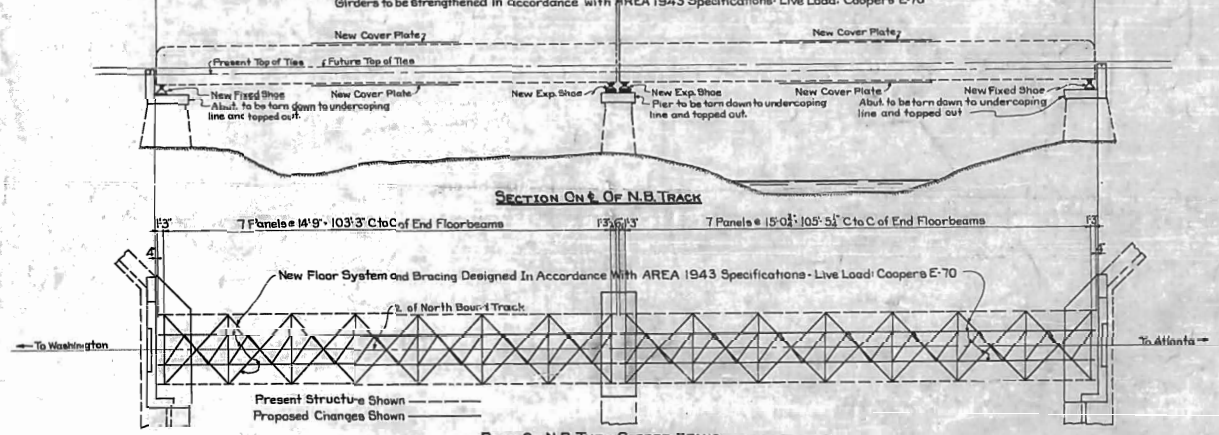
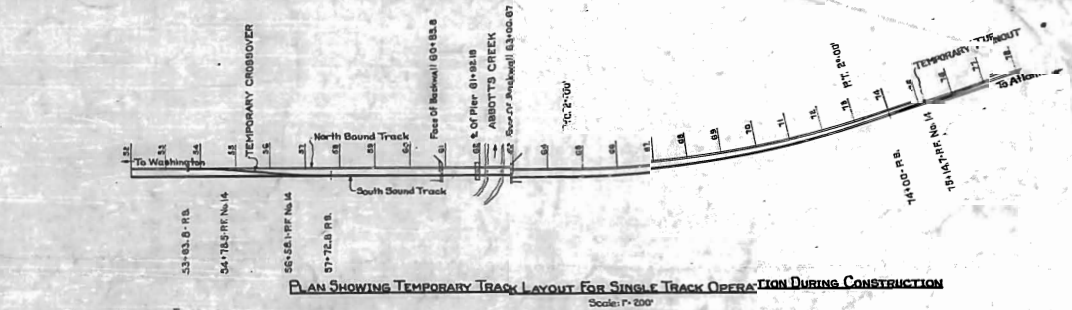


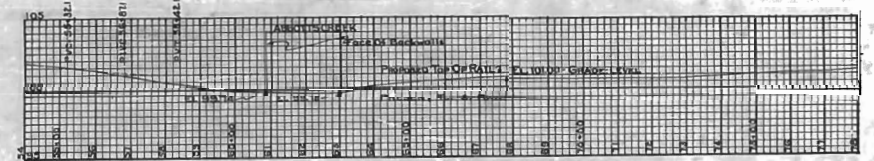
Pres. Thru Girder Span 105'-9" O to O - Built by Phoenix Bridge Co. In 1903. Pres. Thru Girder Span 107'-11" O to O - Built by Phoenix Bridge Co. In 1903  
 Built in accordance with Sou. Ry. Co's 1902 Specifications - Live Load Approx. Coopers E-42 Built in accordance with Sou. Ry. Co's 1902 Specifications - Live Load Approx. Coopers E-42  
 Girders to be strengthened in accordance with AREA 1943 Specifications - Live Load: Coopers E-70



**PLAN OF N.B. THRU GIRDER SPANS**  
 Scale: 7/8" = 1'-0"



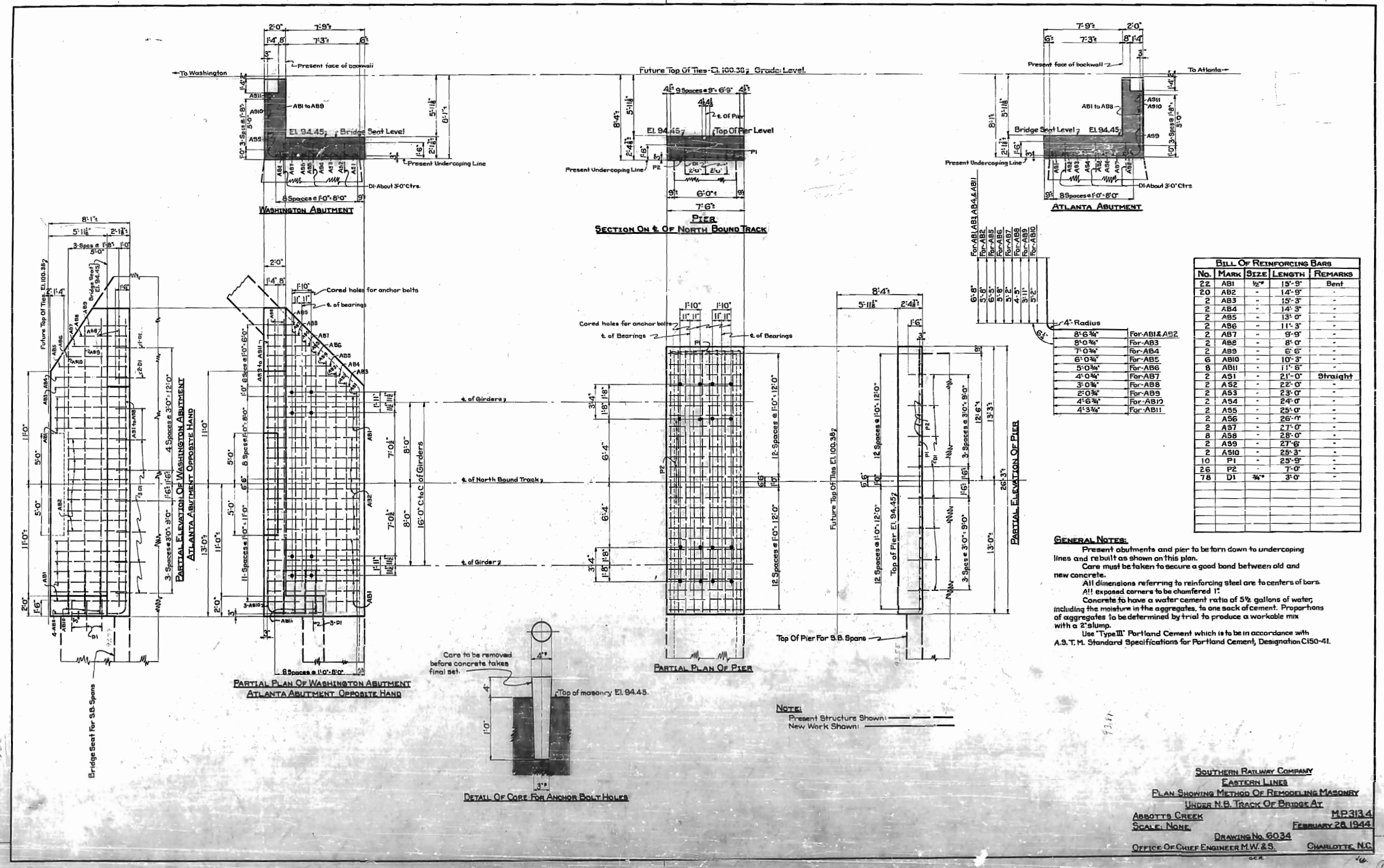
**PLAN SHOWING TEMPORARY TRACK LAYOUT FOR SINGLE TRACK OPERATION DURING CONSTRUCTION**  
 Scale: 1" = 200'



