



# GENERAL NOTES

PROJ. REFERENCE NO.	SHEET NO.
B-4291	TCP-2

CHANGES MAY BE REQUIRED WHEN PHYSICAL DIMENSIONS IN THE DETAIL DRAWINGS, STANDARD DETAILS AND ROADWAY DETAILS ARE NOT ATTAINABLE TO MEET FIELD CONDITIONS, OR RESULT IN DUPLICATE, OR UNDESIRED OVERLAPPING OF DEVICES. MODIFICATION MAY INCLUDE: MOVING, SUPPLEMENTING, COVERING OR REMOVAL OF DEVICES, AS DIRECTED BY THE ENGINEER.

THE FOLLOWING GENERAL NOTES APPLY AT ALL TIMES FOR THE DURATION OF THE CONSTRUCTION PROJECT EXCEPT WHEN OTHERWISE NOTED IN THE PLAN OR AS DIRECTED BY THE ENGINEER.

TIME RESTRICTIONS

A) DO NOT STOP TRAFFIC AS FOLLOWS:

ROAD NAME	DAY AND TIME RESTRICTIONS	DURATION AND OPERATION
1. ALL ROADS	NONE	30 MINUTES MAX./ TRAFFIC OPS/GIRDER PLACEMENT

B) DO NOT CONDUCT ANY HAULING OPERATIONS AGAINST THE FLOW OF TRAFFIC OF AN OPEN TRAVELWAY UNLESS THE HAULING OPERATION IS PROTECTED BY BARRIER OR GUARDRAIL OR AS DIRECTED BY THE ENGINEER.

LANE AND SHOULDER CLOSURE REQUIREMENTS

C) REMOVE LANE CLOSURE DEVICES FROM THE LANE WHEN WORK IS NOT BEING PERFORMED BEHIND THE LANE CLOSURE OR WHEN A LANE CLOSURE IS NO LONGER NEEDED, OR AS DIRECTED BY THE ENGINEER.

D) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN 15 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN SHOULDER USING ROADWAY STANDARD DRAWING NO. 1101.04 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL OR A LANE CLOSURE IS INSTALLED.

E) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING ON THE SHOULDER ADJACENT TO AN UNDIVIDED FACILITY AND WITHIN 5 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN TRAVEL LANE USING ROADWAY STANDARD DRAWING NO. 1101.02 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.

WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING ON THE SHOULDER ADJACENT TO A DIVIDED FACILITY AND WITHIN 10 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN TRAVEL LANE USING ROADWAY STANDARD DRAWING NO. 1101.02 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.

F) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN A LANE OF TRAVEL OF AN UNDIVIDED OR DIVIDED FACILITY, CLOSE THE LANE ACCORDING TO THE TRAFFIC CONTROL PLANS, ROADWAY STANDARD DRAWINGS OR AS DIRECTED BY THE ENGINEER. CONDUCT THE WORK SO THAT ALL PERSONNEL AND/OR EQUIPMENT REMAIN WITHIN THE CLOSED TRAVEL LANE.

G) DO NOT WORK SIMULTANEOUSLY WITHIN 15 FT ON BOTH SIDES OF AN OPEN TRAVELWAY, RAMP OR LOOP WITHIN THE SAME LOCATION UNLESS PROTECTED WITH GUARDRAIL OR BARRIER.

H) PROVIDE TRAFFIC CONTROL FOR APPROPRIATE LANE CLOSURES FOR SURVEYING DONE BY THE DEPARTMENT.

PAVEMENT EDGE DROP OFF REQUIREMENTS

I) BACKFILL AT A 6:1 SLOPE UP TO THE EDGE AND ELEVATION OF EXISTING PAVEMENT IN AREAS ADJACENT TO AN OPENED TRAVEL LANE THAT HAS A DROP-OFF AS FOLLOWS:

BACKFILL DROP-OFFS THAT EXCEED 2 INCHES ON ROADWAYS WITH POSTED SPEED LIMITS OF 45 MPH OR GREATER.

BACKFILL DROP-OFFS THAT EXCEED 3 INCHES ON ROADWAYS WITH POSTED SPEED LIMITS LESS THAN 45 MPH.

BACKFILL WITH SUITABLE COMPACTED MATERIAL, AS APPROVED BY THE ENGINEER, AT NO EXPENSE TO THE DEPARTMENT.

J) DO NOT EXCEED A DIFFERENCE OF 2 INCHES IN ELEVATION BETWEEN OPEN LANES OF TRAFFIC FOR NOMINAL LIFTS OF 1.5 INCHES. INSTALL ADVANCE WARNING "UNEVEN LANES" SIGNS (W8-11) 200 FT IN ADVANCE AND A MINIMUM OF ONCE EVERY HALF MILE THROUGHOUT THE UNEVEN AREA.

