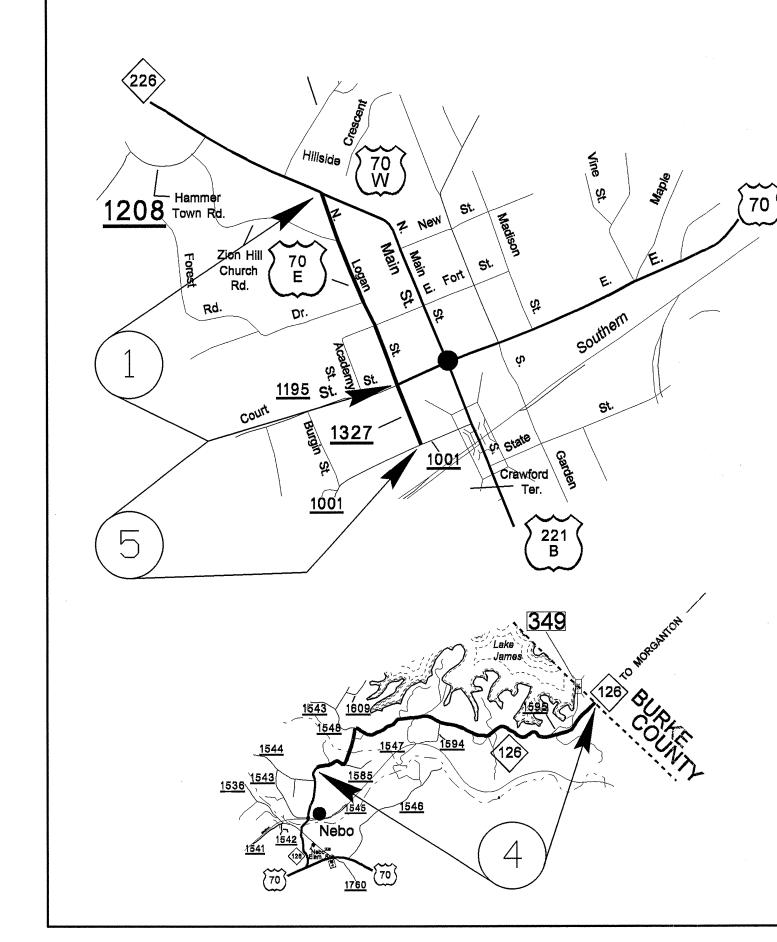
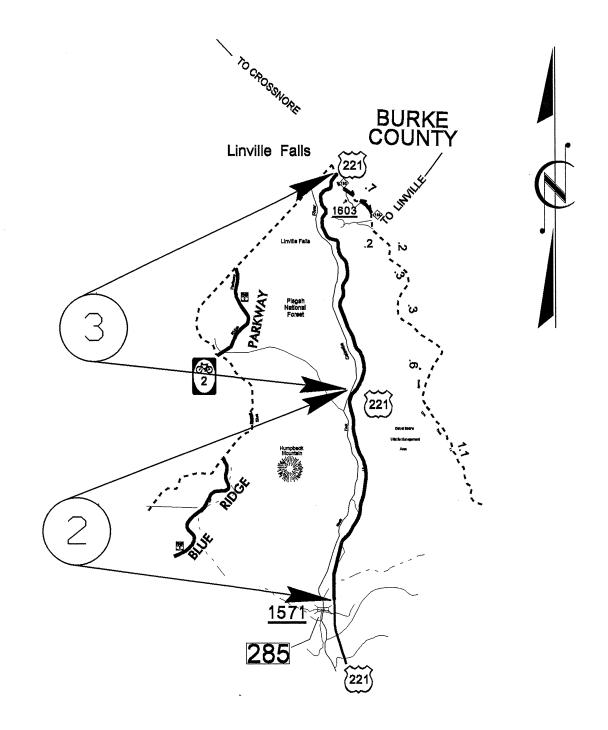
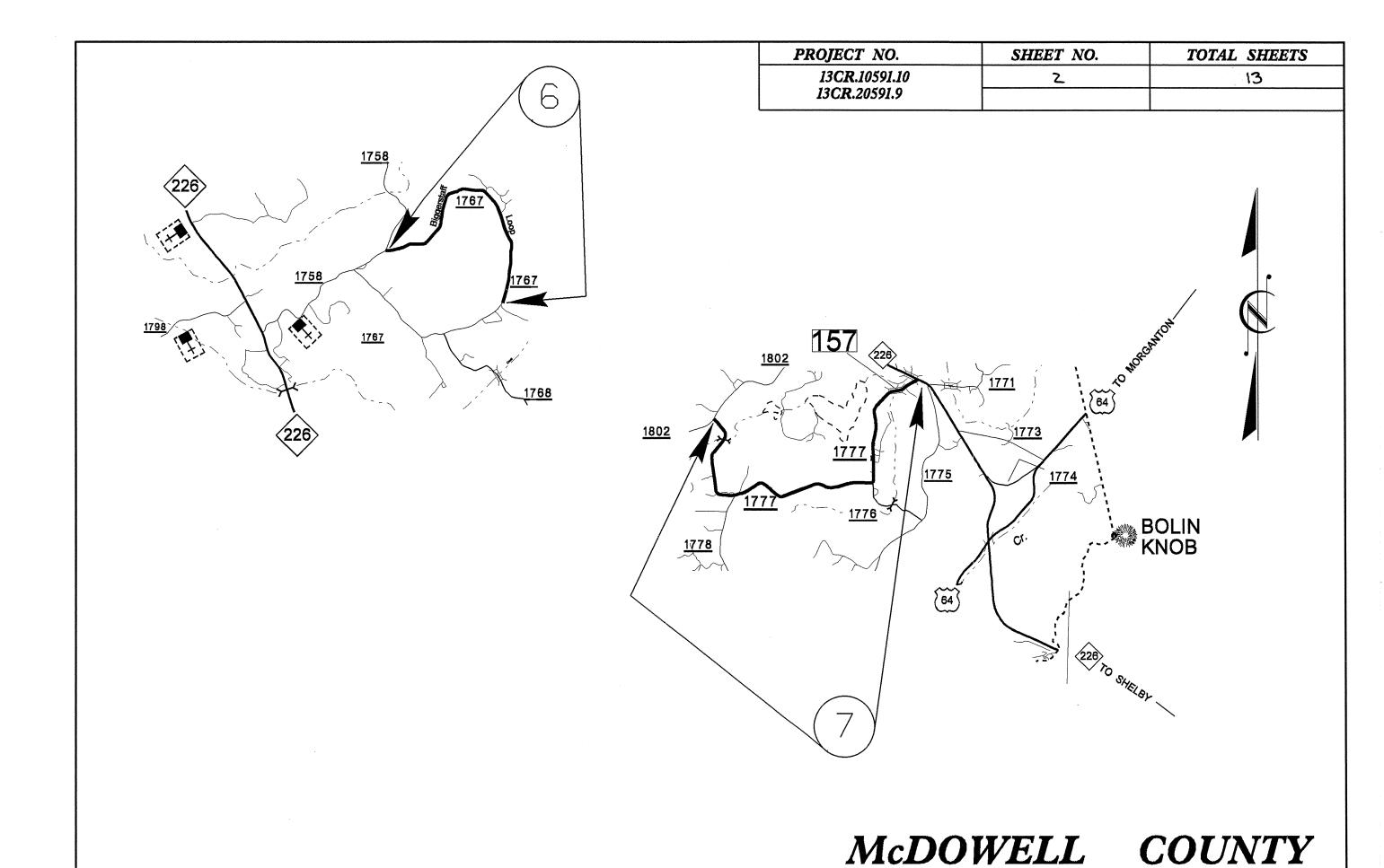
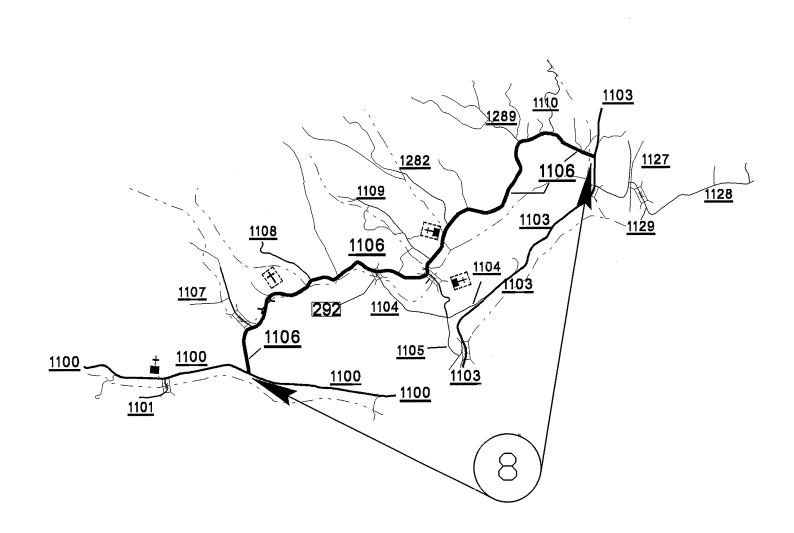
PROJECT NO.	SHEET NO.	TOTAL SHEETS
13CR.10591.10	١	13
13CR.20591.9		