**PROFILE OF N.B. TRACK SHOWING PROPOSED TRACK RAISE**  
 Scales: Vertical 1" = 10' Horizontal 1" = 200'

**SOUTHERN RAILWAY COMPANY**  
 EASTERN LINE  
 PLAN SHOWING PROPOSED STRENGTHENING  
 OF THRU GIRDER SPANS UNDER N.B. TRACK OF BRIDGE AT  
 ABBOTTS CREEK M.P. 313.4  
 SCALE: AS NOTED DRAWING No. 6033 FEBRUARY 16, 1944  
 OFFICE OF CHIEF ENGINEER M.W. & S. CHARLOTTE, N.C.

FILED SEP 22 1944

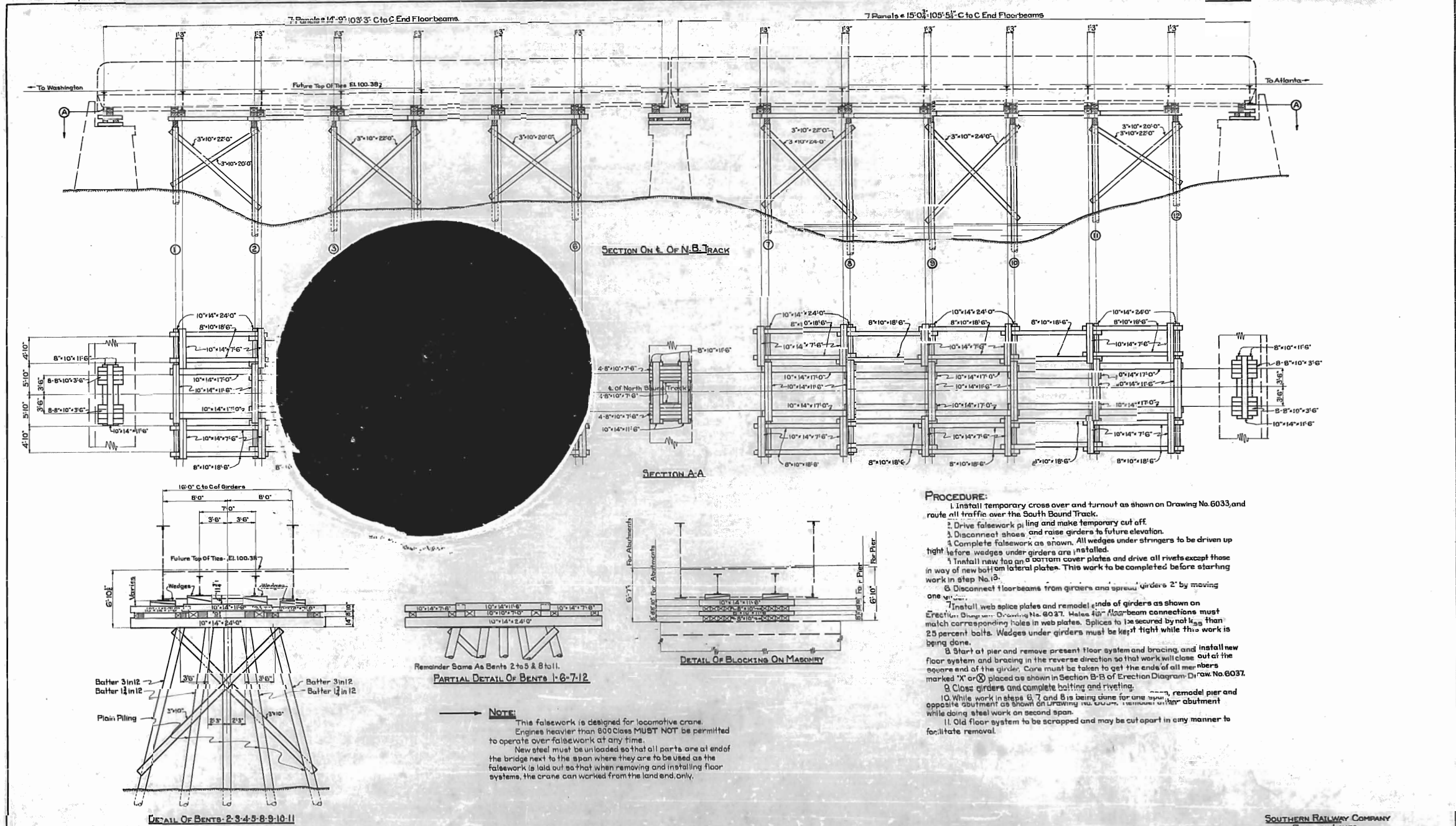


No.	MARK	SIZE	LENGTH	REMARKS
22	AB1	1/2"	15'-3"	Bent
20	AB2	-	14'-9"	-
2	AB3	-	15'-3"	-
2	AB4	-	14'-3"	-
2	AB5	-	13'-0"	-
2	AB6	-	11'-3"	-
2	AB7	-	9'-3"	-
2	AB8	-	8'-0"	-
2	AB9	-	6'-6"	-
6	AB10	-	10'-3"	-
8	AB11	-	11'-3"	-
2	AS1	-	21'-0"	Straight
2	AS2	-	22'-0"	-
2	AS3	-	23'-0"	-
2	AS4	-	24'-0"	-
2	AS5	-	25'-0"	-
2	AS6	-	26'-0"	-
2	AS7	-	27'-0"	-
8	AS8	-	28'-0"	-
2	AS9	-	27'-6"	-
2	AS10	-	28'-3"	-
10	P1	-	25'-9"	-
2	P2	-	3'-0"	-
7	D1	3/4"	3'-0"	-