TRAFFIC PATTERN ALTERATIONS

K) NOTIFY THE ENGINEER TWENTY ONE (21) CALENDAR DAYS PRIOR TO ANY TRAFFIC PATTERN ALTERATION.

SIGNING

L) INSTALL ADVANCE WORK ZONE WARNING SIGNS WHEN WORK IS WITHIN 40 FT FROM THE EDGE OF TRAVEL LANE AND NO MORE THAN THREE (3) DAYS PRIOR TO THE BEGINNING OF CONSTRUCTION.

M) STATE FORCES WILL BE RESPONSIBLE FOR PROVIDING PERMANENT SIGNING.

N) STATE FORCES WILL BE RESPONSIBLE FOR PROVIDING SIGNING REQUIRED FOR THE OFF-SITE DETOUR ROUTE AS SHOWN IN THE TRAFFIC CONTROL PLANS.

O) STATE FORCES WILL COVER OR REMOVE ALL SIGNS REQUIRED FOR THE OFF-SITE DETOUR WHEN THE DETOUR IS NOT IN OPERATION.

P) ENSURE ALL NECESSARY SIGNING IS IN PLACE PRIOR TO ALTERING ANY TRAFFIC PATTERN.

Q) INSTALL BLACK ON ORANGE "DIP" SIGNS (W8-2) AND/OR "BUMP" SIGNS (W8-1) 200 FT IN ADVANCE OF THE UNEVEN AREA, OR AS DIRECTED BY THE ENGINEER.

TRAFFIC CONTROL DEVICES

R) SPACE CHANNELIZING DEVICES IN WORK AREAS NO GREATER IN FEET THAN TWICE THE POSTED SPEED LIMIT (MPH), EXCEPT 10 FT ON-CENTER IN RADIUS, AND 3 FT OFF THE EDGE OF AN OPEN TRAVELWAY, WHEN LANE CLOSURES ARE NOT IN EFFECT. WHEN SKINNY DRUMS ARE ALLOWED REFER TO SECTION 1180 OF STANDARD SPECIFICATIONS FOR ROADS AND STRUCTURES OR AS SHOWN IN THE PLANS.

S) PLACE TYPE III BARRICADES WITH "ROAD CLOSED" SIGN R11-2 ATTACHED OF SUFFICIENT LENGTH TO CLOSE ENTIRE ROADWAY.

PAVEMENT MARKINGS AND MARKERS

T) INSTALL PAVEMENT MARKINGS AND PAVEMENT MARKERS ON THE FINAL SURFACE AS DESCRIBED ON THE PAVEMENT MARKING PLANS.

U) INSTALL TEMPORARY PAVEMENT MARKINGS AND TEMPORARY PAVEMENT MARKERS ON INTERIM LAYERS OF PAVEMENT AS FOLLOWS:

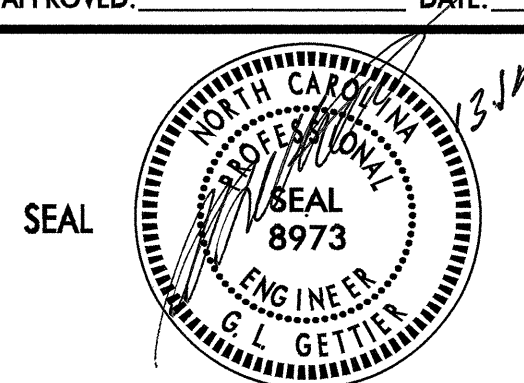
ROAD NAME	MARKING	MARKER
1. ALL ROADS	PAINT	TEMPORARY RAISED

V) PLACE ONE APPLICATION OF PAINT FOR TEMPORARY TRAFFIC PATTERNS. PLACE A SECOND APPLICATION OF PAINT SIX (6) MONTHS AFTER THE INITIAL APPLICATION AND EVERY SIX MONTHS AS DIRECTED BY THE ENGINEER.

W) TIE PROPOSED PAVEMENT MARKING LINES TO EXISTING PAVEMENT MARKING LINES.

X) REMOVE/REPLACE ANY CONFLICTING/DAMAGED PAVEMENT MARKINGS AND MARKERS BY THE END OF EACH DAY'S OPERATION.

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# PHASING

## PHASE I

- STEP 1: - CONTRACTOR SHALL PLACE ADVANCE WORK ZONE WARNING SIGNS ALONG SR 1504 (-L-) AND SR 1518 (-Y-) AS SHOWN ON RSD 1101.01, SHEET 3 OF 3.
- STEP 2: - CONTRACTOR, USING ROADWAY STANDARD DRAWING NO. 1101.02 SHEET 1 OF 15:
- SHALL INSTALL TEMPORARY GUARDRAIL ON THE SOUTH SIDE OF THE EXISTING ROADWAY AS SHOWN ON SHEET TCP-4 AND THE ROADWAY PLANS.
- STEP 3: - CONTRACTOR SHALL:
- CONSTRUCT AS MUCH AS POSSIBLE OF THE PROPOSED -L- ROADWAY UP TO BUT NOT INCLUDING THE FINAL LAYER OF SURFACE COURSE, AND INCLUDING THE PROPOSED STRUCTURE, AWAY FROM TRAFFIC FROM STA. 15+41 +/- -L- TO STA. 22+83 +/- -L- (SEE SHEET TCP-4 AND THE CONSTRUCTION PLANS).

