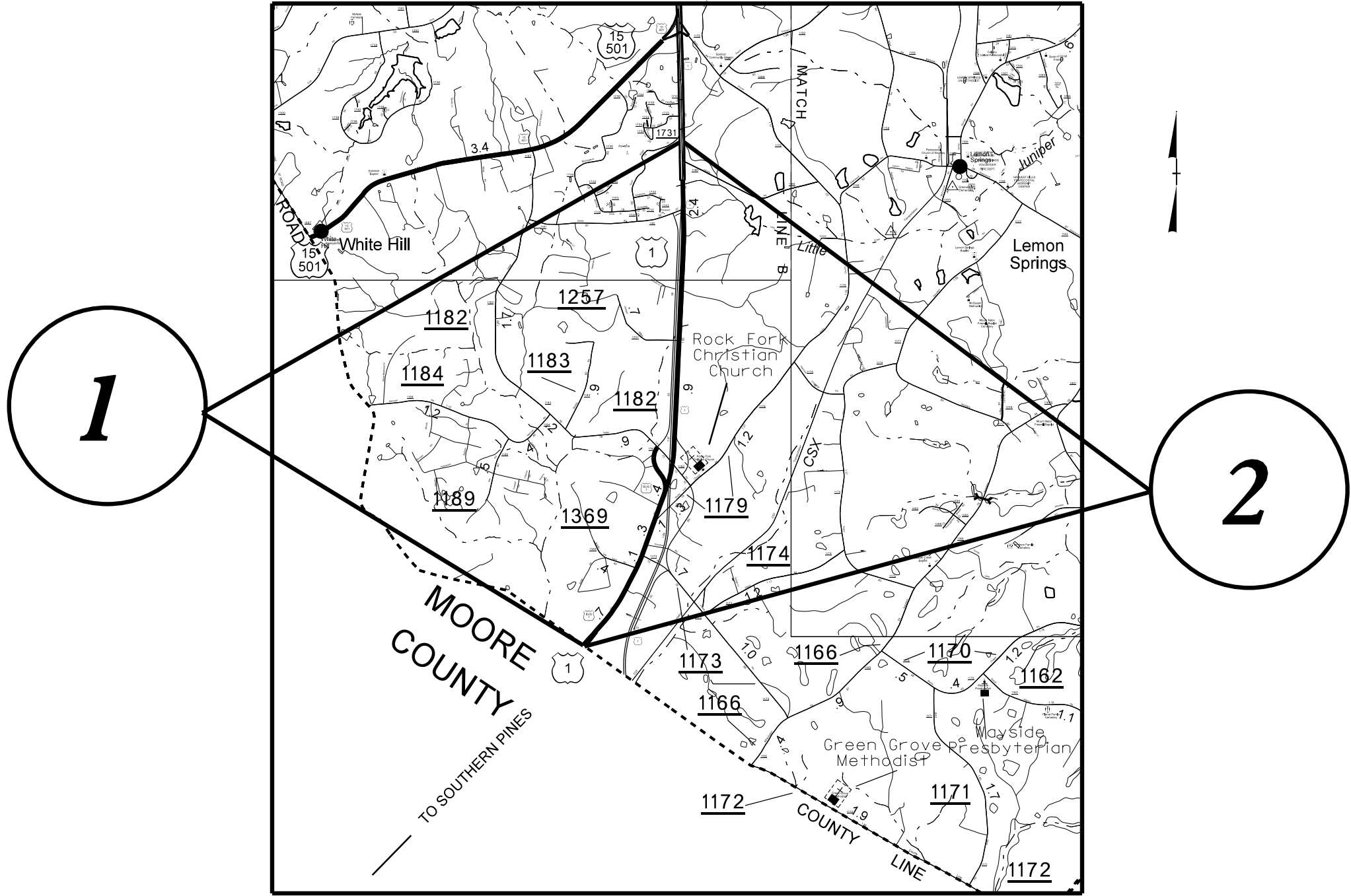
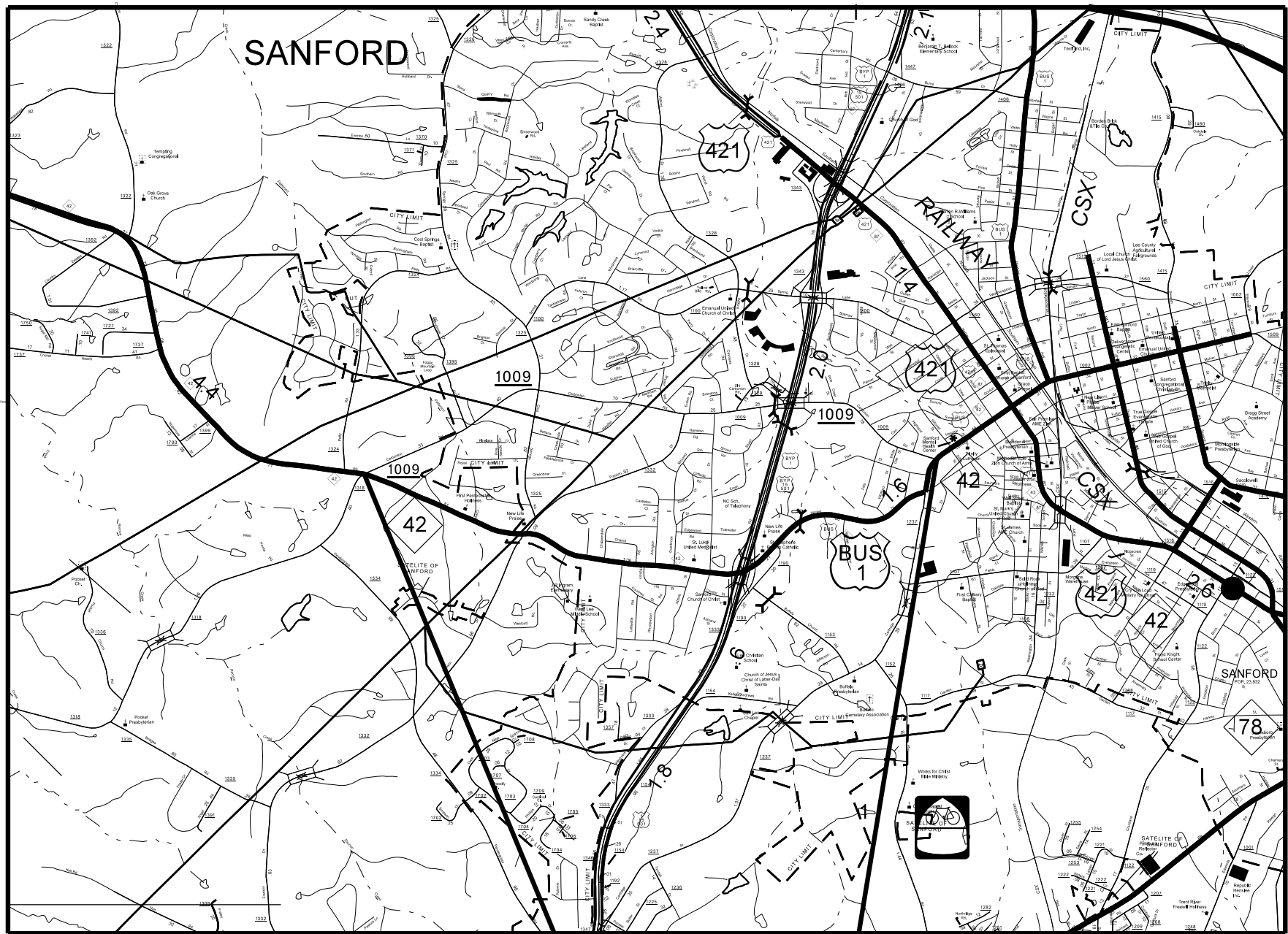


