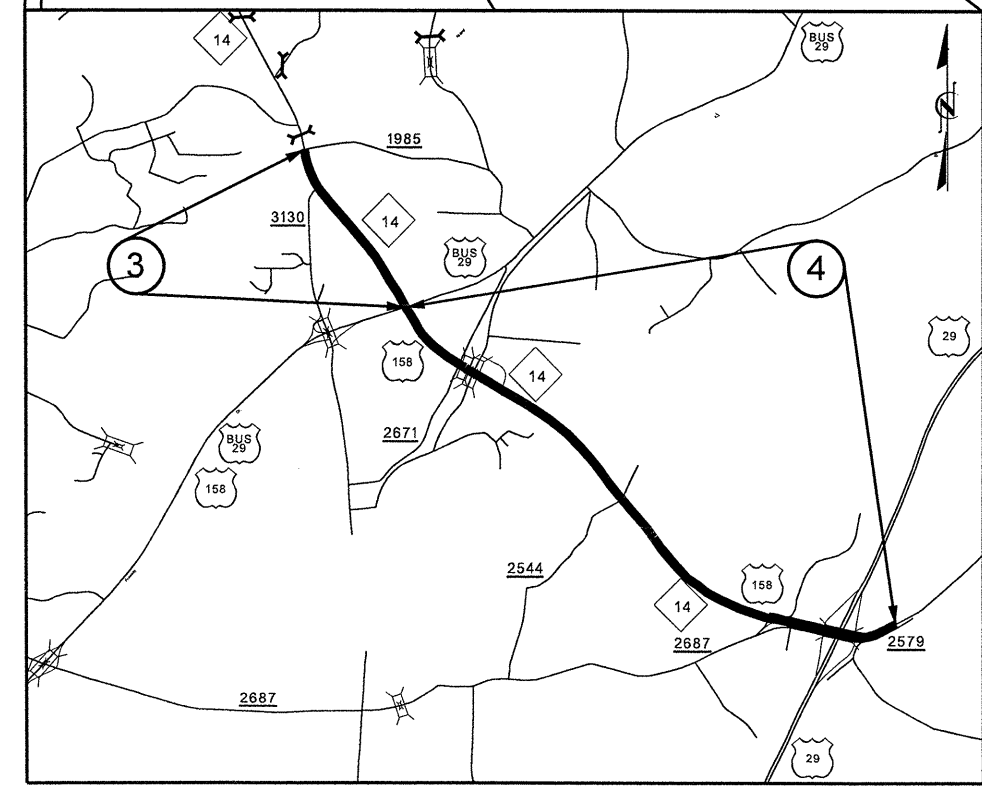
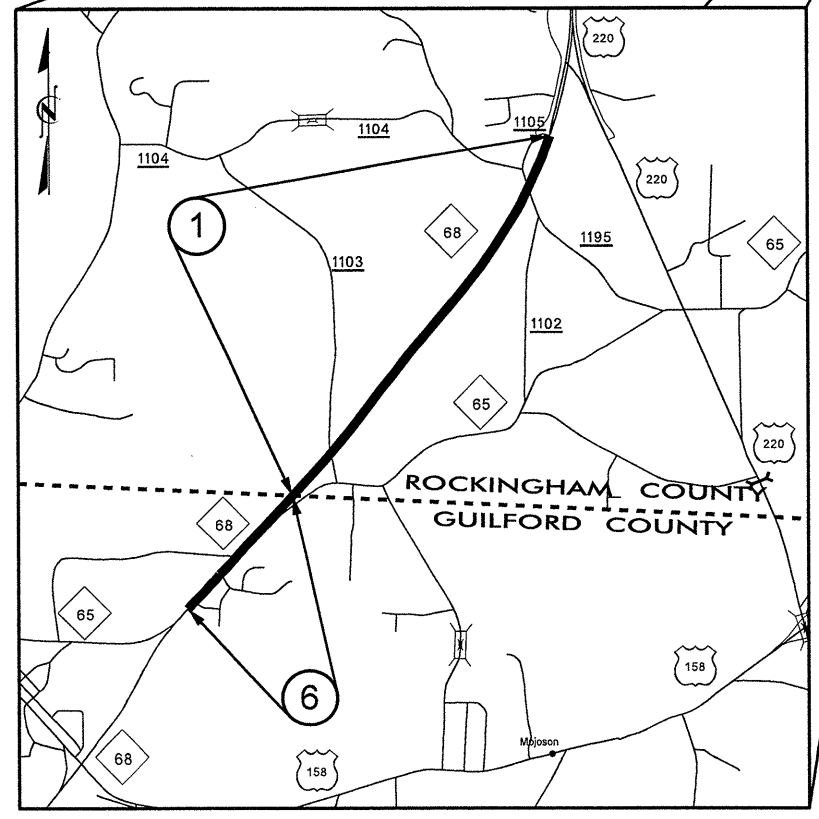
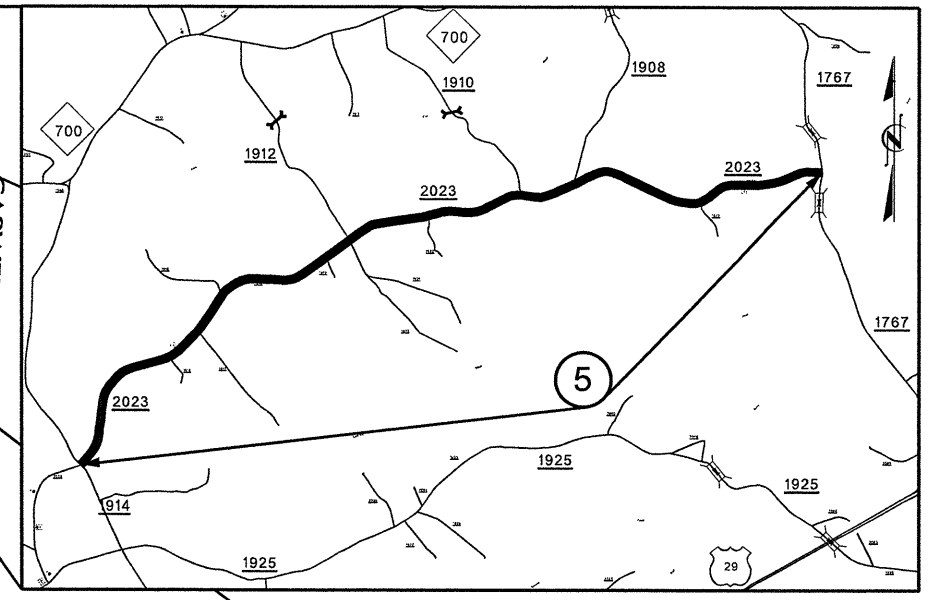
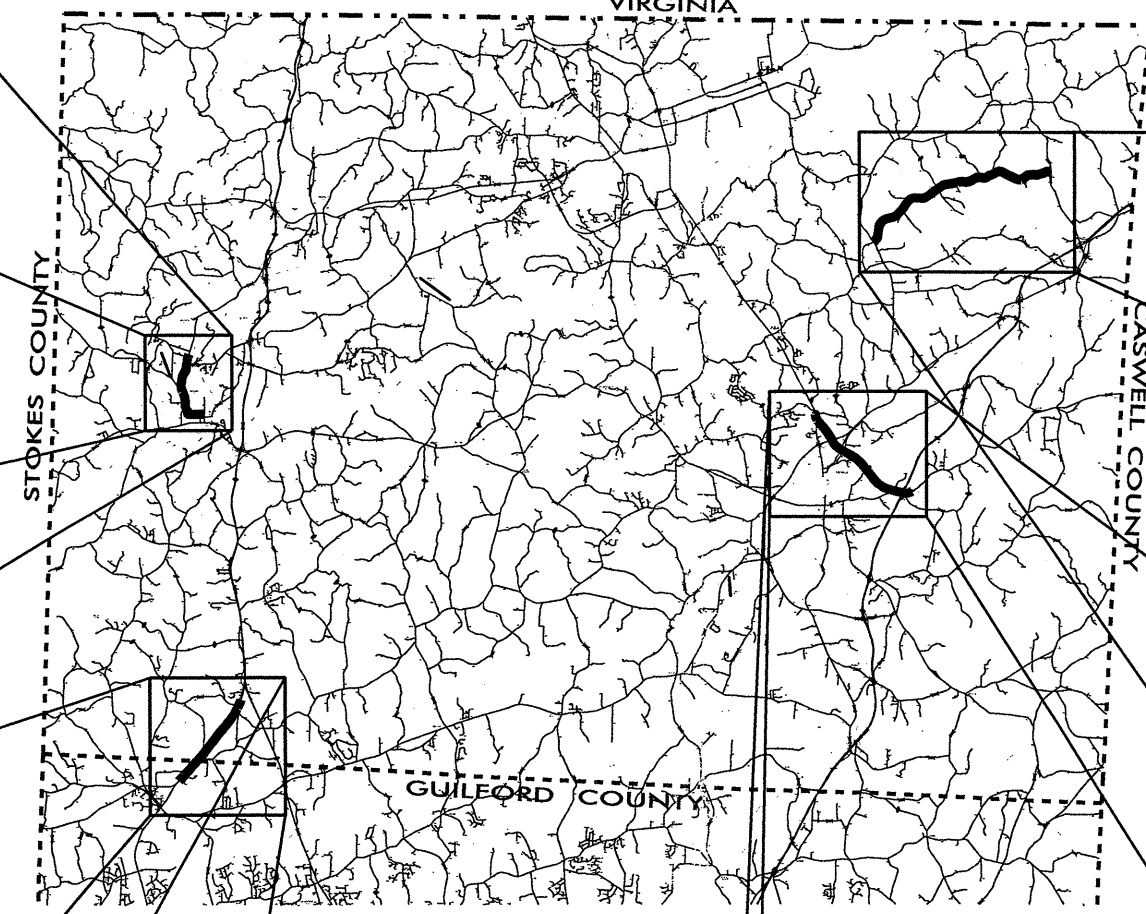
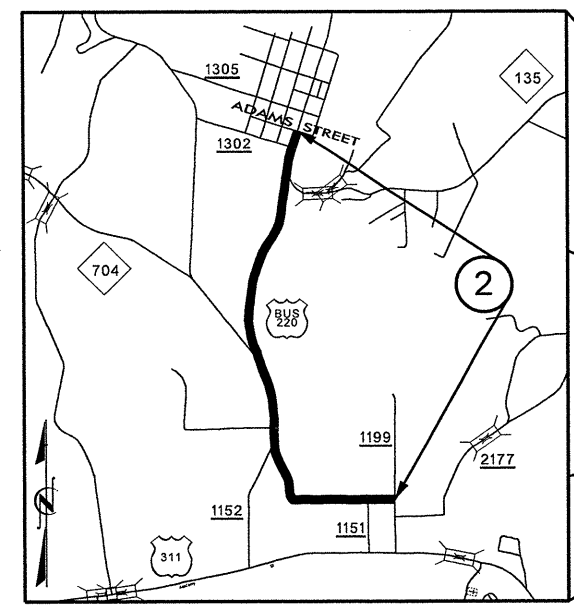


