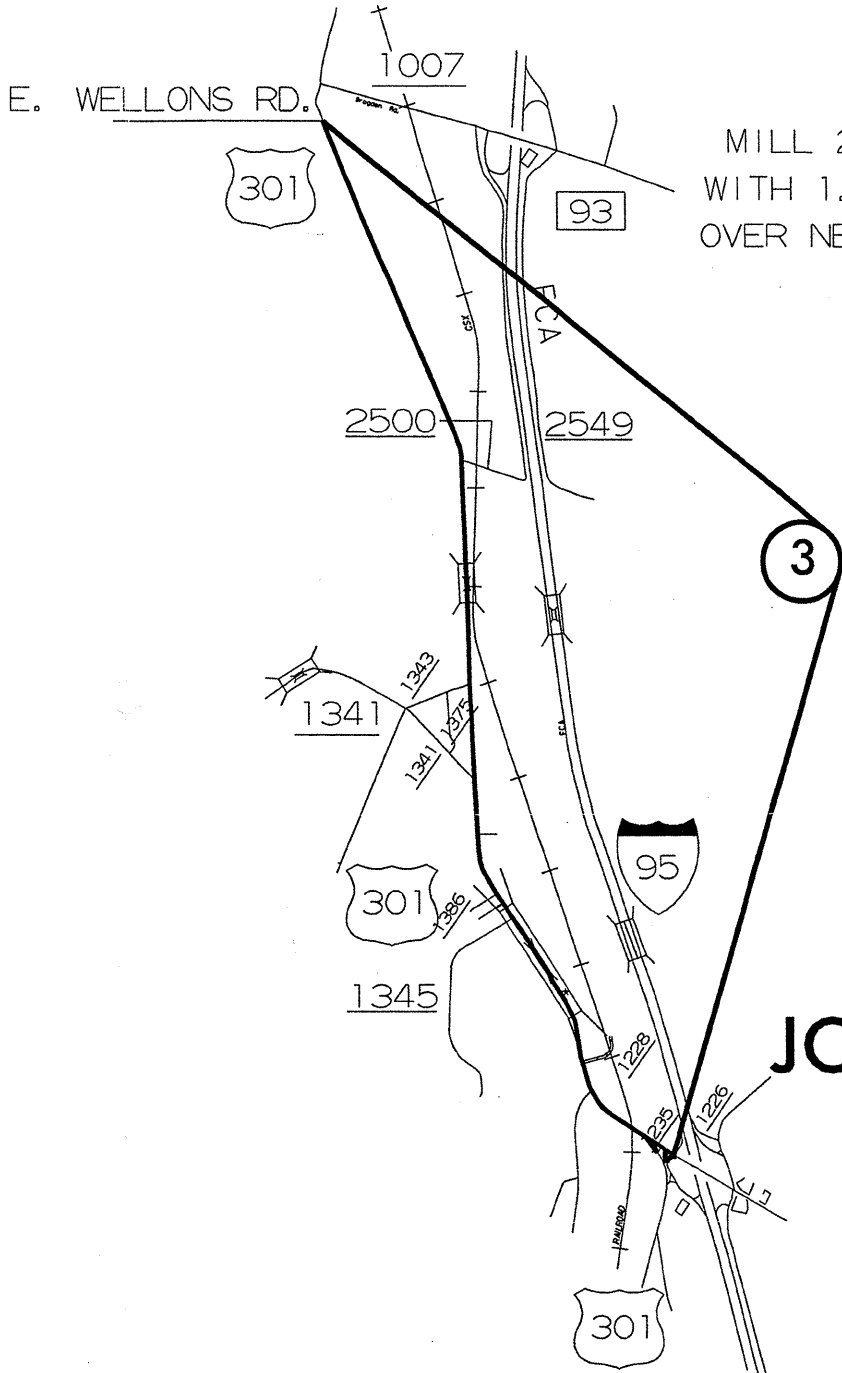
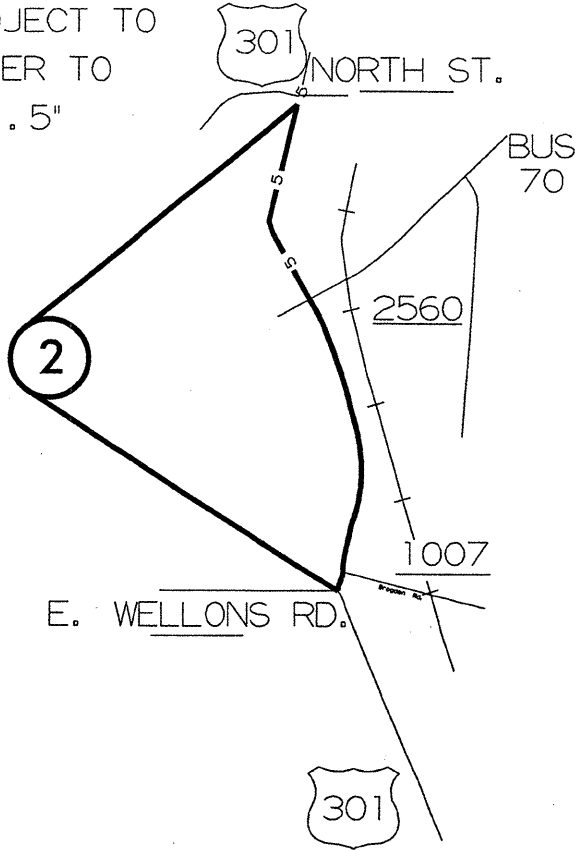