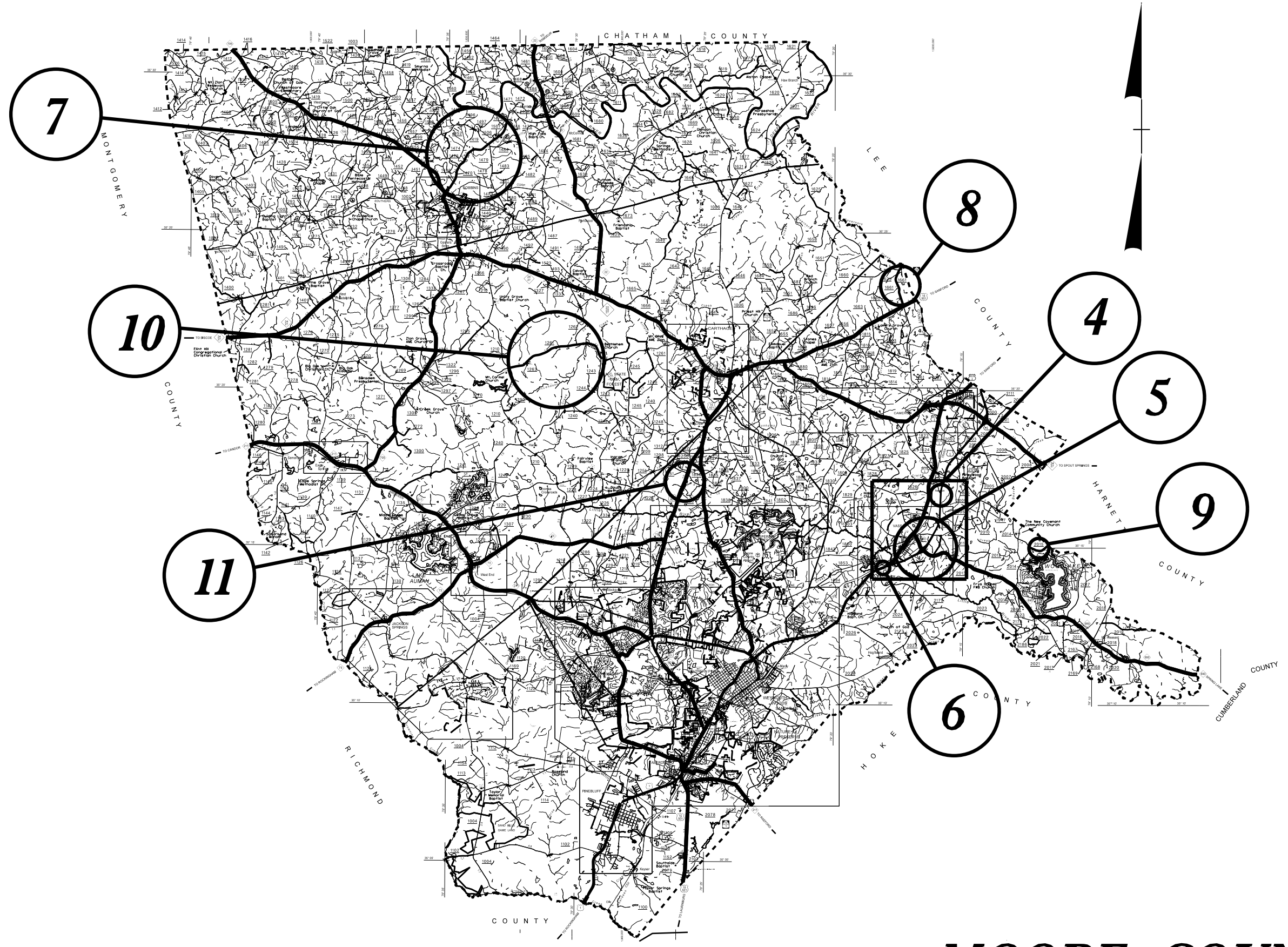
# LEE COUNTY



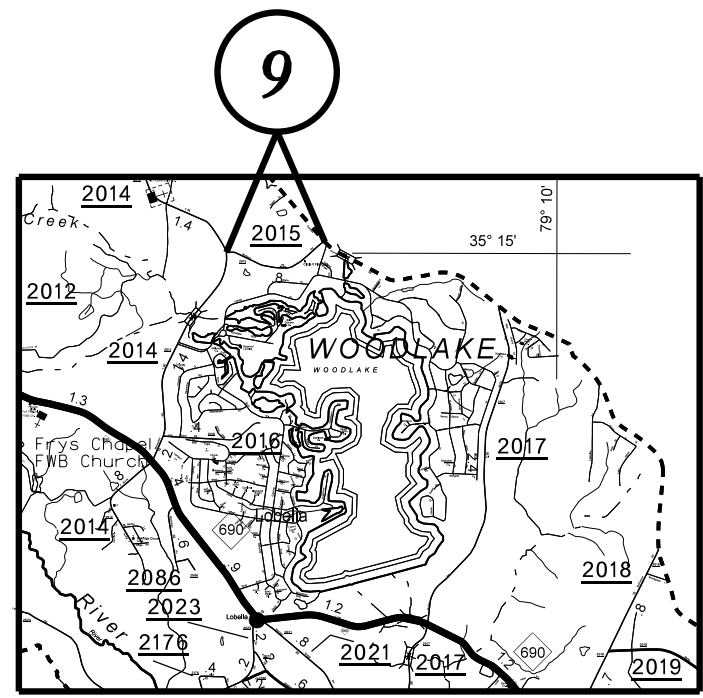
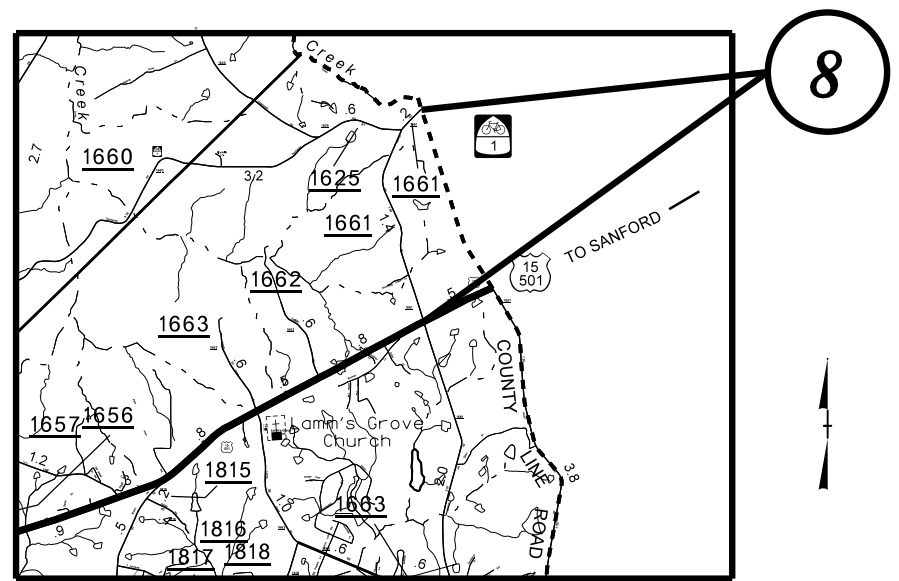
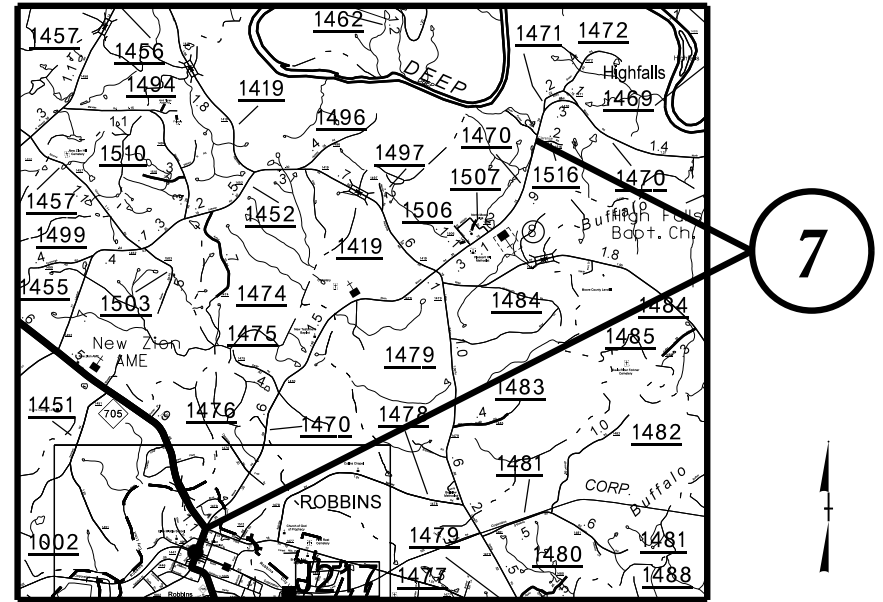
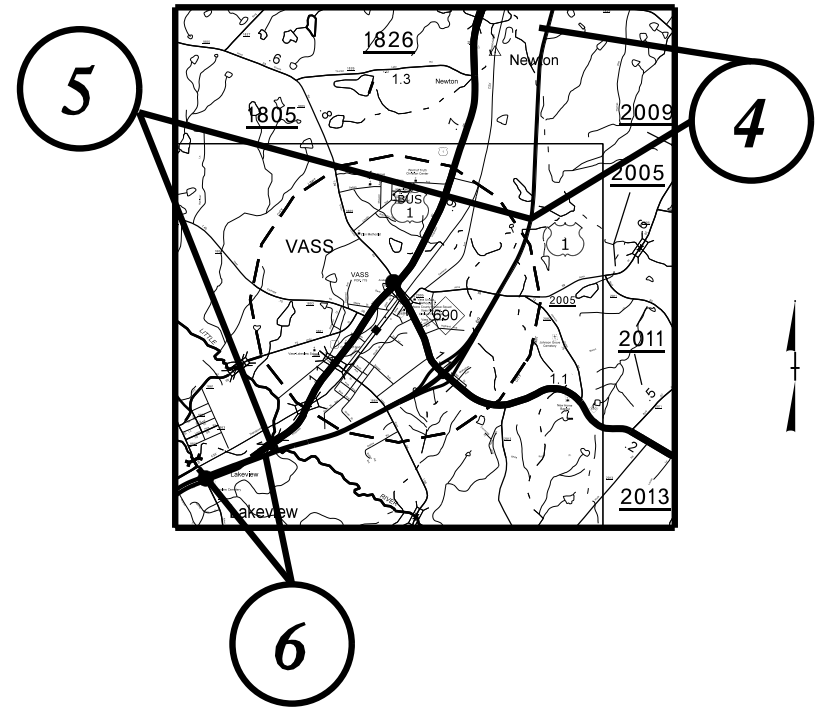


3

5/28/99  
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Author: [unclear]

