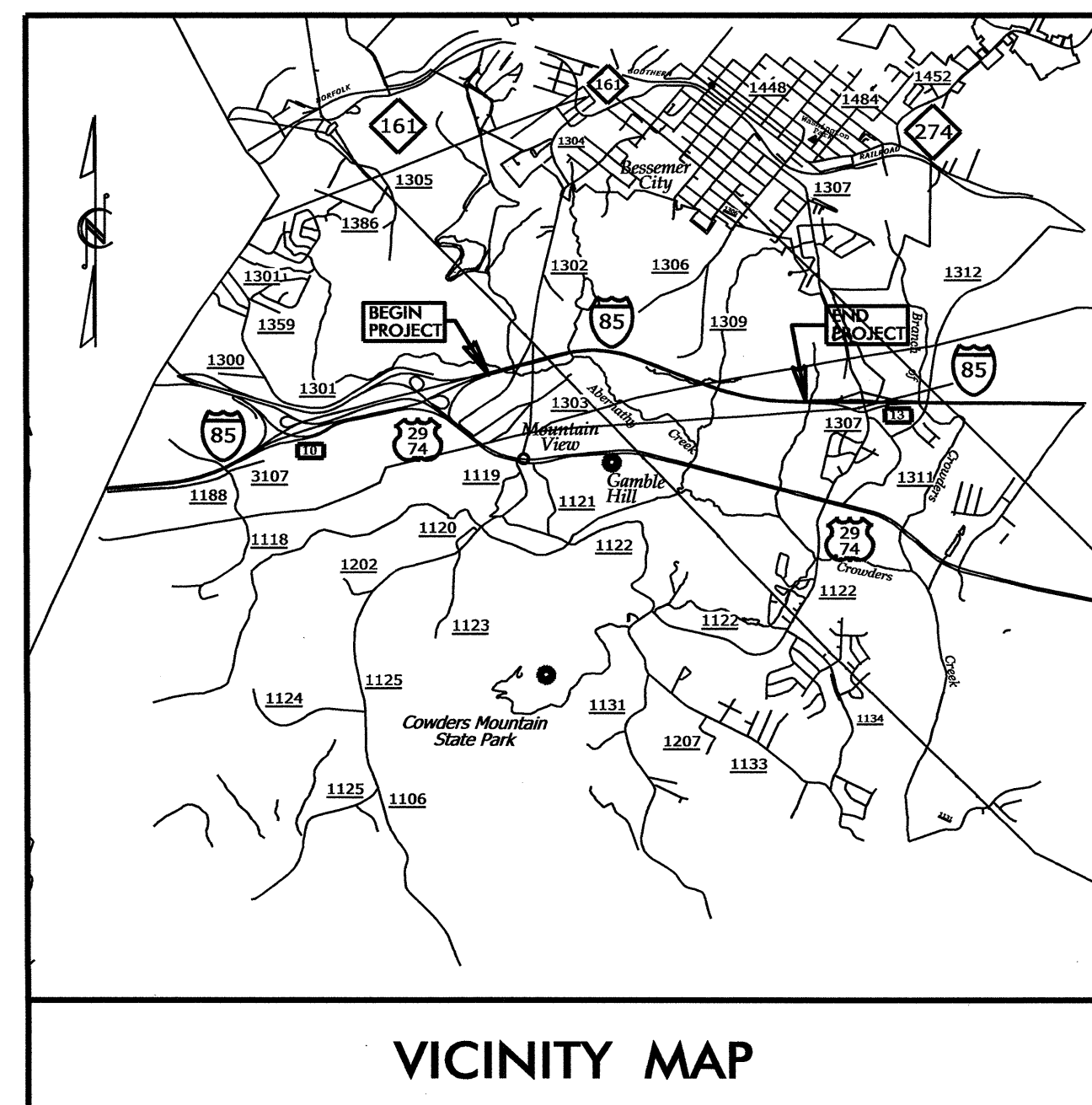
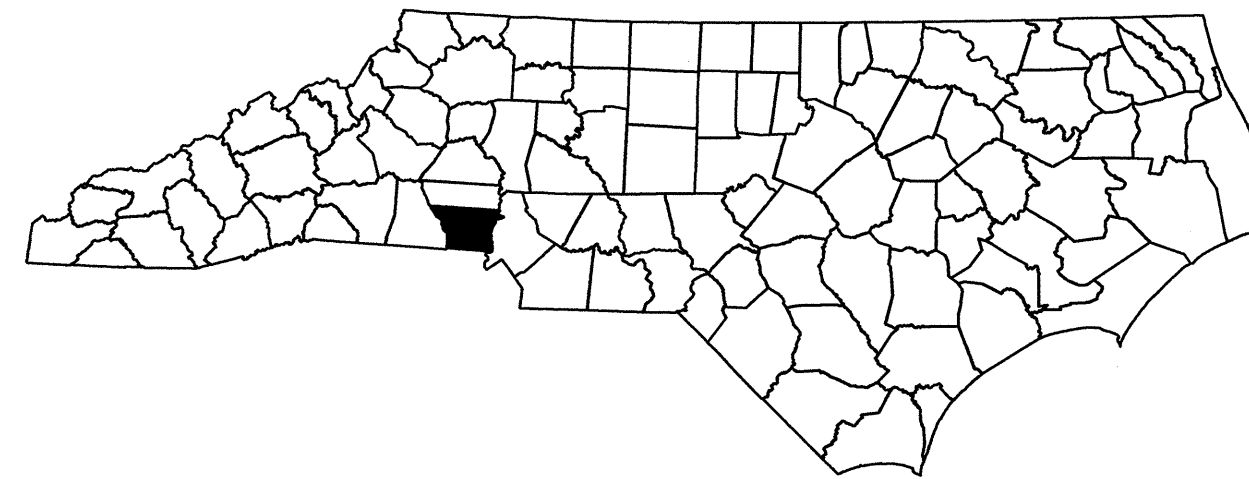


STATE OF NORTH CAROLINA
DIVISION OF HIGHWAYS

TRANSPORTATION MANAGEMENT PLAN

GASTON COUNTY



VICINITY MAP

LOCATION: NEW I-85 NBL WEIGH STATION FROM SR 1302 (CROWDERS MOUNTAIN RD) TO SR 1307 (EDGEWOOD RD).

INDEX OF SHEETS

SHEET NO.	TITLE
TMP-1	TITLE SHEET, VICINITY MAP AND INDEX OF SHEETS
TMP-1A	LIST OF APPLICABLE ROADWAY STANDARD DRAWINGS, AND LEGEND
TMP-1B & TMP-1C	TRANSPORTATION OPERATIONS PLAN: (MANAGEMENT STRATEGIES, GENERAL NOTES AND LOCAL NOTES)
TMP-2	"WORK ZONE" SPEED LIMIT REDUCTION
TMP-2A	\$250 SPEEDING FINE SIGNAGE
TMP-3	TEMPORARY TRAFFIC CONTROL PHASING
TMP-4	TEMPORARY TRAFFIC CONTROL DETAIL
TMP-5	TEMPORARY TRAFFIC CONTROL DETAIL

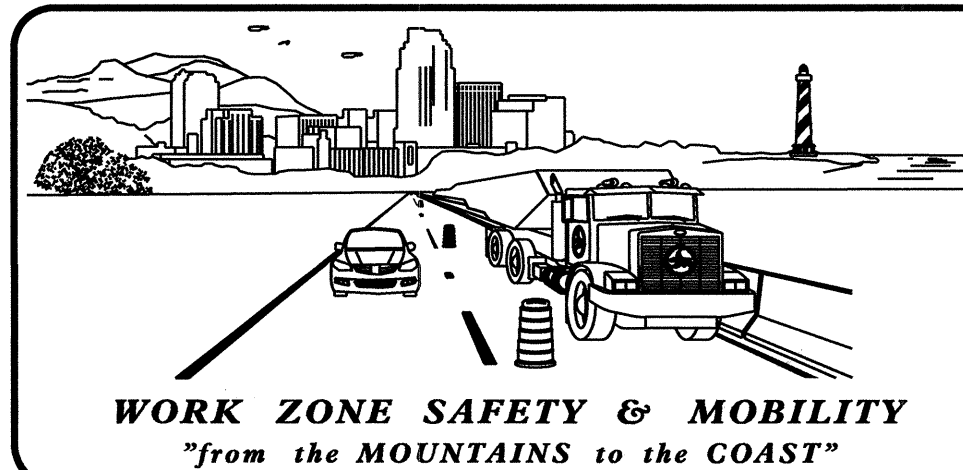
SHEET NO.

TMP-1

I-4928

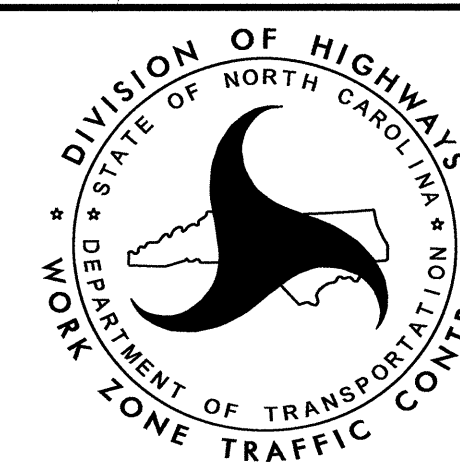
TIP PROJECT:

I7-SEP-2013 10:56
 \\dot\dfsroot\01\Proj\TIPProjects\I4928\Traffic\TrafficControl\TCP\I-4928_TC_TMP_01.dgn
 sngreen AT 1E265817



N.C.D.O.T. WORK ZONE TRAFFIC CONTROL
 1561 MAIL SERVICE CENTER (MSC) RALEIGH, NC 27699-1561
 750 N. GREENFIELD PARKWAY, GARNER, NC 27529 (DELIVERY)
 PHONE: (919) 773-2800 FAX: (919) 771-2745

J. S. BOURNE, P.E. STATE TRAFFIC MANAGEMENT ENGINEER
 G. L. GETTIER, P.E. TRAFFIC CONTROL PROJECT ENGINEER
 J. W. GILSTRAP TRAFFIC CONTROL PROJECT DESIGN ENGINEER
 S. N. GREEN TRAFFIC CONTROL DESIGN ENGINEER