WBS ELEMENT	SHEET NO.	TOTAL SHEETS
4CR.10511.21	1	
		DESCRIPTION

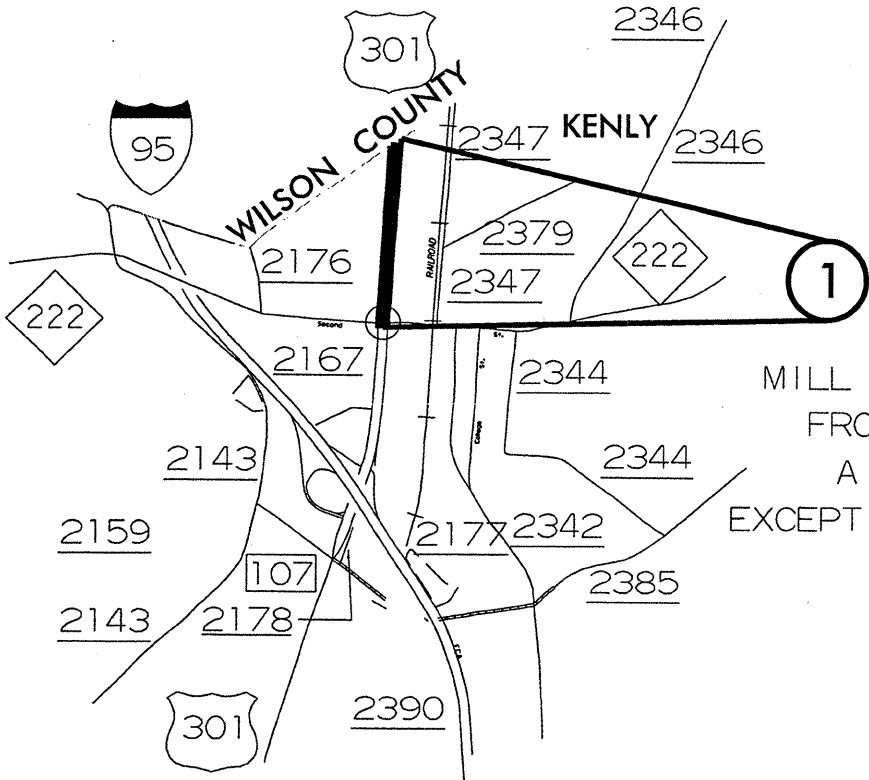
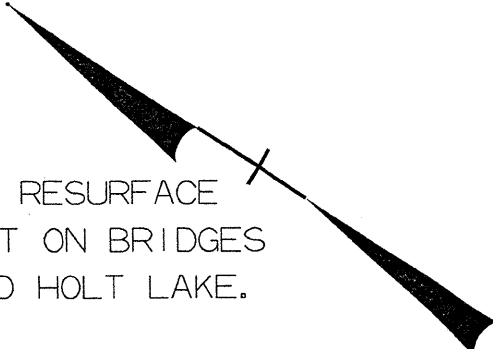
MILL ENTIRE PROJECT TO FRONT OF GUTTER TO A DEPTH OF 1.5"