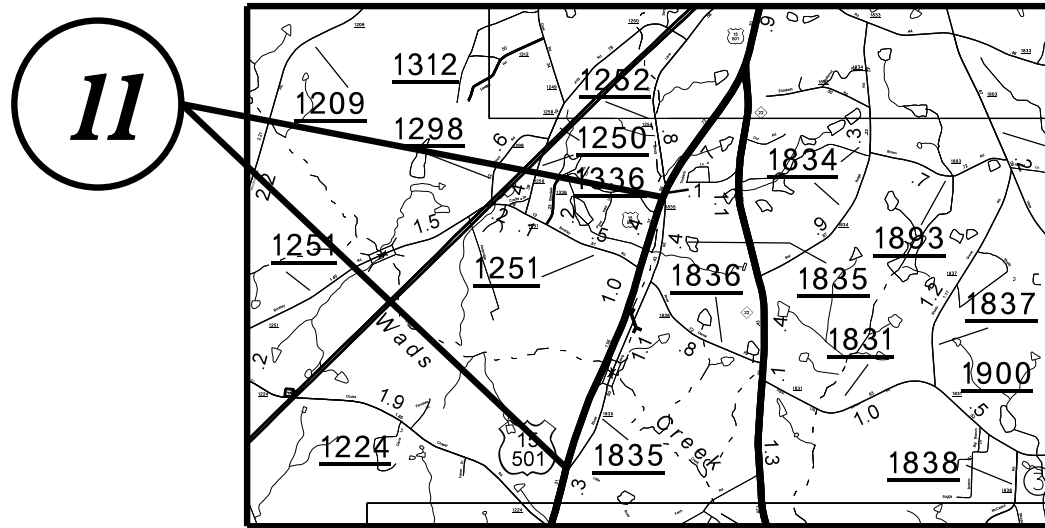
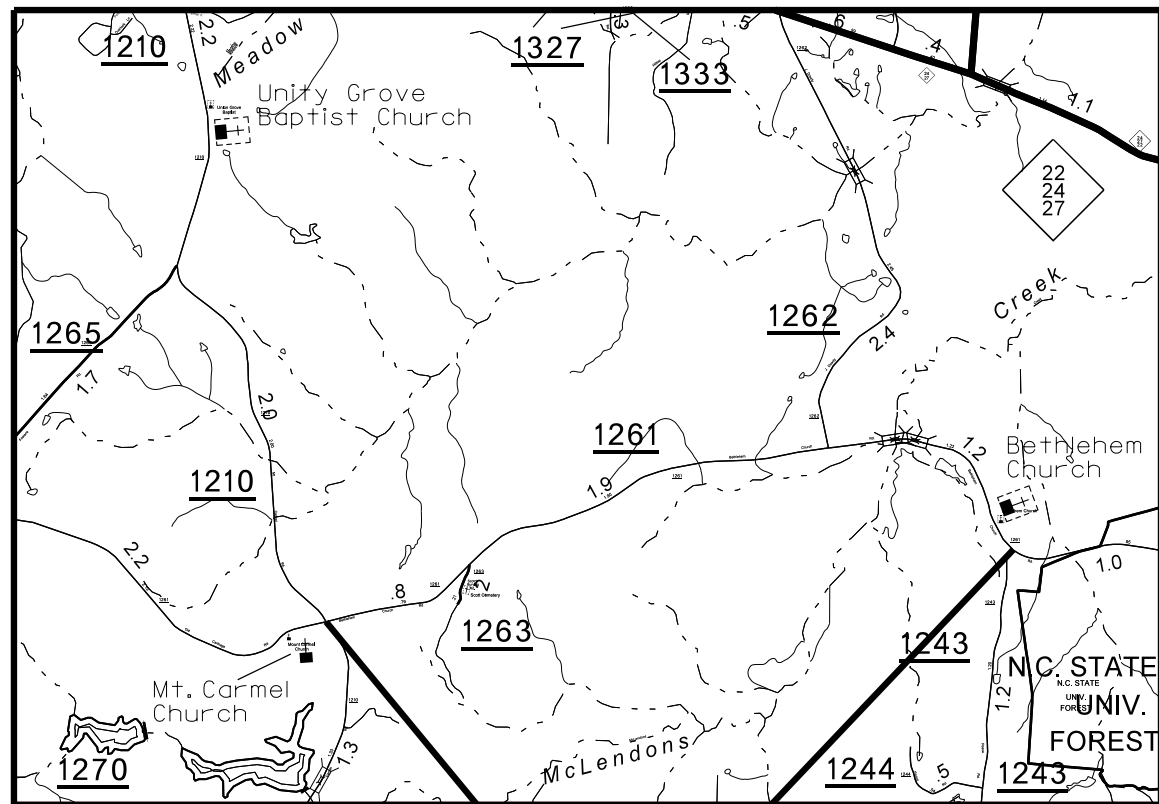


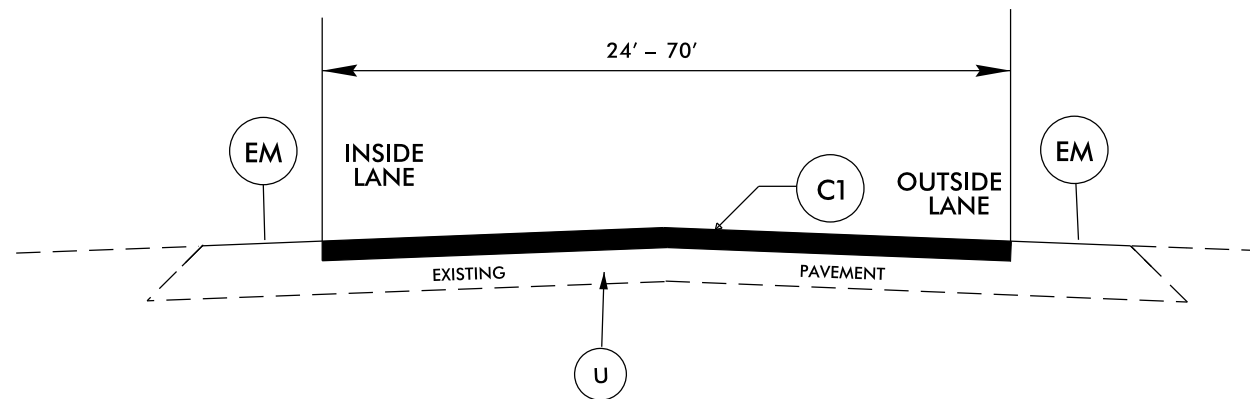
# MOORE COUNTY



27-JAN-2016 14:35  
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 5/28/99

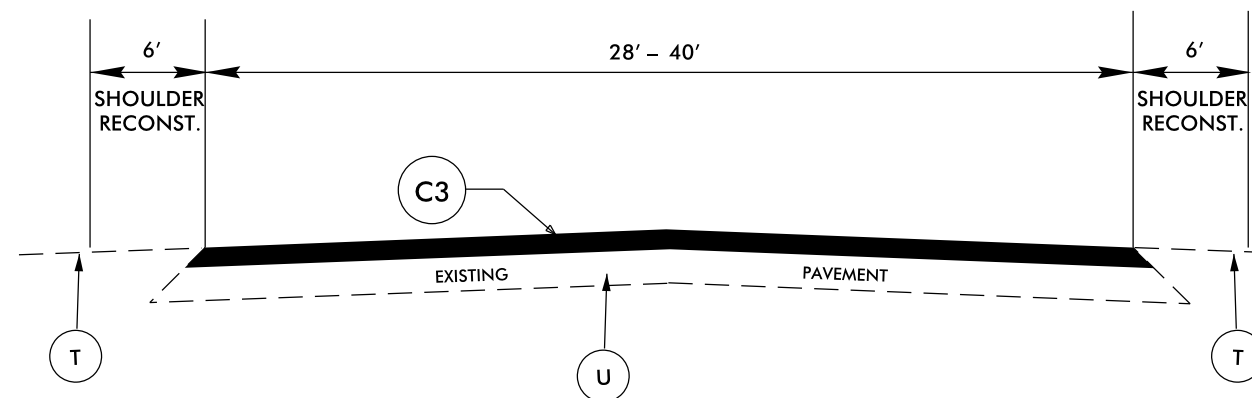
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 10/26/2016



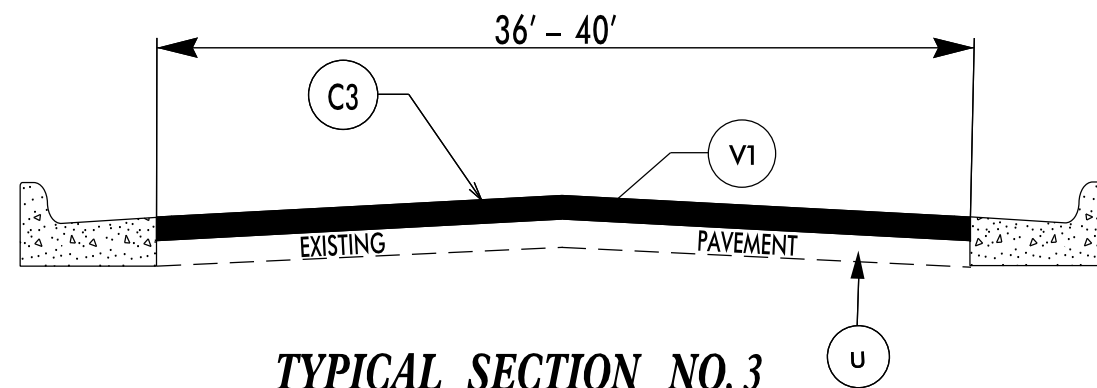


**TYPICAL SECTION NO.1**

NOTE: MICROSURFACING SHALL STOP BEFORE EXISTING RUMBLE STRIP.



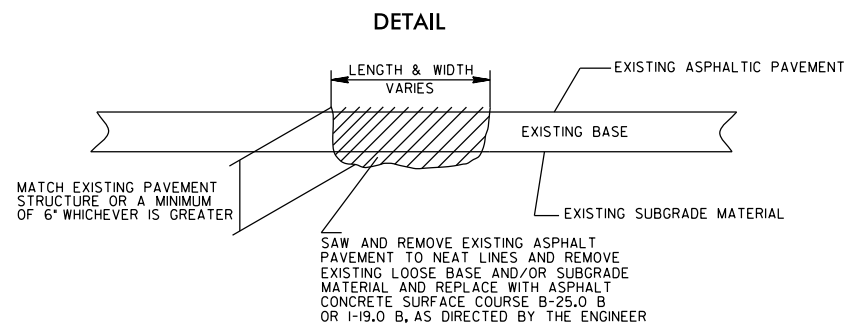
**TYPICAL SECTION NO.2**



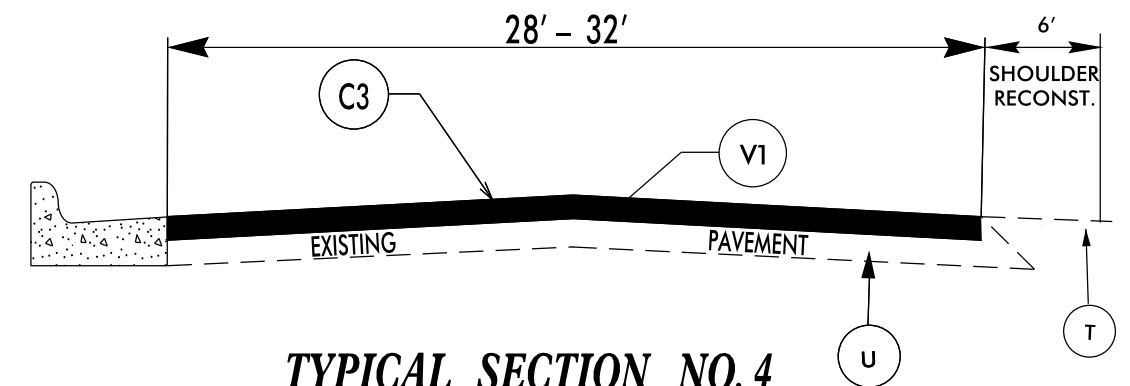
**TYPICAL SECTION NO.3**

NOTE: MILL 1.5" BELOW AND REPLACE FLUSH WITH GUTTER LINE

DETAILS OF PATCHING EXISTING PAVEMENT PRIOR TO RESURFACING



PAVEMENT SCHEDULE	
C1	LATEX MODIFIED MICRO-SURFACING, TYPE III
C3	PROP. APPROX. 1.5" ASPHALT CONC. SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
EM	EXISTING RUMBLE STRIP
T	EARTH MATERIAL
U	EXISTING PAVEMENT
V1	MILLING EXISTING PAVEMENT 1.5"