7CR.10791.48 7CR.20791.48 7CR.10411.48	STATE N.C.	PROJECT NO. 7CR.10791.48, ETC	SHEET NO. 1	TOTAL SHEETS ...
F.A. PROJ. NO.				

2013 ROCKINGHAM COUNTY

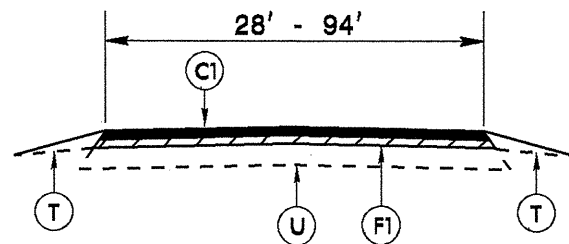


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7CR.10791.48
 7CR.20791.48
 7CR.10411.48

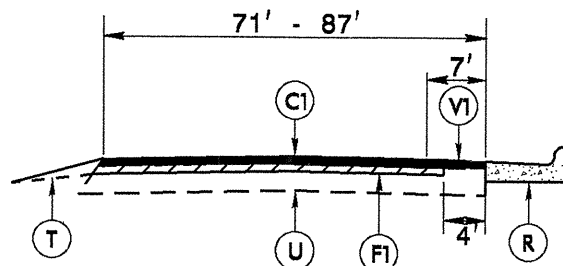
STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
N.C.	7CR.10791.48, ETC	2	



TYPICAL SECTION NO. 1

TO BE USED ON MAPS 1, 3, 4, AND 6

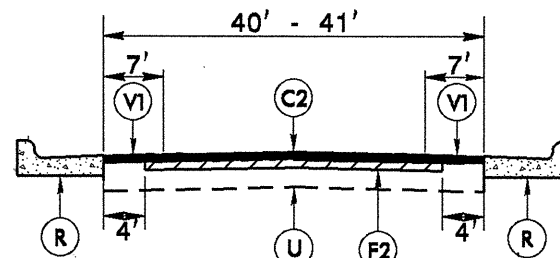
- MAP 3: STA. 14+30 TO STA. 31+00
- STA. 33+60 TO STA. 39+25
- MAP 4: STA. 0+00 TO STA. 5+40
- STA. 10+15 TO STA. 11+35
- STA. 17+25 TO STA. 18+00
- STA. 30+40 TO STA. 31+60
- STA. 48+05 TO STA. 65+35
- STA. 73+40 TO STA. 86+35
- STA. 91+60 TO STA. 104+30
- STA. 111+95 TO STA. 120+15
- STA. 122+95 TO STA. 135+10