MILL 2" INCHES AND RESURFACE WITH 1.5" OF ASPHALT ON BRIDGES OVER NEUSE RIVER AND HOLT LAKE.

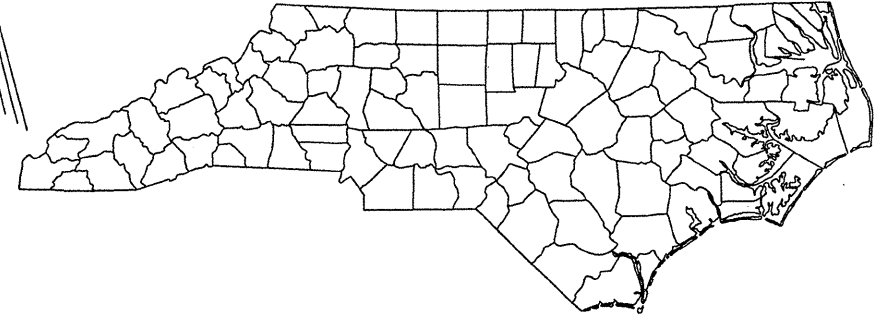
RESURFACE WITH 1.5" OF ASPHALT ON BRIDGE OVER RAILROAD

RESURFACE RADII AT INTERCHANGE US 301 AND US 701