**GENERAL NOTES:**  
 Present abutments and pier to be torn down to undercropping lines and rebuilt as shown on this plan.  
 Care must be taken to secure a good bond between old and new concrete.  
 All dimensions referring to reinforcing steel are to centers of bars.  
 All exposed corners to be chamfered 1".  
 Concrete to have a water cement ratio of 5% gallons of water, including the moisture in the aggregates, to one sack of cement. Proportions of aggregates to be determined by trial to produce a workable mix with a 2" slump.  
 Use "Type III" Portland Cement which is to be in accordance with A.S.T.M. Standard Specifications for Portland Cement, Designation C150-41.

**NOTE:**  
 Present Structure Shown: \_\_\_\_\_  
 New Work Shown: \_\_\_\_\_

SOUTHERN RAILWAY COMPANY  
 EASTERN LINES  
 PLAN SHOWING METHOD OF REMODELING MASONRY  
 UNDER N.B. TRACK OF BRIDGE AT  
 ABBOTT'S CREEK  
 SCALE: NONE  
 DRAWING No. 6034  
 OFFICE OF CHIEF ENGINEER M.W.B.S.  
 FEBRUARY 28, 1944  
 CHARLOTTE, N.C.



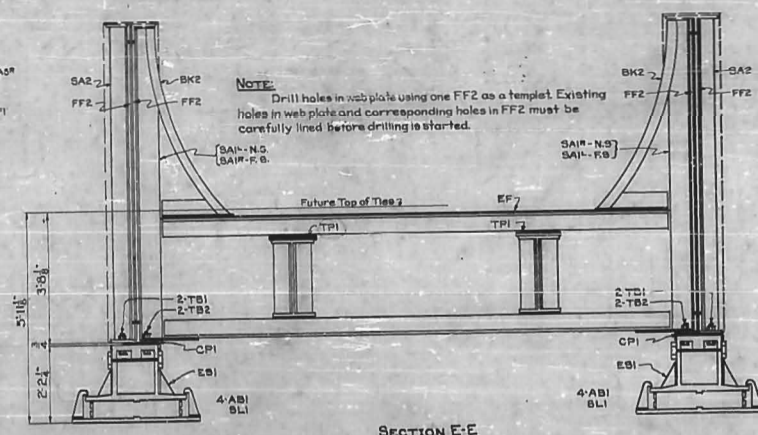
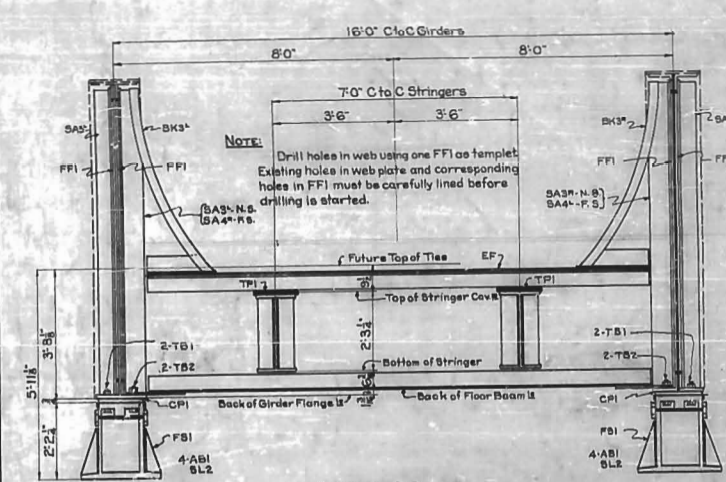
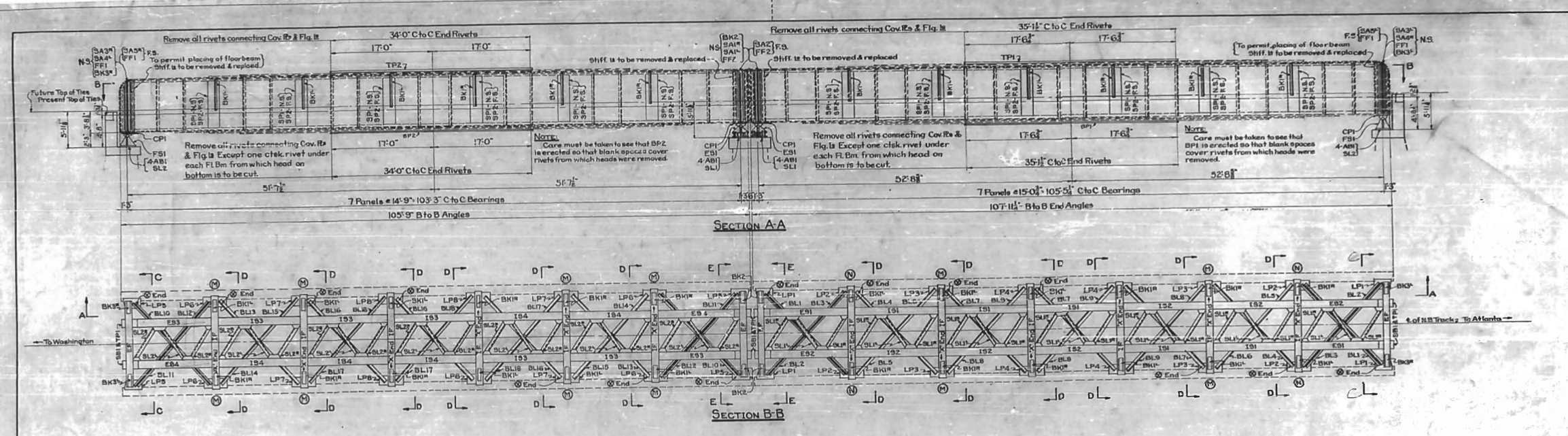
**PROCEDURE:**