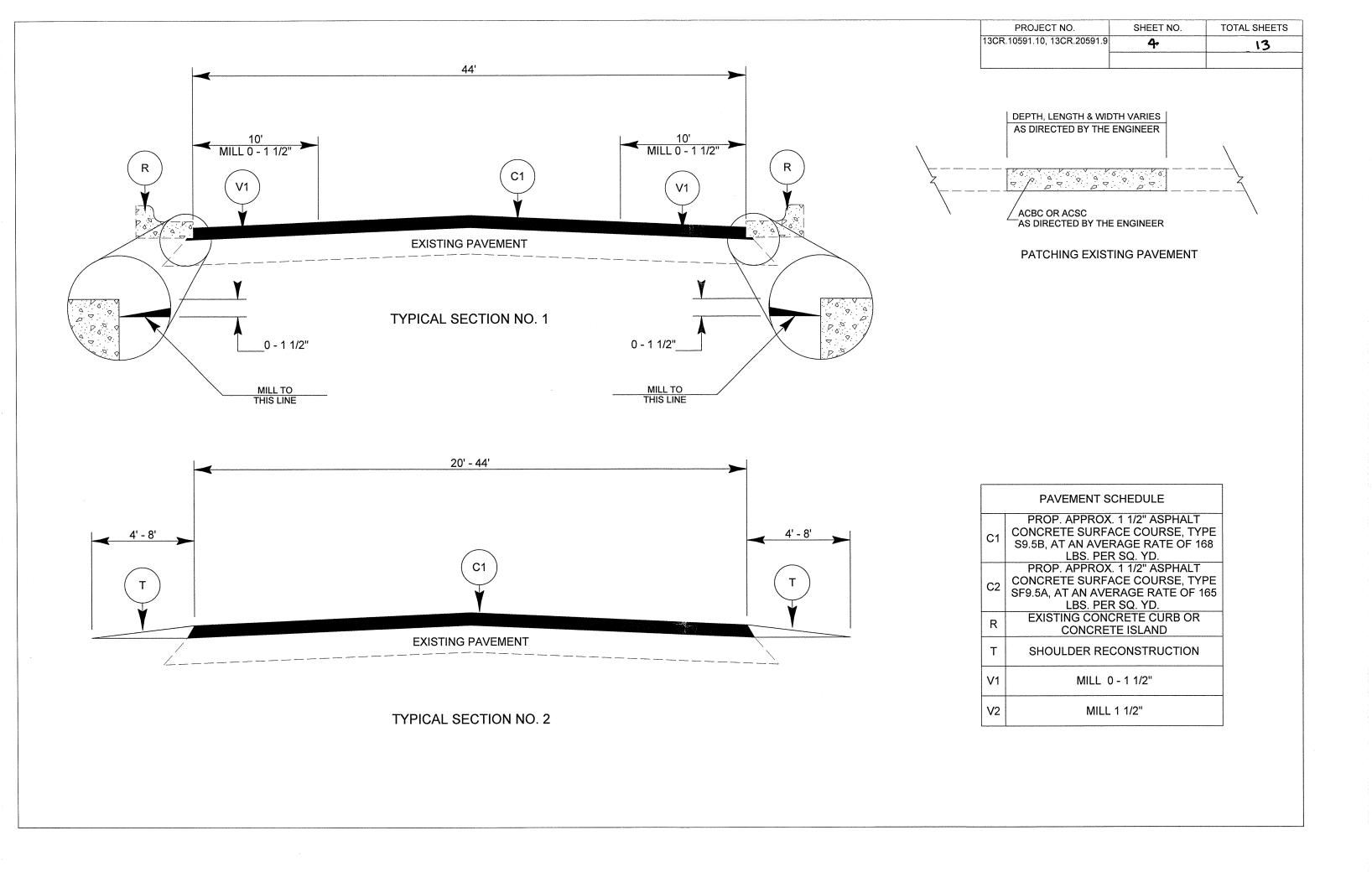


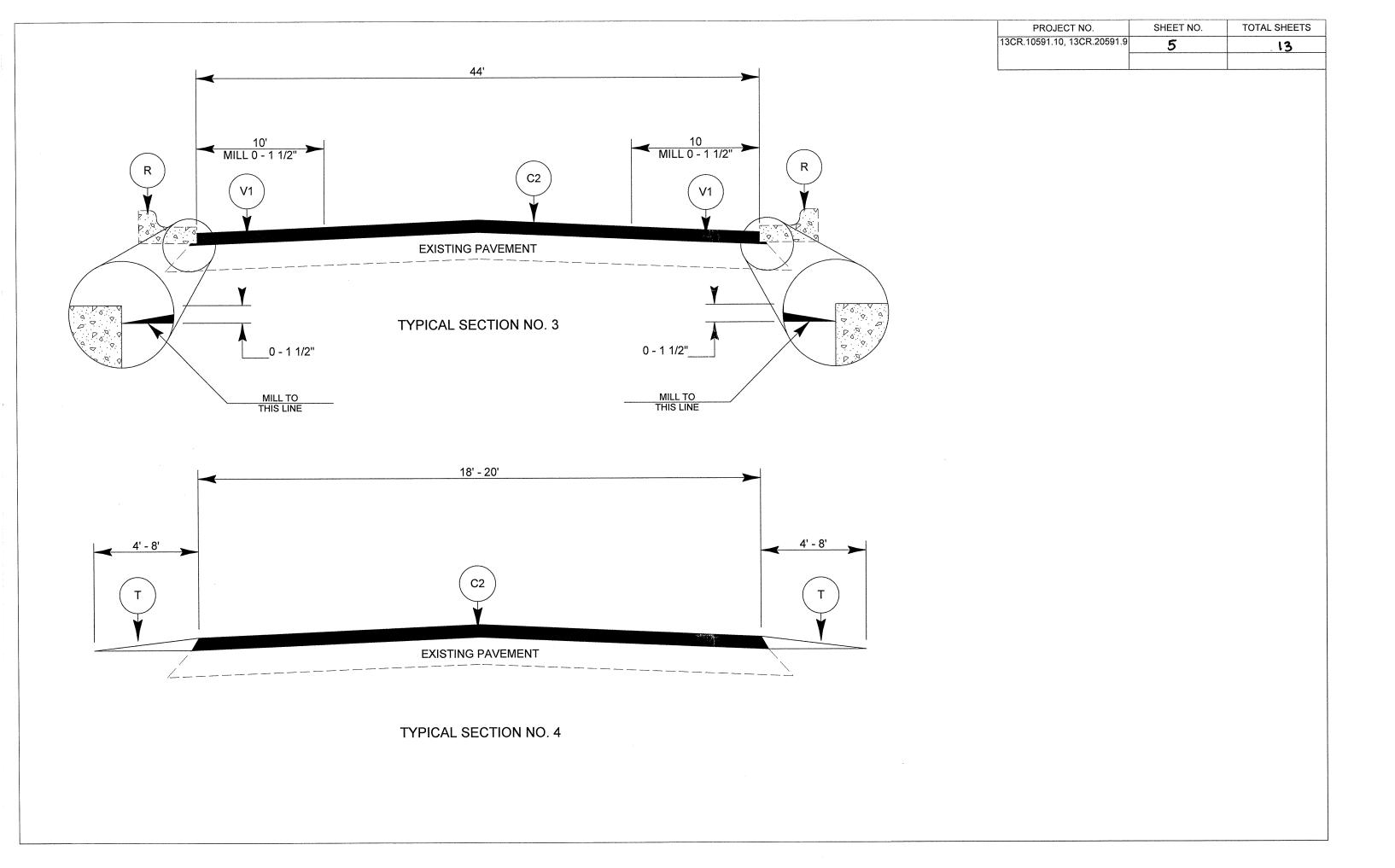


PROJECT NO.	SHEET NO.	TOTAL SHEETS
13CR.10591.10	3	13
13CR.20591.9		

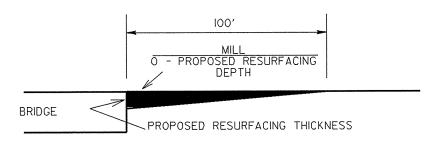




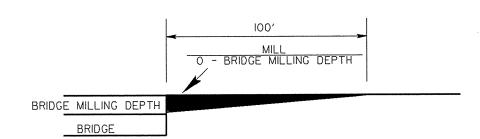




PROJECT NO.	SHEET NO.	TOTAL SHEETS
13CR.10591.10 & 13CR.20591.9	6	13
13CR.10391.10 & 13CR.20391.9		

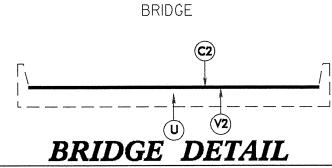


MILLING DETAIL AT BRIDGE APPROACHES
WHERE BRIDGES WILL NOT BE RESURFACED
COST OF MILLING IS INCIDENTAL TO OTHER ITEMS



# MILLING DETAIL AT BRIDGE APPROACHES

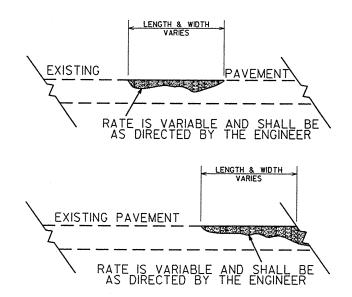
WHERE BRIDGES WILL BE MILLED THEN RESURFACED COST OF MILLING IS INCIDENTAL TO OTHER ITEMS



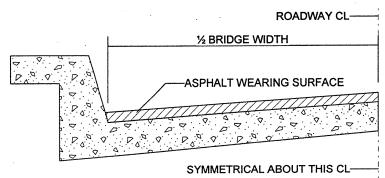
BRIDGE # 157 MAP # 7
MILL 1½" OFF EXISTING PAVEMENT
SEE MAPS FOR BRIDGE LOCATION

	PAVEMENT SCHEDULE