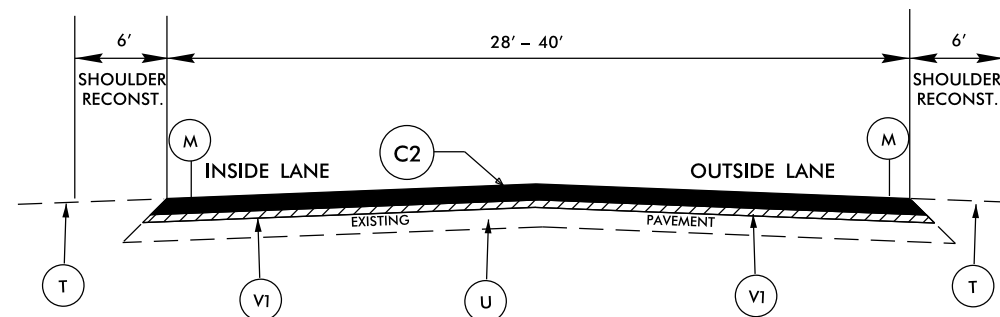


**TYPICAL SECTION NO.4**

NOTE: MILL 1.5" BELOW AND REPLACE FLUSH WITH GUTTER LINE

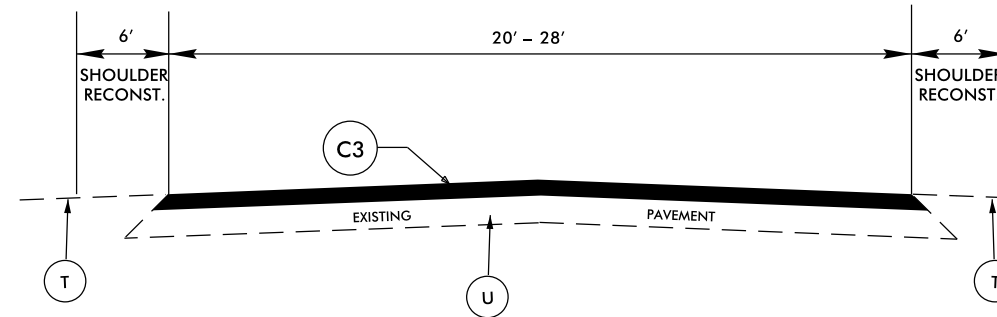
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 11/15/2016

# MOORE COUNTY TYPICALS

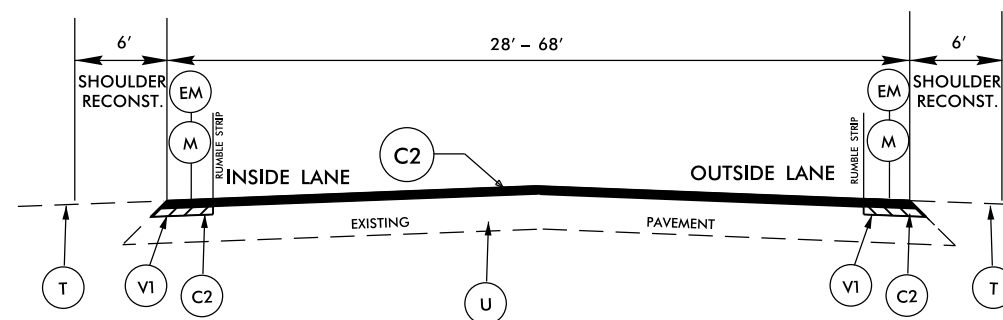


**TYPICAL SECTION NO. 5**

NOTE: US 1 SOUTHBOUND LANES, MILL 1.5" FULL WIDTH AND REPLACE WITH 1.5" OF S9.5C



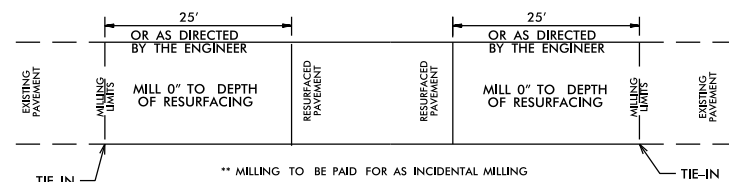
**TYPICAL SECTION NO. 7**



**TYPICAL SECTION NO. 6**

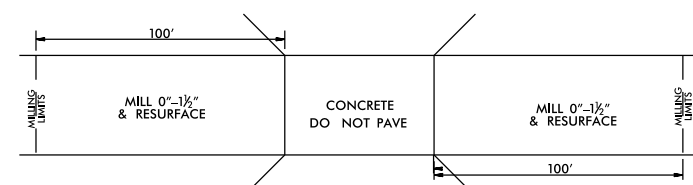
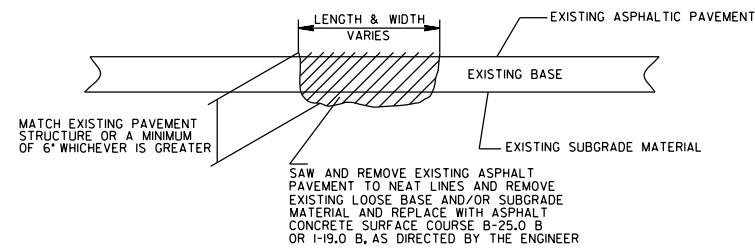
NOTE: MILL RUMBLE STRIP 1.5" AND REPLACE WITH 1.5" OF S9.5C, THEN OVERLAY

PAVEMENT SCHEDULE	
C2	PROP. APPROX. 1.5" ASPHALT CONC. SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
C3	PROP. APPROX. 1.5" ASPHALT CONC. SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
EM	EXISTING RUMBLE STRIP
M	PROPOSED RUMBLE STRIP (RDWY STD. 665.01)
T	EARTH MATERIAL
U	EXISTING PAVEMENT
V1	MILLING EXISTING PAVEMENT 1.5"
V2	MILLING EXISTING PAVEMENT 0" TO 1.5"

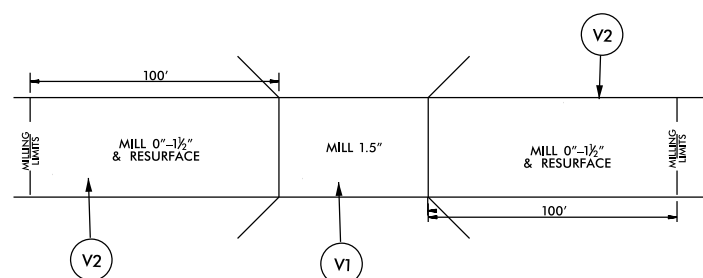


**PAVEMENT TIE-IN DETAIL**

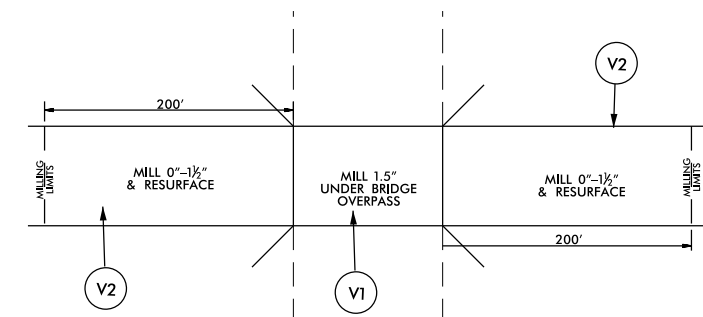
**DETAILS OF PATCHING EXISTING PAVEMENT PRIOR TO RESURFACING**



**BRIDGE DRAWING FOR US 1 SB MAP #5 & #6 (BRIDGE NO. 257)**



**BRIDGE DRAWING FOR MAP #10 (SR 1261 (BRIDGE NO 67 & 68))  
MAP #11 SR 1835 (BRIDGE NO 190)**  
NOTE: BRIDGE NO 67 HAS STEEL PLANK FLOOR WITH 2.5" OF EXISTING ASPHALT