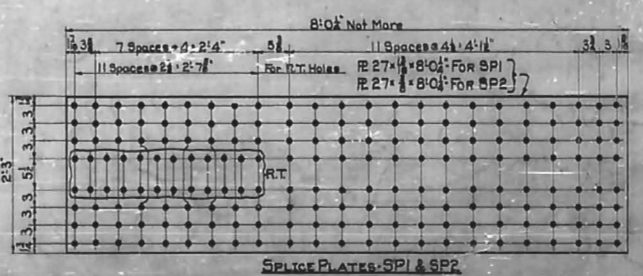
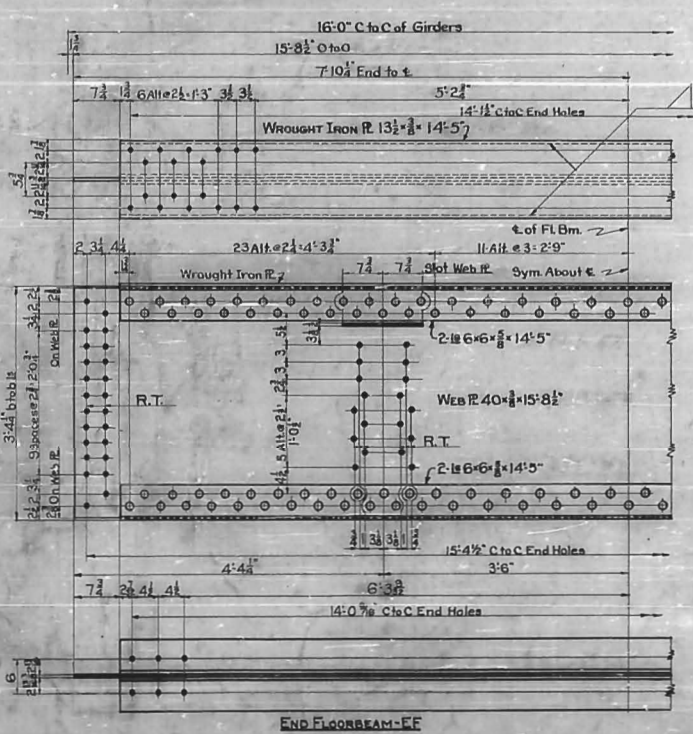
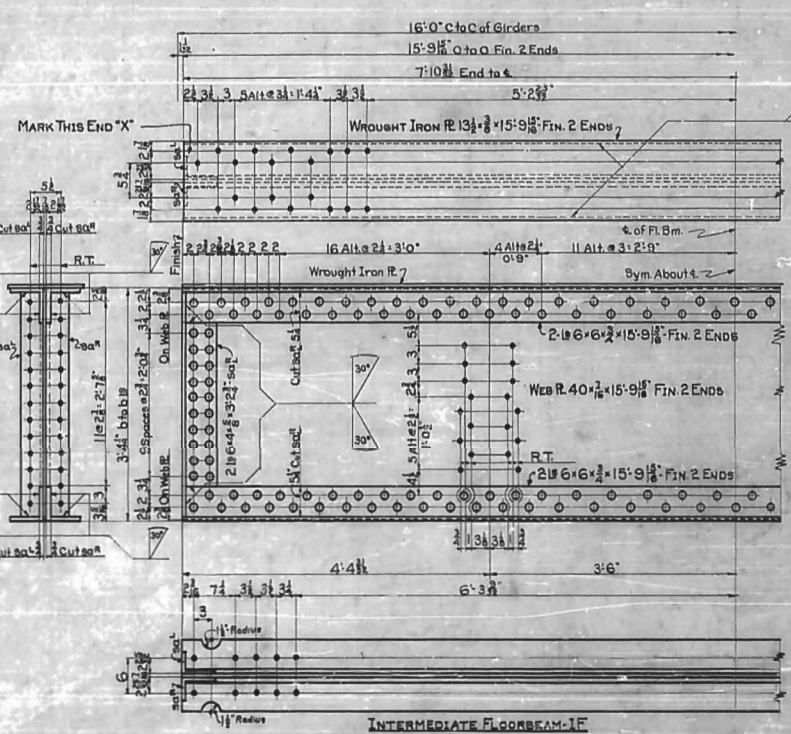
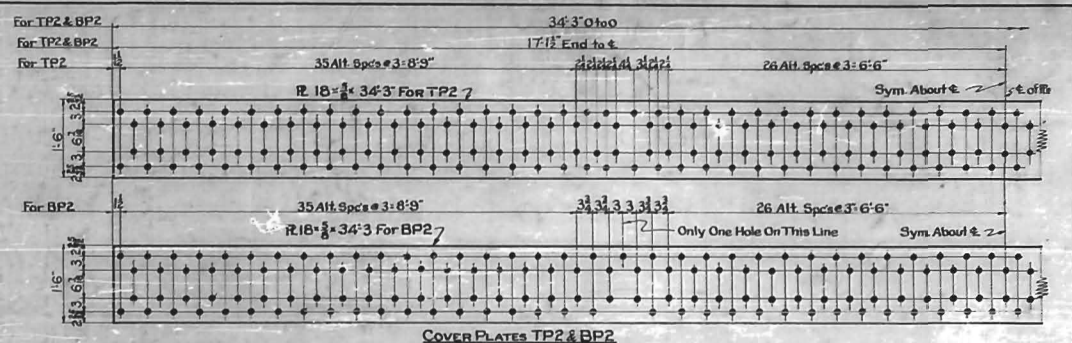
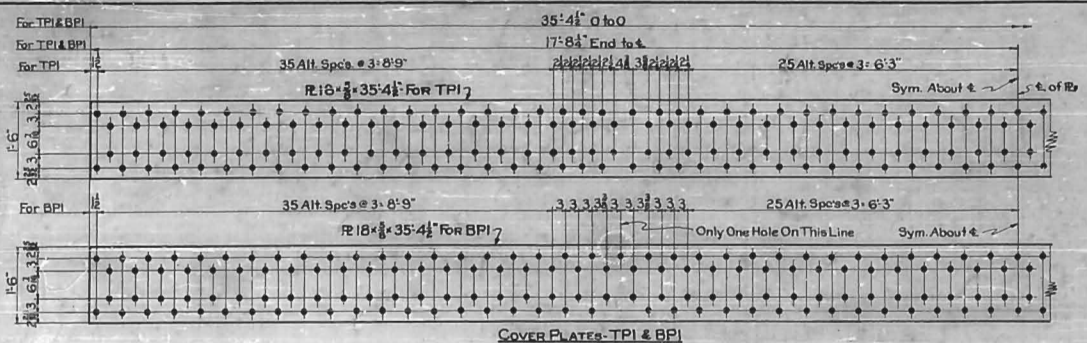
1. Install temporary cross over and turnout as shown on Drawing No. 6033, and route all traffic over the South Bound Track.
2. Drive falsework piling and make temporary cut off.
3. Disconnect shoes and raise girders to future elevation.
4. Complete falsework as shown. All wedges under stringers to be driven up tight before wedges under girders are installed.
5. Install new top cover plates and drive all rivets except those in way of new bottom lateral plates. This work to be completed before starting work in step No. 10.
6. Disconnect floorbeams from girders and spread girders 2' by moving one way.
7. Install web splice plates and remodel ends of girders as shown on Erection Diagram No. 6027. Holes for floor-beam connections must match corresponding holes in web plates. Splices to be secured with not less than 25 percent bolts. Wedges under girders must be kept tight while this work is being done.
8. Start at pier and remove present floor system and bracing and install new floor system and bracing in the reverse direction so that work will close out at the square end of the girder. Care must be taken to get the ends of all members marked 'X' or 'O' placed as shown in Section B-B of Erection Diagram No. 6037.
9. Close girders and complete bolting and riveting.
10. While work in steps 6, 7 and 8 is being done for one span, remodel pier and opposite abutment as shown on Erection Diagram No. 6027. While doing steel work on second span.
11. Old floor system to be scrapped and may be cut apart in any manner to facilitate removal.