TYPICAL SECTION NO. 4

TO BE USED ON MAPS 3 AND 4

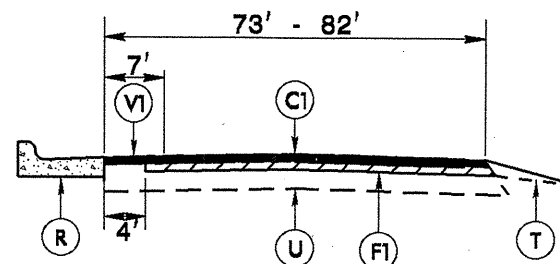
- MAP 3: STA. 0+00 TO STA. 0+75
- STA. 31+00 TO STA. 33+60
- MAP 4: STA. 38+20 TO STA. 48+05
- STA. 104+30 TO STA. 105+15
- STA. 111+40 TO STA. 111+95
- STA. 135+10 TO STA. 140+75



TYPICAL SECTION NO. 2

TO BE USED ON MAP 2

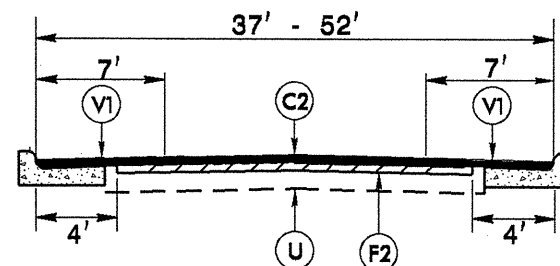
- MAP 2: STA. 0+00 TO STA. 27+60



TYPICAL SECTION NO. 5

TO BE USED ON MAPS 3 AND 4

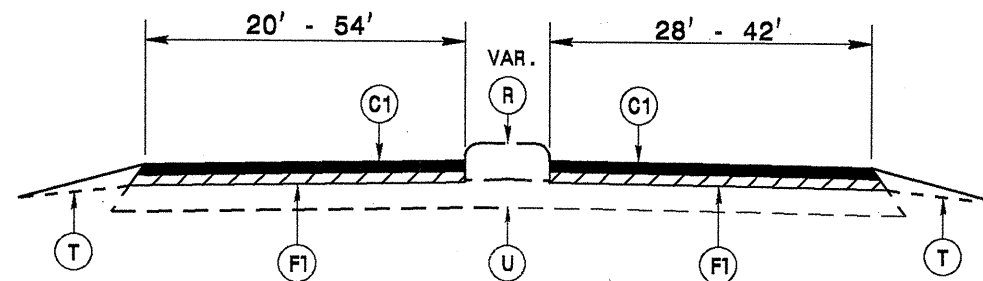
- MAP 3: STA. 0+75 TO STA. 14+30
- MAP 4: STA. 65+35 TO STA. 67+20
- STA. 70+30 TO STA. 73+40
- STA. 86+35 TO STA. 91+60



TYPICAL SECTION NO. 3

TO BE USED ON MAP 2

- STA. 27+60 TO STA. 108+15



**NOTE: NO PAVEMENT ON BRIDGE #151
 STA. 13+15 TO STA. 15+40

TYPICAL SECTION NO. 6

TO BE USED ON MAP 4

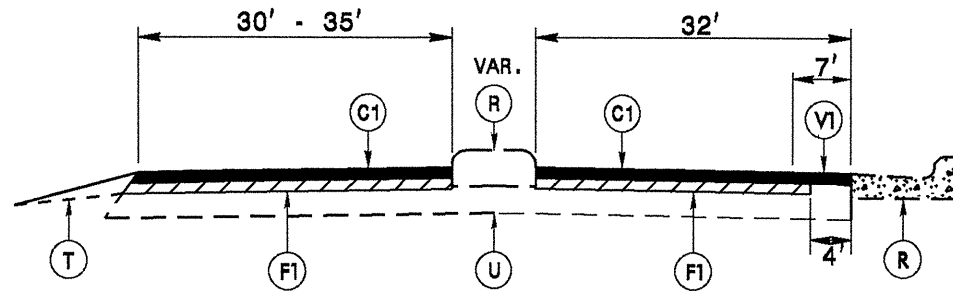
- STA. 5+40 TO STA. 10+15
- STA. 11+35 TO STA. 17+25
- STA. 18+00 TO STA. 30+40
- STA. 31+60 TO STA. 35+35

PAVEMENT SCHEDULE

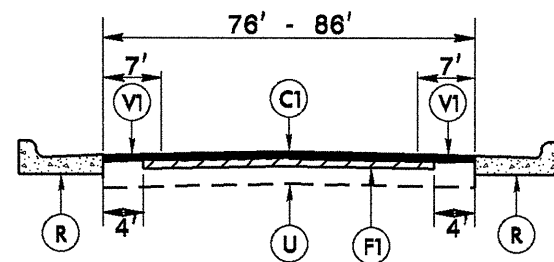
C1	PROP. APPROX. 1½" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
C2	PROP. APPROX. 1¼" ASPHALT CONCRETE SURFACE COURSE, TYPE SF9.5A, AT AN AVERAGE RATE OF 137.5 LBS. PER SQ. YD.
C3	PROP. APPROX. 1½" ASPHALT CONCRETE SURFACE COURSE, TYPE SF9.5A, AT AN AVERAGE RATE OF 165 LBS. PER SQ. YD.
F1	AST MAT COAT #67 STONE
F2	AST MAT COAT, #78M STONE
R	EXISTING CONCRETE STRUCTURE
T	INCIDENTAL STONE BASE IN LOW SHOULDER AREAS, AS DIRECTED BY THE ENGINEER.
U	EXISTING PAVEMENT.
V1	0 - 1½" MILLING

7CR.10791.48
7CR.20791.48
7CR.10411.48

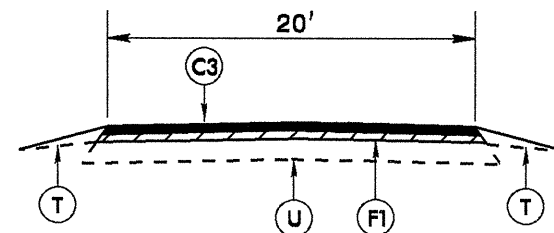
STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
N.C.	7CR.10791.48, ETC	3	



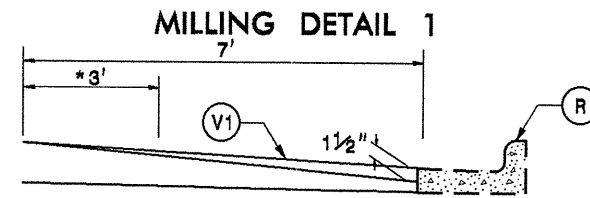
TYPICAL SECTION NO. 7
TO BE USED ON MAP 4
STA. 35+35 TO STA. 38+20



TYPICAL SECTION NO. 8
TO BE USED ON MAP 4
STA. 87+20 TO STA. 70+30
STA. 105+15 TO STA. 111+40
STA. 120+15 TO STA. 122+95

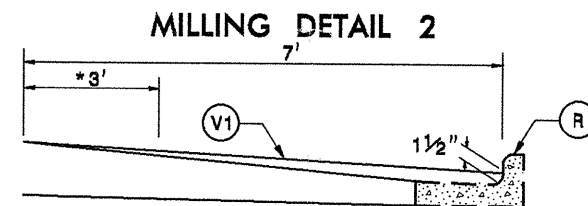


TYPICAL SECTION NO. 9
TO BE USED ON MAP 5



MILLING DETAIL 1
PROFILE MILLING 0 - 1 1/2"
*IF 78M OR #67 SEAL IS INVOLVED, OVERLAP 3'.
PROFILE MILL EXISTING ASPHALT PAVEMENT 0-1 1/2" AT LOCATIONS AS DIRECTED BY THE ENGINEER.