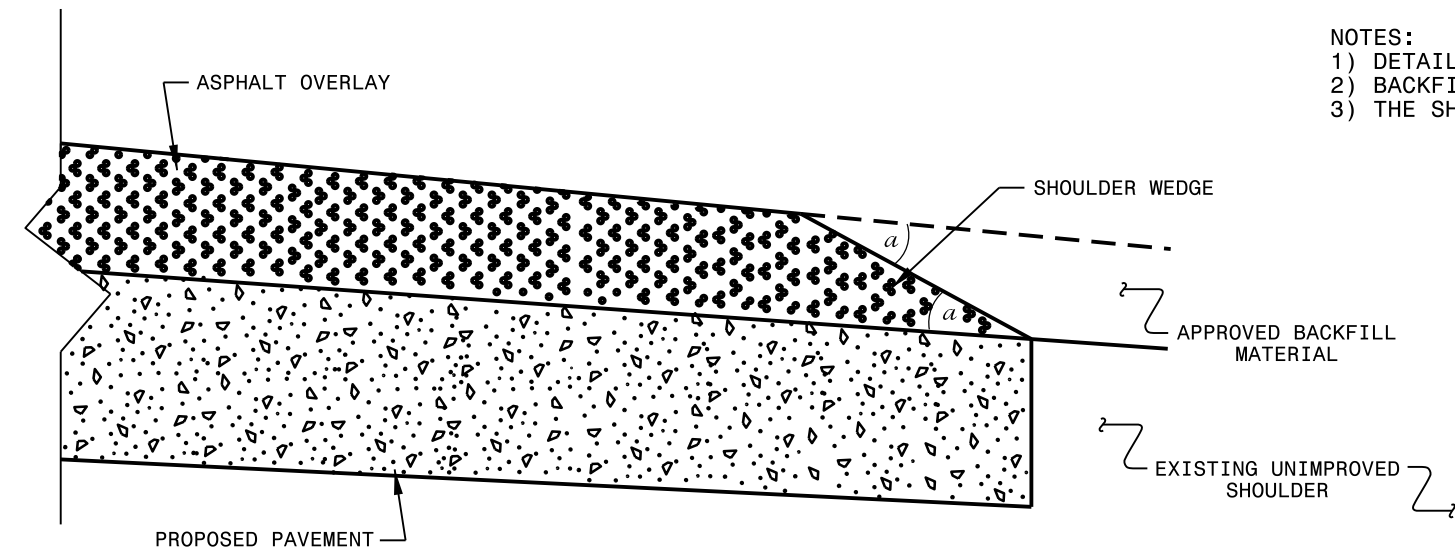


**MAP # 5 OVERPASSES ON US 1 SB**

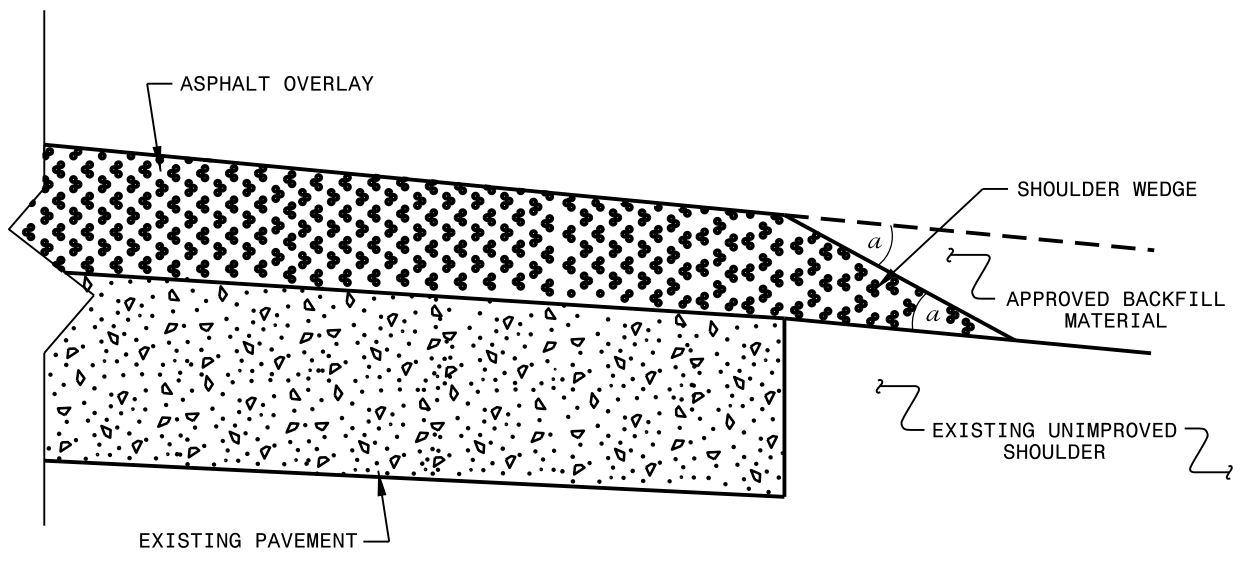
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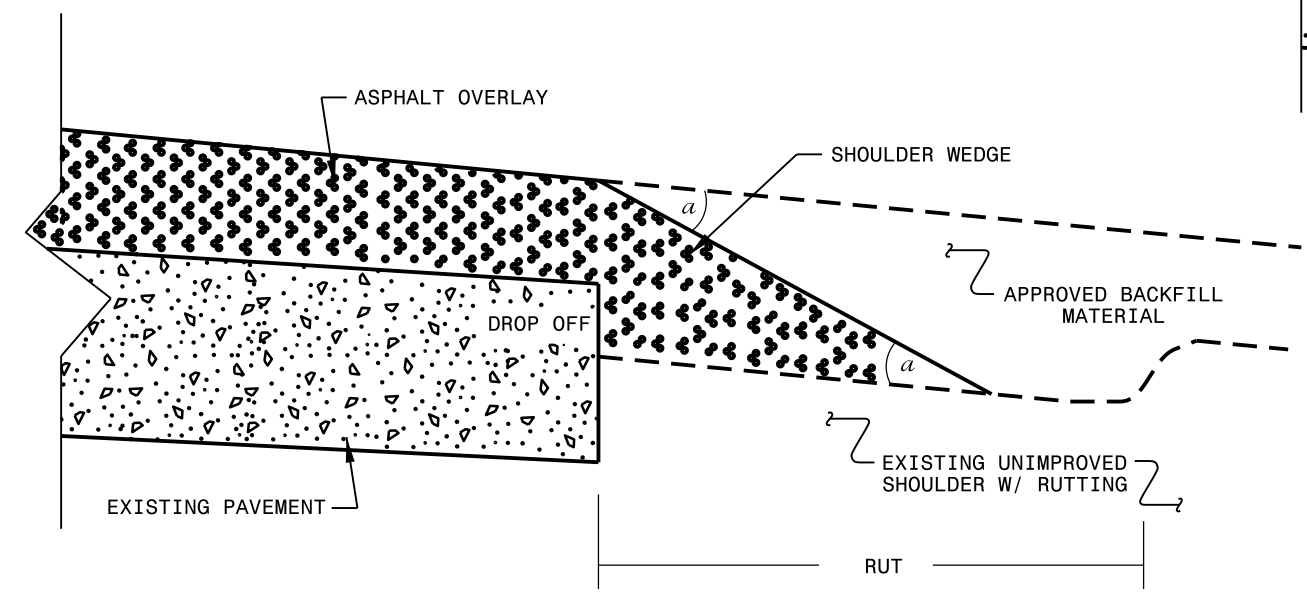
- NOTES:  
 1) DETAIL DOES NOT APPLY TO OGAFD AND ULTRA-THIN BONDED WEARING COURSE.  
 2) BACKFILL SHOULDER WITH APPROVED MATERIAL.  
 3) THE SHOULDER WEDGE DEVICE MAY BE DISENGAGED AT PAVED DRIVEWAYS AND SIDE STREETS.



**SHOULDER WEDGE DETAIL**  
 (Resurfacing Projects w/ Widening or  
 with Existing Paved Shoulder having no dropoffs)



**SHOULDER WEDGE DETAIL**  
 (Resurfacing Projects w/ NO Widening)



**SHOULDER WEDGE DETAIL**  
 (Resurfacing Adjacent to  
 Rutted Shoulder)

- SHOULDER WEDGE ANGLE = 30°

<b>CONTRACT STANDARDS AND DEVELOPMENT UNIT</b>			
Office 919-707-6950		FAX 919-250-4119	
<b>SHOULDER WEDGE DETAILS</b>			
ORIGINAL BY: T.SPELL	DATE: 7-19-11		
MODIFIED BY:	DATE: 10/16/12		
CHECKED BY:	DATE:		
FILE SPEC.: s:usr/details/stand/shoulderwedgedetail.dgn			

S:\JAN-2016\1054  
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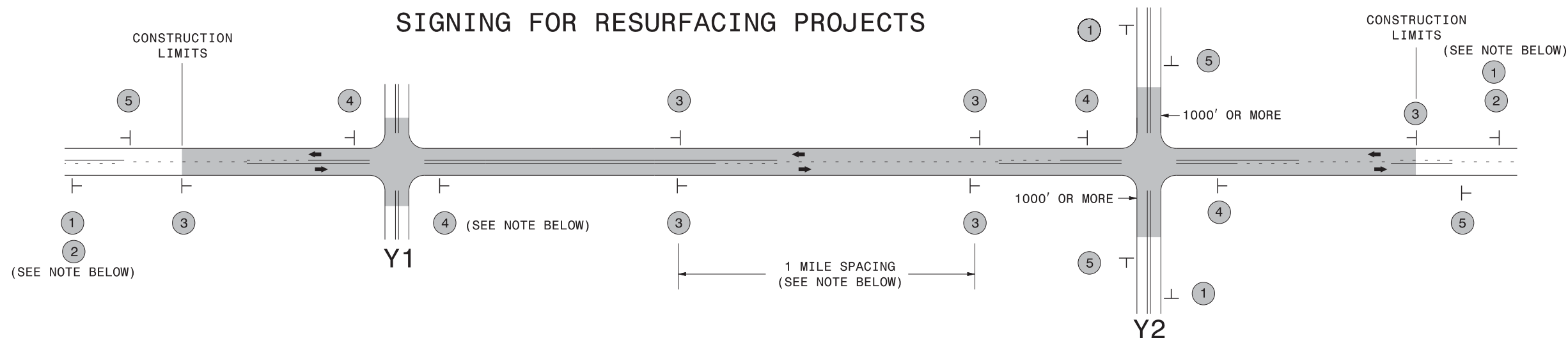
PROJECT NO.	SHEET NO.	TOTAL NO.
2016CPT.08.10.10531, etc	10	