**NOTE:**  
 This falsework is designed for locomotive crane. Engines heavier than 800 Class MUST NOT be permitted to operate over falsework at any time.  
 New steel must be unloaded so that all parts are at end of the bridge next to the span where they are to be used as the falsework is laid out so that when removing and installing floor systems, the crane can work from the land end only.

SOUTHERN RAILWAY COMPANY  
 EASTERN LINES  
 FALSEWORK PLAN FOR  
 STRENGTHENING THRU GIRDERS IN N.B. TRACK OF BRIDGE AT  
 ABBOTTS CREEK M.P. 313.4  
 SCALE: NONE MARCH 1, 1944  
 DRAWING No. 6036  
 OFFICE OF CHIEF ENGINEER M.W. & S. CHARLOTTE, N.C.





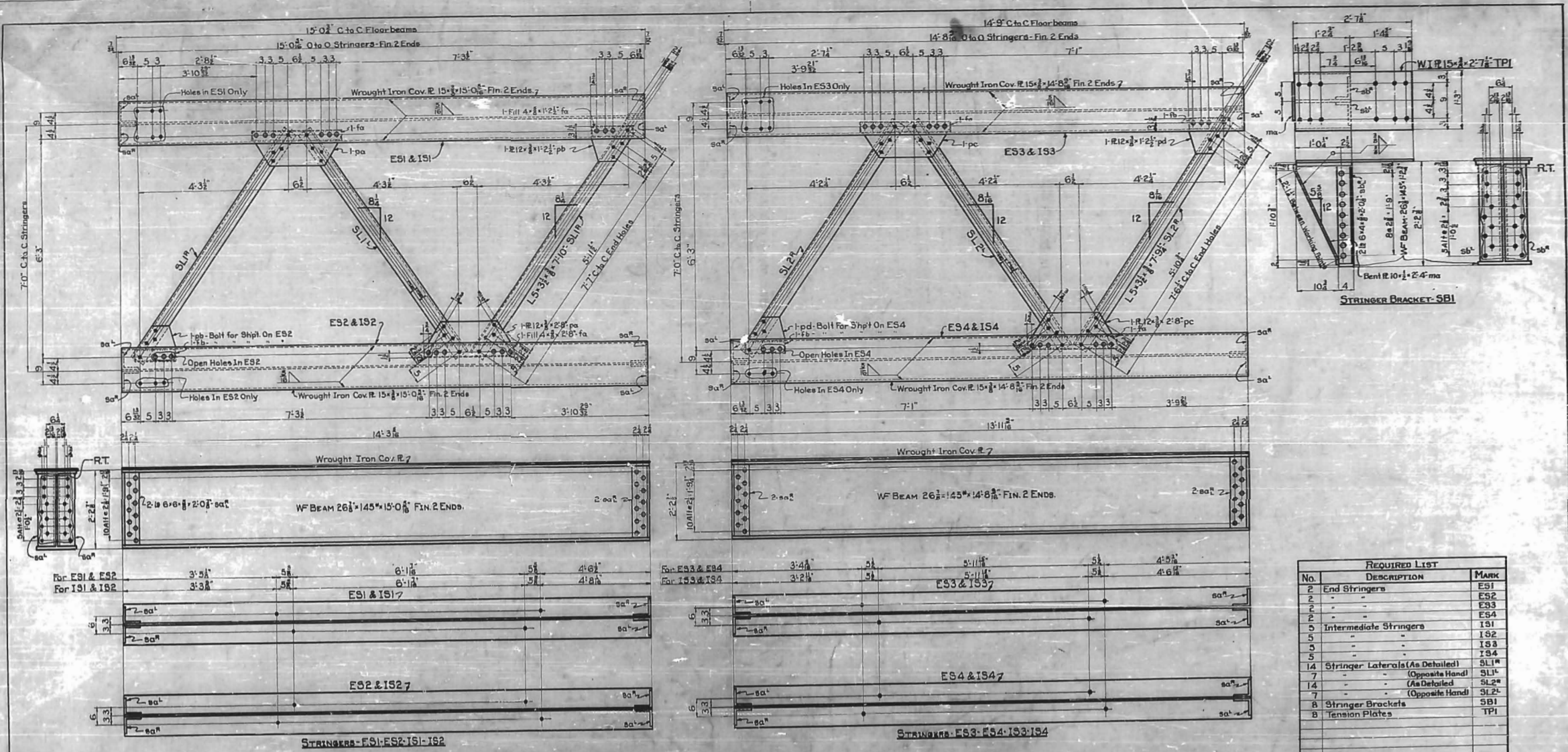


REQUIRED LIST		
No.	DESCRIPTION	MARK
2	Cover Plates	TPI
2	-	BPI
2	-	TP2
2	-	BP2
12	Intermediate Floorbeams	IF
4	End Floorbeams	EF
24	Splice Plates	SPI
24	-	SP2

**GENERAL NOTES:**  
 Holes: 5/8" Diam.  
 Rivets: 5/8" Diam. Unless Noted.  
 Specifications: All material and fabrication to be in accordance with the current A.R.E.A. Specifications.  
 Material: All material to be D.H. Structural Steel, except floorbeam cover plates which are to be Wrought Iron.  
 Razing: All holes marked R.T. to be punched 5/8" diameter and reamed to size thru a metal template not less than 1" thick.  
 Shop Paint: One coat of red lead and linseed oil.