NOTE: UTILIZE DRUMS TO MAINTAIN ACCESS TO DRIVEWAY RIGHT OF STA. 19+50 +/- -L- DURING CONSTRUCTION (SEE SHEET TCP-4).

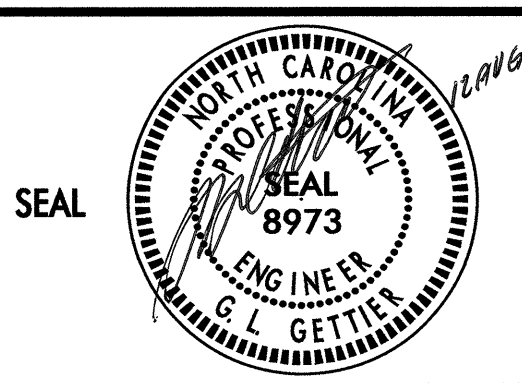
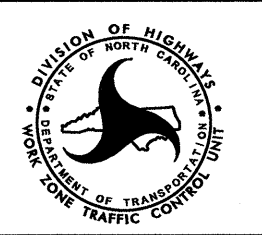
- CONSTRUCT THE PROPOSED -L- ROADWAY UP TO THE EDGE AND ELEVATION OF EXISTING FROM STA. 13+28 -L- TO STA. 15+41 +/- -L- AND STA. 22+83 +/- -L- TO STA. 25+30 -L- USING LANE CLOSURES PER ROADWAY STANDARD DRAWING NO. 1101.02 SHEET 1 OF 15 (SEE SHEET TCP-4 AND THE CONSTRUCTION PLANS).
- COMPLETE CONSTRUCTION OF THE NEW GUARDRAIL (EXCEPT A PORTION OF THE GUARDRAIL LEFT OF STA. 20+75 +/- -L-) USING LANE CLOSURES PER ROADWAY STANDARD DRAWING NO. 1101.02 SHEET 1 OF 15. PLACE A TRUCK MOUNTED IMPACT ATTENUATOR (TMIA) IMMEDIATELY PRIOR TO SHIFTING TRAFFIC TO THE PATTERN SHOWN ON SHEET TCP-5, TO PROTECT THE APPROACH END OF THE BRIDGE.
- BEGIN CONSTRUCTION OF -Y- FROM STA. 12+40 -Y- TO STA. 13+60 +/- -Y- UP TO BUT NOT INCLUDING THE FINAL LAYER OF SURFACE COURSE USING LANE CLOSURES PER ROADWAY STANDARD DRAWING NO. 1101.02, SHEET 1 OF 15 (SEE SHEET TCP-4 AND THE CONSTRUCTION PLANS).

## PHASE II

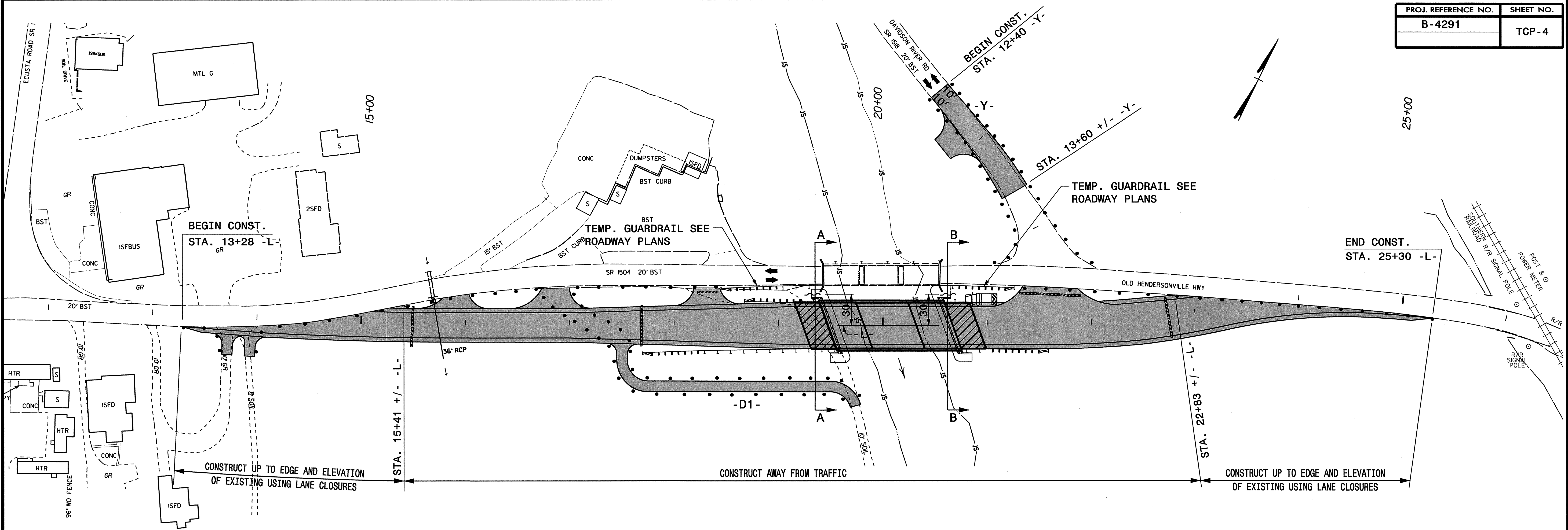
CONTRACTOR SHALL COMPLETE PHASE II, STEPS 1 & 2 IN FIVE CALENDAR DAYS. SEE INTERMEDIATE CONTRACT TIME SPECIAL PROVISION.