C2	PROP. APPROX. $1\frac{1}{2}$ " ASPHALT CONCRETE SURFACE COURSE, TYPE SF9.5A, AT AN AVERAGE RATE OF 165 LBS. PER SQ. YD.
U	EXISTING PAVEMENT
V2	MILL 11/2"

PROJECT NO.	SHEET NO.	TOTAL SHEETS
13CR.10591.10 & 13CR.20591.9	7	13
13CR:10391.10 & 13CR:20391.9		



# DETAIL SHOWING METHOD OF WEDGING



BRIDGE HALF TYPICAL SECTION
FOR BRIDGES WITH FLOOR DRAINS, CARE SHALL BE EXERCISED IN PLACING THE WEARING SURFACE AROUND FLOOR DRAINS SO AS NOT TO HINDER EFFECTIVE DRAINAGE. ALL DRAINS

THE PROPOSED WEARING SURFACE SHALL VARY IN THICKNESS AS NECESSARY TO PROVIDE A SMOOTH RIDING SURFACE. THE MINIMUM THICKNESS SHOULD DEPEND ON PAVEMENT TYPE AS FOLLOWS: \$4.75A ½", \$F9.5A 1.0", \$9.5X 1.5", \$12.5X 2.0", ULTRATHIN HOT MIX ASPHALT-TYPE B 5/8", ULTRATHIN HOT MIX ASPHALT-TYPE B 5/8", ULTRATHIN HOT MIX ASPHALT-TYPE B 5/8", ULTRATHIN HOT MIX ASPHALT-TYPE A \$4", ULTRATHIN HOT MIX ASPHALT-TYPE B 5/8", ULTRATHIN HOT MIX ASPHALT-TYPE C ½".

