016CPT.09.19.20291.1	Davidson	7	SR 3268 CHRISTINE CT.	FROM TARA GAIL LANE END TO BOTH DIRECTIONS OF CHRISTINE CT. INCLUDING CUL-DE-SACS	5	2	2WU	NO	NO	YES	0.356	18								442		30						
TOTAL FOR MAP NO. 7											0.356									442		30						
2016CPT.09.19.20291.1	Davidson	8	SR 1669 NORTH WIND DR.	FROM HICKORY TREE RD. SR 1508 TO END	5	2	2WU	NO	NO	YES	0.852	19								953		64						
TOTAL FOR MAP NO. 8											0.852									953		64						
2016CPT.09.19.20291.1	Davidson	9	SR 3248 MASON KNOLL DR.	FROM HICKORY TREE RD. SR 1508 TO END	6	2	2WU	NO	NO	YES	0.454	19										22						
TOTAL FOR MAP NO. 9											0.454												22					
2016CPT.09.19.20291.1	Davidson	10	SR 3249 BRAMTON CT.	FROM MASON KNOLL DR. SR 3248 TO END	6	2	2WU	NO	NO	YES	0.058	18										66	4					
TOTAL FOR MAP NO. 10											0.058												66	4				
2016CPT.09.19.20291.1	Davidson	11	SR 3250 WINDMERE CT.	FROM MASON KNOLL DR. SR 3248 TO END	6	2	2WU	NO	NO	YES	0.058	18										62	4					
TOTAL FOR MAP NO. 11											0.058												62	4				
TOTAL FOR PROJ NO. 2016CPT.09.19.20291.1											9.393		901	384	15.02	268	1,990	9,533			1,501	452	703				3,004	300
GRAND TOTAL											18.729		2,227	774	35.55	223,651	1,990	17,808	12,888	1,501	452	1,959	1,241	1	1	7,426	743	

NOTE: All Quantities listed include turn lanes and are estimates; Payment will be based on actual field measurements and quantities received.

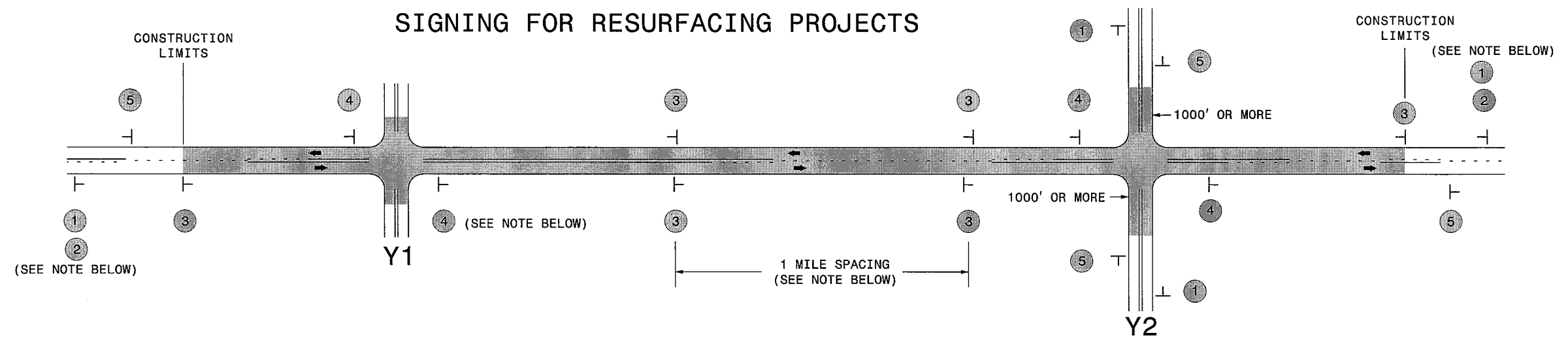
PROJECT NO.	SHEET NO.	TOTAL NO.
2016CPT.09.18.10291.1, 2016CPT.09.19.20291.1	14	