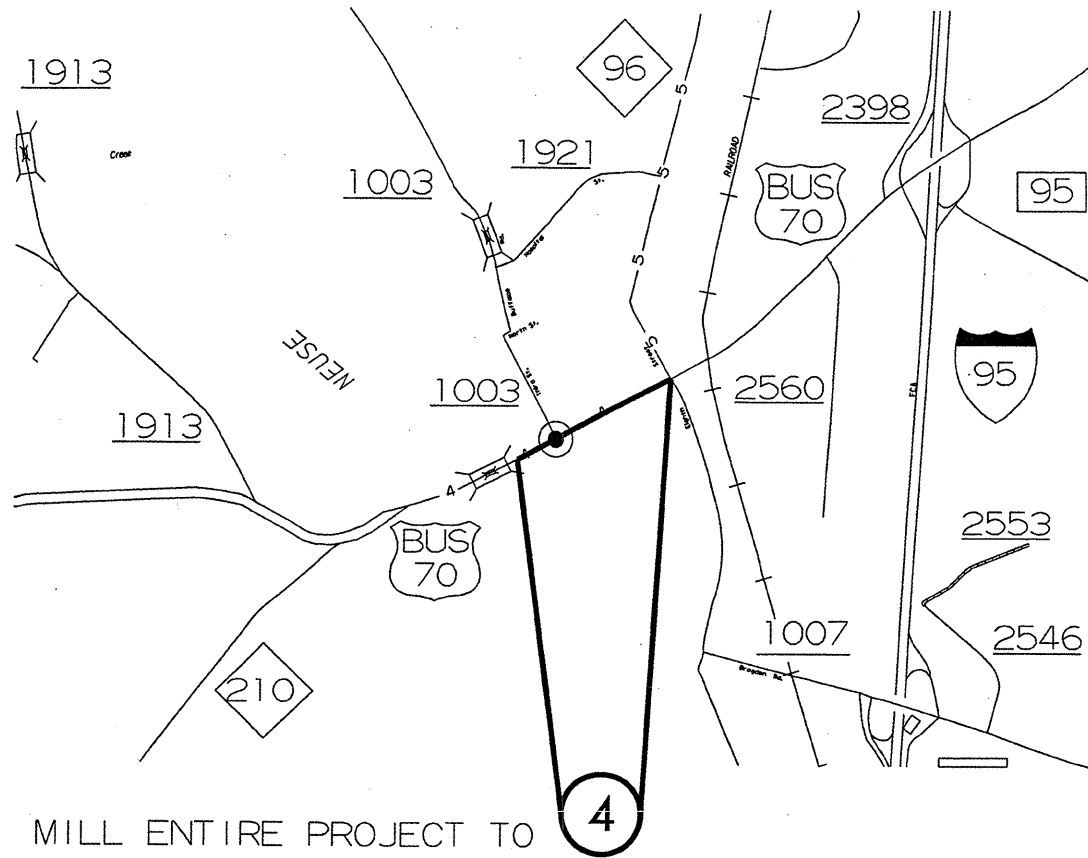


MILL ENTIRE PROJECT TO FRONT OF GUTTER TO A DEPTH OF 1.5" EXCEPT FOR SBL OF NC 222

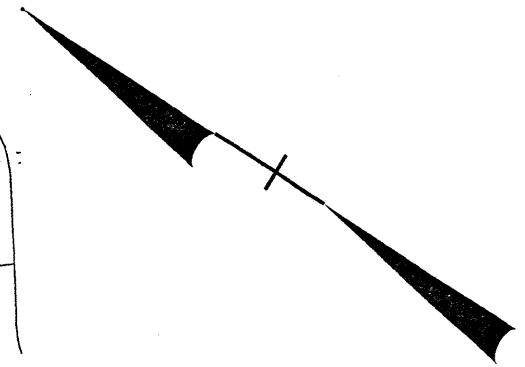
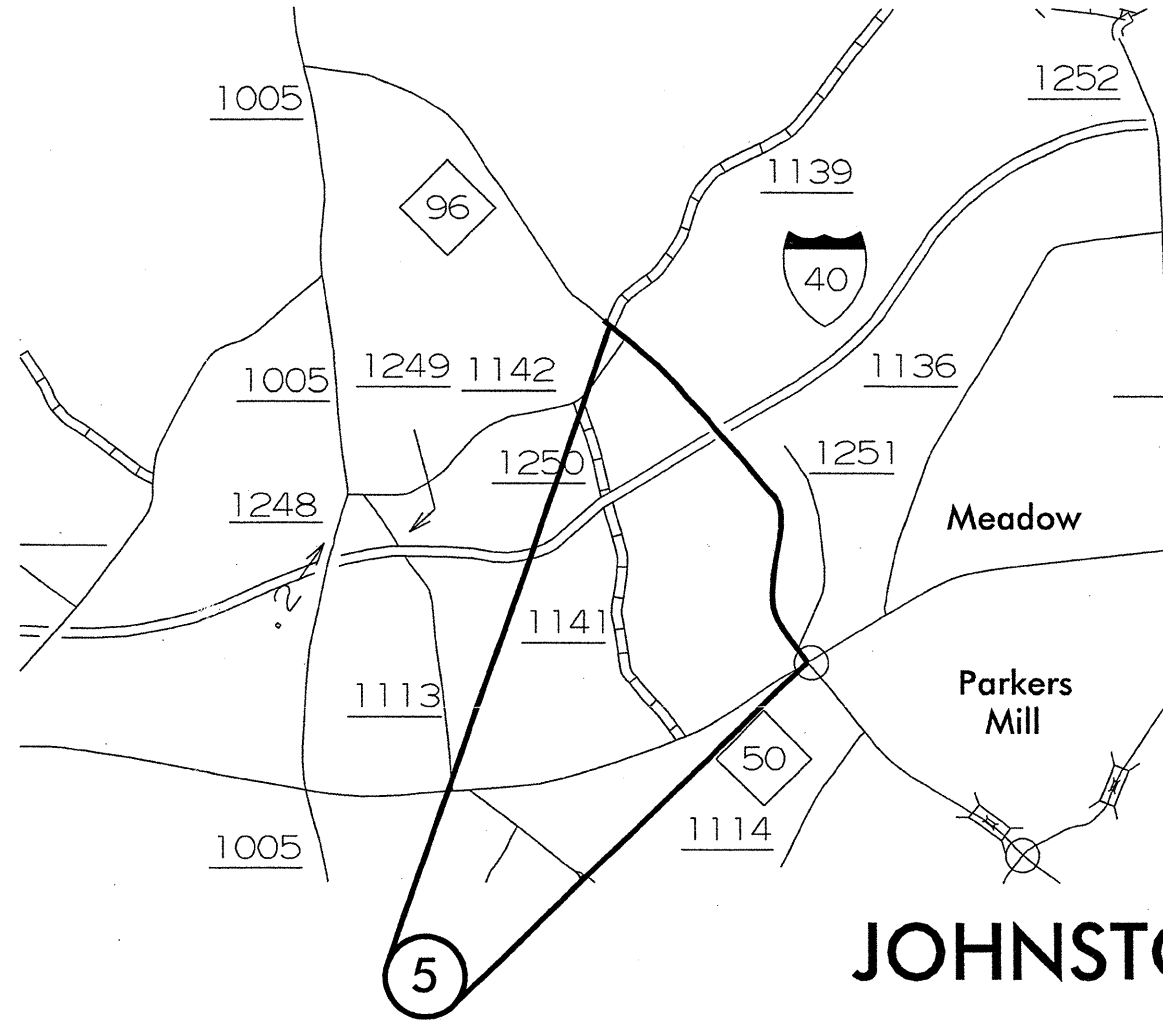
**JOHNSTON COUNTY
NORTH CAROLINA
SHEET 1 OF 3**