- NOTE: TO BE USED IN CONJUNCTION WITH:
- TS. NO. 2 ON MAP 2 STA. 0+00 TO STA. 27+60 RT & LT
 - TS. NO. 4 ON MAP 3 STA. 0+00 TO STA. 0+75 RT
 - TS. NO. 4 ON MAP 3 STA. 31+00 TO STA. 33+60 RT
 - TS. NO. 4 ON MAP 4 STA. 38+20 TO STA. 48+05 RT
 - TS. NO. 4 ON MAP 4 STA. 104+30 TO STA. 105+15 RT
 - TS. NO. 4 ON MAP 4 STA. 111+40 TO STA. 111+95 RT
 - TS. NO. 4 ON MAP 4 STA. 135+10 TO STA. 140+75 RT
 - TS. NO. 5 ON MAP 3 STA. 0+75 TO STA. 14+30 LT
 - TS. NO. 5 ON MAP 4 STA. 65+35 TO STA. 67+20 LT
 - TS. NO. 5 ON MAP 4 STA. 70+30 TO STA. 73+40 LT
 - TS. NO. 5 ON MAP 4 STA. 86+35 TO STA. 91+60 LT
 - TS. NO. 7 ON MAP 4 STA. 35+35 TO STA. 38+20 RT
 - TS. NO. 8 ON MAP 4 STA. 67+20 TO STA. 70+30 RT & LT
 - TS. NO. 8 ON MAP 4 STA. 105+15 TO STA. 111+40 RT & LT
 - TS. NO. 8 ON MAP 4 STA. 120+15 TO STA. 122+95 RT & LT



MILLING DETAIL 2
PROFILE MILLING 0 - 1 1/2"
*IF 78M OR #67 SEAL IS INVOLVED, OVERLAP 3'.
PROFILE MILL EXISTING ASPHALT PAVEMENT 0-1 1/2" AT LOCATIONS AS DIRECTED BY THE ENGINEER.
NOTE: TO BE USED IN CONJUNCTION WITH:
TS. NO. 3 ON MAP 2 STA. 27+60 TO STA. 108+15 RT & LT

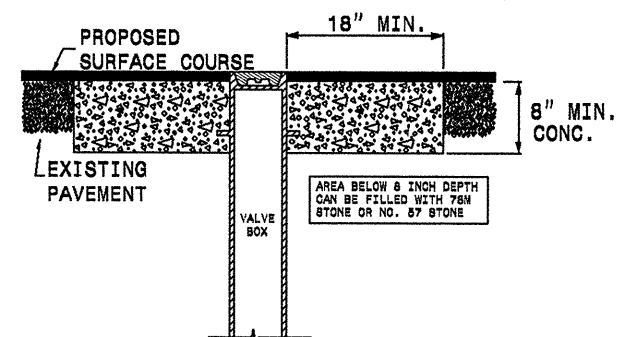
PAVEMENT SCHEDULE

C1	PROP. APPROX. 1 1/2" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
C2	PROP. APPROX. 1 1/4" ASPHALT CONCRETE SURFACE COURSE, TYPE SF9.5A, AT AN AVERAGE RATE OF 137.5 LBS. PER SQ. YD.
C3	PROP. APPROX. 1 1/2" ASPHALT CONCRETE SURFACE COURSE, TYPE SF9.5A, AT AN AVERAGE RATE OF 165 LBS. PER SQ. YD.
F1	AST MAT COAT #67 STONE
F2	AST MAT COAT, #78M STONE
R	EXISTING CONCRETE STRUCTURE
T	INCIDENTAL STONE BASE IN LOW SHOULDER AREAS, AS DIRECTED BY THE ENGINEER.
U	EXISTING PAVEMENT.
V1	0 - 1 1/2" MILLING

7CR.10791.48
7CR.20791.48
7CR.10411.48

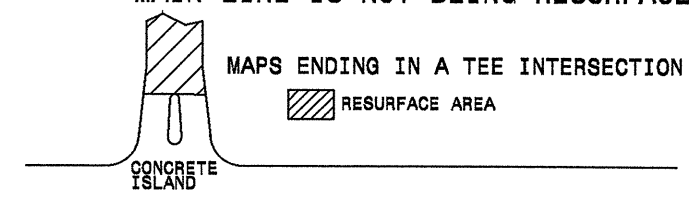
STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
N.C.	7CR.10791.48, ETC	4	

STANDARD CONCRETE ENCASEMENT FOR VALVE CASTINGS IN PAVEMENT



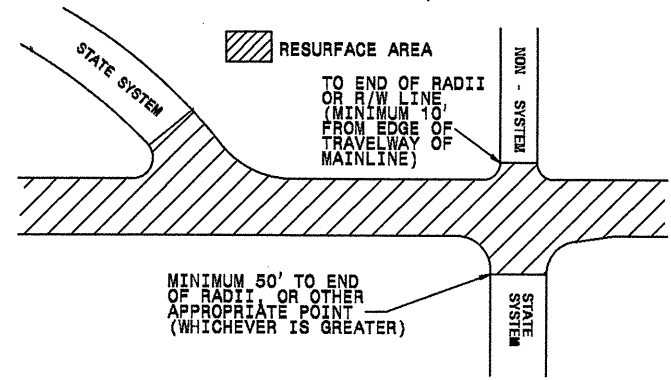
USE RAPID SET GROUT, MORTAR, OR CONCRETE
CLASS B CONCRETE MAY BE USED WHEN ADJUSTMENTS
ARE NOT IN THE TRAVEL LANE.