APPROVED: _____
 DATE: _____

SEAL

ROADWAY STANDARD DRAWINGS

THE FOLLOWING ROADWAY STANDARDS AS SHOWN IN "ROADWAY STANDARD DRAWINGS" - PROJECT SERVICES UNIT - N.C. DEPARTMENT OF TRANSPORTATION - RALEIGH, N.C., DATED JANUARY 2012 ARE APPLICABLE TO THIS PROJECT AND BY REFERENCE HEREBY ARE CONSIDERED A PART OF THESE PLANS:

<u>STD. NO.</u>	<u>TITLE</u>
1101.01	WORK ZONE ADVANCE WARNING SIGNS
1101.02	TEMPORARY LANE CLOSURES
1101.04	TEMPORARY SHOULDER CLOSURES
1101.05	WORK ZONE VEHICLE ACCESSES
1101.11	TRAFFIC CONTROL DESIGN TABLES
1110.01	STATIONARY WORK ZONE SIGNS
1110.02	PORTABLE WORK ZONE SIGNS
1115.01	FLASHING ARROW BOARDS
1130.01	DRUMS
1135.01	CONES
1145.01	BARRICADES
1160.01	TEMPORARY CRASH CUSHION
1165.01	WORK VEHICLE LIGHTING SYSTEMS AND TMA DELINEATION
1170.01	PORTABLE CONCRETE BARRIER
1205.01	PAVEMENT MARKINGS - LINE TYPES AND OFFSETS
1205.02	PAVEMENT MARKINGS - TWO LANE AND MULTILANE ROADWAYS
1205.03	PAVEMENT MARKINGS - EXITS AND ENTRANCE RAMP
1205.06	PAVEMENT MARKINGS - LANE DROPS
1205.13	PAVEMENT MARKINGS - LANE REDUCTIONS
1250.01	RAISED PAVEMENT MARKERS - INSTALLATION SPACING
1251.01	RAISED PAVEMENT MARKERS - (PERMANENT AND TEMPORARY)
1261.01	GUARDRAIL AND BARRIER DELINEATORS - INSTALLATION SPACING
1261.02	GUARDRAIL AND BARRIER DELINEATORS - TYPES AND MOUNTING

LEGEND

GENERAL

- DIRECTION OF TRAFFIC FLOW
- EXIST. PVMT.
- NORTH ARROW
- PROPOSED PVMT.

WORK AREA

REMOVAL

USER DEFINED (IF NEEDED)

USER DEFINED (IF NEEDED)

PAVEMENT MARKINGS

- EXISTING LINES
- TEMPORARY LINES

TRAFFIC CONTROL DEVICES

- BARRICADE (TYPE III)
- CONE
- DRUM SKINNY DRUM TUBULAR MARKER
- TEMPORARY CRASH CUSHION
- FLASHING ARROW BOARD
- FLAGGER
- LAW ENFORCEMENT
- TRUCK MOUNTED ATTENUATOR (TMA)
- CHANGEABLE MESSAGE SIGN

TEMPORARY SIGNING

- PORTABLE SIGN
- STATIONARY SIGN
- STATIONARY OR PORTABLE SIGN

PAVEMENT MARKERS

- CRYSTAL/CRYSTAL
- CRYSTAL/RED
- YELLOW/YELLOW

PAVEMENT MARKING SYMBOLS

- PAVEMENT MARKING SYMBOLS

TEMPORARY PAVEMENT MARKING

- 6" PAINT
- P6 WHITE EDGELINE
 - P7 YELLOW EDGELINE
 - PK WHITE SKIP LINE
 - PL WHITE LANE LINE
 - P11 WHITE MINI SKIP LINE

- TEMPORARY MARKERS
- MI CRYSTAL & RED
 - MH YELLOW & YELLOW

APPROVED: _____ DATE: _____ 		ROADWAY STANDARD DRAWINGS & LEGEND
---------------------------------	--	---

MANAGEMENT STRATEGIES

CONSTRUCTION

CONSTRUCT NEW I-85 NBL WEIGH STATION.

TMP DESIGN PARAMETERS

LANE CLOSURES WILL BE UTILIZED FOR LANE WIDTH REDUCTION, TRAFFIC SHIFTS AND INSTALLATION AND REMOVAL OF PORTABLE CONCRETE BARRIER (PCB).

SPEED LIMIT REDUCTION WILL BE REQUIRED DURING THE CONSTRUCTION PERIOD.

GENERAL NOTES

CHANGES MAY BE REQUIRED WHEN PHYSICAL DIMENSIONS IN THE DETAIL DRAWINGS, STANDARD DETAILS AND ROADWAY DETAILS ARE NOT ATTAINABLE TO MEET FIELD CONDITIONS, OR RESULT IN DUPLICATE, OR UNDESIRED OVERLAPPING OF DEVICES. MODIFICATION MAY INCLUDE: MOVING, SUPPLEMENTING, COVERING OR REMOVAL OF DEVICES, AS DIRECTED BY THE ENGINEER.

THE FOLLOWING GENERAL NOTES APPLY AT ALL TIMES FOR THE DURATION OF THE CONSTRUCTION PROJECT EXCEPT WHEN OTHERWISE NOTED IN THE PLAN OR DIRECTED BY THE ENGINEER.

TIME RESTRICTIONS

A) DO NOT CLOSE OR NARROW TRAVEL LANES AS FOLLOWS:

ROAD NAME	DAY AND TIME RESTRICTIONS
1. ONE LANE ON I-85 NB	MONDAY THRU SUNDAY 6:00AM-8:00PM
2. TWO LANES ON I-85 NB	MONDAY THRU SUNDAY 6:00AM-10:00PM

B) DO NOT CLOSE OR NARROW TRAVEL LANES DURING HOLIDAYS AND SPECIAL EVENTS AS FOLLOWS:

ROAD NAME

1. I-85 NB

HOLIDAY

- FOR ANY UNEXPECTED OCCURRENCE THAT CREATE UNUSUALLY HIGH TRAFFIC VOLUMES, AS DIRECTED BY THE ENGINEER.
- FOR NEW YEAR'S, BETWEEN THE HOURS OF 6:00 A.M. DECEMBER 31st TO 8:00 P.M. JANUARY 2ND. IF NEW YEAR'S DAY IS ON A FRIDAY, SATURDAY, SUNDAY, OR MONDAY THEN UNTIL 8:00 P.M. THE FOLLOWING TUESDAY.
- FOR EASTER, BETWEEN THE HOURS OF 6:00 A.M. THURSDAY AND 8:00 P.M. MONDAY.
- FOR MEMORIAL DAY, BETWEEN THE HOURS OF 6:00 A.M. FRIDAY TO 8:00 P.M. TUESDAY.
- FOR INDEPENDENCE DAY, BETWEEN THE HOURS OF 6:00 A.M. THE DAY BEFORE INDEPENDENCE DAY AND 8:00 P.M. THE DAY AFTER INDEPENDENCE DAY.

IF INDEPENDENCE DAY IS ON A FRIDAY, SATURDAY, SUNDAY, OR MONDAY THEN BETWEEN THE HOURS OF 8:00 P.M. THE THURSDAY BEFORE INDEPENDENCE DAY AND 8:00 P.M. THE TUESDAY AFTER INDEPENDENCE DAY.
- FOR LABOR DAY, BETWEEN THE HOURS OF 6:00 A.M. FRIDAY AND 8:00 P.M. TUESDAY.
- FOR THANKSGIVING DAY, BETWEEN THE HOURS OF 6:00 A.M. TUESDAY TO 8:00 P.M. MONDAY.
- FOR CHRISTMAS, BETWEEN THE HOURS OF 6:00 A.M. THE FRIDAY BEFORE THE WEEK OF CHRISTMAS DAY AND 8:00 P.M. THE FOLLOWING TUESDAY AFTER THE WEEK OF CHRISTMAS.
- FOR ANY NASCAR CAR RACING EVENT AT THE CHARLOTTE MOTOR SPEEDWAY, BETWEEN THE HOURS OF 6:00 A.M. THE WEDNESDAY BEFORE THE FIRST RACE AND 8:00 P.M. THE MONDAY AFTER THE LAST RACE.

C) DO NOT CLOSE ROADS AS FOLLOWS:

ROAD NAME	DAY AND TIME RESTRICTIONS
1. I-85 NB	NEVER

D) DO NOT STOP TRAFFIC AS FOLLOWS:

ROAD NAME	DAY AND TIME RESTRICTIONS	DURATION AND OPERATION
1. I-85 NB AND ALL RAMP & LOOPS	MONDAY THRU SUNDAY, 5:00AM-12:00 MIDNIGHT	15 MINUTES FOR OVERHEAD SIGN AND STRUCTURE INSTALLATION

E) DO NOT CONDUCT ANY HAULING OPERATIONS AGAINST THE FLOW OF TRAFFIC OF AN OPEN TRAVELWAY UNLESS THE HAULING OPERATION IS PROTECTED BY BARRIER OR GUARDRAIL OR AS DIRECTED BY THE ENGINEER.

LANE AND SHOULDER CLOSURE REQUIREMENTS

- F) REMOVE LANE CLOSURE DEVICES FROM THE LANE WHEN WORK IS NOT BEING PERFORMED BEHIND THE LANE CLOSURE OR WHEN A LANE CLOSURE IS NO LONGER NEEDED, OR AS DIRECTED BY THE ENGINEER.
- G) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN 15 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN SHOULDER USING ROADWAY STANDARD DRAWING NO. 1101.04 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL OR A LANE CLOSURE IS INSTALLED.
- H) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING ON THE SHOULDER ADJACENT TO AN UNDIVIDED FACILITY AND WITHIN 5 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN TRAVEL LANE USING ROADWAY STANDARD DRAWING NO. 1101.02 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.

WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING ON THE SHOULDER ADJACENT TO A DIVIDED FACILITY AND WITHIN 10 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN TRAVEL LANE USING ROADWAY STANDARD DRAWING NO. 1101.02 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.

I) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN A LANE OF TRAVEL OF AN UNDIVIDED OR DIVIDED FACILITY, CLOSE THE LANE ACCORDING TO THE TRAFFIC CONTROL PLANS, ROADWAY STANDARD DRAWINGS OR AS DIRECTED BY THE ENGINEER. CONDUCT THE WORK SO THAT ALL PERSONNEL AND/OR EQUIPMENT REMAIN WITHIN THE CLOSED TRAVEL LANE.

J) DO NOT WORK SIMULTANEOUSLY WITHIN 15 FT ON BOTH SIDES OF AN OPEN TRAVELWAY, RAMP OR LOOP WITHIN THE SAME LOCATION UNLESS PROTECTED WITH GUARDRAIL OR BARRIER.

K) PROVIDE TRAFFIC CONTROL FOR APPROPRIATE LANE CLOSURES FOR SURVEYING DONE BY THE DEPARTMENT.

PAVEMENT EDGE DROP OFF REQUIREMENTS

L) BACKFILL AT A 6:1 SLOPE UP TO THE EDGE AND ELEVATION OF EXISTING PAVEMENT IN AREAS ADJACENT TO AN OPENED TRAVEL LANE THAT HAS A DROP-OFF AS FOLLOWS:

BACKFILL DROP-OFFS THAT EXCEED 2 INCHES ON ROADWAYS WITH POSTED SPEED LIMITS OF 45 MPH OR GREATER.

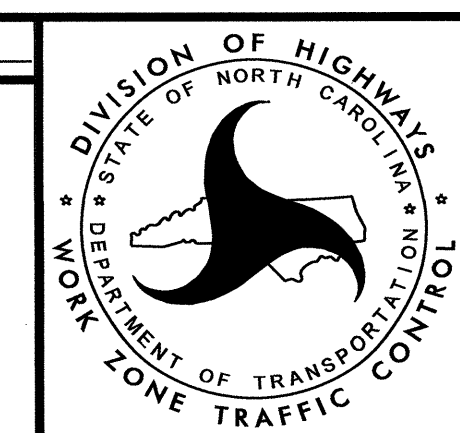
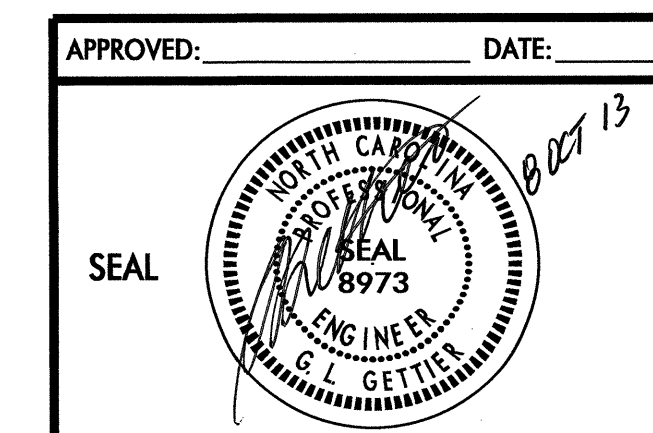
BACKFILL DROP-OFFS THAT EXCEED 3 INCHES ON ROADWAYS WITH POSTED SPEED LIMITS LESS THAN 45 MPH.

BACKFILL WITH SUITABLE COMPACTED MATERIAL, AS APPROVED BY THE ENGINEER, AT NO EXPENSE TO THE DEPARTMENT.

M) DO NOT EXCEED A DIFFERENCE OF 2 INCHES IN ELEVATION BETWEEN OPEN LANES OF TRAFFIC FOR NOMINAL LIFTS OF 1.5 INCHES. INSTALL ADVANCE WARNING "UNEVEN LANES" SIGNS (W8-11) (1,000 FT) IN ADVANCE AND A MINIMUM OF ONCE EVERY HALF MILE THROUGHOUT THE UNEVEN AREA.

TRAFFIC PATTERN ALTERATIONS

N) NOTIFY THE ENGINEER TWENTY ONE (21) CALENDAR DAYS PRIOR TO ANY TRAFFIC PATTERN ALTERATION.



TRANSPORTATION OPERATIONS & PROJECT NOTES

GENERAL NOTES

SIGNING

- O) INSTALL ADVANCE WORK ZONE WARNING SIGNS WHEN WORK IS WITHIN 40 FT FROM THE EDGE OF TRAVEL LANE AND NO MORE THAN THREE (3) DAYS PRIOR TO THE BEGINNING OF CONSTRUCTION.
- P) ENSURE ALL NECESSARY SIGNING IS IN PLACE PRIOR TO ALTERING ANY TRAFFIC PATTERN.

TRAFFIC BARRIER

- Q) INSTALL TEMPORARY BARRIER ACCORDING TO THE TRAFFIC CONTROL PLANS A MAXIMUM OF TWO (2) WEEKS PRIOR TO BEGINNING WORK IN ANY LOCATION. ONCE TEMPORARY BARRIER IS INSTALLED AT ANY LOCATION, PROCEED IN A CONTINUOUS MANNER TO COMPLETE THE PROPOSED WORK IN THAT LOCATION UNLESS OTHERWISE STATED IN THE TRAFFIC CONTROL PLANS OR AS DIRECTED BY THE ENGINEER.

DO NOT PLACE BARRIER DIRECTLY ON ANY SURFACE OTHER THAN ASPHALT OR CONCRETE.

ONCE TEMPORARY BARRIER IS INSTALLED AT ANY LOCATION AND NO WORK IS PERFORMED BEHIND THE TEMPORARY BARRIER FOR A PERIOD LONGER THAN TWO (2) MONTHS, REMOVE/RESET TEMPORARY BARRIER AT NO COST TO THE DEPARTMENT UNLESS OTHERWISE STATED IN THE TRAFFIC CONTROL PLANS, TEMPORARY BARRIER IS PROTECTING A HAZARD, OR AS DIRECTED BY THE ENGINEER.

INSTALL TEMPORARY BARRIER WITH THE TRAFFIC FLOW, BEGINNING WITH THE UPSTREAM SIDE OF TRAFFIC. REMOVE TEMPORARY BARRIER AGAINST THE TRAFFIC FLOW, BEGINNING WITH THE DOWNSTREAM SIDE OF TRAFFIC.

INSTALL AND SPACE DRUMS NO GREATER THAN TWICE THE POSTED SPEED LIMIT (MPH) TO CLOSE OR KEEP THE SECTION OF THE ROADWAY CLOSED UNTIL THE TEMPORARY BARRIER CAN BE PLACED OR AFTER THE TEMPORARY BARRIER IS REMOVED.

- R) PROTECT THE APPROACH END OF MOVABLE/PORTABLE CONCRETE BARRIER AT ALL TIMES DURING THE INSTALLATION AND REMOVAL OF THE BARRIER BY EITHER A TRUCK MOUNTED IMPACT ATTENUATOR (MAXIMUM 72 HOURS) OR A TEMPORARY CRASH CUSHION.

PROTECT THE APPROACH END OF MOVABLE/PORTABLE CONCRETE BARRIER FROM ONCOMING TRAFFIC AT ALL TIMES BY A TEMPORARY CRASH CUSHION UNLESS THE APPROACH END OF MOVABLE/PORTABLE CONCRETE BARRIER IS OFFSET FROM ONCOMING TRAFFIC AS FOLLOWS OR AS SHOWN IN THE PLANS:

POSTED SPEED LIMIT	MINIMUM OFFSET
40 OR LESS	15 FT
45-50	20 FT
55	25 FT
60 MPH or HIGHER	30 FT

TRAFFIC CONTROL DEVICES

- S) WHEN LANE CLOSURES ARE NOT IN EFFECT SPACE CHANNELIZING DEVICES IN WORK AREAS NO GREATER IN FEET THAN TWICE THE POSTED SPEED LIMIT (MPH) EXCEPT, 10 FT ON-CENTER IN RADII, AND 3 FT OFF THE EDGE OF AN OPEN TRAVELWAY. REFER TO STANDARD SPECIFICATIONS FOR ROADS AND STRUCTURES SECTIONS 1130 (DRUMS), 1135 (CONES) AND 1180 (SKINNY DRUMS) FOR ADDITIONAL REQUIREMENTS.
- T) PLACE TYPE III BARRICADES WITH "ROAD CLOSED" SIGN R11-2 ATTACHED OF SUFFICIENT LENGTH TO CLOSE ENTIRE ROADWAY.
- U) PLACE ADDITIONAL SETS OF THREE CHANNELIZING DEVICES (DRUMS, CONES OR SKINNY DRUMS) PERPENDICULAR TO THE EDGE OF TRAVELWAY ON 1,000 FT CENTERS WHEN UNOPENED LANES ARE CLOSED TO TRAFFIC.

PAVEMENT MARKINGS AND MARKERS

- V) INSTALL TEMPORARY PAVEMENT MARKINGS AND TEMPORARY PAVEMENT MARKERS ON INTERIM LAYERS OF PAVEMENT AS FOLLOWS:

ROAD NAME	MARKING	MARKER
1. I-85	PAINT	TEMPORARY RAISED

- W) PLACE ONE APPLICATION OF PAINT FOR TEMPORARY TRAFFIC PATTERNS. PLACE A SECOND APPLICATION OF PAINT SIX (6) MONTHS AFTER THE INITIAL APPLICATION AND EVERY SIX MONTHS AS DIRECTED BY THE ENGINEER.

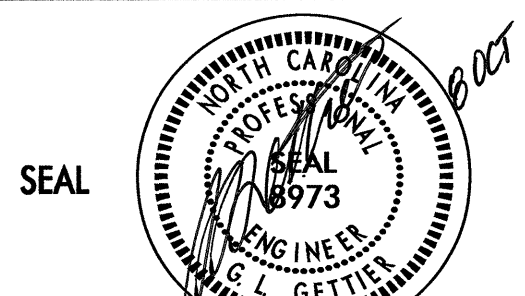
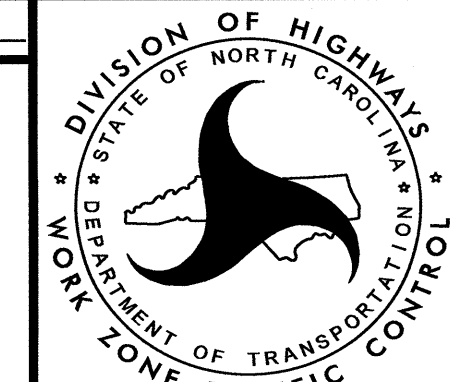
- X) TIE PROPOSED PAVEMENT MARKING LINES TO EXISTING PAVEMENT MARKING LINES.