THERMOPLASTIC AND PAINT QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP	LANES	LANE TYPE	LENGTH	WIDTH	4413000000-E	4457000000-N	4685000000-E	4686000000-E	4705000000-E	4710000000-E	4721000000-E	4725000000-E	4810000000-E	4820000000-E	4830000000-E	4835000000-E	4840000000-N	4847000000-E	4847100000-E	4905000000-N									
										WORK ZONE ADVANCE/GENERAL WARNING SIGNING SF	TEMPORARY TRAFFIC CONTROL	4" X 90 M WHITE THERMO	4" X 120 M YELLOW THERMO	4" X 120 M WHITE THERMO	16" X 120 M WHITE THERMO	24" X 120 M WHITE THERMO	THERMO MSG ONLY 120 M	THERMO RXR 120 M	THERMO LT ARROW 90 M	4" WHITE PAINT	4" YELLOW PAINT	8" WHITE PAINT	16" WHITE PAINT	24" WHITE PAINT	PAINT MSG RXR	4" WHITE POLYUREA (HIGHLY REFLECTIVE ELEMENTS) LF	4" YELLOW POLYUREA (HIGHLY REFLECTIVE ELEMENTS) LF	8" WHITE POLYUREA (HIGHLY REFLECTIVE ELEMENTS) LF	SNOW PLOWABLE MARKERS					
2016CPT.09.18.10291.1	Davidson	1	US 64	FROM PAVEMENT JT. AT END OF MEDIAN AT LINDA PLACE TO NOSE OF CONC. MEDIAN NEAR ENT. TO SMART START OF DAVIDSON CO. INCLUDES RAMPS AS SHOWN ON MAPS		1	5	2WU	2,789	71	2,258				79	16		88	46,740	41,483	1,669				79	46,740	41,483	1,669	736					
TOTAL FOR MAP NO. 1									2,789		2,258			79	16		88	46,740	41,483	1,669				79	46,740	41,483	1,669	736						
2016CPT.09.18.10291.1	Davidson	2	NC 8	FROM BRIDGE # 59 TO BRIDGE # 48		2	2	2WU	4,247	22									46,122	45,123							46,122	45,123	280					
TOTAL FOR MAP NO. 2									4,247										46,122	45,123						46,122	45,123	280						
2016CPT.09.18.10291.1	Davidson	3	NC 8	FROM BRIDGE #48 TO APPROX. 100 FEET SOUTH OF DRIVE AT STREET ADDRESS "18846 NC 8"		2	2	2WU	2.3	20				50	25				24,844	24,288			50	25	2	24,844	24,288	152						
TOTAL FOR MAP NO. 3									2.3				50	25					24,844	24,288			50	25	2	24,844	24,288	152						
TOTAL FOR PROJ NO. 2016CPT.09.18.10291.1									9,336		2,258	1		50	104	16	2	88	117,706	110,894	1,669		50	104	2	117,706	110,894	1,669	1,168					
																		228,600								228,600								
2016CPT.09.19.20291.1	Davidson	4	SR 1836 BETHESDA RD.	FROM OLD US 52 TO PAVEMENT JT. AT BUDD SINK RD. SR 1837		3	2	2WU	2,483	23			26,717	26,220	92													454	454					
TOTAL FOR MAP NO. 4									2,483				26,717	26,220	92													454	454					
2016CPT.09.19.20291.1	Davidson	5	SR 2310 SHIPTONTOWN RD.	FROM NC 8 EDGE OF PAVEMENT TO NC 47 EDGE OF PAVEMENT		4	2	2WU	5,027	20			54,091	53,085	60																			
TOTAL FOR MAP NO. 5									5,027				54,091	53,085	60																			
2016CPT.09.19.20291.1	Davidson	6	SR 3206 TARA GAIL LANE	FROM HICKORY TREE RD. SR 1508 TO CHRISTINE CT. SR 3268		5	2	2WU	0.105	18																								
TOTAL FOR MAP NO. 6									0.105																									
2016CPT.09.19.20291.1	Davidson	7	SR 3268 CHRISTINE CT.	FROM TARA GAIL LANE END TO BOTH DIRECTIONS OF CHRISTINE CT. INCLUDING CUL-DE-SACS		5	2	2WU	0.356	18																								
TOTAL FOR MAP NO. 7									0.356																									
2016CPT.09.19.20291.1	Davidson	8	SR 1669 NORTH WIND DR.	FROM HICKORY TREE RD. SR 1508 TO END		5	2	2WU	0.852	19																								
TOTAL FOR MAP NO. 8									0.852																									
2016CPT.09.19.20291.1	Davidson	9	SR 3248 MASON KNOLL DR.	FROM HICKORY TREE RD. SR 1508 TO END		6	2	2WU	0.454	19																								
TOTAL FOR MAP NO. 9									0.454																									
2016CPT.09.19.20291.1	Davidson	10	SR 3249 BRAMTON CT.	FROM MASON KNOLL DR. SR 3248 TO END		6	2	2WU	0.058	18																								
TOTAL FOR MAP NO. 10									0.058																									
2016CPT.09.19.20291.1	Davidson	11	SR 3250 WINDMERE CT.	FROM MASON KNOLL DR. SR 3248 TO END		6	2	2WU	0.058	18																								
TOTAL FOR MAP NO. 11									0.058																									
TOTAL FOR PROJ NO. 2016CPT.09.19.20291.1									9,393			80,808	79,305	152													454	454						
																		79,457								908								
GRAND TOTAL									18,729	2,258	1	80,808	79,305	152	50	104	16	2	88	117,706	110,894	1,669	50	104	2	118,160	111,348	1,669	1,168					
																		79,457								18			228,600			229,508		