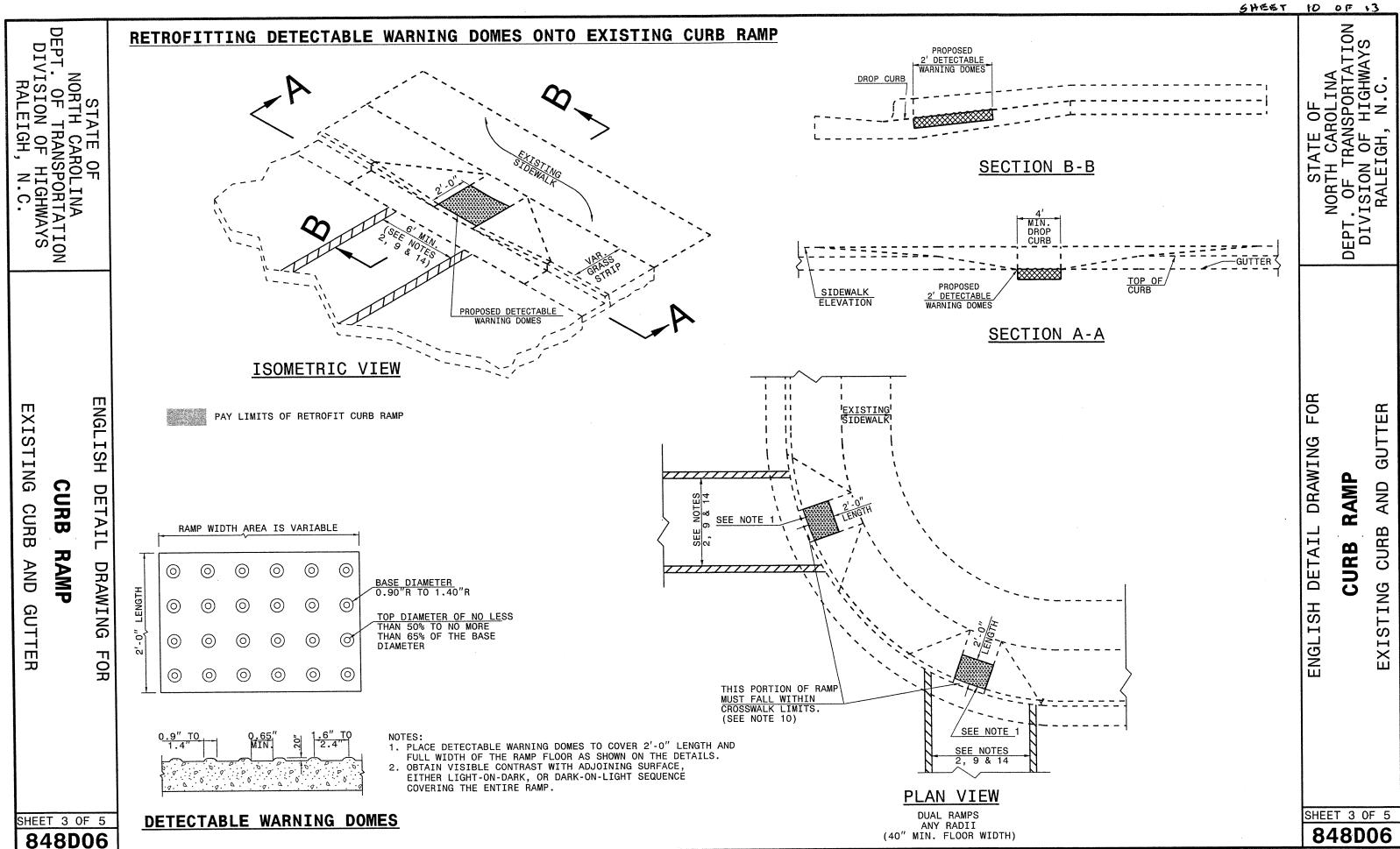
### NOTES

ALL UNPAVED ROADS TO BE RESURFACED 50' FROM EDGE OF PAVEMENT OF MAIN PROJECT. ALL PAVED S.R. ROADS TO BE RESURFACED TO THE ENDS OF THE RADII, OR AS DIRECTED BY THE ENGINEER.

BRIDGES ARE TO BE RESURFACED AT LOCATIONS AND TO DEPTH AS DIRECTED BY THE

SHEET 8 OF 13 STATE OF
NORTH CAROLINA
DEPT. OF TRANSPORTATION
DIVISION OF HIGHWAYS
RALEIGH, N.C. 8 CURB RAMP AND EXISTING SIDEWALK WITH GRASS STRIP STATE OF
NORTH CAROLINA
DEPT. OF TRANSPORTATION
DIVISION OF HIGHWAYS
RALEIGH, N.C. RETROFITTED CURB RAMP RETROFITTED SIDEWALK (4' MIN.) 2' DETECTABLE WARNING DOMES DROP CURB (EXISTING CURB & GUTTER SHOWN) 0.083 MAX. RAMP SLOPE SECTION B-B 5'-0" CURB DEP. MIN. DROP CURB CURB TRANSITION TRANSITION GUTTER VAR. LENGTH TO 12'-6' WITH VARIABLE SLOPE EXISTING DETECTABLE WARNING DOMES SIDEWALK **ELEVATION** SECTION A-A ISOMETRIC VIEW FOR 11 **ENGL** GRASS EXISTING STRIP SIDEWALK **EXISTING** GUTTER EXPANSION JOINT (SEE STD. 846.01) DRAWING HSI PAY LIMITS OF RETROFIT CURB RAMP NOTE: A PORTION OF ONE OR BOTH RAMPS MAY EXTEND OUTSIDE RAMP THE RETURN. **CURB** AND DE: CURB TAI SEE NOTE 1 CURB RAMP WIDTH AREA IS VARIABLE DETAIL EXPANSION JOINT (SEE STD. 846.01) CURB RAMP AND DRAWING 0 (0)0 0 0 0 EXISTING SIDEWALK BASE DIAMETER 0.90"R TO 1.40"R EXISTING 0 0 ENGLISH GUTTER TOP DIAMETER OF NO LESS THAN 50% TO NO MORE THAN 65% OF THE BASE 2'-0" 0 0 0 0 DIAMETER П 9 0 0 0 0 0 THIS PORTION OF RAMP MUST FALL WITHIN CROSSWALK LIMITS. (SEE NOTE 10) SEE NOTE 1 NOTES: 2, 9 & 14 1. PLACE DETECTABLE WARNING DOMES TO COVER 2'-0" LENGTH AND FULL WIDTH OF THE RAMP FLOOR AS SHOWN ON THE DETAILS. 2. OBTAIN VISIBLE CONTRAST WITH ADJOINING SURFACE, EITHER LIGHT-ON-DARK, OR DARK-ON-LIGHT SEQUENCE COVERING THE ENTIRE RAMP. PLAN VIEW SHEET 1 OF 5 DUAL RAMPS DETECTABLE WARNING DOMES SHEET 1 OF 5 ANY RADII (40" MIN. FLOOR WIDTH) 848D06 848D06