PAVING DETAIL 1 MAIN LINE IS NOT BEING RESURFACED

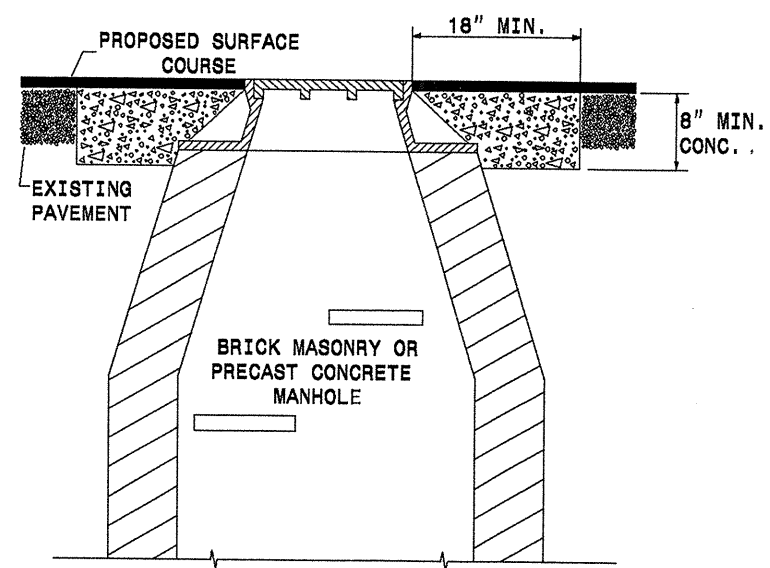
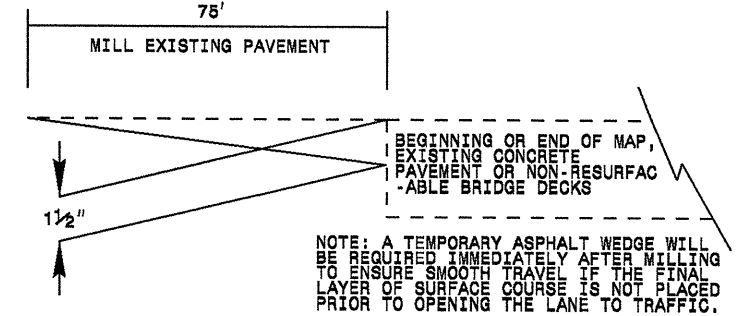


PAVING DETAIL 2 MAIN LINE IS BEING RESURFACED

NOTE: NON-SYSTEM (CITY STREET, PRIVATE DRIVE,
SCHOOL BUS DRIVE)

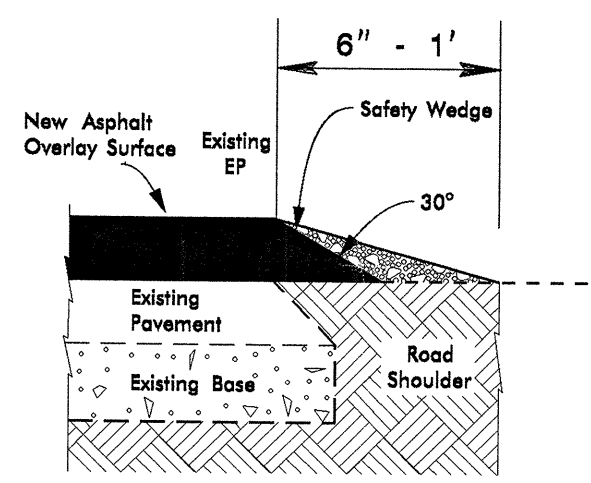


INCIDENTAL MILLING DETAIL



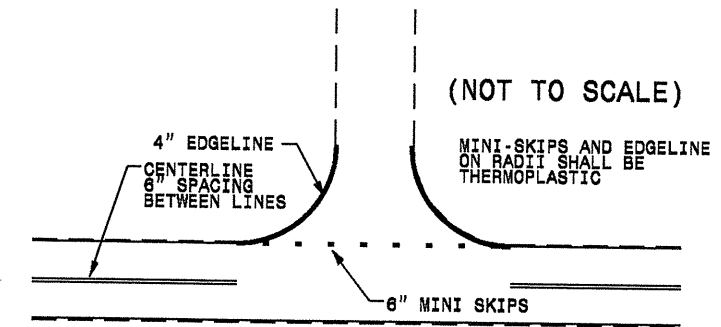
- NOTES:
1. MORTAR SHALL BE MIXED TO NCDOT SPECIFICATIONS.
 2. ALL FAULTY EXISTING BRICKWORK TO BE REMOVED AND REPLACED WITH NEW BRICK MASONRY.
 3. EXCAVATION FOR THE ADJUSTMENT SHALL BE SHEER CUT ON ALL SIDES.
 4. USE RAPID SET GROUT, MORTAR OR CONCRETE AS NOTED IN PROJECT SPECIAL PROVISIONS. CLASS B CONCRETE MAY BE USED WHEN THE ADJUSTMENTS ARE NOT IN THE TRAVEL LANE.

INCIDENTAL STONE SHOULDER DETAIL WITH SHOULDER WEDGE



NOTE: ASB SHOULD BE PLACED AT THE DISCRETION OF THE ENGINEER
NEW ASPHALT OVERLAY SURFACE CAN VARY IN DEPTH

TO BE USED AT ALL NON-SIGNALIZED INTERSECTIONS



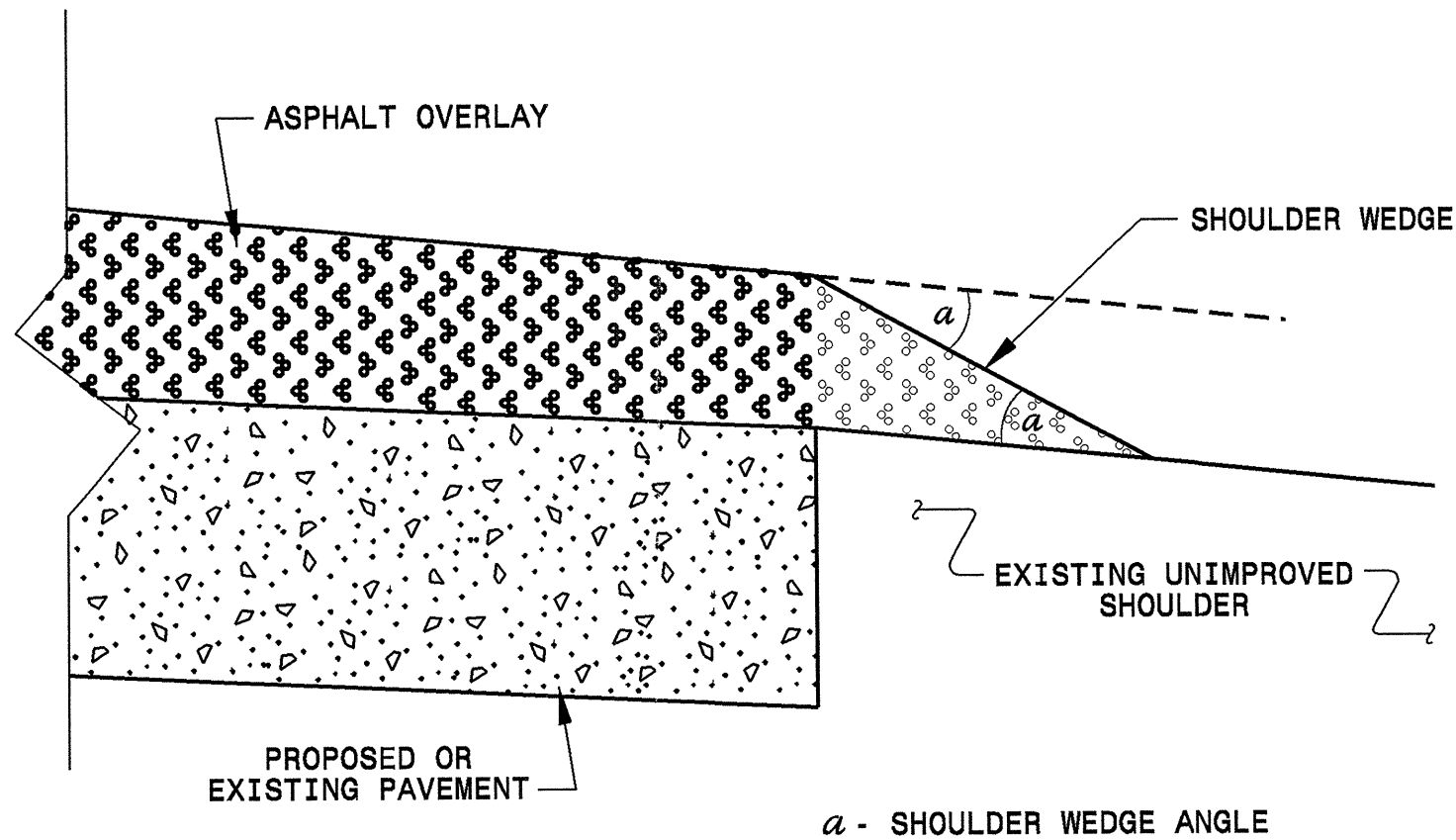
NOTE: MINI SKIPS SHALL BE PLACED ON A 8' CYCLE, CONTAINING AN 8' AND 2' SKIP, THE WIDTH OF THE SKIP SHALL BE 6'.