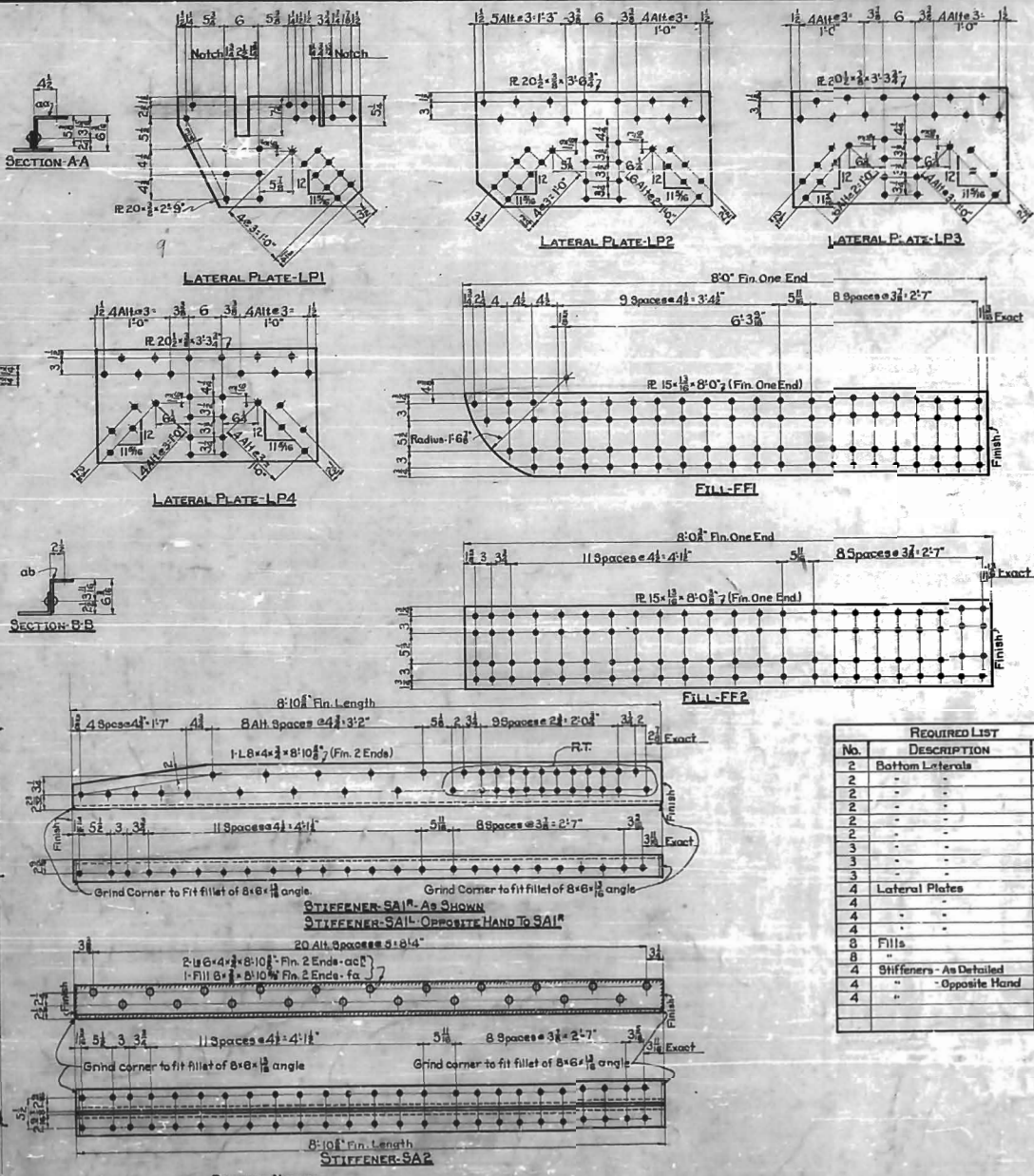
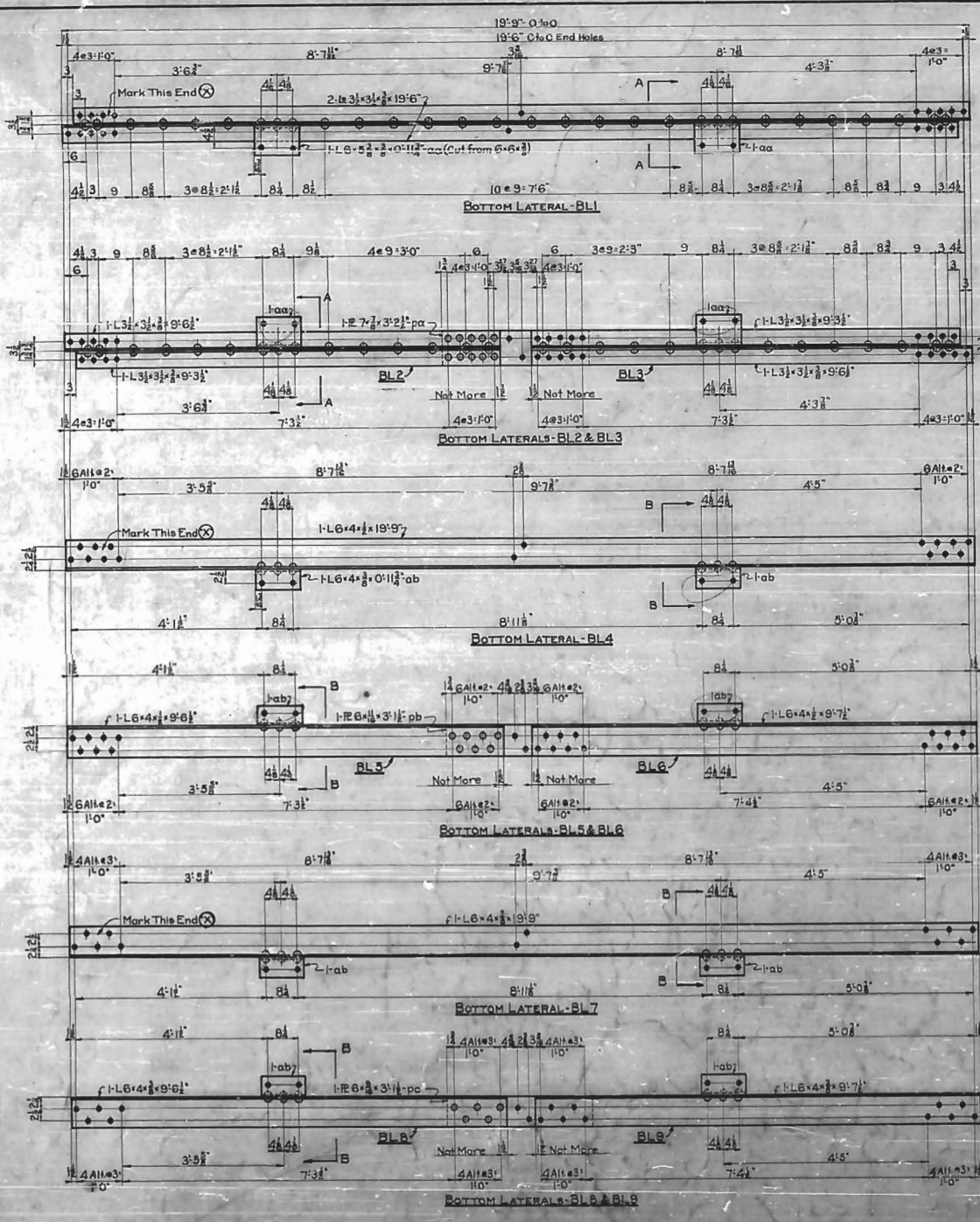
**SOUTHERN RAILWAY COMPANY**  
**EASTERN LINE**  
 DETAILS OF FL. BEAM, COV. PL., SP. PL., & CLIP ANGLES FOR STRENGTHENING THRU GIRDERS IN N.B. TRACK OF BRIDGE AT ABBOTTS CREEK. MP 313.4  
 SCALE: NONE NOVEMBER 23, 1943.  
 DRAWING No. 6038  
 OFFICE OF CHIEF ENGINEER M.W.S. CHARLOTTE, N.C.