NOTE: All Quantities listed include turn lanes and are estimates; Payment will be based on actual field measurements and quantities received.

SIGNING FOR RESURFACING PROJECTS



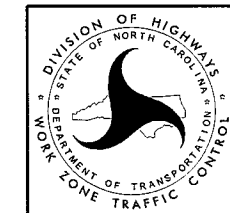
LEGEND	
⊥	STATIONARY SIGN
→	DIRECTION OF TRAFFIC FLOW

MAINLINE (-L-) SIGNING

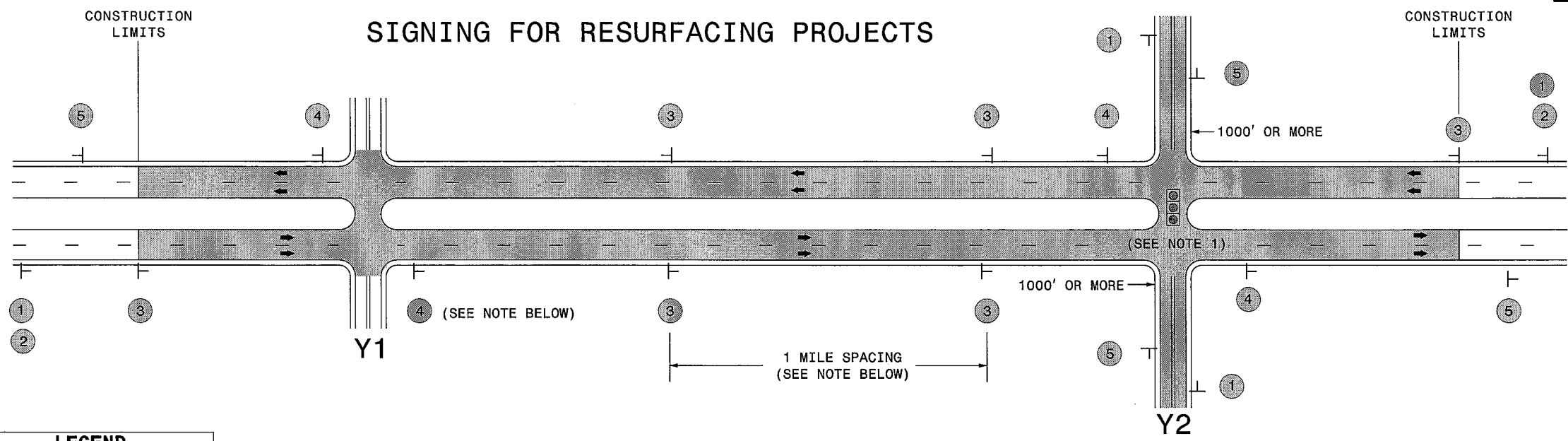
-Y- LINE SIGNING

SIGNING NOTES AND PLACEMENT PER DIRECTION	MAINLINE (-L-) SIGNING		-Y- LINE SIGNING	
	1	 W20-1 48" X 48"	NO REQUIRED STATIONARY SIGNING FOR THE FOLLOWING -Y- LINE CONDITIONS: 1) LESS THAN 1000' OF RESURFACING ALONG -Y- LINE 2) SUBDIVISION ROADS 3) DEAD END ROADS	
	2	 W7-3aP 24" X 18"	WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, ADVANCE WARNING PORTABLE SIGNS SHALL BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.	
	3	 SP 13107 48" X 48"	 W20-1 48" X 48" W20-7 A 48" X 48"	
	4	 SP 13106 48" X 48"	PLACED 500' IN ADVANCE OF FLAGGER. PLACED 250' IN ADVANCE OF FLAGGER.	
5	 G20-2 A 48" X 24"	PLACE 500' FOLLOWING THE END OF CONSTRUCTION LIMITS.		