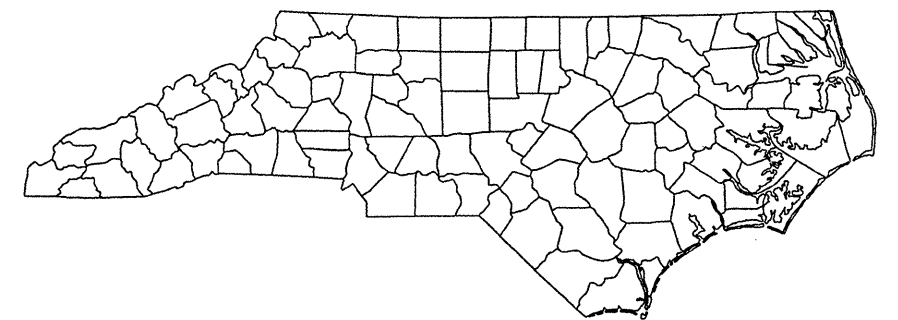
WBS ELEMENT	SHEET NO.	TOTAL SHEETS
4CR.10511.21	2	
DESCRIPTION		



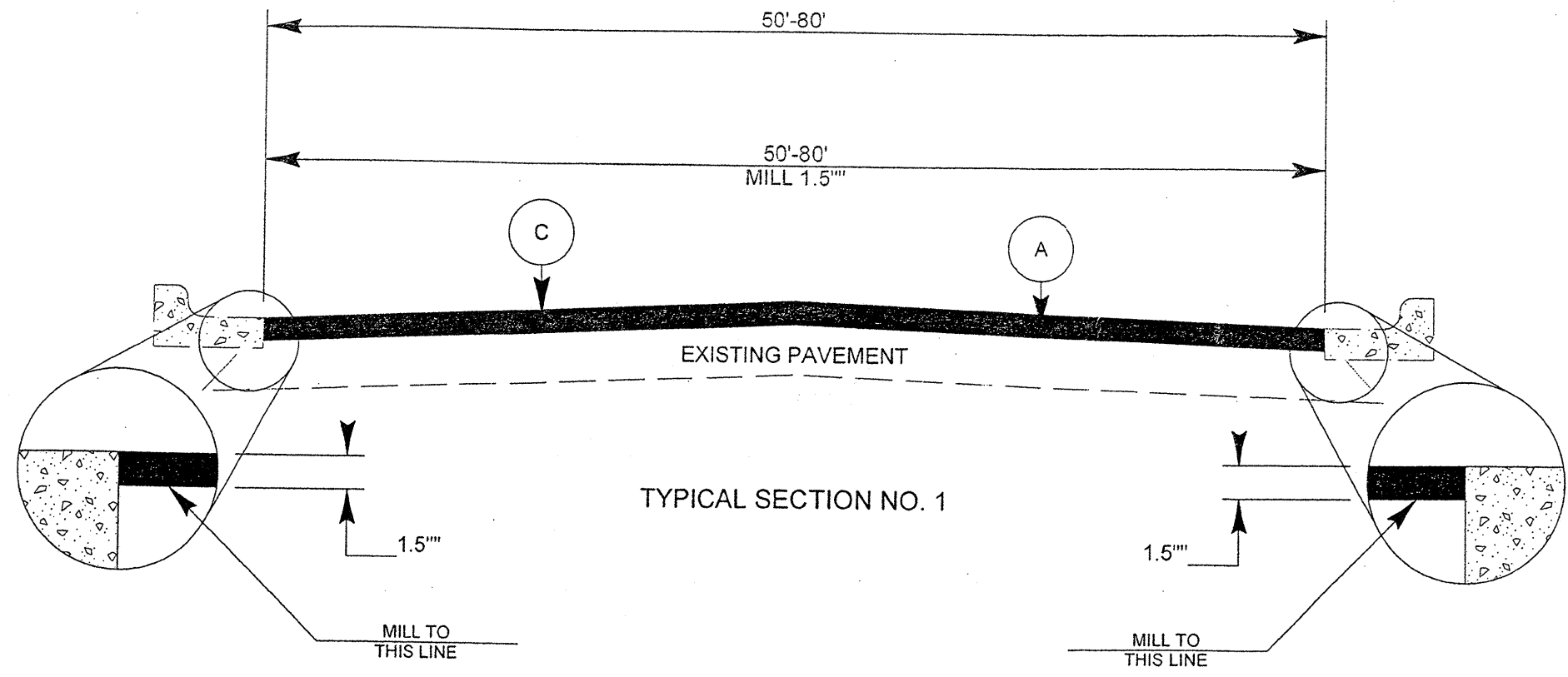
MILL ENTIRE PROJECT TO
FRONT OF GUTTER TO
A DEPTH OF 1.5"