- STEP 1: - CONTRACTOR SHALL:
- CONSTRUCT THE -L- TIE-INS USING PAVING AND WEDGING OPERATIONS UP TO BUT NOT INCLUDING THE FINAL LAYER OF SURFACE COURSE FROM STA. 13+28 -L- TO STA. 15+41 +/- -L- AND FROM STA. 22+83 +/- -L- TO STA. 25+30 -L- USING ROADWAY STANDARD DRAWING NO. 1101.02, SHEET 1 OF 15 (SEE SHEET TCP-5 AND THE CONSTRUCTION PLANS).
  - INSTALL TEMPORARY PAVEMENT MARKINGS (PAINT) & TEMPORARY RAISED PAVEMENT MARKERS ON -L- USING ROADWAY STANDARD DRAWING NO. 1101.02, SHEET 1 OF 15 AND SHIFT TRAFFIC TO THE NEW PATTERN AS SHOWN ON SHEET TCP-5. NOTE: INSTALL DRUMS TO KEEP LEFT TURN LANE CLOSED.
- STEP 2: - CONTRACTOR, USING ROADWAY STANDARD NO. 1101.03 SHEETS 1 & 2 OF 9, AND USING THE DETOUR ROUTE SHOWN ON SHEET TCP-5:
- SHALL CONSTRUCT -Y- FROM STA. 12+40 +/- -Y- TO STA. 14+30 +/- -Y- UP TO BUT NOT INCLUDING THE FINAL LAYER OF SURFACE COURSE.
  - SHALL INSTALL TEMPORARY PAVEMENT MARKINGS (PAINT) & TEMPORARY RAISED PAVEMENT MARKERS ON -Y-, REMOVE DRUMS AT -L- TURN LANE THEN OPEN TO THE FINAL TRAFFIC PATTERN.
- STEP 3: - CONTRACTOR, USING ROADWAY STANDARD DRAWING NOS. 1101.02, SHEETS 1 & 2 OF 15, AND/OR 1101.04 SHEET 1 OF 1 (SEE CONSTRUCTION PLANS AND SHEET TCP-5):
- SHALL COMPLETE GUARDRAIL INSTALLATION BEGUN IN PHASE I, STEP 3.
  - SHALL DEMO/REMOVE EXISTING BRIDGE AND ROADWAY AND COMPLETE ANY REMAINING GRADING, DRAINAGE AND LANDSCAPING.
- STEP 4: - CONTRACTOR, USING ROADWAY STANDARD DRAWING NO. 1101.02 SHEETS 1 & 2 OF 15:
- SHALL PAVE THE FINAL LAYER OF SURFACE COURSE ON ALL ROADS, THEN INSTALL FINAL PAVEMENT MARKINGS AND PAVEMENT MARKERS AS DESCRIBED ON THE PAVEMENT MARKING PLANS (SEE CONSTRUCTION PLANS).
- STEP 5: - CONTRACTOR SHALL REMOVE ANY REMAINING TRAFFIC CONTROL DEVICES.