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 User: jmgarcia



RESURFACING
 ADVANCE WARNING SIGNS
 FOR
 RURAL AND SUBURBAN
 2 LANE ROADWAYS



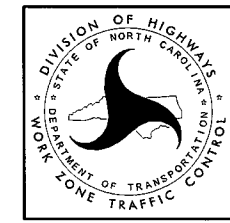
LEGEND
 T STATIONARY SIGN
 ← DIRECTION OF TRAFFIC FLOW

MAINLINE (-L-) SIGNING

-Y- LINE SIGNING

SIGNING NOTES AND PLACEMENT PER DIRECTION	1		PLACE 1000' PRIOR TO BEGINNING OF CONSTRUCTION LIMITS. ONLY USED ON -Y- LINES IF RESURFACING LIMITS EXTEND 1000' ALONG -Y- LINE.	<p>NO REQUIRED STATIONARY SIGNING FOR THE FOLLOWING -Y- LINE CONDITIONS:</p> <ol style="list-style-type: none"> LESS THAN 1000' OF RESURFACING ALONG -Y- LINE SUBDIVISION ROADS DEAD END ROADS <p>WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, ADVANCE WARNING PORTABLE SIGNS SHALL BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.</p> <div style="display: flex; justify-content: space-around;"> <div style="text-align: center;"> <small>W20-1 48" X 48"</small> </div> <div style="text-align: center;"> <small>W20-7 A 48" X 48"</small> </div> </div> <p>PLACED 500' IN ADVANCE OF FLAGGER. PLACED 250' IN ADVANCE OF FLAGGER.</p> <p>NOTES:</p> <ol style="list-style-type: none"> MAY USE LAW ENFORCEMENT TO CONTROL TRAFFIC AT SIGNALIZED INTERSECTIONS AS DIRECTED BY THE ENGINEER. PROVIDE PORTABLE "ROAD WORK AHEAD" (W20-1) SIGNS 500' IN ADVANCE ALONG BOTH APPROACHES FROM THE SIDE STREETS WHEN PAVING PROCEEDS THROUGH THE INTERSECTION.
	2		#2 SIGN ONLY USED WHEN RESURFACING LIMITS ARE 2 OR MORE MILES IN LENGTH. ROUND UP TO NEXT WHOLE NUMBER. (NO FRACTIONAL OR DECIMAL NUMBERS)	
	3		PLACE INITIALLY AT THE CONSTRUCTION LIMITS AND SPACED 1 MILE APART THEREAFTER. IF NO -Y- LINES EXIST, PLACE 2ND SET 1/2 MILE FROM THE CONSTRUCTION LIMITS AND THEN SPACE 1 MILE THEREAFTER.	
	4		THESE ARE FOR -Y- LINES THAT ARE "THROUGH" ROADWAYS. DEAD END AND SUBDIVISION ROADS ARE NOT "THROUGH" ROADWAYS. INSTALL 500' +/- FROM EACH -Y- LINE APPROACH AS SHOWN ABOVE. FOR MULTIPLE -Y- LINES THAT ARE SEPARATED BY 0.25 MILES OR LESS, TREAT AS A SINGLE UNIT AND INSTALL WITHIN 500' OF EACH APPROACH. A MAXIMUM OF 2 SIGN SETS PER MILE. DO NOT INSTALL WHEN -Y- LINES ARE WITHIN 0.5 MILES FROM "END ROAD WORK" SIGN.	
	5		PLACE 500' FOLLOWING THE END OF CONSTRUCTION LIMITS.	