SHEET 9 OF 13 STATE OF
NORTH CAROLINA
DEPT. OF TRANSPORTATION
DIVISION OF HIGHWAYS
RALEIGH, N.C. STATE OF
NORTH CAROLINA
DEPT. OF TRANSPORTATION
DIVISION OF HIGHWAYS
RALEIGH, N.C. CURB RAMPS AND EXISTING SIDEWALK ADJACENT TO CURB RETROFITTED CURB RAMP AND EXISTING SIDEWALK 4' MIN LANDING 2' DETECTABLE WARNING DOMES DROP CURB (EXISTING CURB & GUTTER SHOWN) 0.083 MAX. RAMP SLOPE VAR WITH VARIABLE SLOPE SECTION B-B SIDEWALK TRANSITION 6'-0" CURB 6'-0' CURB DROP CURB TRANSITION TRANSITION -GUTTER 7 VAR. LENGTH TO 12'-6' WITH VARIABLE SLOPE TOP OF CURB EXISTING SIDEWALK DETECTABLE WARNING DOMES **ELEVATION** SECTION A-A ISOMETRIC VIEW 11 FOR **ENGLISH** | EXISTING EXISTING GUTTER EXPANSION JOINT (SEE STD. 846.01) DRAWING PAY LIMITS OF CURB RAMP RAMP **CURB** AND DETAIL **CURB** SEE NOTE 1 RAMP WIDTH AREA IS VARIABLE CURB DETAIL CURB RAMP AND DRAWING 0 0 0 0 BASE DIAMETER 0.90"R TO 1.40"R EXISTING LENGTH 0  $\odot$ 0 0 **ENGLISH** GUTTER EXISTING TOP DIAMETER OF NO LESS THAN 50% TO NO MORE SIDEWALK THAN 65% OF THE BASE 0 0 0 0 0 DIAMETER **FOR** 0 0 0 0 0 THIS PORTION OF RAMP MUST FALL WITHIN CROSSWALK LIMITS. (SEE NOTE 10) SEE NOTE 1 SEE NOTES 2, 9 & 14 1. PLACE DETECTABLE WARNING DOMES TO COVER 2'-0" LENGTH AND FULL WIDTH OF THE RAMP FLOOR AS SHOWN ON THE DETAILS. 2. OBTAIN VISIBLE CONTRAST WITH ADJOINING SURFACE, EITHER LIGHT-ON-DARK, OR DARK-ON-LIGHT SEQUENCE PLAN VIEW COVERING THE ENTIRE RAMP. DUAL RAMPS SHEET 2 OF 5 DETECTABLE WARNING DOMES SHEET 2 OF 5 ANY RADII (40" MIN. FLOOR WIDTH) 848D06 848D06



SHEET II

H CAROLINA TRANSPORTATION N OF HIGHWAYS STATE NORTH CAP OF T DEPT

> GUTTER AND

RAMP CURB

FOR

DRAWING

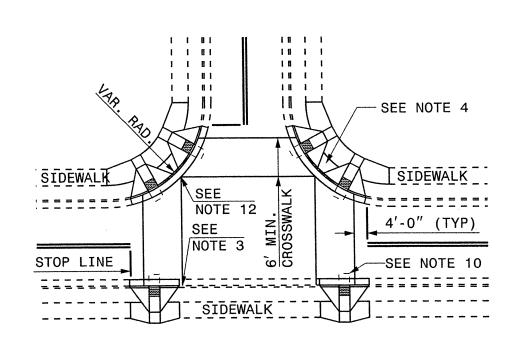
CURB EXISTING

DETAIL ENGLISH

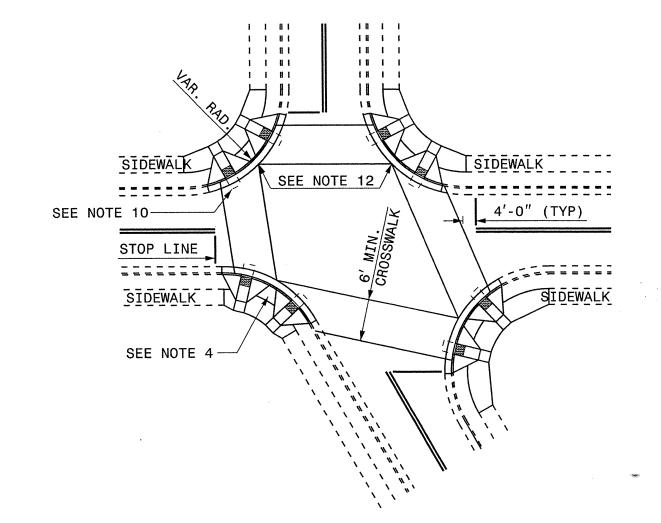
SHEET 4 OF 5

848D06

## CURB RAMPS AND EXISTING SIDEWALK



DETAIL SHOWING TYPICAL LOCATION OF CURB RAMPS,
PEDESTRIAN CROSSWALKS AND STOP LINES FOR TEE INTERSECTIONS



RAMPS, PEDESTRIAN CROSSWALKS AND STOP LINES

DETAIL SHOWING TYPICAL LOCATION OF CURB

ALLOWABLE LOCATIONS

DUAL RAMP RADII.....ANY

## RESURFACING PROJECTS

PROPOSED CURB RAMP W/ LANDING FOR RESURFACING PROJECTS EXISTING SIDEWALK

SHEET 4 OF 5

STATE OF
NORTH CAROLINA
DEPT. OF TRANSPORTATION
DIVISION OF HIGHWAYS
RALEIGH, N.C.

**ENGLISH** 

DETAIL

DRAWING

FOR

EXISTING

CURB

AND

GUTTER

CURB

RAMP

848D06

SHEET 12 OF 13

STATE OF
NORTH CAROLINA
DEPT. OF TRANSPORTATION
DIVISION OF HIGHWAYS
RALEIGH, N.C. 

RAMP

CURB

CUR

STING

DRAWING DETAIL ENGLISH

### CURB RAMP AND EXISTING SIDEWALK

#### NOTES:

DEPT DIV

STATE OF
NORTH CAROLINA
T. OF TRANSPORTATION
VISION OF HIGHWAYS
RALEIGH, N.C.

S

HSI

DETAIL

DRAWING

**FOR** 

**URB** 

RAMP

 $S_{G}$ 

**CURB** 

AND

GUTT

Ш

- 1. CONSTRUCT THE RAMP SURFACE TO BE STABLE, FIRM, AND SLIP RESISTANT. CONSTRUCT THE CURB RAMP TYPE AS SHOWN IN THE PAVEMENT MARKING PLANS OR AS DIRECTED BY THE ENGINEER.
- 2. LOCATE CURB RAMPS AND PLACE PEDESTRIAN CROSSWALK MARKINGS AS SHOWN IN THE PAVEMENT MARKING PLANS. WHEN FIELD ADJUSTMENTS REQUIRE MOVING CURB RAMPS OR MARKINGS AS SHOWN, CONTACT THE SIGNING AND DELINEATION UNIT OR LOCATE AS DIRECTED BY THE ENGINEER.
- 3. COORDINATE THE CURB RAMP AND THE PEDESTRIAN CROSSWALK MARKINGS SO A  $4' \times 4'$  CLEAR SPACE AT THE BASE OF THE CURB RAMP WILL FALL WITHIN THE PEDESTRIAN CROSSWALK LINES.
- 4. SET BACK DISTANCE FROM INSIDE CROSSWALK MARKING TO NEAREST EDGE OF TRAVEL LANE IS 4' MINIMUM.
- 5. REFER TO THE PAVEMENT MARKING PLANS FOR STOP BAR LOCATIONS AT SIGNALIZED INTERSECTIONS. IF A PAVEMENT MARKING PLAN IS NOT PROVIDED, CONTACT THE SIGNAL DESIGN SECTION FOR THE STOP BAR LOCATIONS OR LOCATE AS DIRECTED BY THE ENGINEER.
- TERMINATE PARKING A MINIMUM OF 20' BACK OF A PEDESTRIAN CROSSWALK.
- 7. CONSTRUCT CURB RAMPS A MINIMUM OF 4' WIDE.
- 8. CONSTRUCT THE RUNNING SLOPE OF THE RAMP 8.33% MAXIMUM.
- 9. ALLOWABLE CROSS SLOPE ON SIDEWALKS AND CURB RAMPS WILL BE 2% MAXIMUM.
- 10. CONSTRUCT THE SIDE FLARE SLOPE A MAXIMUM OF 10% MEASURED ALONG THE CURB LINE.
- 11. CONSTRUCT THE COUNTER SLOPE OF THE GUTTER OR STREET AT THE BASE OF THE CURB RAMP A MAXIMUM OF 5% AND MAINTAIN A SMOOTH TRANSITION.
- 12. CONSTRUCT LANDINGS FOR SIDEWALK A MINIMUM OF 4'x4' WITH A MAXIMUM SLOPE OF 2% IN ANY DIRECTION. CONSTRUCT LANDINGS FOR MEDIAN ISLANDS A MINIMUM OF 5'x5' WITH A MAXIMUM SLOPE OF 2% IN ANY DIRECTION.
- 13. TO USE A MEDIAN ISLAND AS A PEDESTRIAN REFUGE AREA, MEDIAN ISLANDS WILL BE A MINIMUM OF 6' WIDE. CONSTRUCT MEDIAN ISLANDS TO PROVIDE PASSAGE OVER OR THROUGHT THE ISLAND.
- 14. SMALL CHANNELIZATION ISLANDS THAT CAN NOT PROVIDE A 5'X5' LANDING AT THE TOP OF A RAMPS, WILL BE CUT THROUGH LEVEL WITH THE SURFACE STREET.
- 15. CURB RAMPS WITH RETURNED CURBS MAY BE USED ONLY WHERE PEDESTRIANS WOULD NOT NORMALLY WALK ACROSS THE RAMP. THE ADJACENT SURFACE IS PLANTING OR OTHER NON-WALKING SURFACE OR THE SIDE APPROACH IS SUBSTANTIALLY OBSTRUCTED.
- 16. PLACE A 1/2" EXPANSION JOINT WHERE THE CONCRETE CURB RAMP JOINS THE CURB AS SHOWN IN ROADWAY STANDARD DRAWING 848.01
- PLACE ALL PEDESTRIAN PUSH BUTTON ACTUATORS AND CROSSING SIGNALS AS SHOWN IN THE PLANS OR AS SHOWN IN THE MUTCD.
- 18. CURB RAMPS THROUGH MEDIAN ISLANDS, SINGLE RAMPS AT DUAL CROSSWALKS OR LIMITED R/W SITUATIONS, WILL BE HANDLED BY SPECIAL DETAILS. CONTACT THE CONTRACT STANDARDS AND DEVELOPMENT UNIT FOR THE DETAILS OR FOR A SPECIAL DESIGN.