PAVEMENT SCHEDULE

C1	PROP. APPROX. 1 1/2" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
C2	PROP. APPROX. 1 1/4" ASPHALT CONCRETE SURFACE COURSE, TYPE SF9.5A, AT AN AVERAGE RATE OF 137.5 LBS. PER SQ. YD.
C3	PROP. APPROX. 1 1/2" ASPHALT CONCRETE SURFACE COURSE, TYPE SF9.5A, AT AN AVERAGE RATE OF 165 LBS. PER SQ. YD.
F1	AST MAT COAT #67 STONE
F2	AST MAT COAT, #78M STONE
R	EXISTING CONCRETE STRUCTURE
T	INCIDENTAL STONE BASE IN LOW SHOULDER AREAS, AS DIRECTED BY THE ENGINEER.
U	EXISTING PAVEMENT.
V1	0 - 1 1/2" MILLING

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PROJECT REFERENCE NO.	SHEET NO.
7CR.10791.48, ETC	5



SHOULDER WEDGE DETAIL

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 P:\porter

CONTRACT STANDARDS AND DEVELOPMENT UNIT Office 919-707-6950 FAX 919-260-4119	
SHOULDER WEDGE DETAIL	
ORIGINAL BY: T.SPELL	DATE: 7-19-11
MODIFIED BY:	DATE:
CHECKED BY:	DATE:
FILE SPEC: s:\user\detail\stand\shoulderwedge\std1.dgn	

PROJECT NO.	SHEET NO.	TOTAL NO.
7CR.10791.48, 7CR.20791.48 7CR.10411.48,	6	

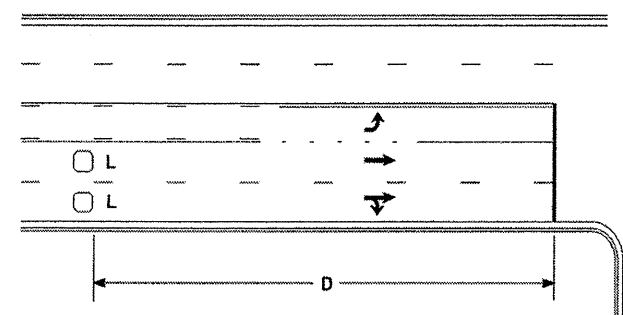
SUMMARY OF QUANTITIES

PROJECT NO.	COUNTY	MAP NO.	ROUTE	DESCRIPTION	TYP	FINAL SURFACE TESTING REQUIRED	AGGREGATE SHOULDER BORROW ALLOWED	LENGTH MI	WIDTH FT	INCIDENTAL STONE BASE TONS	MILLING ASPHALT PAVEMENT, 0" TO 1-1/2" DEPTH SY	INCIDENTAL MILLING SY	ASPHALT CONC SURFACE COURSE, TYPE S9.5B TONS	ASPHALT CONC SURFACE COURSE, TYPE SF9.5A TONS	ASPHALT BINDER FOR PLANT MIX TONS	ASPHALT SURFACE TREATMENT, MAT COAT, #78M STONE SY	ASPHALT SURFACE TREATMENT, MAT COAT, #67 STONE SY	RETROFIT EXISTING CURB RAMPS EA	ADJ. OF DROP INLET EA	ADJ. OF MANHOLES EA	ADJ. OF METER BOXES OR VALVE BOXES EA	TRENCHING (PAVED) (1)(2") LF	TRENCHING (UNPAVED) (1)(2") LF	JUNCTION BOX (STANDARD SIZE) EA	2" RISER W/ WEATHERHEAD EA	INDUCTIVE LOOP SAW CUT LF	LEAD-IN CABLE (14-2) LF							
7CR.10411.48	Guilford	6	NC 68	FROM JOINT 550' NORTH OF NC 65 WEST - 25.39 TO ROCKINGHAM COUNTY LINE - 26.20	1	NO	YES	0.047	30-36	78		275	77		5		910																	
					1	NO	YES	0.042	36-53							93		6		1,109														
					1	NO	YES	0.034	53								99		6		1,057													
					1	NO	YES	0.086	41								174		10		2,069													
					1	NO	YES	0.045	41-52								105		6		1,241													
					1	NO	YES	0.035	52								100		6		1,068													
					1	NO	YES	0.048	41								97		6		1,155													
					1	NO	YES	0.048	28-41								83		5		986													
					1	NO	YES	0.426	28								640		38		6,998													
					TOTAL FOR MAP NO. 6								0.811		78		275	1,468		88		16,593												
TOTAL FOR PROJ NO. 7CR.10411.48								0.811		78		275	1,468		88		16,593																	
7CR.10791.48	Rockingham	1	NC 68	FROM GUILFORD COUNTY LINE - 0.00 TO JOINT JUST NORTH OF SR 1105 (LEMONS LOOP)/SR 1195 (SYLVANIA ROAD) - 1.75	1	NO	YES	1.745	28	136		233	2,559		154		28,665																	
					TOTAL FOR MAP NO. 1								1.745		136		233	2,559		154		28,665												
		2	US 220 BUSINESS	FROM SR 1199 (MARKET STREET) - 1.60 TO NON-SYSTEM (ADAMS STREET) - 3.65	2	NO	YES	0.488	40			4,008	333		869	58	9,156		8		6	6												
					2	NO	YES	0.035	40-41				287			68	5	668		2		2	1											
					3	NO	YES	0.037	45-48				304			70	5	834			1													
					3	NO	YES	0.242	48				1,988			540	36	5,689		3		6	5		200	4	2	278	300					
					3	NO	YES	0.842	52				6,916			2,109	141	21,731		8		15	12		200	200	7	3	1,300	600				
					3	NO	YES	0.087	49-52				715			179	12	2,172				2												
					3	NO	YES	0.141	49				1,158			339	23	3,394				2				160	5	2	600	250				
					3	NO	YES	0.106	37-49				871			204	14	2,178		2		2	2											
					3	NO	YES	0.07	37				575	308		105	7	1,192																
					TOTAL FOR MAP NO. 2								2.048				16,822	641		4,483	301	47,014		23	1	35	26	200	560	16	7	2,178	1,150	
		3	NC 14	FROM SR 1985 (WILSON ROAD) - 3.25 TO 400' NORTH OF US 29 BUSINESS - 2.51	4	NO	YES	0.014	73	29		57	608		50		3		575															
					5	NO	YES	0.028	82				115			133		8		1,300														
					5	NO	YES	0.027	73-82				111			104		6		1,184														
					5	NO	YES	0.062	73				255			223		13		2,492														
					5	NO	YES	0.03	73-82				123			116		7		1,307														
					5	NO	YES	0.035	82				144			162		10		1,603						900	7	2	900	1,150				
					5	NO	YES	0.074	73				304			267		16		2,990														
					1	NO	YES	0.02	66-73							69		4		821														
1	NO				YES	0.027	66-78							96		6		1,141																
1	NO				YES	0.057	78							219		13		2,608																
1	NO				YES	0.123	66							401		24		4,763																
1	NO				YES	0.026	66-78							92		6		1,098																
1	NO				YES	0.064	78							246		15		2,929																
4	NO				YES	0.049	73					201		177		11		1,993																
1	NO				YES	0.014	66							46		3		542																
1	NO				YES	0.015	66-78							53		3		634																
1	NO				YES	0.078	78							650		18		3,569																
TOTAL FOR MAP NO. 3								0.743		29		1,310	1,258		2,754		166		31,549					900	7	2	900	1,150						