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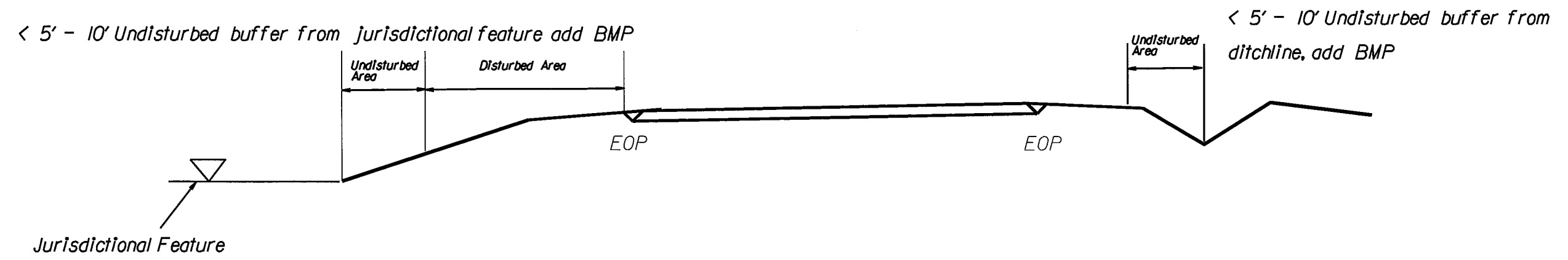
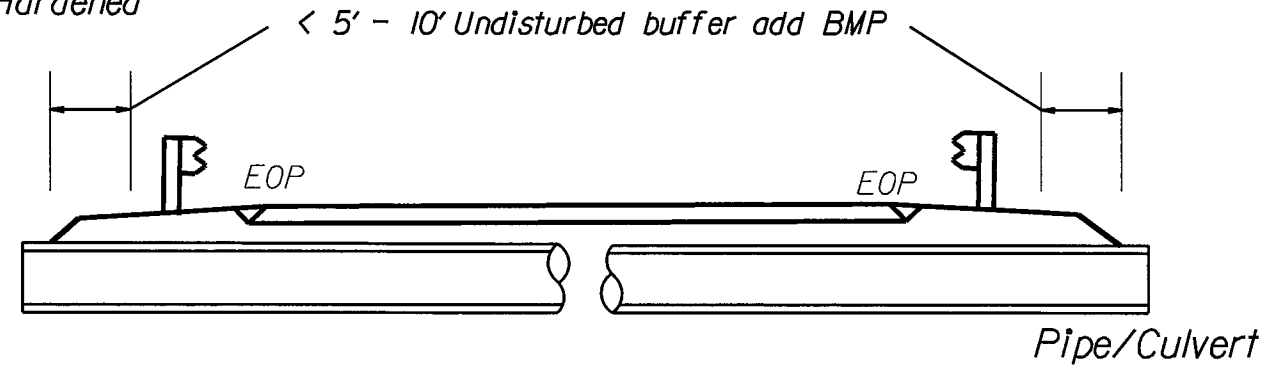


RESURFACING ADVANCE WARNING SIGNS FOR RURAL AND SUBURBAN MULTI-LANE ROADWAYS W/ SHOULDER SECTIONS

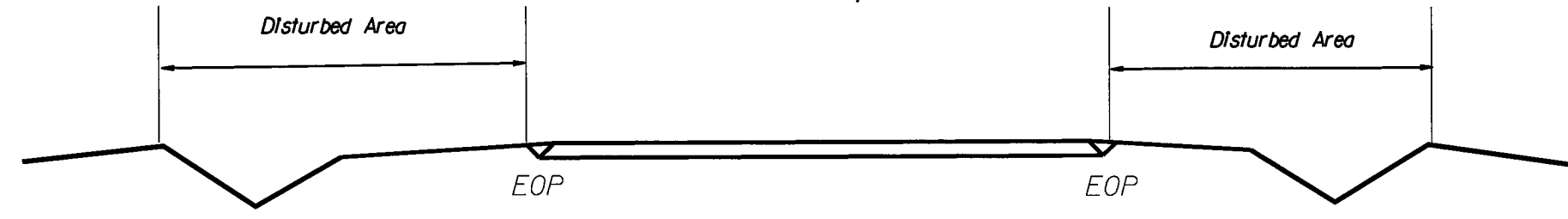
NOTES: Less than 5' - 10' undisturbed buffer from ROW, ditchline, water feature, or drainage inlet, add BMP.

BMP Options: Wattle, Silt Fence or Hardened Aggregate.