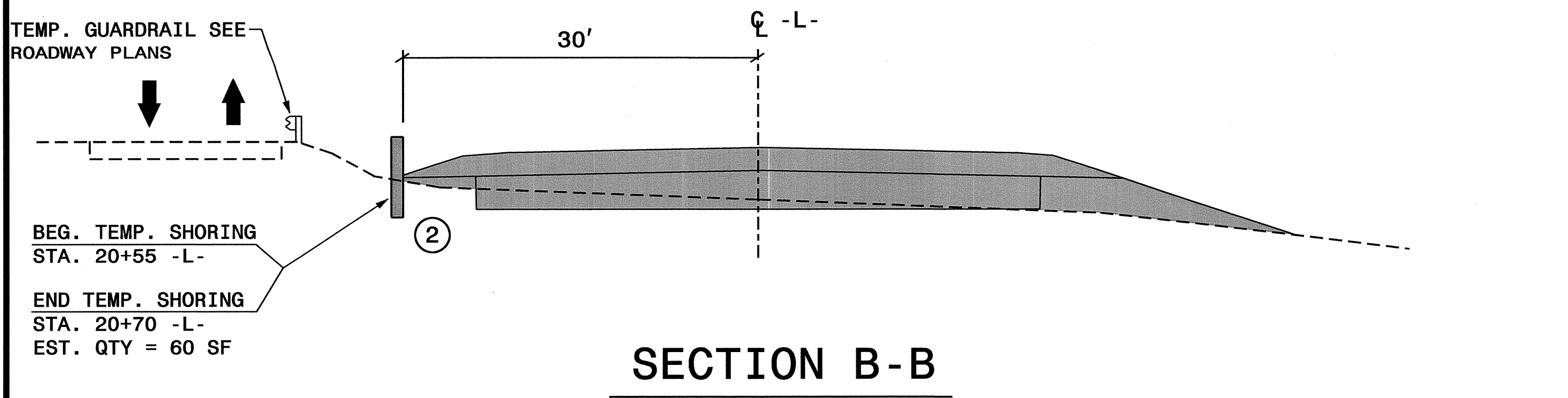
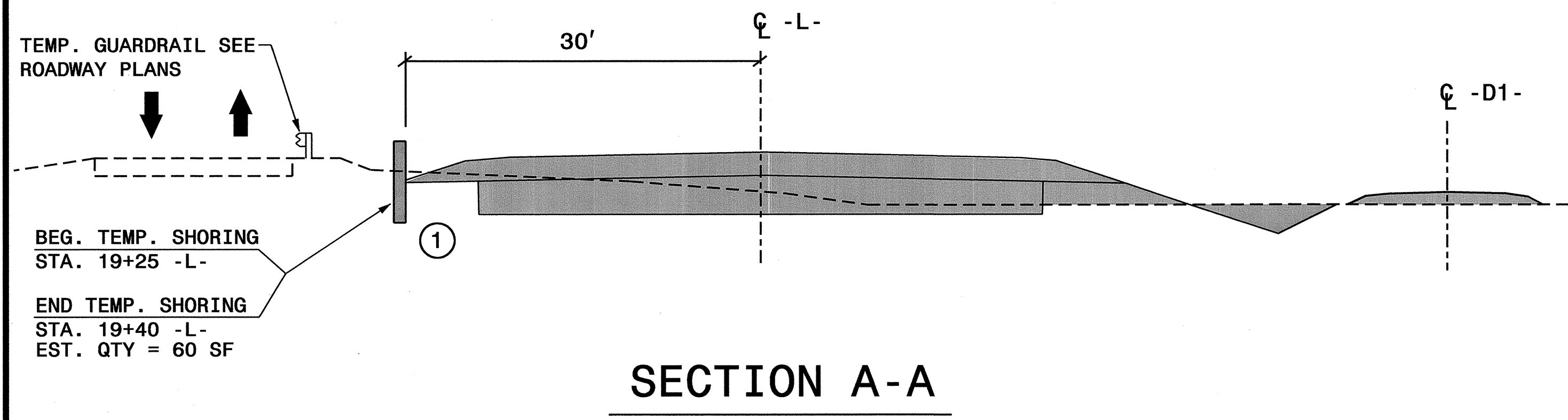
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### TEMPORARY SHORING NOTES



#### Temporary Shoring No. 1

FOR TEMPORARY SHORING AND POSITIVE PROTECTION FOR TEMPORARY SHORING, SEE PLANS AND TEMPORARY SHORING PROVISION.

BEFORE BEGINNING TEMPORARY SHORING DESIGN OR CONSTRUCTION, SURVEY EXISTING GROUND ELEVATIONS IN THE VICINITY OF SHORING LOCATIONS TO DETERMINE ACTUAL SHORING HEIGHTS.

DO NOT USE A TEMPORARY WALL FOR TEMPORARY SHORING FROM STATION 19+25.00+/- -L-, 30 FT. LEFT OF -L-, TO STATION 19+40.00+/- -L-, 30 FT. LEFT OF -L-.

AT THE CONTRACTOR'S OPTION, USE STANDARD TEMPORARY SHORING FOR TEMPORARY SHORING FROM STATION 19+25.00+/- -L-, 30 FT. LEFT OF -L-, TO STATION 19+40.00+/- -L-, 30 FT. LEFT OF -L-. SEE STANDARD DRAWING NO. 1801.01 FOR STANDARD TEMPORARY SHORING.