SUMMARY OF QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	FINAL SURFACE TESTING REQUIRED	AGGREGATE SHOULDER BORROW ALLOWED	LENGTH MI	WIDTH FT	INCIDENTAL STONE BASE TONS	MILLING ASPHALT PAVEMENT, 0" TO 1-1/2" DEPTH SY	INCIDENTAL MILLING SY	ASPHALT CONC SURFACE COURSE, TYPE S9.5B TONS	ASPHALT CONC SURFACE COURSE, TYPE SF9.5A TONS	ASPHALT BINDER FOR PLANT MIX TONS	ASPHALT SURFACE TREATMENT, MAT COAT, #78M STONE SY	ASPHALT SURFACE TREATMENT, MAT COAT, #67 STONE SY	RETROFIT EXISTING CURB RAMPS EA	ADJ. OF DROP INLET EA	ADJ. OF MANHOLES EA	ADJ. OF METER BOXES OR VALVE BOXES EA	TRENCHING (PAVED) (1) (2") LF	TRENCHING (UNPAVED) (1) (2") LF	JUNCTION BOX (STANDARD SIZE) EA	2" RISER W/ WEATHERHEAD EA	INDUCTIVE LOOP SAW CUT LF	LEAD-IN CABLE (14 2) LF							
7CR.10791.48	Rockingham	4	US 158 / NC 14	FROM SR 2823 (BROOKS ROAD) - 20.60 TO 50' EAST OF RADIUS OF US 29 BUSINESS (FREEWAY DRIVE) - 17.93	1	NO	YES	0.041	32	107			85		5		770																	
					1	NO	YES	0.062	32-50							126		8		1,491														
					6	NO	YES	0.032	49-55							82		5		976														
					6	NO	YES	0.058	55								158		9		1,872													
					1	NO	YES	0.023	73								123		7		985													
					6	NO	YES	0.034	70								583		7		1,396													
					6	NO		0.043	70																									
					6	NO	YES	0.035	70								583		7		1,437													
					1	NO	YES	0.014	87										6		715													
					6	NO	YES	0.149	70										31		6,119													
					6	NO	YES	0.037	70-83										8		1,671													
					6	NO	YES	0.02	83										5		974													
					6	NO	YES	0.029	74										8		1,259													
					1	NO	YES	0.023	87										11		1,174													
					6	NO	YES	0.037	82										9		1,780													
					6	NO	YES	0.023	62-82										5		972													
					6	NO	YES	0.011	62-64										2		407													
					7	NO	YES	0.054	62-67										10		1,916													
					4	NO	YES	0.187	71										39		7,333													
					1	NO	YES	0.019	66-71										4		769													
					1	NO	YES	0.301	66										59		11,655													
					1	NO	YES	0.008	66-73										2		329													
					5	NO	YES	0.023	71									94		81		893												
					5	NO	YES	0.012	71-78										3		509													
					8	NO	YES	0.059	76										13		2,342													
					5	NO	YES	0.01	71-78										2		431													
					5	NO	YES	0.029	71										6		1,154													
					5	NO	YES	0.019	71-79										4		789													
					1	NO	YES	0.01	74-81										2		458													
					1	NO	YES	0.015	74-77										3		669													
					1	NO	YES	0.025	77										8		1,129													
					1	NO	YES	0.054	66-77										12		2,281													
					1	NO	YES	0.129	66										25		4,995													
					1	NO	YES	0.012	66-73										2		493													
					5	NO	YES	0.099	71										21		3,908													
					1	NO	YES	0.013	66-73										3		534													
					1	NO	YES	0.212	66										41		8,209													
					1	NO	YES	0.015	66-73										3		616													
					4	NO	YES	0.016	71-79										4		671													
					8	NO	YES	0.079	77										18		3,182													
					8	NO	YES	0.04	77-85										10		1,703													
					4	NO	YES	0.01	82-87										3		492													
					1	NO	YES	0.037	82										13		1,780													
					1	NO	YES	0.095	66										19		3,678													
					1	NO	YES	0.024	66-79										5		1,028													
8	NO	YES	0.025	79										6		1,026																		
8	NO	YES	0.028	79-86										7		1,242																		
1	NO	YES	0.012	83-86										3		598																		
1	NO	YES	0.025	94										14		1,379																		
1	NO	YES	0.007	75										2		308																		
1	NO	YES	0.028	67-75										6		1,166																		
1	NO	YES	0.146	67										29		5,739																		
1	NO	YES	0.012	67-73										2		493																		
4	NO	YES	0.03	71										6		1,191																		
4	NO	YES	0.047	82-85										12		2,208																		
4	NO	YES	0.029	82										7		1,343																		
TOTAL FOR MAP NO. 4								2.666		107			4,218	1,849	9,426		104,637																	
TOTAL FOR PROJ NO. 7CR.10791.48								7.202		272			22,350	3,981	14,739	4,483	1,187																	
7CR.20791.48	Rockingham	5	SR 2023 (GUERRANT SPRINGS ROAD)	FROM SR 1767 (MAYFIELD ROAD) - 1.00 TO SR 1914 (OREGON HILL ROAD) - 6.59	9	NO.	YES	5.591	20	539				5,626	377		65,601																	
TOTAL FOR MAP NO. 5								5.591		539				5,626	377		65,601																	
TOTAL FOR PROJ NO. 7CR.20791.48								5.591		539				5,626	377		65,601																	
GRAND TOTAL								13.604		889			22,350	4,256	16,207	10,109	1,652		47,014	247,045	23	1	35	26	200	3,510	43	15	5,328	5,700				