## SUMMARY OF QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP	LANES	LANE TYPE	FINAL SURFACE TESTING REQUIRED	WARM MIX ASPHALT REQUIRED	AGGREGATE SHOULDER BORROW ALLOWED	LENGTH MI	WIDTH FT	BORROW EXCAVATION CY	INCIDENTAL STONE BASE TONS	SHOULDER RECONSTRUCTION SMI	1.5" MILLING SY	0" TO 1.5" MILLING SY	INCIDENTAL MILLING SY	SURFACE COURSE, S9.5B TONS	SURFACE COURSE, S9.5C TONS	ASPHALT BINDER FOR PLANT MIX TONS	PATCHING EXISTING PAVEMENT TONS	MILLED RUMBLE STRIPS LF	LATEX MODIFIED MICRO SURFACING, TYPE III SY	ADJ. OF MAN-HOLES EA	ADJ. OF METER OR VALVE BOXES EA	TEMP. SILT FENCE LF	WATTLE LF	SEED & MULCHING AC
2016CPT.08.10.10531	Lee	1	US 1 SB	FROM CONSTRUCTION JT NEAR SR 1731 (QUAIL RIDGE DR) TO MOORE CO. LINE	1	2	MD	NO	NO	YES	4.109	34						105				1,020		81,961	1		825	130	5.98
		2	US 1 NB	FROM MOORE COUNTY LINE TO CONSTRUCTION JOINT NEAR SR 1731 (QUAIL RIDGE DR)	1	2	MD	NO	NO	YES	4.102	34							130				625		81,821	2		825	130
<b>TOTAL FOR PROJ NO. 2016CPT.08.10.10531</b>											<b>8.211</b>							<b>235</b>				<b>1,645</b>		<b>163,782</b>	<b>3</b>		<b>1,650</b>	<b>260</b>	<b>11.95</b>
2016CPT.08.10.20531	Lee	3	SR 1009 (CARBONTON RD)	FROM NC 42 TO US 1 BUS. (CARTHAGE ST)	2,3,4	2	2WU	NO	NO	YES	2.89	28	425		4.72	12,515		180	4,535		272	3,075			10	20	420	70	3.03
<b>TOTAL FOR PROJ NO. 2016CPT.08.10.20531</b>											<b>2.89</b>		<b>425</b>		<b>4.72</b>	<b>12,515</b>		<b>180</b>	<b>4,535</b>		<b>272</b>	<b>3,075</b>			<b>10</b>	<b>20</b>	<b>420</b>	<b>70</b>	<b>3.03</b>
2016CPT.08.10.10631	Moore	4	US 1 SB	FROM BRIDGE AT CRANE'S CREEK TO END OF GUARDRAIL NORTH OF SR 2005 (ALMA ST) OVERPASS	5	2	M2	NO	NO	YES	1.286	34	190		2.57	26,710		460		2,275	134		13,584				257	40	1.87
		5	US 1 SB	FROM END OF GUARDRAIL NORTH OF SR 2005 (ALMA ST) OVERPASS TO BRIDGE AT LITTLE RIVER (INCLUDE ON AND OFF RAMPS)	6	2	M2	NO	NO	YES	2.028	28-64	300		4.06	3,855	2,270	605		5,480	323	1,525	21,412				410	70	2.95
		6	US 1 SB	FROM BRIDGE AT LITTLE RIVER TO PAVEMENT JOINT PAST SR 1853 (CRYSTAL LAKE ROAD)	6	2	M2	NO	NO	YES	0.409	36-68	60		0.82	255		500		1,075	63	155	4,324				85	20	0.60
<b>TOTAL FOR PROJ NO. 2016CPT.08.10.10631</b>											<b>3.723</b>		<b>550</b>		<b>7.45</b>	<b>30,820</b>	<b>2,270</b>	<b>1,565</b>		<b>8,830</b>	<b>520</b>	<b>1,680</b>	<b>39,320</b>				<b>752</b>	<b>130</b>	<b>5.42</b>
2016CPT.08.10.20631	Moore	7	SR 1470 (NORTH MOORE RD)	FROM SR 1516 (CLARENCE RD TO NC 705)	7	2	2WU	NO	NO	YES	3.739	22	550	235	7.48			120	4,455		267	3,060					750	120	5.44
		8	SR 1661 (STANTON RD)	FROM US 15-501 TO LEE COUNTY LINE	7	2	2WU	NO	NO	YES	1.415	20	210	185	2.83			100	1,550		93	775					285	50	2.06
		9	SR 2015 (MURDOCK C RD)	FROM SR 2014 (MCLAUCHLIN RD) TO HARNETT COUNTY LINE	7	2	2WU	NO	NO	YES	0.73	20	110	80	1.46			115	800		48	760					150	30	1.06
		10	SR 1261 (BETHLEHEM CHURCH RD)	FROM SR 1210 (BEULAH HILL CHURCH RD) TO SR 1243 (PEACE RD)	7	2	2WU	NO	NO	YES	3.905	24	575	155	7.81	270	1,100	600	5,075		305	760					785	120	5.68
		11	SR 1835 (BLUES SIDING RD)	FROM US 15-501 TO LITTLE RIVER FARM RD	7	2	2WU	NO	NO	YES	1.434	20	215	35	2.87	165	445	100	1,530		92	365					290	50	1.04
<b>TOTAL FOR PROJ NO. 2016CPT.08.10.20631</b>											<b>11.223</b>		<b>1,660</b>	<b>690</b>	<b>22.45</b>	<b>435</b>	<b>1,545</b>	<b>1,035</b>	<b>13,410</b>		<b>805</b>	<b>5,720</b>					<b>2,260</b>	<b>370</b>	<b>15.28</b>
<b>GRAND TOTAL</b>											<b>26.047</b>		<b>2,635</b>	<b>690</b>	<b>34.62</b>	<b>43,770</b>	<b>3,815</b>	<b>3,015</b>	<b>17,945</b>	<b>8,830</b>	<b>1,597</b>	<b>12,120</b>	<b>39,320</b>	<b>163,782</b>	<b>13</b>	<b>20</b>	<b>5,082</b>	<b>830</b>	<b>35.68</b>



# SIGNING FOR RESURFACING PROJECTS



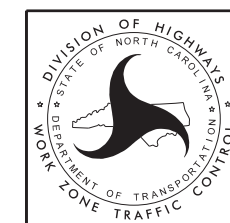
LEGEND	
T	STATIONARY SIGN
←	DIRECTION OF TRAFFIC FLOW

## MAINLINE (-L-) SIGNING

## -Y- LINE SIGNING

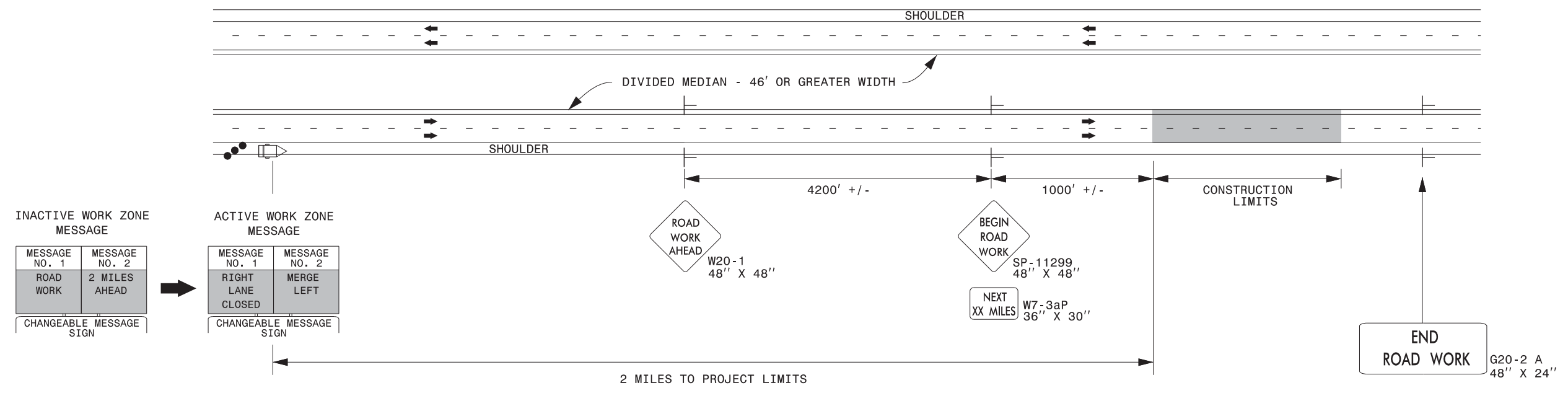
SIGNING NOTES AND PLACEMENT PER DIRECTION	MAINLINE (-L-) SIGNING		-Y- LINE SIGNING	
	1	 W20-1 48" X 48"	PLACE 1000' PRIOR TO BEGINNING OF CONSTRUCTION LIMITS. ONLY USED ON -Y- LINES IF RESURFACING LIMITS EXTEND 1000' ALONG -Y- LINE.	<p>NO REQUIRED STATIONARY SIGNING FOR THE FOLLOWING -Y- LINE CONDITIONS:</p> <ol style="list-style-type: none"> <li>1) LESS THAN 1000' OF RESURFACING ALONG -Y- LINE</li> <li>2) SUBDIVISION ROADS</li> <li>3) DEAD END ROADS</li> </ol> <p>WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, ADVANCE WARNING PORTABLE SIGNS SHALL BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.</p> <div style="display: flex; justify-content: space-around;"> <div style="text-align: center;">             W20-1            48" X 48"         </div> <div style="text-align: center;">             W20-7 A            48" X 48"         </div> </div> <p>PLACED 500' IN ADVANCE OF FLAGGER. PLACED 250' IN ADVANCE OF FLAGGER.</p>
	2	 W7-3aP 24" X 18"	#2 SIGN ONLY USED WHEN RESURFACING LIMITS ARE 2 OR MORE MILES IN LENGTH. ROUND UP TO NEXT WHOLE NUMBER. (NO FRACTIONAL OR DECIMAL NUMBERS)	
	3	 SP 13107 48" X 48"	PLACE INITIALLY AT THE CONSTRUCTION LIMITS AND SPACED 1 MILE APART THEREAFTER. IF NO -Y- LINES EXIST, PLACE 2ND SET 1/2 MILE FROM THE CONSTRUCTION LIMITS AND THEN SPACE 1 MILE THEREAFTER.	
	4	 SP 13106 48" X 48"	THESE ARE FOR -Y- LINES THAT ARE "THROUGH" ROADWAYS. DEAD END AND SUBDIVISION ROADS ARE NOT "THROUGH" ROADWAYS. INSTALL 500' +/- FROM EACH -Y- LINE APPROACH AS SHOWN ABOVE. FOR MULTIPLE -Y- LINES THAT ARE SEPARATED BY 0.25 MILES OR LESS, TREAT AS A SINGLE UNIT AND INSTALL WITHIN 500' OF EACH APPROACH. A MAXIMUM OF 2 SIGN SETS PER MILE. DO NOT INSTALL WHEN -Y- LINES ARE WITHIN 0.5 MILES FROM "END ROAD WORK" SIGN.	
5	 G20-2 A 48" X 24"	PLACE 500' FOLLOWING THE END OF CONSTRUCTION LIMITS.		