**GENERAL NOTES:**  
 Holes: 1 1/8" Diam.  
 Rivets: 3/8" Diam.  
 Specifications: All material and fabrication to be in accordance with the current A.R.E.A. Specifications.  
 Material: All material to be C.H. Structural Steel, except stringer cover plates and tension plates TP1 which are to be Wrought Iron.  
 Drilling: All holes in material over 1/2" thick to be drilled.  
 Reaming: All holes marked R.T. to be punched 1/8" diameter and reamed to size thru a metal template not less than 1" thick.  
 Shop Paint: One coat of red lead and lined oil.

**SOUTHERN RAILWAY COMPANY**  
**EASTERN LINES**  
**DETAILS OF STRINGERS AND LATERALS FOR**  
**STRENGTHENING THREE GIRDERS IN N.B. TRACK OF BRIDGE AT**  
**ABBOTT'S CREEK M.P. 313.4**  
**SCALE: NONE** **OCTOBER 19 1943**  
**DRAWING NO. 6039**  
**OFFICE OF CHIEF ENGINEER, H.W. & S. CHARLOTTE, N.C.**

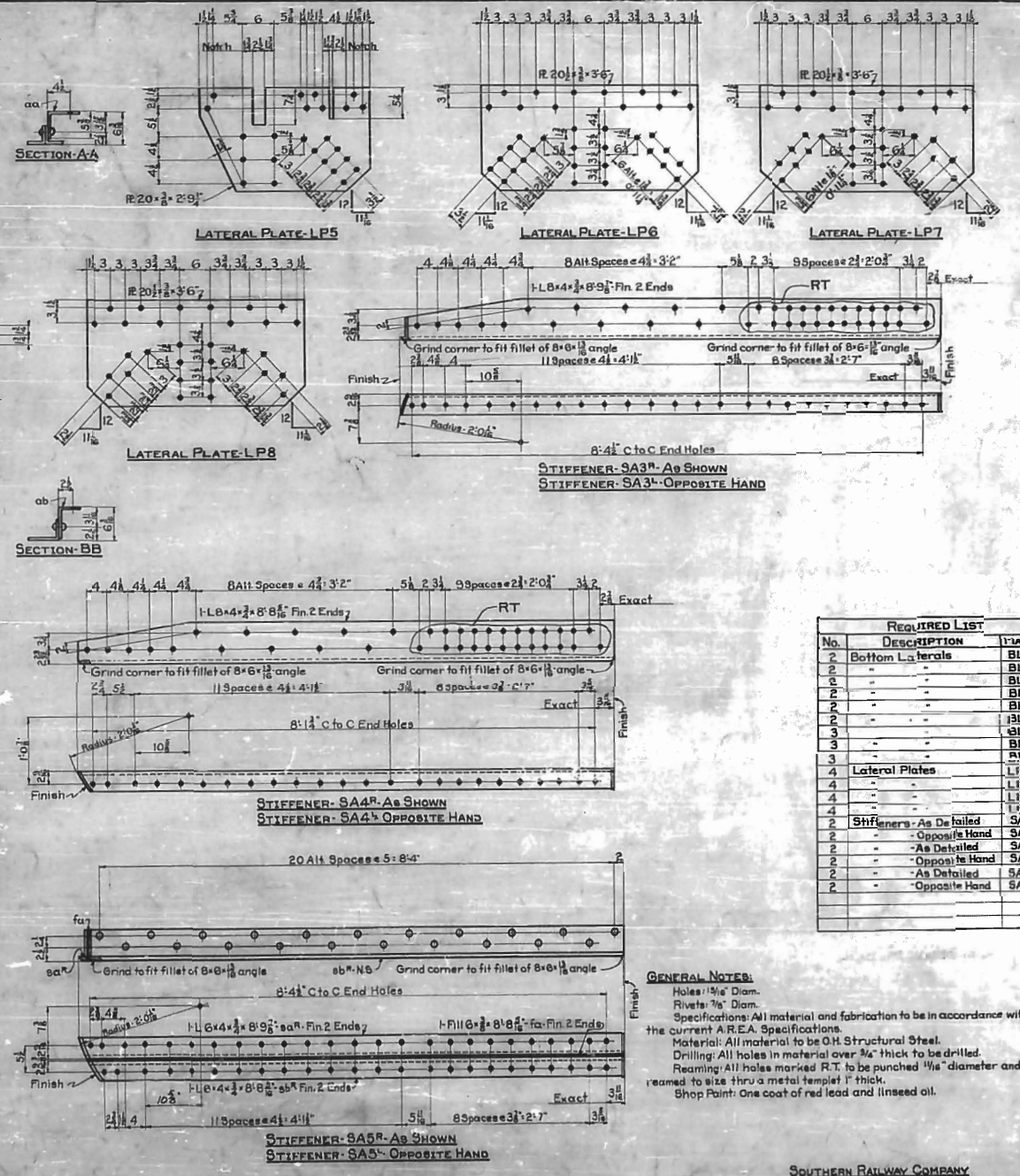
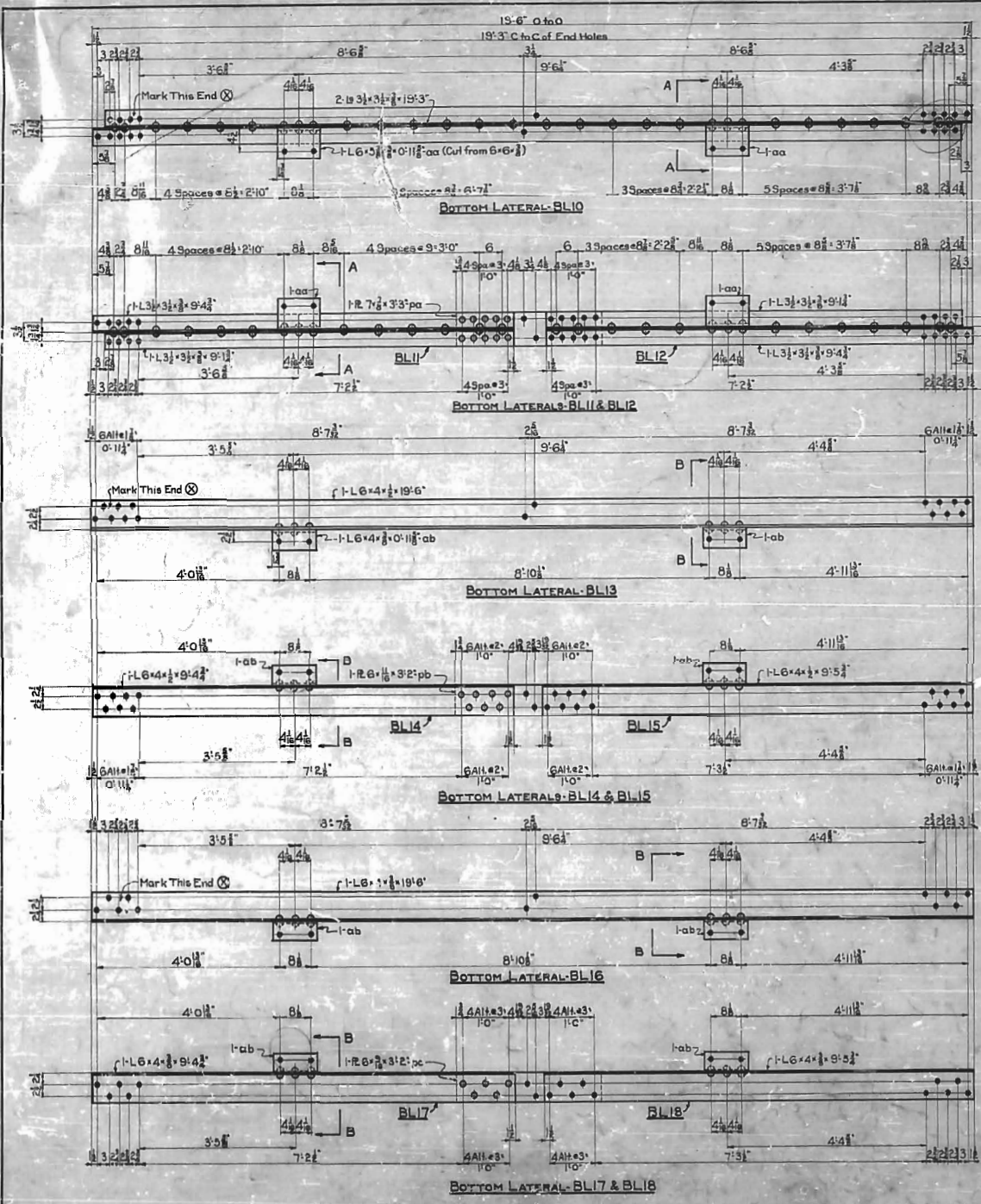