HEET 5 OF 5

848D06

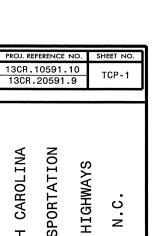
PROJECT NO.	SHEET NO.	TOTAL NO.
13CR.10591.10, 13CR.20591.9	13	13

# SUMMARY OF QUANTITIES

									<u> </u>	J IVI IVI A IV		QUANT			·~		·			,	
PROJECT	COUNTY	MAP	ROUTE	DESCRIPTION	TYP	FINAL	LENGTH	WIDTH	INCIDENTAL	SHOULDER	1½" MILLING	0" TO 1%" MILLING	ASPHALT CONC	ASPHALT CONC	ASPHALT	PATCHING	CONCRETE CURB		ADJUSTMENT OF	1	PORTABLE
						SURFACE			STONE BASE	RECONSTRUCTION			SURFACE COURSE,	SURFACE COURSE,	BINDER FOR	EXISTING	RAMPS	OF CATCH BASIN	MANHOLES	OF METER	LIGHTING
		1				TESTING							TYPE S9.5B	TYPE SF9.5A	PLANT MIX	PAVEMENT				BOXES OR	
	1					REQUIRED														VALVE BOXES	
NO		NO			NO	negomes	MI	FT	TON	SMI	SY	SY	TON	TON	TON	TON	EA	EA	EA	EA	LS
	ļ	+		FROM US 221 BUS MP 18.05 TO SR																	
13CR.10591.10	McDowell	1 1	US 70	1327 MP 18.34	1	NO	0.27	44				3,168	646		39	50	5	1	4	8	11
13011.10331.10	Wichard	╁╧		FROM 0.33 MILES NORTH OF SR 1571																	
				MP 32.63 TO BEG PASSING LANE MP																	
	McDowell	1 2	1	1	2	NO	1.73	24	86.50	3.46			2,262		136	350					
		1												l							
	ł	1		FROM BEG PASSING LANE MP 34.36																	
	McDowel	1 3	US 221	TO BURKE CO. LINE MP 36.42	2	NO	2.06	20-44	103.00	4.12			2,654		159	515					
														1	1						
				FROM 0.20 MILES WEST OF SR 1547				1													
	McDowel	1 4	NC 126	MP 0.97 TO BURKE CO. LINE MP 3.85	2	NO	2.88	20	144.00	5.76			3,142		189	748					
TOTAL FOR							6.94		333.50	13.34		3,168	8,704		523	1,663	5	1 1	4	8	1
												,		.,		·		,	r	·	
	1	T		FROM US 70 MP 0 TO SR 1001 MP																	
13CR.20591.9	McDowel	1 5	SR 1327	0.09	3	NO	0.09	44				1,056		211	14	50	4	3	1	8	
				FROM SR 1758 MP 0 TO PAVEMENT																	
	McDowel	1 6	SR 1767	CHANGE MP 1.65	4	NO	1.65	18	82.50	3.30				1,592	107	500					
		T		FROM NC 226 MP 0 TO SR 1802 MP												İ					1
	McDowel	1 7	SR 1777	3.62	4	NO	3.6	20	180.00	7.20	66			3,857	258	1,300					
		1		FROM SR 1100 MP 0 TO SR 1103 MP																	ı
	McDowel	11 8	SR 1106	3.73	4	NO	3.7	18	185.00	7.40				3,570	239	1,100	ļ				
TOTAL FOR	PROJ NO. 1	13CR.20	591.9				9.04		447.50	17.90	66	1,056		9,230	618	2,950	4	3	1 1	8	
												<del>,</del>		·	T	r		T	ı		
	GRAND TOT	AL					15.98		781.00	31.24	66	4,224	8,704	9,230	1,141	4,613	<u> 9</u>	1 4	5	16	11