3/19/2015  
 C:\Users\rmgarrett\Downloads\Resurfacing\_AdvWarn.2Ln (2).dgn  
 User:rmgarrett

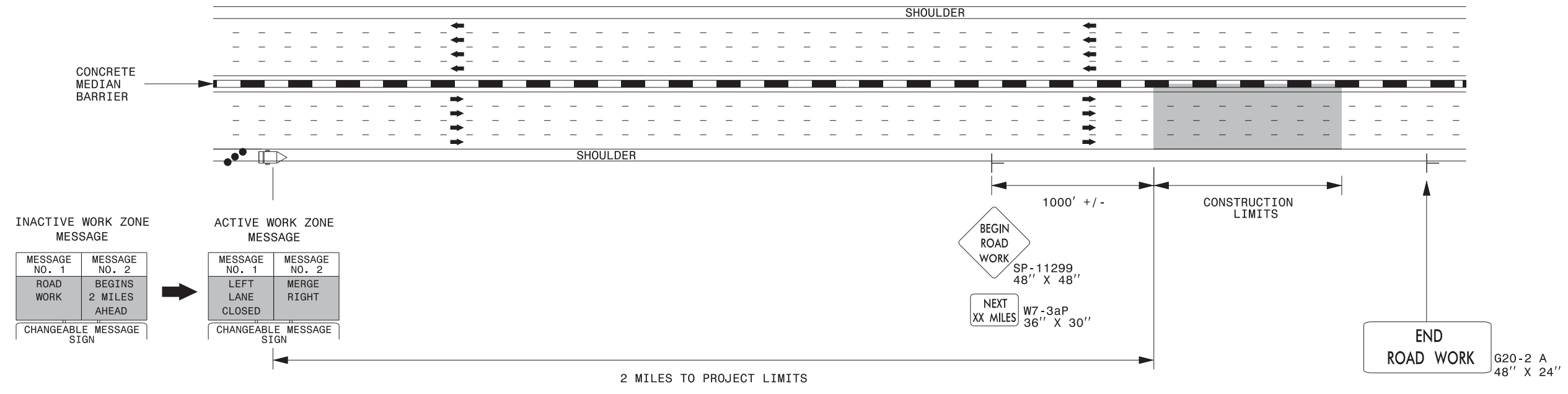


**RESURFACING  
 ADVANCE WARNING SIGNS  
 FOR  
 RURAL AND SUBURBAN  
 2 LANE ROADWAYS**

## DIVIDED MEDIANS WITH WIDTHS 46' OR GREATER



## DIVIDED MEDIANS WITH WIDTHS LESS THAN 46' OR WITH PERMANENT MEDIAN BARRIER

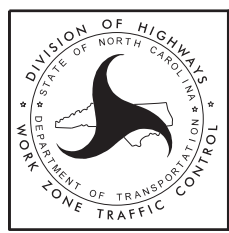


**NOTES:**

- 1) LATERAL CLEARANCE AT ALL SIGN LOCATIONS SHALL BE 6' AS MEASURED FROM THE EDGE OF PAVEMENT.
- 2) MOUNT SIGNS THAT ARE LARGER THAN 10 SQUARE FEET IN AREA ON TWO OR MORE WOOD OR U-CHANNEL SUPPORTS. PERFORATED SQUARE TUBING SUPPORT SYSTEMS MAY SUPPORT LARGER AREAS ON A SINGLE SUPPORT. FOLLOW MANUFACTURER'S RECOMMENDATIONS. THESE SYSTEMS SHALL BE NCHRP 350 COMPLIANT AND NCDOT APPROVED.
- 3) FOR MEDIAN WIDTHS LESS THAN 46' (MEASURED EDGELINE TO EDGELINE) USE THE BOTTOM DRAWING.
- 4) IF STATIONARY GENERAL WARNING SIGNS ARE USED, THEY WILL BE PAID FOR PER SECTION 104 OF THE NCDOT STANDARD SPECIFICATIONS AS EXTRA WORK.
- 5) INSTALL "ROAD WORK AHEAD" (W20-1) ALONG ENTRANCE RAMP 500' PRIOR TO RAMP TERMINAL, AND "END ROAD WORK" (G20-2a) AT THE END OF EXIT RAMP WITHIN THE WORK ZONE.
- 6) IF MILLED AREAS ARE NOT PAVED BACK BY THE END OF THE WORK DAY, PORTABLE SIGNS SHALL BE USED TO WARN DRIVERS OF THE PRESENT CONDITIONS. THESE ARE TO INCLUDE, BUT NOT LIMITED TO "ROUGH ROAD" W8-8, "UNEVEN LANES" W8-11, "GROOVED PAVEMENT" W8-15 w/MOTORCYCLE PLAQUE MOUNTED BELOW. THESE ARE TO BE DOUBLE INDICATED ON MULTI-LANE ROADWAYS WITH SPEED LIMITS 45 MPH AND GREATER AND WITH DIVIDED MEDIANS OF 46' OR GREATER. THESE PORTABLE SIGNS ARE INCIDENTAL TO THE OTHER ITEMS OF WORK INCLUDED IN THE TEMPORARY TRAFFIC CONTROL (LUMP SUM) PAY ITEM.

**LEGEND**

- CHANGEABLE MESSAGE SIGN (CMS)
- STATIONARY SIGN
- DIRECTION OF TRAFFIC FLOW
- TRAFFIC DRUM



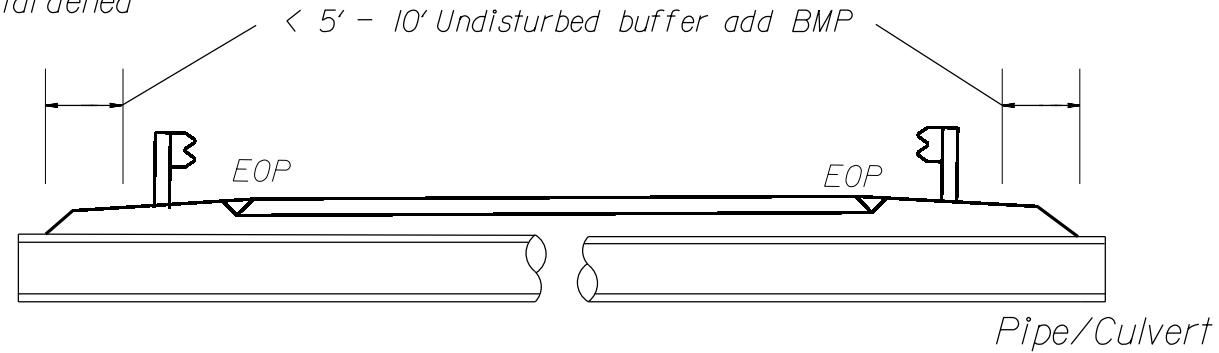
**RESURFACING ADVANCE  
WARNING SIGNS FOR  
HIGH SPEED FACILITIES  
≥ 60 MPH**

3/23/2015 C:\Users\rmgarrrett\Downloads\Resurfacing\_AdvWarn\_HSpd.dgn User:rmgarrrett

NOTES: Less than 5' - 10' undisturbed buffer from ROW, ditchline, water feature, or drainage inlet, add BMP.

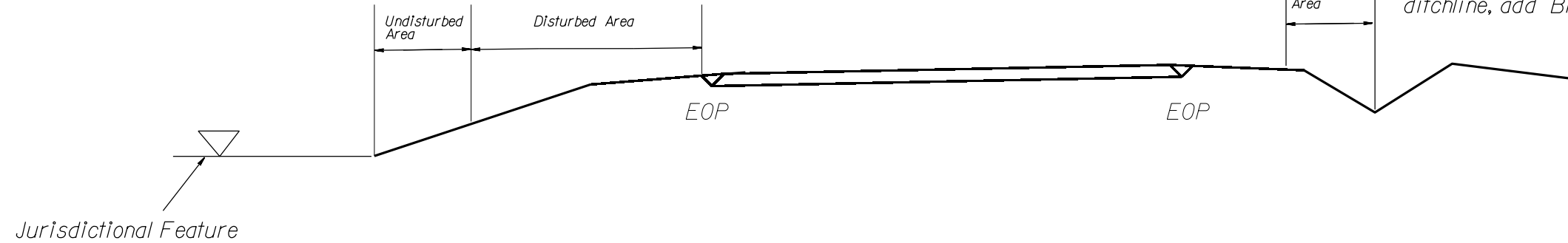
# EROSION CONTROL DETAIL

BMP Options: Wattle, Silt Fence or Hardened Aggregate.