High Speed Detection [≥40 mph (64 km/hr)]

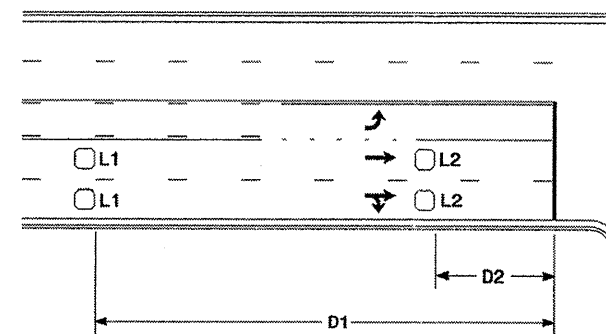


Speed Limit mph (km/hr)	D ft (m)
40 (64)	250 (75)
45 (72)	300 (90)
50 (80)	355 (110)
55 (88)	420 (130)

L = 6ft X 6ft (1.8m X 1.8m)
Wired in series for TS1
Controllers
Wired separately for TS2,
170, and 2070L Controllers

Volume Density Operation

OR

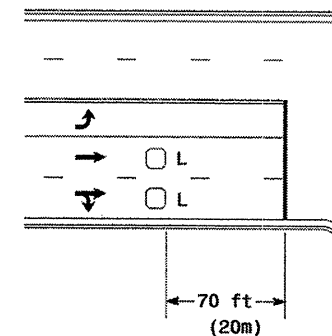


Speed Limit mph (km/hr)	D1 ft (m)	D2 ft (m)
40 (64)	250 (75)	80 (25)
45 (72)	300 (90)	90 (27)
50 (80)	355 (110)	100 (30)
55 (88)	420 (130)	110 (35)

L1 = 6ft X 6ft
(1.8m X 1.8m)
Wired in series
L2 = 6ft X 6ft
(1.8m X 1.8m)
Wired in series

"Stretch" Operation

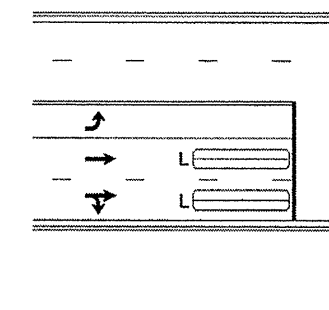
Low Speed Detection [≤35 mph (56 km/hr)]



L = 6ft X 6ft (1.8m X 1.8m)
Wired in series

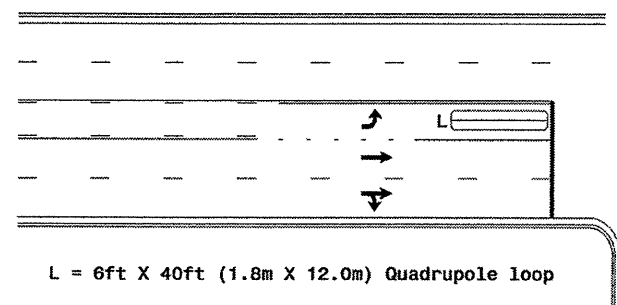
70 ft (20m)

OR



L = 6ft X 40ft (1.8m X 12.0m)
Quadrupole loop, wired separately

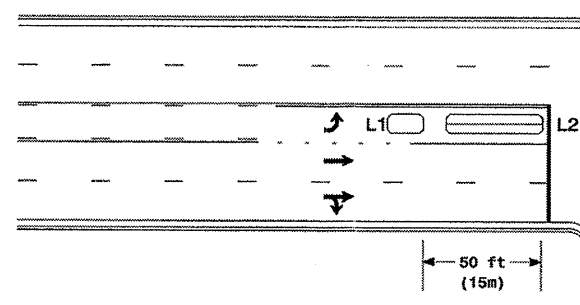
Left Turn Lane Detection



L = 6ft X 40ft (1.8m X 12.0m) Quadrupole loop

Presence Loop Detection

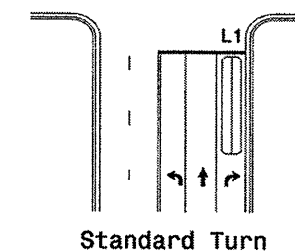
OR



L1 = 6ft X 15ft (1.8m X 4.6m) Queue detector
L2 = 6ft X 40ft (1.8m X 12.0m) Quadrupole loop

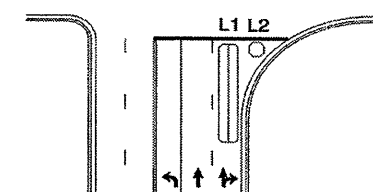
Queue Loop Detection

Right Turn Lane Detection

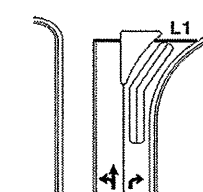


Standard Turn

L1 = 6ft X 40ft (1.8m X 12.0m) Quadrupole loop
L2 = 6ft X 6ft (1.8m X 1.8m) [Minimum] Presence loop
Wired separately
L3 = 6ft X 20ft (1.8m X 6.0m) Quadrupole loop
Wired in series

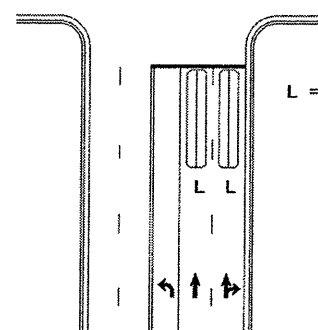


Wide Radius Turn



Channelized Turn

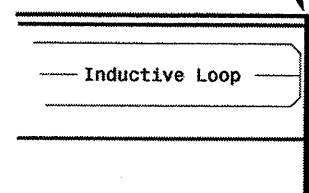
Side Street Detection



L = 6ft X 40ft (1.8m X 12.0m)
Quadrupole loop
Wired to separate
detectors/channels

Presence Loop Placement at Stop Lines

Locate loop slightly
behind leading
edge of stop line



Note:
Loop may be located in advance
of stop line when stop line is
greater than 15' (4.5m) from edge
of intersecting roadway; or, when
loop detects a permissive or
protected/permissive left turn.

Recommended Number of Turns

Single 6' X 6' (1.8m X 1.8m)
loop (wired separately):

Length of Lead-in ft (m)	Number of Turns
< 250 (75)	3
250-375 (75-115)	4
375-525 (115-160)	5
> 525 (160)	6

Quadrupole loops: Use 2-4-2 turns
6' X 15' (1.8m X 4.6m) Loops:
Lead-in < 150' (45 m), use 2 turns
Lead-in > 150' (45 m), use 3 turns

	<p>Typical Loop Locations</p>		
	<p>PLAN DATE: June 2006</p> <p>PREPARED BY: P L Alexander</p>	<p>REVIEWED BY:</p> <p>REVIEWED BY:</p>	