## THERMOPLASTIC AND PAINT QUANTITIES

T		1 1			———Т		4685000000-E	4686	000000-E	4697000000-E	4710000000-E		4725000000-E		481000	00000-E	4820000000-E	4835000000-E	<u> </u>	48450	00000-N		4905000000-N
PROJECT	COUNTY	МАР	ROUTE	DESCRIPTION	LENGTH	WIDTH	4" X 90 M WHITE THERMO	4" X 120 M YELLOW		8" X 120 M WHITE	24" X 120 M WHITE THERMO	PAVEMENT MARKING		THERMOPLASTIC PAVEMENT	PAINT PAVEMENT	PAINT PAVEMENT	PAINT PAVEMENT	PAVEMENT	PAINT PAVEMENT MARKING	PAINT PAVEMENT MARKING	PAINT PAVEMENT MARKING	PAINT PAVEMENT MARKING	SNOWPLOWABLE PAVEMENT MARKERS
								THERMO				ARROW)	SYMBOL (90 MILS, RT ARROW)	MARKING SYMBOL (90 MILS, STR & RT ARROW)	MARKING LINES (4") WHITE	MARKING LINES (4") YELLOW	MARKING LINES (8") WHITE	MARKING LINES (24") WHITE	SYMBOL (LT ARROW)	SYMBOL (RT ARROW)	SYMBOL (STR & LT ARROW)	SYMBOL (STR & RT ARROW)	1
NO		NO					LF	LF	LF	LF	LF	EA	EA	EA	LF	LF	ĿF	LF	EA	EA	EA	EA	EA
13CR.10591.10	McDowell	1	US 70	FROM US 221 BUS MP 18.05 TO SR 1327 MP 18.34	0.27	44	2,851	2,851		325	72	6	2	2									36
		_		FROM 0.33 MILES NORTH OF SR 1571 MP 32.63 TO BEG PASSING LANE MP																			
	McDowell	1 2	1 1	34.36	1.73	24	18,268	18,268													<u> </u>		173
	McDowell		116 221	FROM BEG PASSING LANE MP 34.36 TO BURKE CO. LINE MP 36.42	2.06	20	21,754	21.754	740										1				206
	MicDowell	1 3	03 221	FROM 0.20 MILES WEST OF SR 1547	2.00	20	21,754	21,754	/														
	McDowell	1 4	NC 126	MP 0.97 TO BURKE CO. LINE MP 3.85		20	30,413	30,413		90					<u> </u>								288 703
TOTAL FOR PI	ROJ NO. 13	3CR.105	91.10		6.94		73,286	73,286	740 74,026	415	72	6	10	2		L				L	L		703
			·		,	·						·	T	Т	1	Τ	T	Т	1	Г	T	T	T
13CR.20591.9	McDowell	1 5	SR 1327	FROM US 70 MP 0 TO SR 1001 MP 0.09	0.09	44				,					1,901	1,901	250	60	2	2	2	2	
	McDowell	1 6	SR 1767	FROM SR 1758 MP 0 TO PAVEMENT CHANGE MP 1.65	1.65	18									34,848	34,848							
	McDowell	1 7	SR 1777	FROM NC 226 MP 0 TO SR 1802 MP 3.62	3.6	20									76,032	76,032							
	McDowell			FROM SR 1100 MP 0 TO SR 1103 MP 3.73	3.7	18									78,144	78,144							
TOTAL FOR F					9.04								<u></u>	<u></u>	190,925 381	190,925 1,850	250	60	2	2	8	2	
																				1 -	7	T	702
-	RAND TOTA	· A I			15.98		73,286	73,286	740 74.026	415	72	6	10	2	190,925	190,925 L.850	250	60	2	2		z	703



9F

DIVISION

DRAWING FOR Y UNDIVIDED WARNING SIGNS

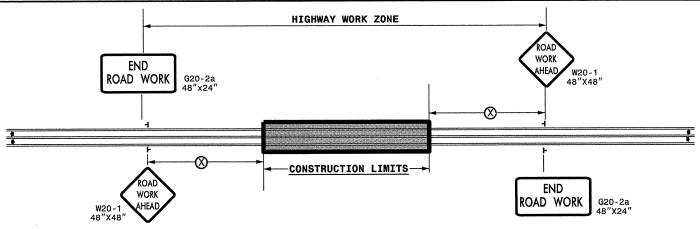
DETAIL E TWO-WAY SK ZONE V

WORK

FOR

RALEIGH,

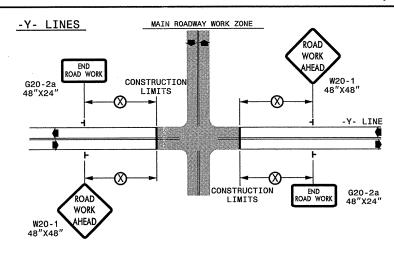
TWO-WAY UNDIVIDED \*\* (L-LINES)



RECOMMENDED MINIMUM SIGN SPACING POSTED SPEED LIMIT ≤ 50 1000'

TRANSPORTATION NORTH R P STATE DEPT.

ROADWAYS INTERSECTING ALONG 2 WAY UNDIVIDED WORK ZONE (Y-LINES)



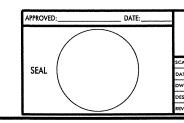
#### **GENERAL NOTES**

- USE FLUORESCENT ORANGE SHEETING (TYPE VII OR HIGHER) ON ALL ADVANCED WORK ZONE SIGNS.
- DO NOT INSTALL ADVANCE WARNING SIGNS MORE THAN 3 DAYS PRIOR TO BEGINNING OF WORK.
- SIGNS SHOWN ARE REQUIRED FOR WORK ZONES THAT WILL REMAIN IN EFFECT OVERNIGHT. FOR SHORT-TERM DAILY MAINTENANCE TYPE OPERATIONS THIS SIGNING APPLICATION IS OPTIONAL; MAY USE ONLY APPLICABLE ROADWAY STANDARD DRAWINGS INSTEAD. HOWEVER, IF THIS SIGNING APPLICATION IS USED, SIGNS MAY BE PORTABLE MOUNTED.
- ALL SIGN SPACING DIMENSIONS ARE APPROXIMATE, FIELD ADJUST AS NECESSARY OR AS DIRECTED.
- USE 3LB STEEL U-CHANNEL POST OR 4" X 4" WOOD POST FOR ALL WORK ZONE SIGNS. 3LB STEEL U-CHANNEL POSTS MUST MEET THE REQUIREMENTS OF STANDARD SPECIFICATION SECTION 1094-1(B), MAY BE GALVANIZED STEEL, OR MAY BE PAINTED GREEN BY THE POST MANUFACTURER. SQUARE STEEL TUBING POSTS HAVING EQUIVALENT STRENGTH OF THE 3 LB STEEL U-CHANNEL POST ARE ALSO ACCEPTABLE FOR USE. ERECT SIGNS PER ROADWAY STANDARD DRAWING 1110.01. PAYMENT FOR WOOD POSTS, 3LB STEEL U-CHANNEL AND SQUARE STEEL TUBING POSTS WITH SIGNS WILL BE MADE ACCORDING TO STANDARD SPECIFICATION "WORK ZONE SIGNS" SECTION 1110.
- WHEN NECESSARY, USE SPLICING IN ACCORDANCE WITH ROADWAY STANDARD DRAWING NO. 1110.01. REMOVE ENTIRE POST WHEN REMOVING SIGNS WITH SPLICED POSTS.
- DO NOT BACK BRACE SIGN SUPPORTS.
- \*\* TWO-WAY UNDIVIDED ADVANCE WARNING SIGN CONFIGURATION MAY BE USED ON URBAN MULTI-LANE FACILITIES WHERE CONDITIONS LIMIT THE USE OF DUAL MOUNTED SIGNS AS DETERMINED BY THE ENGINEER.