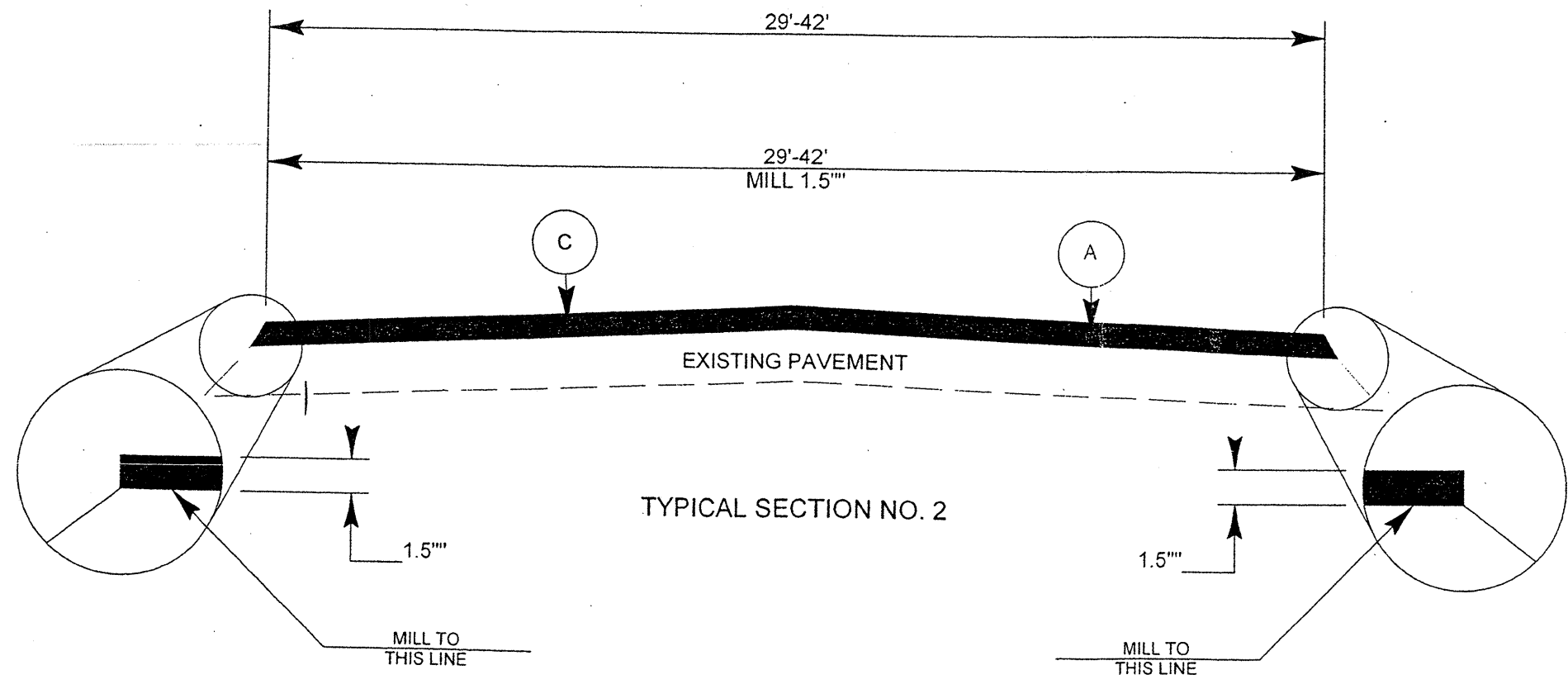
JOHNSTON COUNTY
NORTH CAROLINA
SHEET 2 OF 3



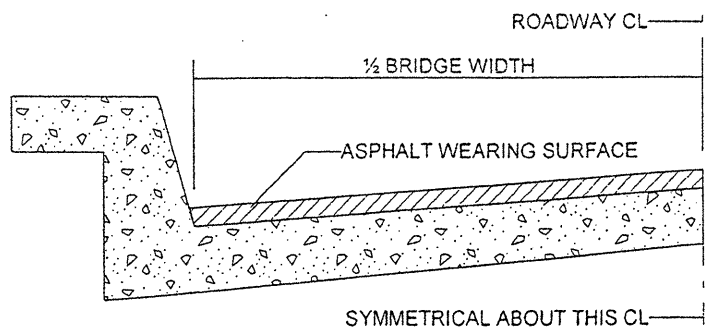
PROJECT NO. 4CR.10511.21	SHEET NO. 4	TOTAL SHEETS
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TYPICAL SECTION NO. 1



TYPICAL SECTION NO. 2



BRIDGE HALF TYPICAL SECTION

FOR BRIDGES WITH FLOOR DRAINS, CARE SHALL BE EXERCISED IN PLACING THE WEARING SURFACE AROUND FLOOR DRAINS SO AS NOT TO HINDER EFFECTIVE DRAINAGE. ALL DRAINS SHALL BE LEFT OPEN

THE PROPOSED WEARING SURFACE SHALL VARY IN THICKNESS AS NECESSARY TO PROVIDE A SMOOTH RIDING SURFACE. THE MINIMUM THICKNESS SHOULD DEPEND ON PAVEMENT TYPE AS FOLLOWS: S4.75A 1/2", SF9.5A 1.0", S9.5X 1.5", S12.5X 2.0", ULTRATHIN HOT MIX ASPHALT-TYPE A 1/4", ULTRATHIN HOT MIX ASPHALT-TYPE B 5/8", ULTRATHIN HOT MIX ASPHALT-TYPE C 1/2". THE MAXIMUM THICKNESS SHOULD DEPEND ON PAVEMENT TYPE AS FOLLOWS: S4.75A 1.0", SF9.5A 1.5", S9.5X 2.0", S12.5X 2.0", ULTRATHIN HOT MIX ASPHALT-TYPE A 1/4", ULTRATHIN HOT MIX ASPHALT-TYPE B 5/8", ULTRATHIN HOT MIX ASPHALT-TYPE C 1/2".