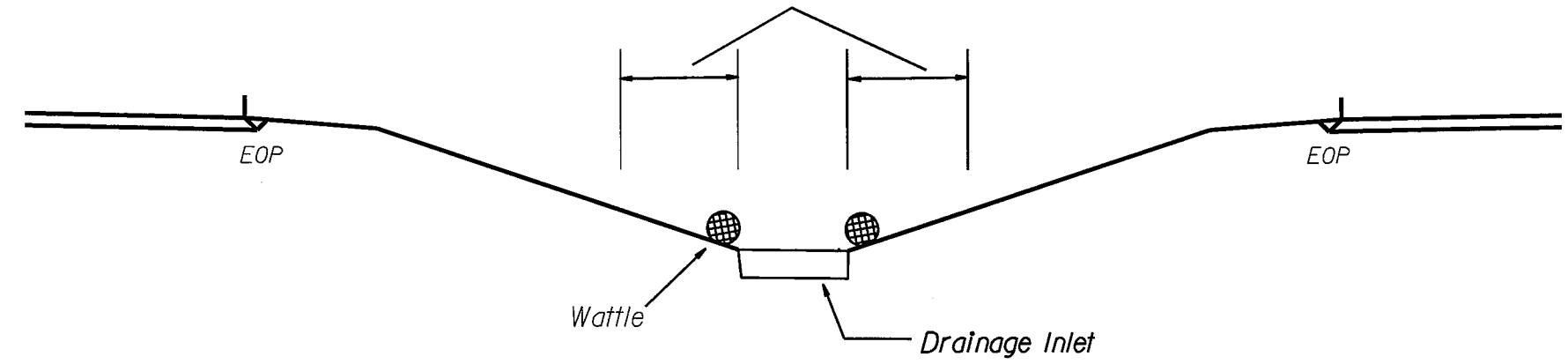
EROSION CONTROL DETAIL



Use BMP's if shoulders and/or frontslopes and/or ditchline and/or backslopes are disturbed