< 5' - 10' Undisturbed buffer from jurisdictional feature add BMP

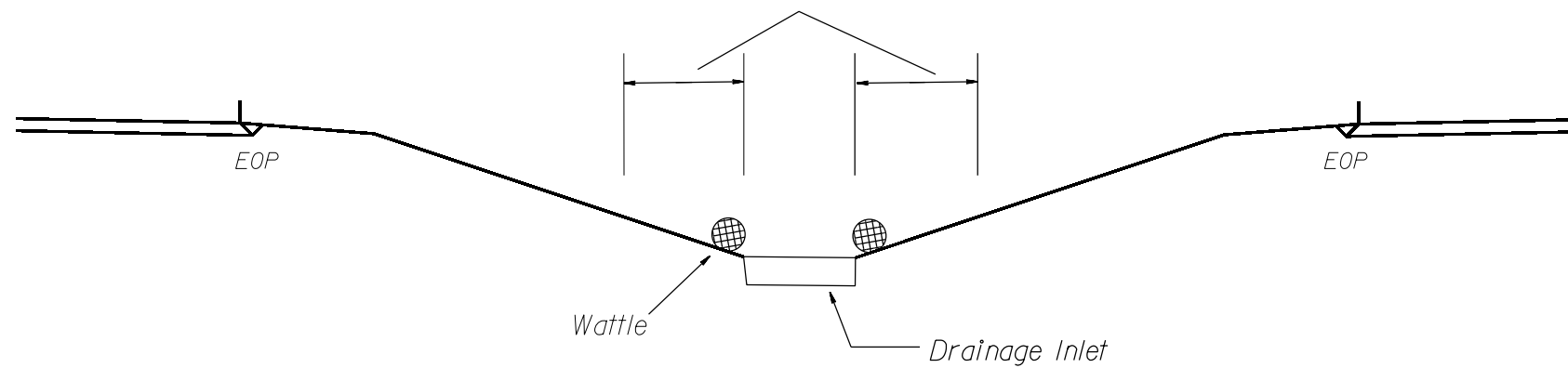
< 5' - 10' Undisturbed buffer from ditchline, add BMP



Use BMP's if shoulders and/or frontslopes and/or ditchline and/or backslopes are disturbed

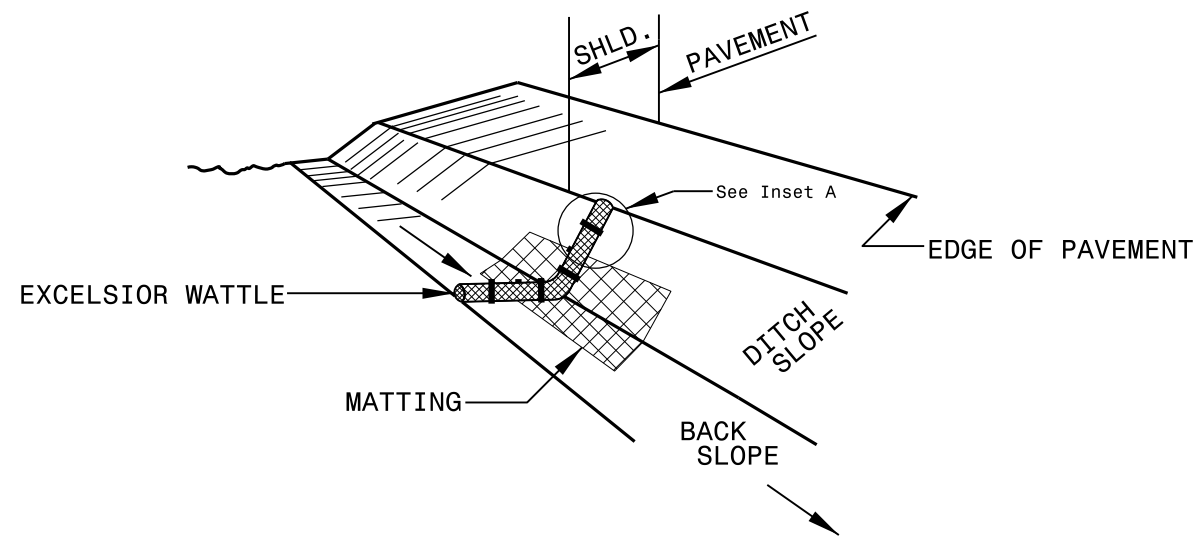


< 5' - 10' Undisturbed buffer from inlet, add wattle

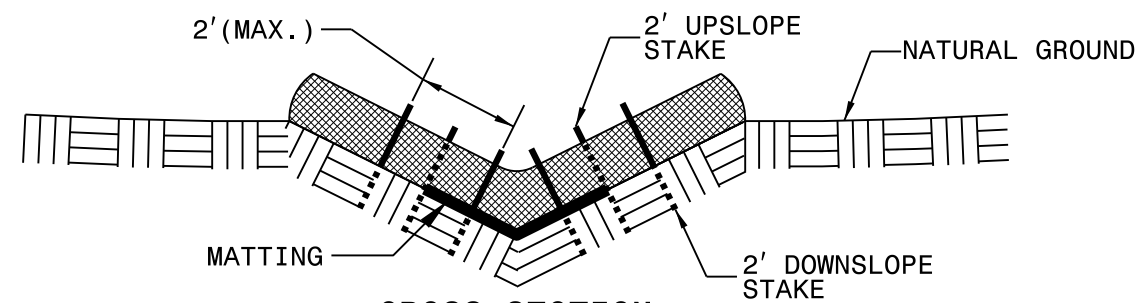


NOT TO SCALE

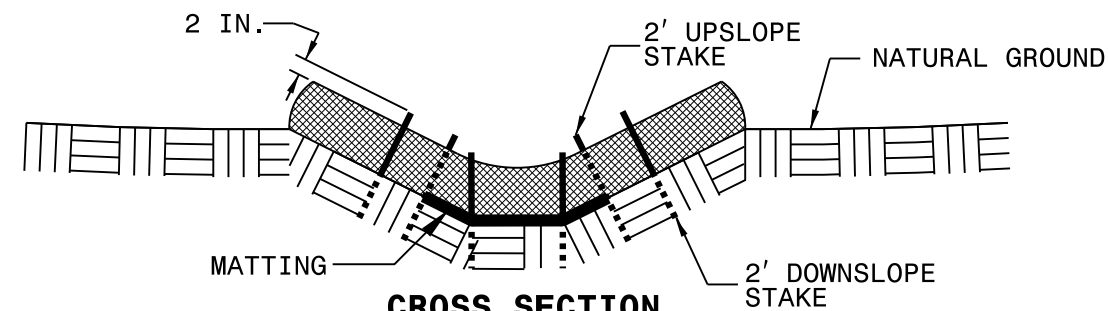
# WATTLE DETAIL



**ISOMETRIC VIEW**



**CROSS SECTION  
VEE DITCH**



**CROSS SECTION  
TRAPEZOIDAL DITCH**

**NOTES:**

USE MINIMUM 12 IN. DIAMETER EXCELSIOR WATTLE.

USE 2 FT. WOODEN STAKES WITH A 2 IN. BY 2 IN. NOMINAL CROSS SECTION.

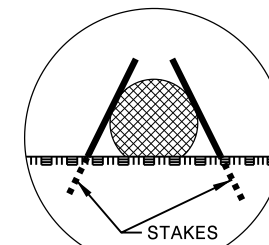
ONLY INSTALL WATTLE(S) TO A HEIGHT IN DITCH SO FLOW WILL NOT WASH AROUND WATTLE AND SCOUR DITCH SLOPES AND AS DIRECTED.

INSTALL A MINIMUM OF 2 UPSLOPE STAKES AND 4 DOWNSLOPE STAKES AT AN ANGLE TO WEDGE WATTLE TO BOTTOM OF DITCH.

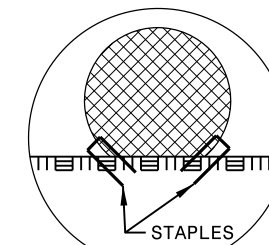
PROVIDE STAPLES MADE OF 0.125 IN. DIAMETER STEEL WIRE FORMED INTO A U SHAPE NOT LESS THAN 12" IN LENGTH.

INSTALL STAPLES APPROXIMATELY EVERY 1 LINEAR FOOT ON BOTH SIDES OF WATTLE AND AT EACH END TO SECURE IT TO THE SOIL.

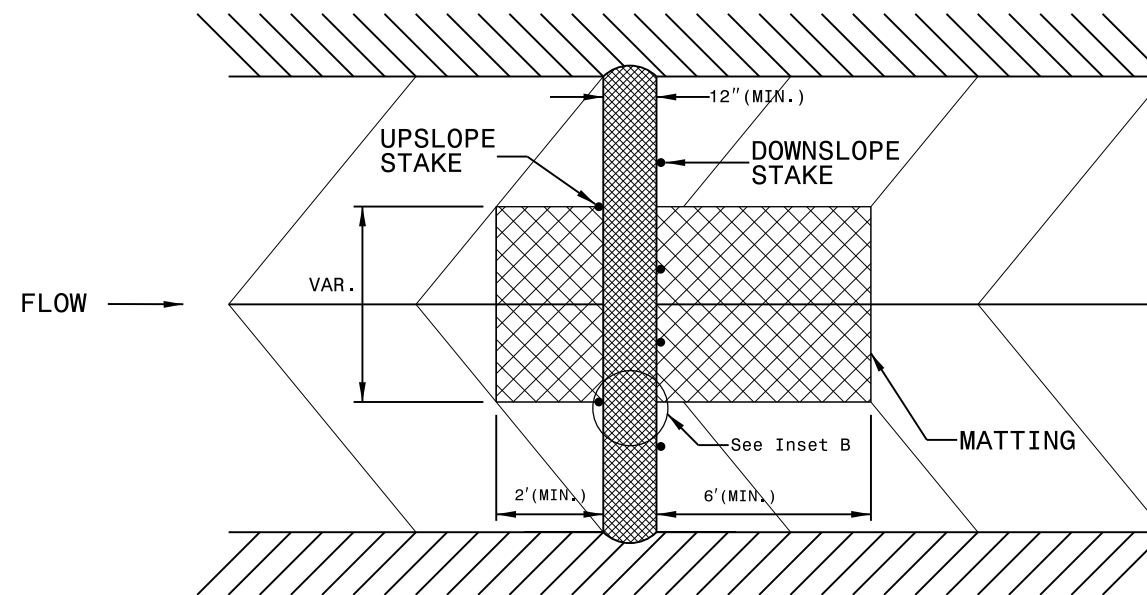
INSTALL MATTING IN ACCORDANCE WITH SECTION 1631 OF THE STANDARD SPECIFICATIONS.



**INSET A**



**INSET B**



**TOP VIEW**