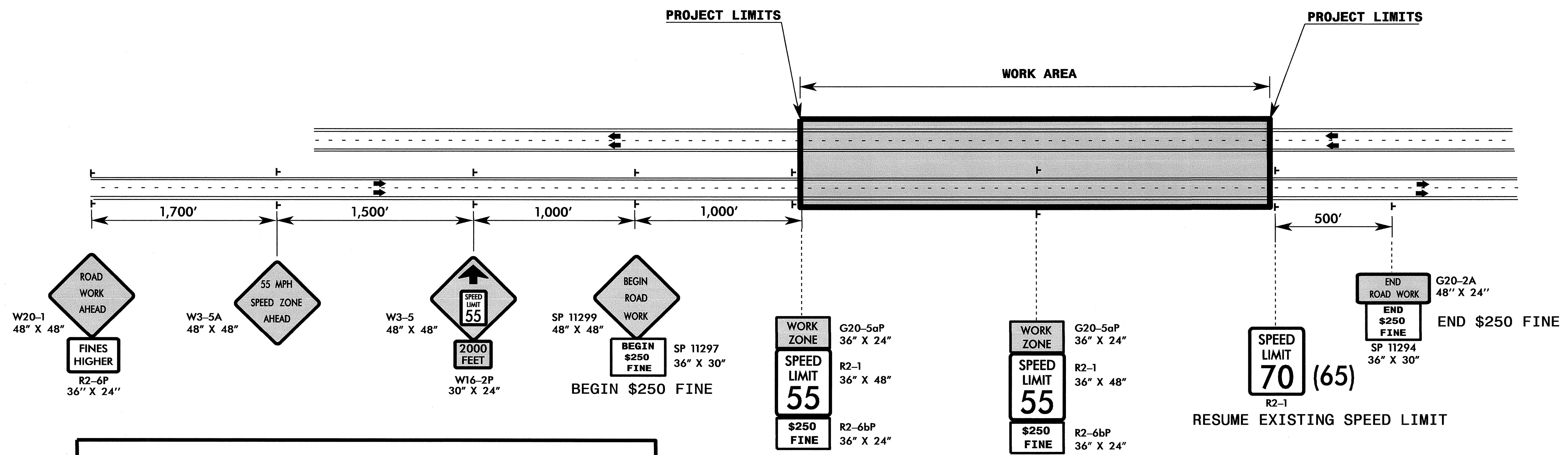
- Y) REMOVE/REPLACE ANY CONFLICTING/DAMAGED PAVEMENT MARKINGS AND MARKERS BY THE END OF EACH DAY'S OPERATION.

MISCELLANEOUS

- Z) LAW ENFORCEMENT MAY BE USED TO MAINTAIN TRAFFIC THROUGH THE WORK AREA AND/OR INTERSECTIONS, AS DIRECTED BY THE ENGINEER.

I7-SEP-2013 11:16 \\dot\dfsroot\proj\TIP\Projects\I4928\TrafficControl\TCP\I-4928_TC_TMP_01.dgn AT TE265817 sngreen

APPROVED: _____ DATE: _____ 		<h2 style="margin: 0;">TRANSPORTATION OPERATIONS & PROJECT NOTES</h2>
--	---	---



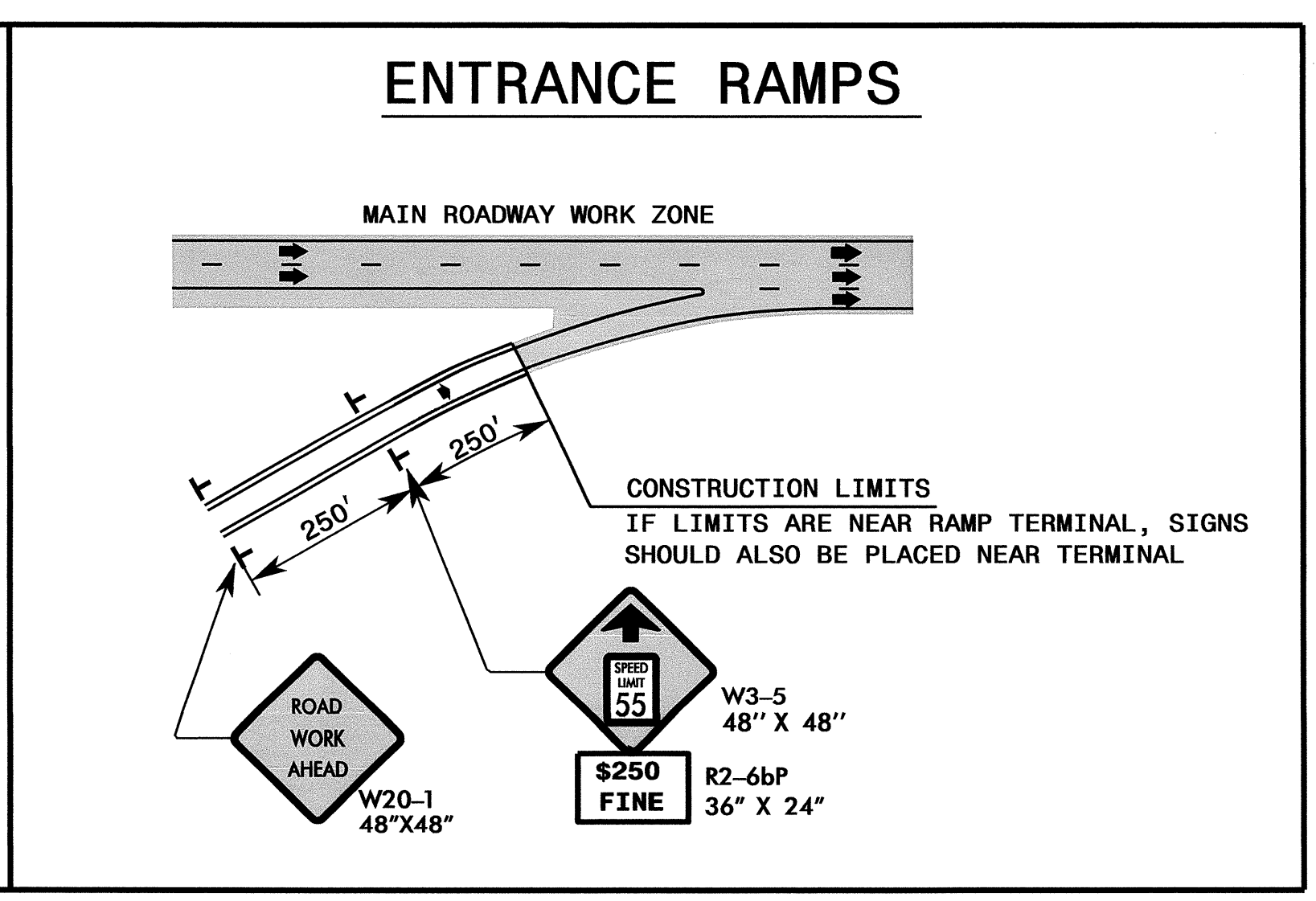
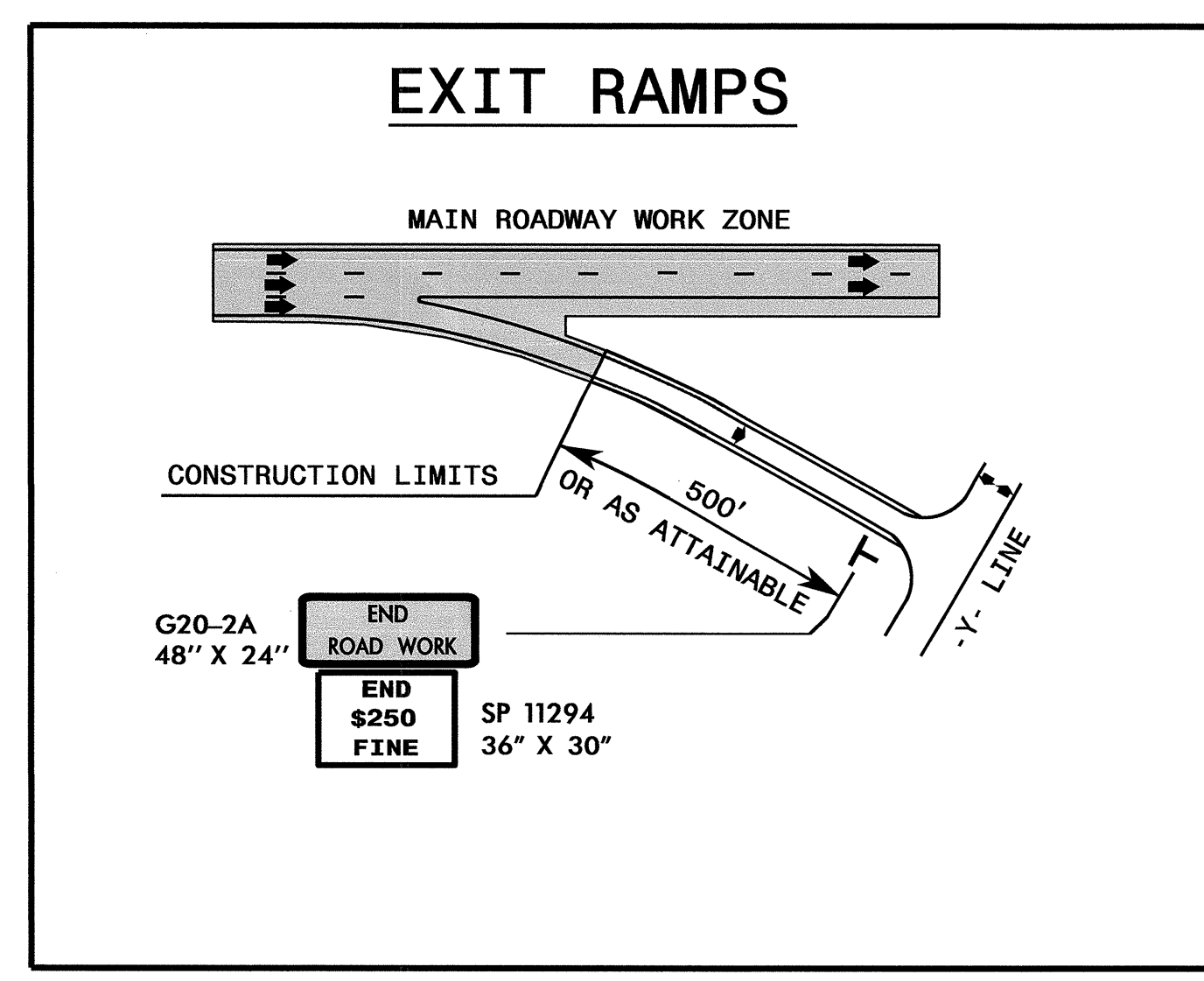
NOTES