DESIGN TEMPORARY SHORING FROM STATION 19+25.00+/- -L-, 30 FT. LEFT OF -L-, TO STATION 19+40.00+/- -L-, 30 FT. LEFT OF -L-. FOR THE FOLLOWING ASSUMED SOIL PARAMETERS AND GROUNDWATER ELEVATION:  
 UNIT WEIGHT,  $(\gamma) = 120$  PCF  
 FRICTION ANGLE,  $\phi = 30$  DEGREES  
 COHESION,  $c = 0$  PSF

LIMITED SUBSURFACE INFORMATION IS AVAILABLE IN THE VICINITY OF THE TEMPORARY SHORING FROM STATION 19+25.00+/- -L-, 30 FT. LEFT OF -L-, TO STATION 19+40.00+/- -L-, 30 FT. LEFT OF -L-. THE INFORMATION PROVIDED FOR DESIGN WAS ASSUMED AND MAY NOT BE APPLICABLE TO THE ACTUAL SITE CONDITIONS ENCOUNTERED DURING CONSTRUCTION.

#### Temporary Shoring No. 2

FOR TEMPORARY SHORING AND POSITIVE PROTECTION FOR TEMPORARY SHORING, SEE PLANS AND TEMPORARY SHORING PROVISION.

BEFORE BEGINNING TEMPORARY SHORING DESIGN OR CONSTRUCTION, SURVEY EXISTING GROUND ELEVATIONS IN THE VICINITY OF SHORING LOCATIONS TO DETERMINE ACTUAL SHORING HEIGHTS.

DO NOT USE A TEMPORARY WALL FOR TEMPORARY SHORING FROM STATION 20+55.00+/- -L-, 30 FT. LEFT OF -L-, TO STATION 20+70.00+/- -L-, 30 FT. LEFT OF -L-.

AT THE CONTRACTOR'S OPTION, USE STANDARD TEMPORARY SHORING FOR TEMPORARY SHORING FROM STATION 20+55.00+/- -L-, 30 FT. LEFT OF -L-, TO STATION 20+70.00+/- -L-, 30 FT. LEFT OF -L-. SEE STANDARD DRAWING NO. 1801.01 FOR STANDARD TEMPORARY SHORING.

