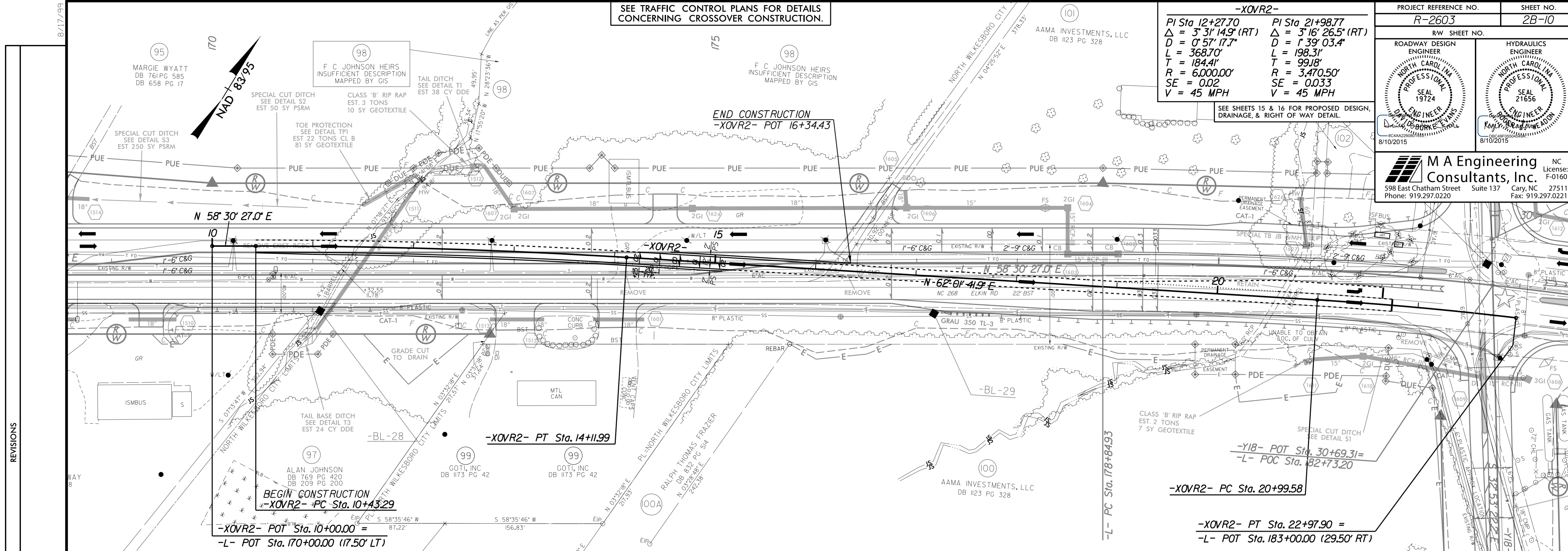


SEE TRAFFIC CONTROL PLANS FOR DETAILS CONCERNING CROSSOVER CONSTRUCTION.

-XOVR2-	
PI Sta 12+27.70	PI Sta 21+98.77
$\Delta = 3' 31" 14.9" (RT)$	$\Delta = 3' 16" 26.5" (RT)$
$D = 0' 57" 17.7"$	$D = 1' 39" 03.4"$
$L = 368.70'$	$L = 198.31'$
$T = 184.41'$	$T = 99.18'$
$R = 6,000.00'$	$R = 3,470.50'$
$SE = 0.02$	$SE = 0.033$
$V = 45 MPH$	$V = 45 MPH$

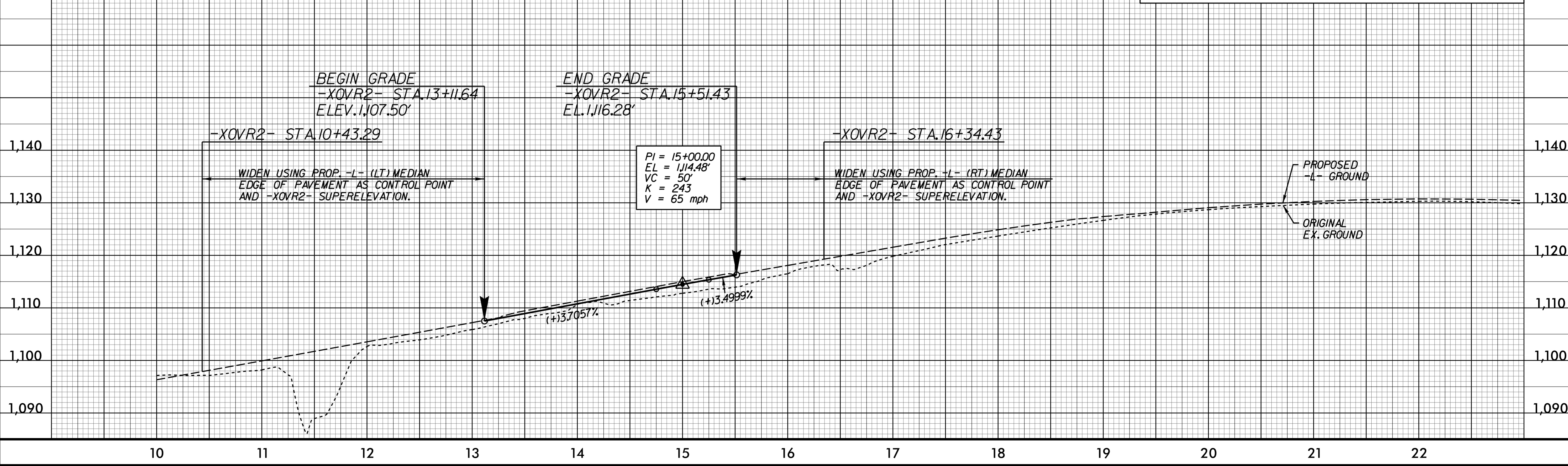
PROJECT REFERENCE NO. R-2603	SHEET NO. 2B-10
RW SHEET NO.	
ROADWAY DESIGN ENGINEER DR. J. L. BURKE, P.E. SEAL 19724 8/10/2015	HYDRAULICS ENGINEER ROY L. BARNETT, P.E. SEAL 21656 8/10/2015

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-XOVR2-

NOTE:
THE GRADE AS SHOWN TIES TO THE PROPOSED -L- CONSTRUCTION 1.5' BELOW FINAL GRADE. THE GRADE MAY BE ADJUSTED TO SUIT FIELD CONDITIONS AT THE TIME OF CROSSOVER CONSTRUCTION AT THE DIRECTION OF THE ENGINEER.



REVISIONS

7/2/2015 10:52:34 AM pr-o:\p2603.Rdy-sh02B-10.dgn