- 1) THE WORK ZONE SPEED LIMIT WILL BE ESTABLISHED IN COLLABORATION BETWEEN THE REGIONAL TRAFFIC ENGINEER, THE DIVISION AND THE WORK ZONE TRAFFIC CONTROL SECTION. THIS DRAWING SHOWS THE TYPICAL APPLICATION OF REDUCING THE "WORK ZONE SPEED LIMIT" TO 55 MPH.
- 2) IF THE "WORK ZONE SPEED LIMIT" ONLY APPLIES TO A SPECIFIC PORTION AND NOT THE ENTIRE PROJECT, THE EXISTING SPEED LIMIT IS TO BE REESTABLISHED INSIDE THE PROJECT LIMITS. THE EXISTING SPEED LIMIT SIGNS AND THE "END \$250 FINE" SIGNS ARE TO BE INSTALLED AT THE LOCATION WHERE THE EXISTING SPEED LIMIT IS TO RESUME. (SEE GUIDELINE- D)
- 3) IF THE WORK ZONE SPEED LIMIT REDUCTION IS INSIDE THE WORK AREA, SIGNS W3-5A, W3-5, AND THE R2-1'S ALONG WITH THE SPEEDING FINE SIGNS ARE TO BE INSTALLED AT THE DISTANCE SHOWN ABOVE IN ADVANCE OF WHERE THE SPEED LIMIT IS REDUCED.
- 4) THE WORK ZONE SPEED LIMIT SIGNS ARE TO BE MOUNTED FROM 7' ABOVE EDGE OF PAVEMENT ELEVATION.
- 5) WHEN TEMPORARY LANE CLOSURES ARE INSTALLED AT THE BEGINNING OF THE PROJECT LIMITS, THE PORTABLE LANE CLOSURE SIGNS ARE TO BE ADJUSTED TO AVOID SIGN OVERLAP/CLUTTER
- 6) THE NEED AND LOCATION OF ADDITIONAL POSTED "WORK ZONE SPEED LIMIT" SIGNS WITHIN THE WORK AREA IS TO BE DETERMINED BY THE REGIONAL TRAFFIC ENGINEER.

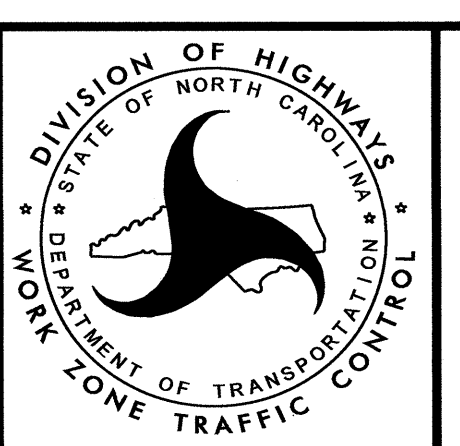
"WORK ZONE" SPEED LIMIT "WORK ZONE" SPEED LIMIT
SEE NOTE #4

GUIDELINES

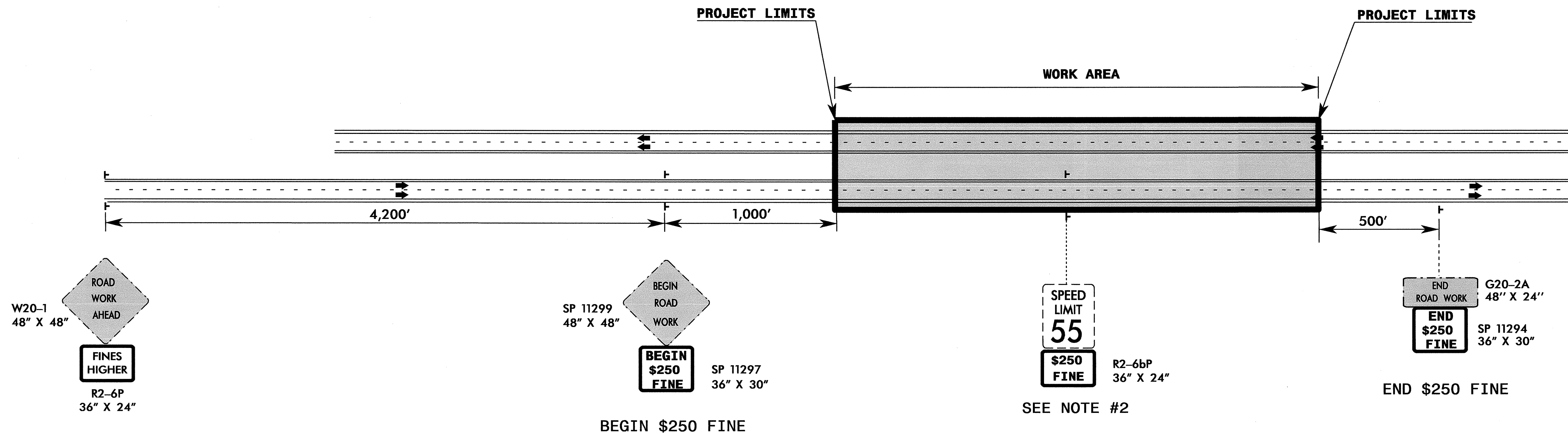
- A) THIS DRAWING IS FOR USE ONLY AFTER AN ENGINEERING INVESTIGATION AND CRITERIA REVIEW HAS BEEN PERFORMED BY THE REGIONAL TRAFFIC ENGINEER AND THE WORK ZONE TRAFFIC CONTROL SECTION. THE WORK ZONE SPEED LIMIT REDUCTION IS INTENDED FOR USE ON FREEWAYS WITH SPEED LIMITS 65 MPH OR GREATER. SEE WORK ZONE SPEED LIMIT GUIDELINES FOR CRITERIA.
- B) THE STATE TRAFFIC ENGINEER HAS TO ORDINANCE THE SPEED LIMIT REDUCTION IN ORDER FOR THE REDUCTION TO BE VALID AND ENFORCEABLE. NO SPEED LIMIT SIGNS SHALL BE INSTALLED PRIOR TO RECEIVING A SIGNED ORDINANCE. IN ADDITION, THE \$250 SPEEDING FINE ALSO REQUIRES A SEPARATE SIGNED ORDINANCE BY THE STATE TRAFFIC ENGINEER.
- C) EACH DIRECTION OF THE PROJECT IS TO BE EVALUATED FOR THE "WORK ZONE" SPEED LIMIT REDUCTION. THIS DRAWING INTENTIONALLY HAS 1 DIRECTION SIGNED AS A REMINDER TO CAREFULLY CONSIDER WHETHER BOTH DIRECTIONS OF THE PROJECT NEED TO HAVE THE SPEED LIMIT REDUCED.
- D) FOR PROJECTS THAT EXCEED 2 MILES IN LENGTH, AN EVALUATION IS TO BE MADE TO DETERMINE IF THE "WORK ZONE" SPEED LIMIT REDUCTION APPLIES TO THE ENTIRE PROJECT LENGTH OR IF ONLY A PORTION OF THE PROJECT LENGTH. THE "WORK ZONE" SPEED LIMIT REDUCTION MAY TERMINATE BEFORE THE END OF THE PROJECT LIMITS. THE DRAWING IS TO BE MODIFIED AS NEEDED TO REFLECT THESE CONDITIONS.
- E) THE \$250 SPEEDING FINE APPLIES FOR ALL PROJECTS THAT QUALIFY FOR A "WORK ZONE" SPEED LIMIT REDUCTION.
- F) ALL "WORK ZONE" SPEED LIMIT REDUCTION SIGNAGE SHALL BE REMOVED WHEN THE CONDITION/S THAT WARRANTED THE REDUCTION AND FINE IS REMOVED. THE REGIONAL TRAFFIC ENGINEER WILL BE NOTIFIED BY THE RESIDENT ENGINEER AT THIS TIME TO RESCIND THE ORDINANCES AND RETURN THE EXISTING POSTED SPEED LIMIT. THIS SHOULD TAKE PLACE BEFORE THE PROJECT IS 100% COMPLETE AND ACCEPTED FOR MAINTENANCE.