DESIGN TEMPORARY SHORING FROM STATION 20+55.00+/- -L-, 30 FT. LEFT OF -L-, TO STATION 20+70.00+/- -L-, 30 FT. LEFT OF -L-. FOR THE FOLLOWING ASSUMED SOIL PARAMETERS AND GROUNDWATER ELEVATION:  
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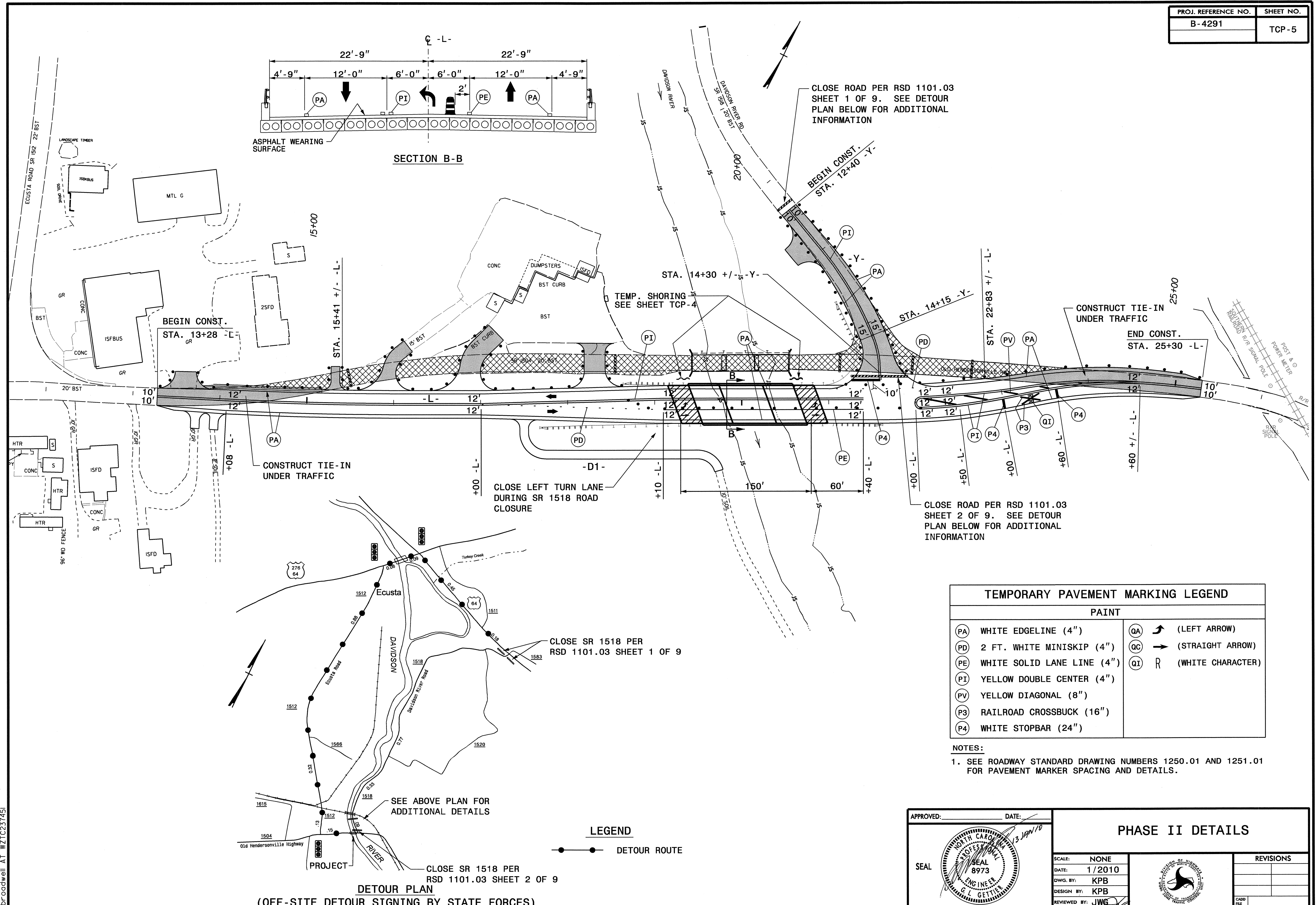
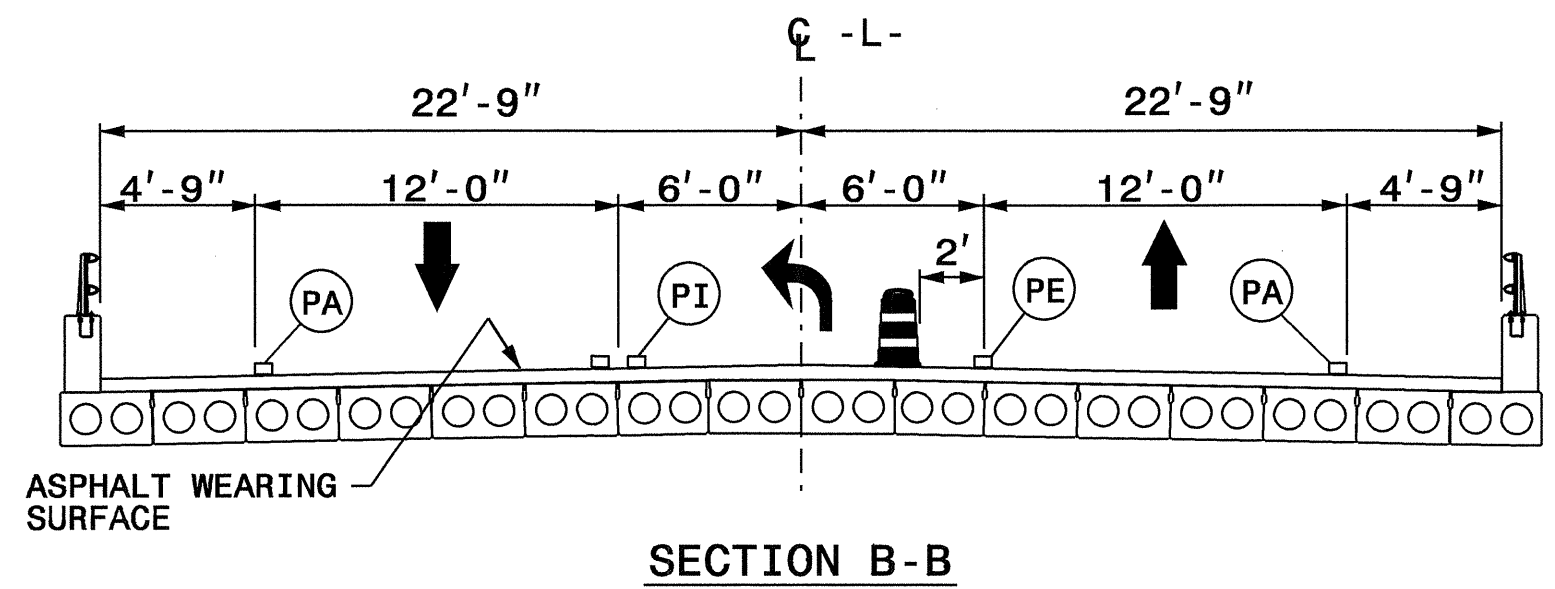
**PHASE I DETAILS**