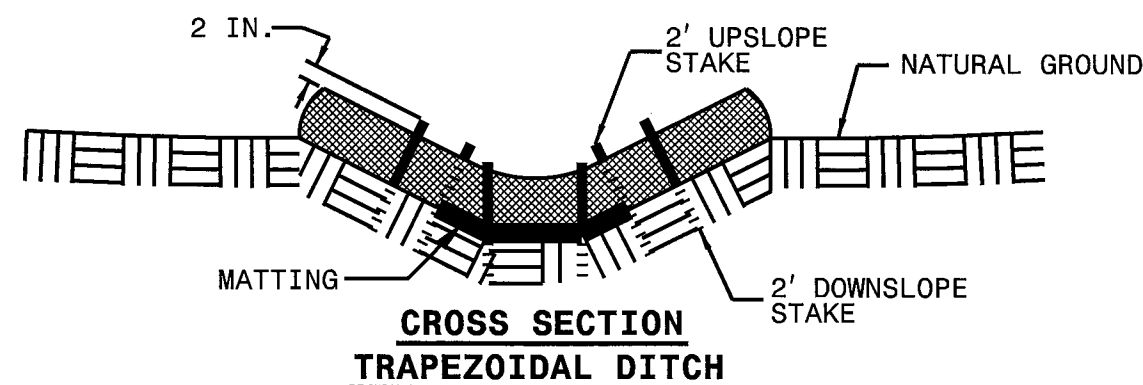
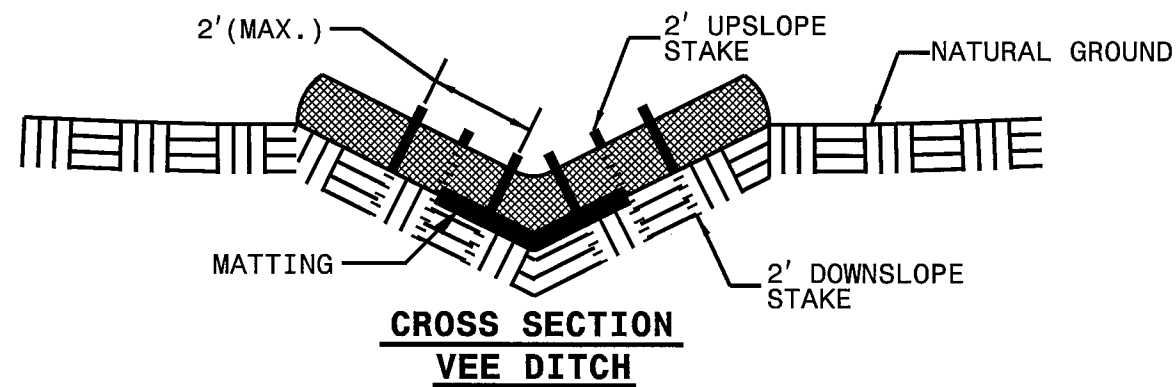
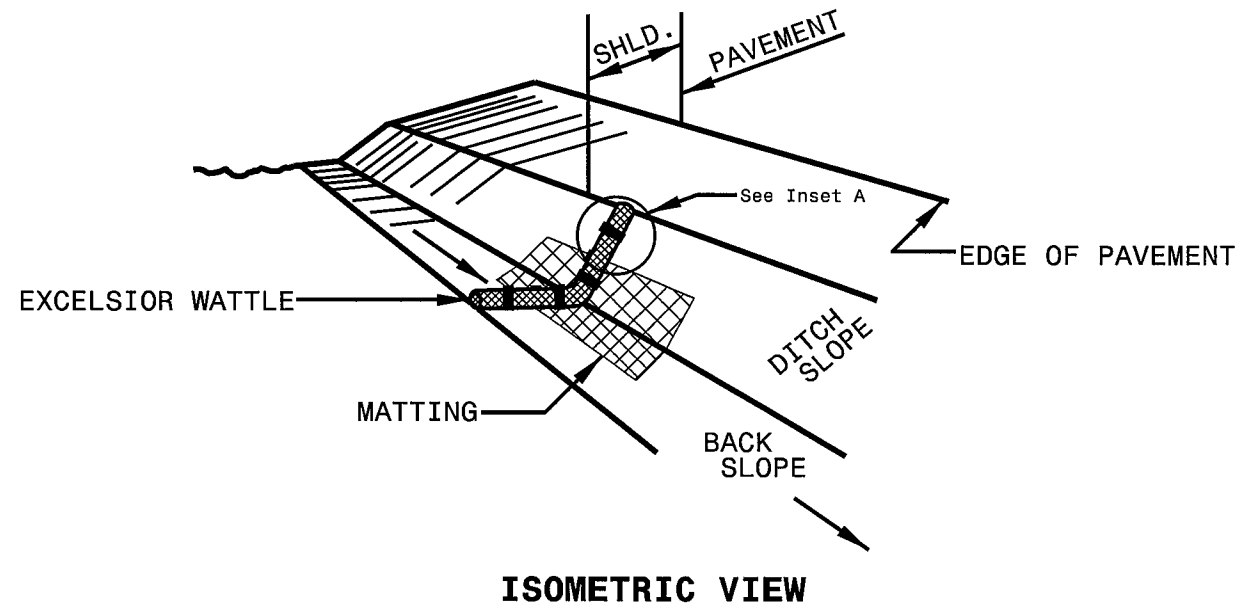


< 5' - 10' Undisturbed buffer from inlet, add wattle



NOT TO SCALE

WATTLE DETAIL



NOTES:

USE MINIMUM 12 IN. DIAMETER EXCELSIOR WATTLE.

USE 2 FT. WOODEN STAKES WITH A 2 IN. BY 2 IN. NOMINAL CROSS SECTION.

ONLY INSTALL WATTLE(S) TO A HEIGHT IN DITCH SO FLOW WILL NOT WASH AROUND WATTLE AND SCOUR DITCH SLOPES AND AS DIRECTED.

INSTALL A MINIMUM OF 2 UPSLOPE STAKES AND 4 DOWNSLOPE STAKES AT AN ANGLE TO WEDGE WATTLE TO BOTTOM OF DITCH.

PROVIDE STAPLES MADE OF 0.125 IN. DIAMETER STEEL WIRE FORMED INTO A U SHAPE NOT LESS THAN 12" IN LENGTH.

INSTALL STAPLES APPROXIMATELY EVERY 1 LINEAR FOOT ON BOTH SIDES OF WATTLE AND AT EACH END TO SECURE IT TO THE SOIL.

INSTALL MATTING IN ACCORDANCE WITH SECTION 1631 OF THE STANDARD SPECIFICATIONS.