APPROVED: _____ DATE: _____



"WORK ZONE" SPEED LIMIT REDUCTION

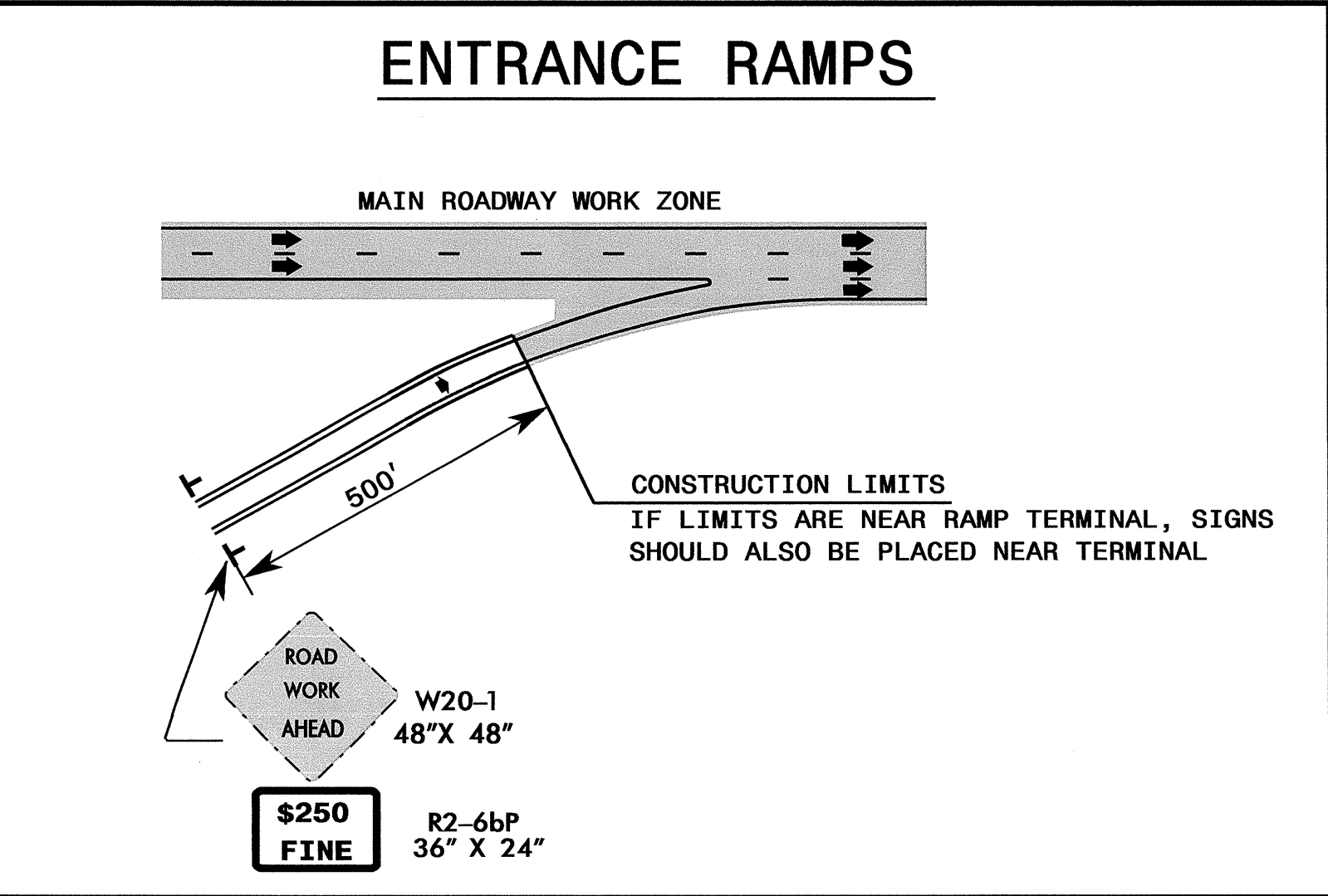
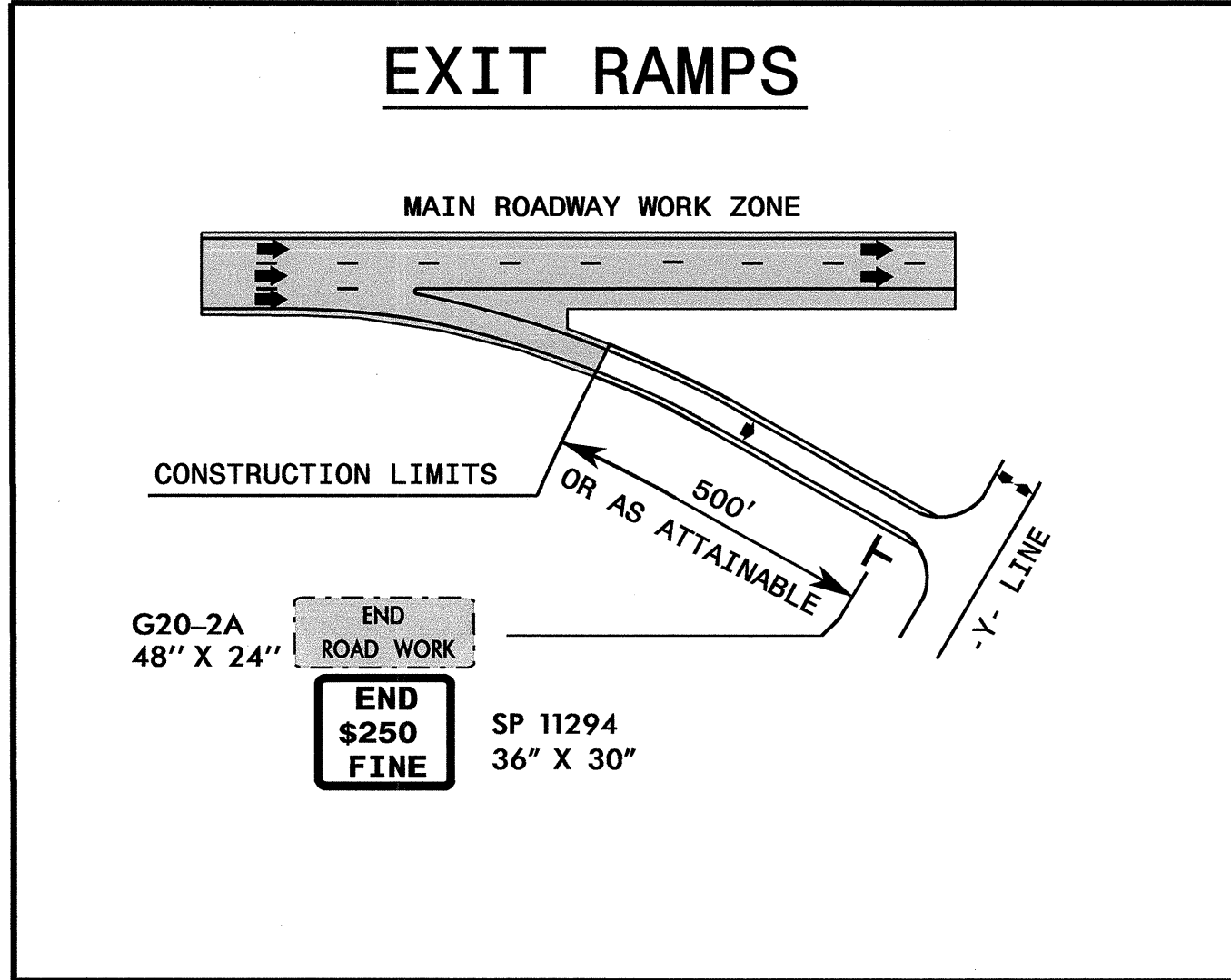


NOTES

- 1) THE \$250 SPEEDING FINE WILL BE DETERMINED BY THE ESTABLISHED CRITERIA AND COLLABORATION BETWEEN THE REGIONAL TRAFFIC ENGINEER, THE DIVISION AND THE WORK ZONE TRAFFIC CONTROL SECTION.
- 2) ONCE THE PROJECT HAS BEEN ORDINANCED FOR THE \$250 SPEEDING FINE, EACH EXISTING SPEED LIMIT SIGN WITHIN THE WORK AREA SHALL HAVE THE R2-6bP SIGN MOUNTED BELOW.
- 3) ONCE THE PROJECT HAS BEEN ORDINANCED FOR THE \$250 SPEEDING FINE, EACH "ROAD WORK AHEAD" SIGN INSTALLED ALONG ENTRANCE RAMP SHALL HAVE THE R2-6P SIGN MOUNTED BELOW.

GUIDELINES

- A) THIS DRAWING IS FOR USE ONLY AFTER AN ENGINEERING INVESTIGATION HAS BEEN PERFORMED BY THE REGIONAL TRAFFIC ENGINEER AND THE WORK ZONE TRAFFIC CONTROL SECTION. THE \$250 SPEEDING FINE IS INTENDED FOR USE ON FREEWAYS WITH SPEED LIMITS 60 MPH OR GREATER. SEE \$250 SPEEDING FINE GUIDELINES FOR THE CRITERIA.
- B) THE STATE TRAFFIC ENGINEER HAS TO ORDINANCE THE \$250 SPEEDING FINE IN ORDER FOR IT BE VALID AND ENFORCEABLE. NO "\$250 SPEEDING FINE" SIGNS SHALL BE INSTALLED PRIOR TO RECEIVING A SIGNED ORDINANCE.
- C) EACH DIRECTION OF THE PROJECT IS TO BE EVALUATED FOR THE "\$250 SPEEDING FINE". THIS DRAWING INTENTIONALLY HAS 1 DIRECTION SIGNED AS A REMINDER TO CAREFULLY CONSIDER WHETHER BOTH DIRECTIONS OF THE PROJECT NEED THE \$250 SPEEDING FINE.
- D) ALL "\$250 SPEEDING FINE" SIGNAGE SHALL BE REMOVED WHEN THE CONDITION/S THAT WARRANTED THE FINE IS REMOVED. THE REGIONAL TRAFFIC ENGINEER WILL BE NOTIFIED BY THE RESIDENT ENGINEER AT THIS TIME TO RESCIND THE ORDINANCE. THIS SHOULD TAKE PLACE BEFORE THE PROJECT IS 100% COMPLETE AND ACCEPTED FOR MAINTENANCE.



APPROVED: _____ DATE: _____

SEAL

\$250 SPEEDING FINE SIGNAGE

PHASING

NOTE: CONTRACTOR SHALL ONLY PLACE WORK ZONE SIGNS ALONG I-85 NB (-L-).

PHASE I

STEP 1: - CONTRACTOR SHALL INSTALL ALL ADVANCE WORK ZONE WARNING SIGNS. (SEE ROADWAY STANDARD DRAWING NO. 1101.01, SHEET 1 OF 3 AND SHEETS TMP-2 & TMP-2A).

STEP 2: - USING ROADWAY STANDARD DRAWING NO. 1101.02, SHEETS 3, 4, 8, 9 & 10 OF 15:

-- CONTRACTOR SHALL REDUCE TRAVEL LANES ON I-85 NB (-L-) TO ELEVEN (11) FEET WIDE AND SHIFT TRAFFIC FIVE (5) FEET TO THE MEDIAN SHOULDER AS SHOWN ON SHEETS TMP-4 AND TMP-5. (SEE CONSTRUCTION PLANS).