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REVISIONS

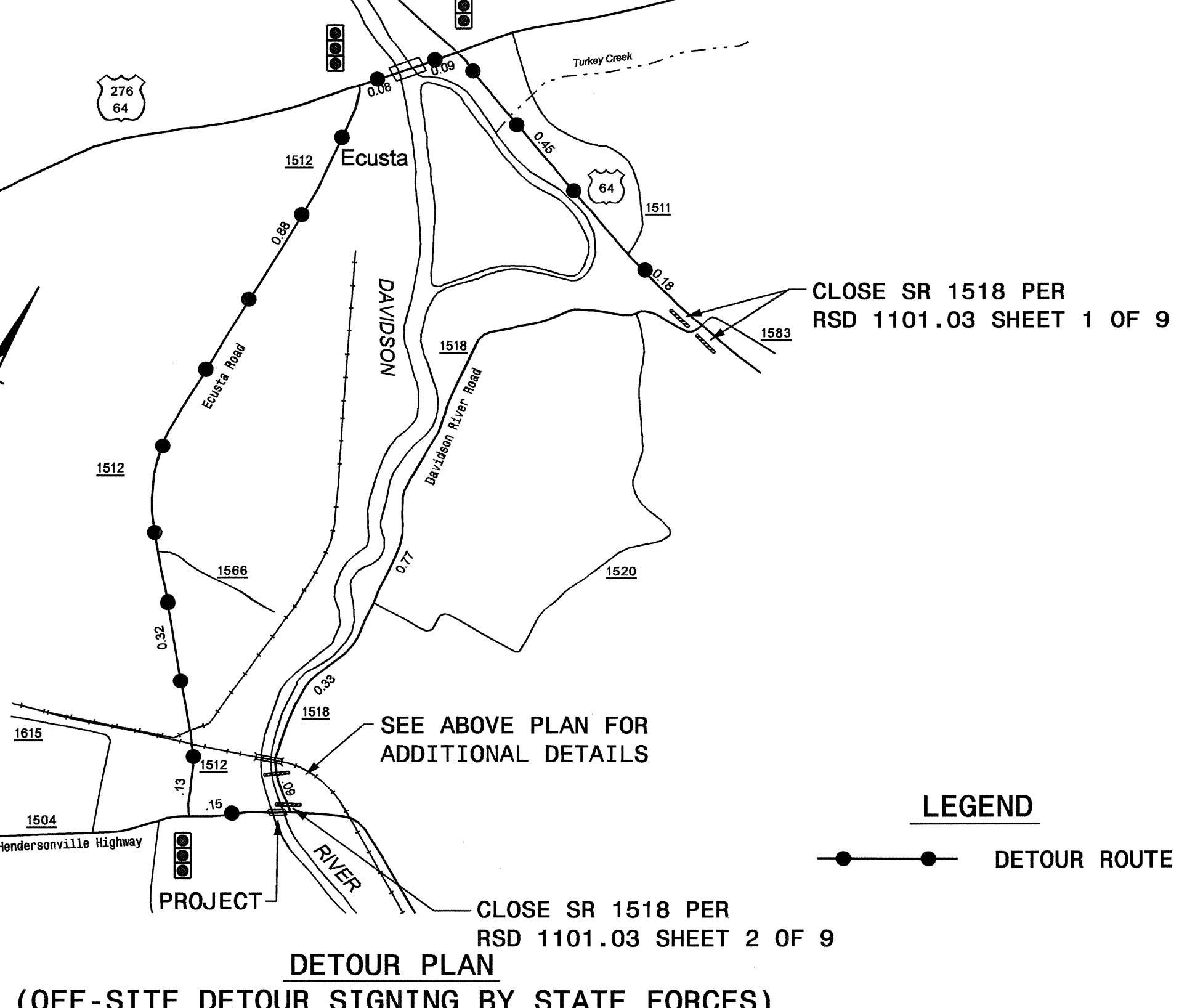
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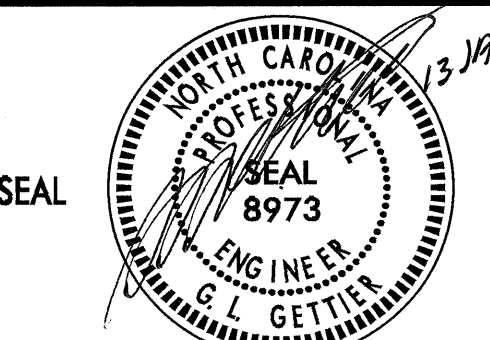

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TEMPORARY PAVEMENT MARKING LEGEND	
PAINT	
(PA) WHITE EDGELINE (4")	(QA) (LEFT ARROW)
(PD) 2 FT. WHITE MINISKIP (4")	(QC) (STRAIGHT ARROW)
(PE) WHITE SOLID LANE LINE (4")	(QI) R (WHITE CHARACTER)
(PI) YELLOW DOUBLE CENTER (4")	
(PV) YELLOW DIAGONAL (8")	
(P3) RAILROAD CROSSBUCK (16")	
(P4) WHITE STOPBAR (24")	

NOTES:  
 1. SEE ROADWAY STANDARD DRAWING NUMBERS 1250.01 AND 1251.01 FOR PAVEMENT MARKER SPACING AND DETAILS.



APPROVED:	DATE:	<b>PHASE II DETAILS</b>				
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