LEGEND - STATIONARY SIGN

dIRECTION OF TRAFFIC FLOW

SHEET 1 OF 1



DETAIL	DRAWIN	IG FOR	TWO-W	ΙΑΥ
UNDIVID	ED AND	URBAN	FREE	WAYS
ADVANCED	WORK ZO	ONE WAR	RNING	SIGNS

NONE	"HOINEER!
Y:	CONTROL

REVISIONS							
7-98	10/01						
10-98	03/04						
01/01	11/04						
CADD FILE							

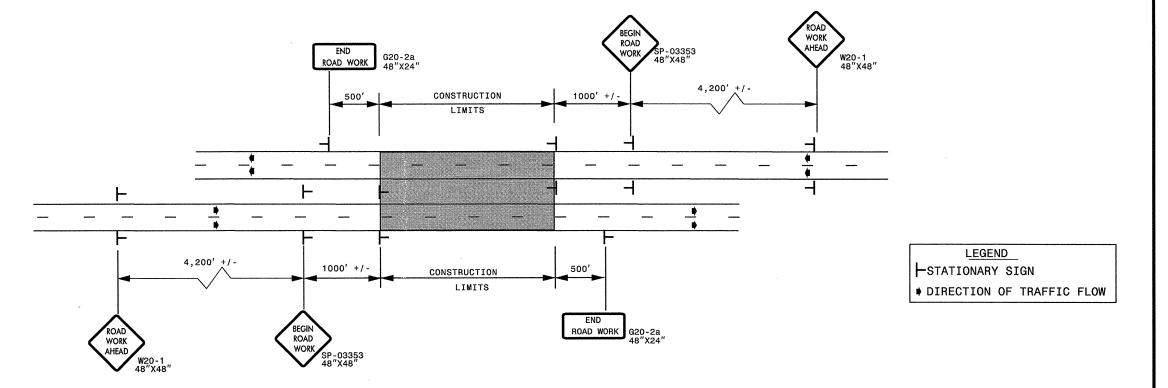
## ADVANCED WORK ZONE WARNING SIGNING FOR FREEWAYS (4 LANES OR GREATER)

PROJ. REFERENCE NO. SHEET NO.

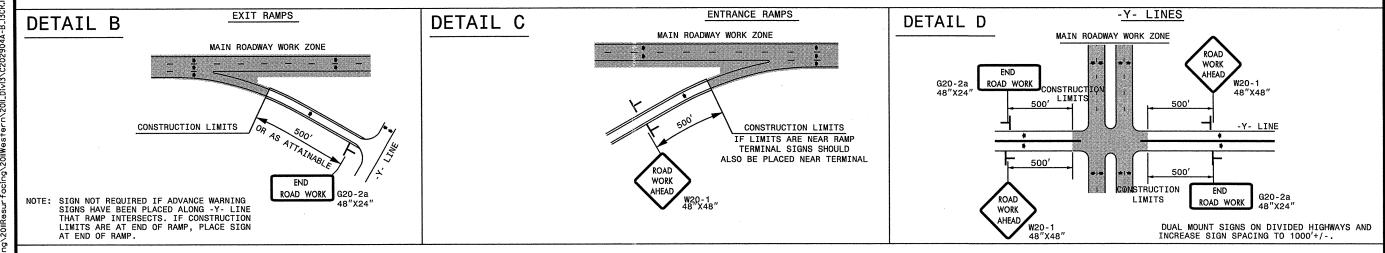
13CR.10591.10

13CR.20591.9

### DETAIL A

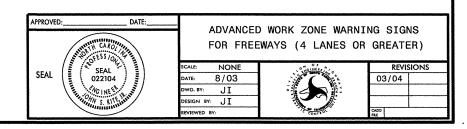


★ USE THE "\$250 SPEEDING PENALTY" SIGN, SPEED LIMIT SIGN, AND ORANGE PANEL; ONLY WHEN A "\$250 SPEEDING PENALTY" ORDINANCE HAS BEEN ISSUED BY THE REGIONAL TRAFFIC ENGINEER.

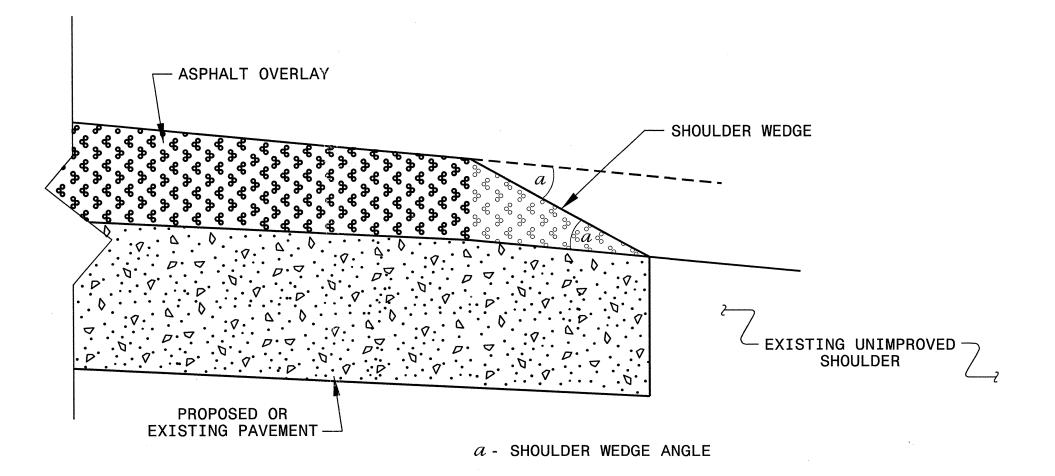


#### **GENERAL NOTES**

- USE FLUORESCENT ORANGE SHEETING (TYPE VII OR HIGHER) ON ALL ADVANCED WORK ZONE SIGNS.
- DO NOT INSTALL ADVANCE WARNING SIGNS MORE THAN 3 DAYS PRIOR TO BEGINNING OF WORK.
- SIGNS SHOWN ARE REQUIRED FOR WORK ZONES THAT WILL REMAIN IN EFFECT OVERNIGHT. FOR SHORT-TERM DAILY MAINTENANCE TYPE OPERATIONS, THIS SIGNING APPLICATION IS OPTIONAL; MAY USE ONLY APPLICABLE ROADWAY STANDARD DRAWINGS INSTEAD. HOWEVER, IF THIS SIGNING APPLICATION IS USED, SIGNS MAY BE PORTABLE MOUNTED.
- ALL SIGN SPACING DIMENSIONS ARE APPROXIMATE, FIELD ADJUST AS NECESSARY OR AS DIRECTED.
- USE 3LB STEEL U-CHANNEL POST OR 4" X 4" WOOD POST FOR ALL WORK ZONE SIGNS. 3LB STEEL U-CHANNEL POSTS MUST MEET THE REQUIREMENTS OF STANDARD SPECIFICATION SECTION 1094-1(B), MAY BE GALVANIZED STEEL, OR MAY BE PAINTED GREEN BY THE POST MANUFACTURER. SQUARE STEEL TUBING POSTS HAVING EQUIVALENT STRENGTH OF THE 3 LB STEEL U-CHANNEL POST ARE ALSO ACCEPTABLE FOR USE. ERECT SIGNS PER ROADWAY STANDARD DRAWING 1110.01. PAYMENT FOR WOOD POSTS, 3LB STEEL U-CHANNEL AND SQUARE STEEL TUBING POSTS WITH SIGNS WILL BE MADE ACCORDING TO STANDARD SPECIFICATION "WORK ZONE SIGNS" SECTION 1110.
- WHEN NECESSARY, USE SPLICING IN ACCORDANCE WITH ROADWAY STANDARD DRAWING NO. 1110.01. REMOVE ENTIRE POST WHEN REMOVING SIGNS WITH SPLICED POSTS.
- DO NOT BACK BRACE SIGN SUPPORTS.



PROJECT REFERENCE NO.



# SHOULDER WEDGE DETAIL

CONTRACT STANDARDS AND DEVELOPMENT UNIT Office 919-707-6950 FAX 919-250-4119

# SHOULDER WEDGE DETAIL

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