NOTE: IN THE CASE OF EXISTING MEDIAN GRATES PRESENT WITHIN THE PROJECT LIMITS, CONTRACTOR SHALL ENSURE ALL GRATES ARE LOAD BEARING AND WELDED IN PLACE, AS DIRECTED BY THE ENGINEER.

-- PLACE TEMPORARY PAVEMENT MARKINGS AND TEMPORARY RAISED MARKERS ON I-85 NB (-L-) AND SHIFT TRAFFIC TO TEMPORARY PATTERN. (SEE SHEETS TMP-4 & TMP-5).

NOTE: CONTRACTOR SHALL PLACE TEMPORARY RAISED PAVEMENT MARKERS ON 20' CENTERS IN ALL TAPERS.

-- USING ROADWAY STANDARD DRAWING NO. 1101.02, SHEETS 3, 8 & 9 OF 15, CONTRACTOR SHALL INSTALL PORTABLE CONCRETE BARRIER (PCB) ALONG THE OUTSIDE TRAVEL LANE OF I-85 NB (-L-) AS SHOWN ON SHEETS TMP-4 & TMP-5. (SEE CONSTRUCTION PLANS).

NOTE: CONTRACTOR SHALL HAVE NO MORE THAN 1 INGRESS AND 1 EGRESS AT ONE TIME, ON I-85 NB (-L-) NEAR THE WEIGH STATION COMPLEX TO ACCESS THE WORK AREA, UNLESS OTHERWISE APPROVED BY THE ENGINEER. (SEE ROADWAY STANDARD DRAWING NO. 1101.05, SHEET 1 OF 1).

STEP 4: - AWAY FROM TRAFFIC, CONTRACTOR SHALL CONSTRUCT THE PROPOSED WEIGH STATION FACILITIES, RAMPS, PARKING AREA, SIGNAGE, DRAINAGE AND ITS LOOPS. (SEE CONSTRUCTION PLANS AND TMP-4 & TMP-5).

NOTE: FOR PROTECTION OF WORK AREA FOR PROPOSED SIGNAGE ALONG OUTSIDE SHOULDER OF I-85 NB, CONTRACTOR SHALL USE ROADWAY STANDARD DRAWING NO. 1101.02 SHEET 3 OR 4 OF 15 TO INSTALL PROPOSED GUARDRAIL PRIOR TO CONSTRUCTION OF PROPOSED SIGNING STRUCTURES. (SEE CONSTRUCTION PLANS AND SIGNING PLANS).

PHASE II

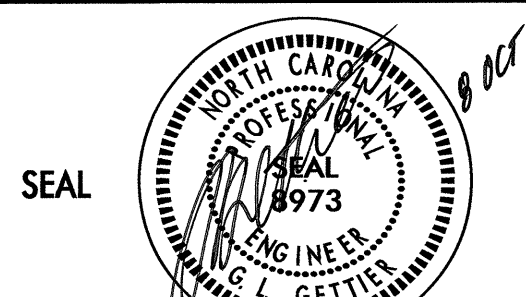
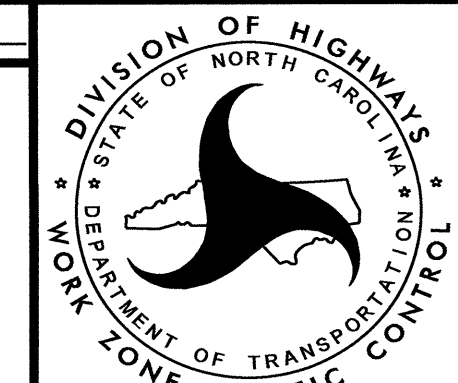
STEP 1: - USING ROADWAY STANDARD DRAWING NO. 1101.02, SHEETS 3, 8, 9 & 10 OF 15, REMOVE THE PORTABLE CONCRETE BARRIER (PCB), PLACE TEMPORARY MARKINGS AND TEMPORARY RAISED MARKERS FOR FINAL PATTERN ON I-85 NB ONLY AND PLACE TRAFFIC IN FINAL TRAFFIC PATTERN. (SEE FINAL PAVEMENT MARKING PLANS).

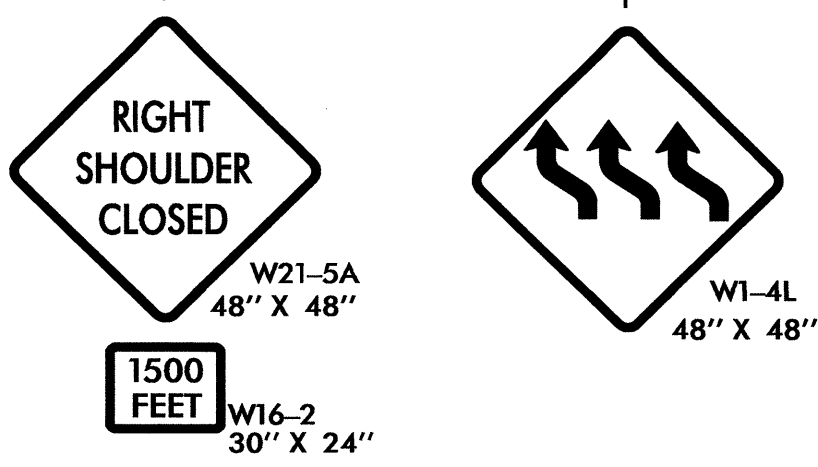
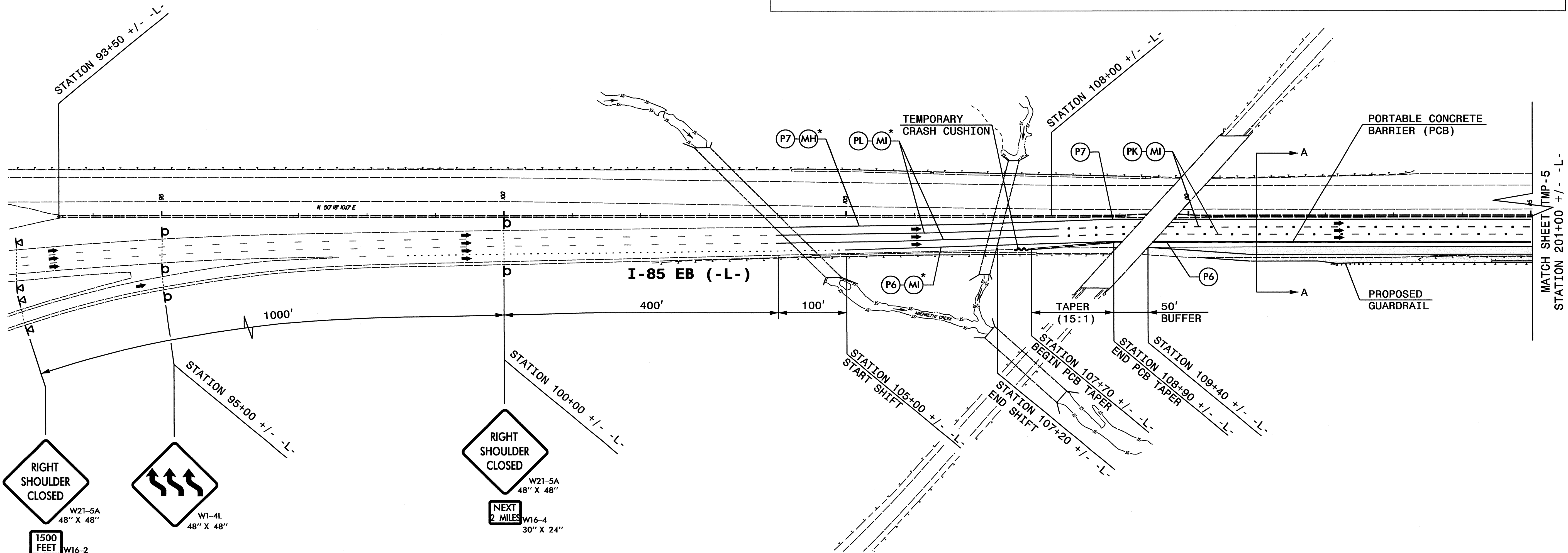
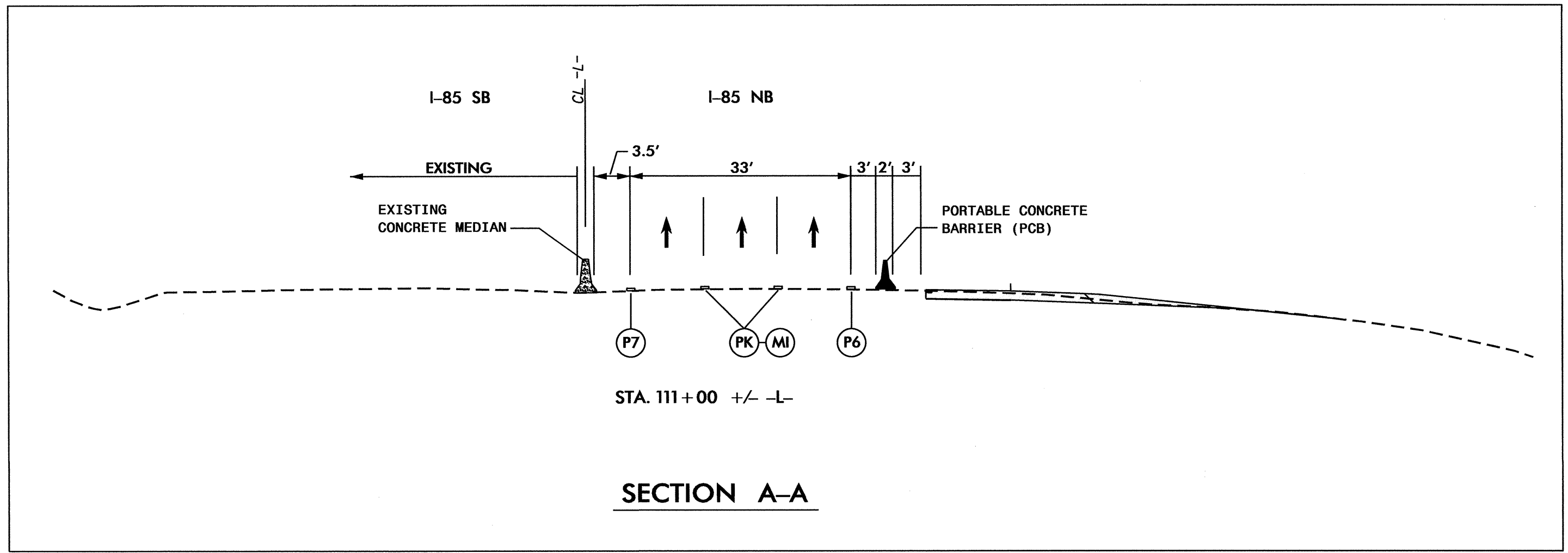
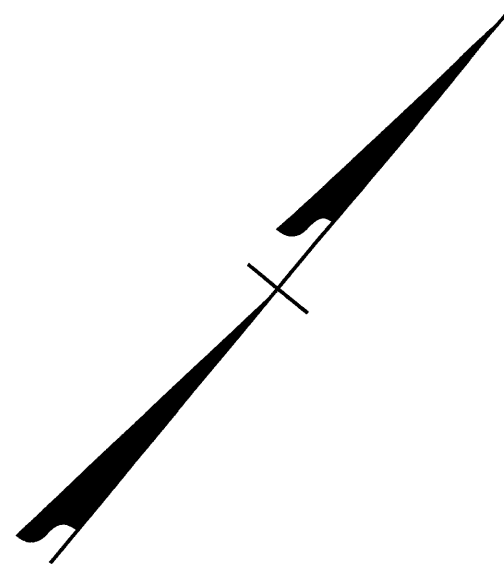
NOTE: CONTRACTOR SHALL PLACE TYPE III BARRICADES AND DRUMS TO KEEP PROPOSED WEIGH STATION FACILITIES CLOSED TO TRAFFIC AS DIRECTED BY THE ENGINEER.