REQUIRED LIST		
No.	DESCRIPTION	MARK
2	Bottom Laterals	BL1
2	"	BL2
2	"	BL3
2	"	BL4
2	"	BL5
2	"	BL6
3	"	BL7
3	"	BL8
3	"	BL9
4	Lateral Plates	LP1
4	"	LP2
4	"	LP3
4	"	LP4
8	Fills	FF1
8	"	FF2
4	Stiffeners - As Detailed	SAIL
4	" - Opposite Hand	SAIL
4	"	SAIL

**GENERAL NOTES:**  
 Holes: 1 1/4" Diam.  
 Rivets: 3/4" Diam.  
 Specifications: All material and fabrication to be in accordance with the current A.R.E.A. Specifications.  
 Material: All material to be O.H. Structural Steel.  
 Drilling: All holes in material over 1/4" thick to be drilled.  
 Reaming: Holes marked R.T. to be punched 1/16" diameter and reamed to size thru a metal template 1" thick.  
 Shop Paint: One coat of red lead and linseed oil.

SOUTHERN RAILWAY COMPANY  
 EASTERN LINES  
 DETAILS OF BOTTOM LATERALS, LATERAL PLATES, FILLS & STIFFENERS FOR  
 STRENGTHENING THREE GIRDERS IN N.B. TRACK OF BRIDGE AT  
 ABBOTT'S CREEK  
 M.P. 313.4  
 SCALE: NONE  
 DRAWING No. 6040  
 OFFICE OF CHIEF ENGINEER  
 CHARLOTTE, N.C.  
 DECEMBER 15, 1943





REQUIRED LIST		
No.	DESCRIPTION	MARK
2	Bottom Laterals	BL10
2	-	BL11
2	-	BL12
2	-	BL13
2	-	BL14
2	-	BL15
2	-	BL16
2	-	BL17
2	-	BL18
4	Lateral Plates	LP5
4	-	LP6
4	-	LP7
4	-	LP8
2	Stiffeners - As Detailed	SA3
2	-	SA3 <sup>o</sup>
2	-	SA4
2	-	SA4 <sup>o</sup>
2	-	SA5
2	-	SA5 <sup>o</sup>

**GENERAL NOTES:**  
 Holes: 1/8" Diam.  
 Rivets: 3/8" Diam.  
 Specifications: All material and fabrication to be in accordance with the current A.R.E.A. Specifications.  
 Material: All material to be O.H. Structural Steel.  
 Drilling: All holes in material over 1/2" thick to be drilled.  
 Reaming: All holes marked R.T. to be punched 1/16" diameter and reamed to size thru a metal template 1" thick.  
 Shop Paint: One coat of red lead and linseed oil.

SOUTHERN RAILWAY COMPANY  
 EASTERN LINES  
 DETAILS OF BOTTOM LATERALS, LATERAL PLATES & STIFFENERS FOR  
 STRENGTHENING THRU GIRGERS IN N.B. TRACK OF BRIDGE AT  
 ABBOTT'S CREEK M.P. 313.4  
 SCALE: NONE DECEMBER 18, 1943  
 DRAWING No. 6041  
 OFFICE OF CHIEF ENGINEER M.W. & S. CHARLOTTE, N.C.