NOTE: TRAFFIC IS IN FINAL TRAFFIC PATTERN ON I-85 NB AND WEIGH STATION FACILITY IS CLOSED.

STEP 2: - USING ROADWAY STANDARD DRAWING NO. 1101.02, SHEETS 3, 8, 9 & 10 OF 15, CONTRACTOR SHALL INSTALL REQUIRED ITS LOOPS, PAVE THE FINAL LAYER OF SURFACE COURSE FOR PROPOSED WEIGH STATION, RESURFACING I-85 NB (-L-) AND INSTALL THE FINAL PAVEMENT MARKINGS AND OPEN PROPOSED WEIGH STATION. (SEE CONSTRUCTION PLANS AND FINAL PAVEMENT MARKING PLANS).

17-SEP-2013 11:50
 \\dot\dfsroot\01\Proj\TIP\Projects\I4928\TrafficControl\TCP\I-4928_TC_TMP_03.dgn
 AT TE265817
 sngreen

APPROVED: _____ DATE: _____ <div style="text-align: center;">  </div>		<h1 style="margin: 0;">TEMPORARY TRAFFIC CONTROL PHASING</h1>
---	---	---



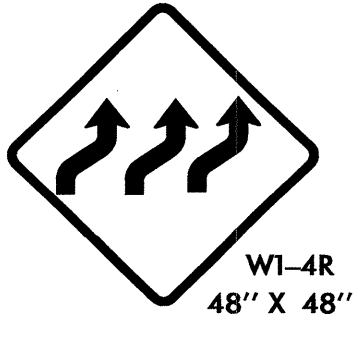
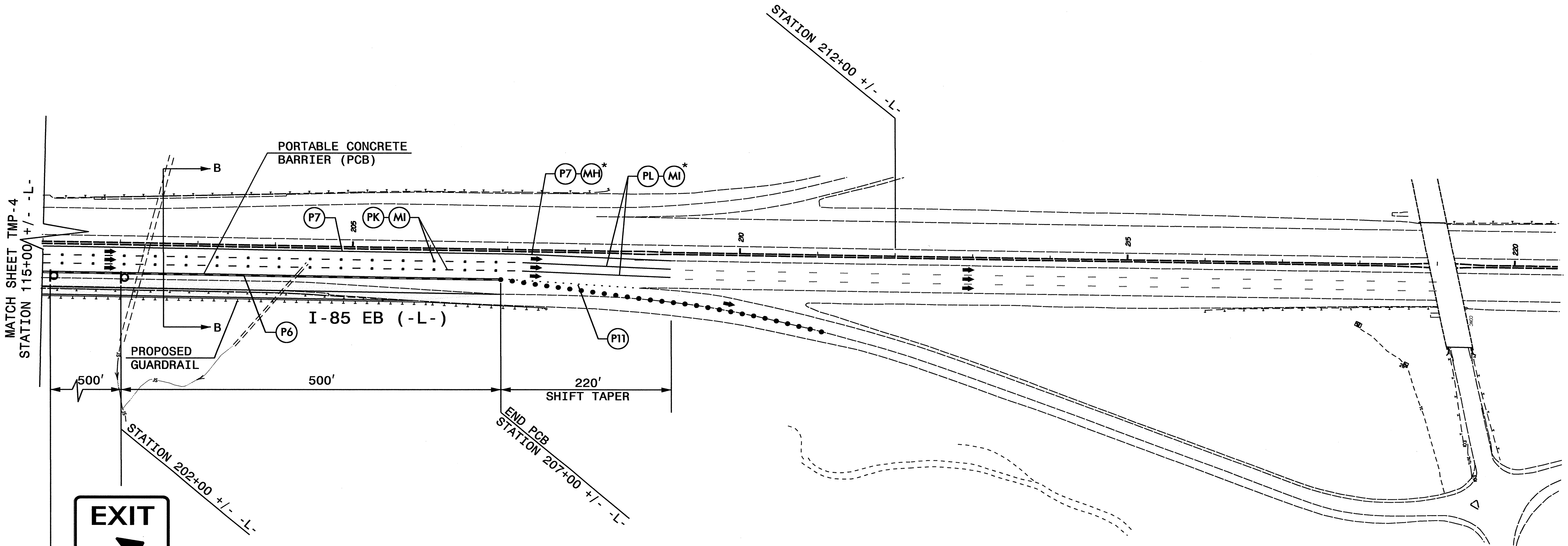
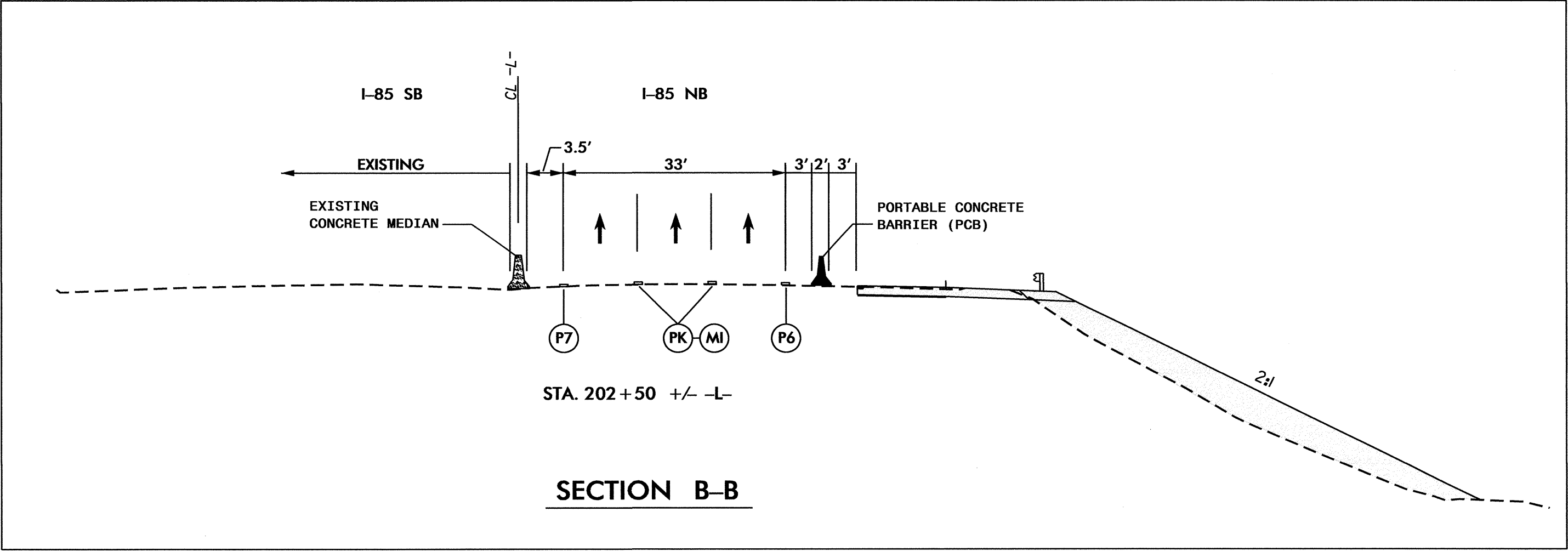
NOTES:
* INSTALL TEMPORARY RAISED PAVEMENT MARKERS ON 20' CENTERS IN ALL TAPERS.

WORK AREA

APPROVED: _____ DATE: _____

TEMPORARY TRAFFIC CONTROL DETAIL

18-SEP-2013 16:25 \\dot\dfsroot\proj\TIPProjects\I-4928\TrafficControl\TCPN-4928-TC-TMP-04.dgn sngreen AT 1E26587



NOTES:
 * INSTALL TEMPORARY RAISED PAVEMENT MARKERS ON 20' CENTERS IN ALL TAPERS.

WORK AREA

APPROVED: _____ DATE: _____

**TEMPORARY
TRAFFIC CONTROL
DETAIL**

18-SEP-2013 16:28
 \\dot\dfsroot\01\proj\18-SEP-2013\18-SEP-2013\TrafficControl\I-4928\TrafficControl\I-4928.TC_TMP-05.dgn
 sngreen AT 12:58:17