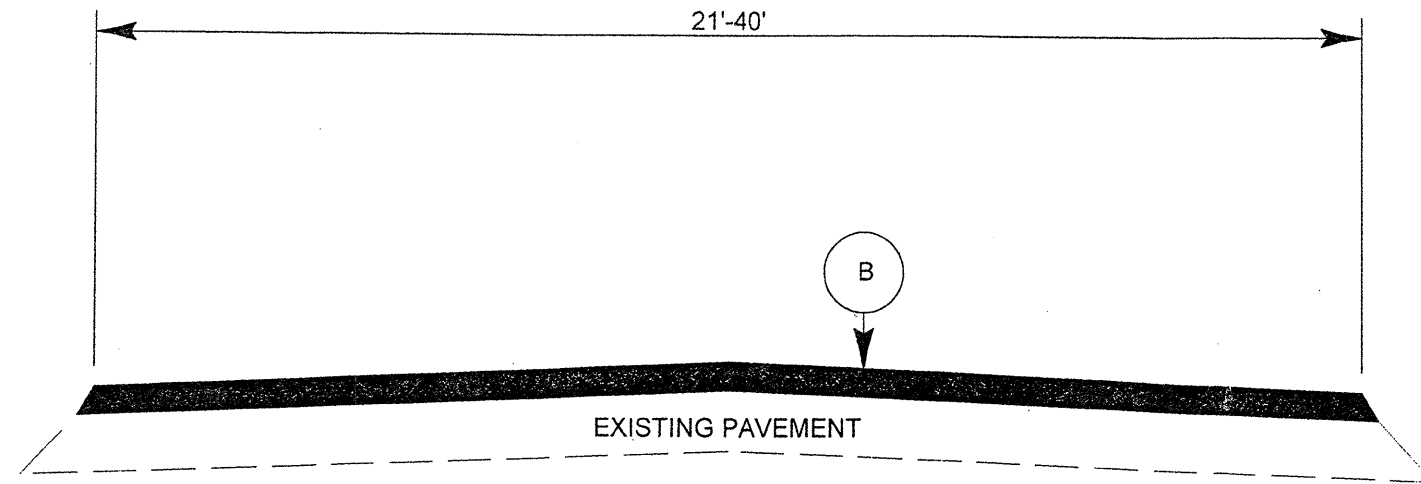
NOTES

ALL UNPAVED ROADS TO BE RESURFACED 50' FROM EDGE OF PAVEMENT OF MAIN PROJECT. ALL PAVED S.R. ROADS TO BE RESURFACED TO THE ENDS OF THE RADII, OR AS DIRECTED BY THE ENGINEER. EDGES, PAVEMENT WIDENING, INTERSECTIONS AND BRIDGE FLARES ARE INCLUDED IN THE TABLE OF QUANTITIES. SHOULDERS AND DITCHES ARE TO BE CONSTRUCTED BY OTHERS UNLESS OTHERWISE INDICATED. BRIDGES ARE TO BE RESURFACED AT LOCATIONS AND TO DEPTH AS DIRECTED BY THE ENGINEER.

PAVEMENT SCHEDULE	
A	PROPOSED APPROX 1.5" OF S9.5C AT AN AVERAGE RATE OF 168 LBS. SQ. YD.
B	PROPOSED 1.5" OF S9.5B AT AN AVERAGE RATE OF 168 LBS. SQ. YD.
C	1.5" MILLING

SHOULDER RECONSTRUCTION TO BE PERFORMED BY STATE FORCES

PROJECT NO.	SHEET NO.	TOTAL SHEETS
4CR.10511.21	5	



TYPICAL SECTION NO. 3

PAVEMENT SCHEDULE	
A	PROPOSED APPROX 1.5" OF S9.5C AT AN AVERAGE RATE OF 168 LBS. SQ. YD.
B	PROPOSED 1.5" OF S9.5B AT AN AVERAGE RATE OF 168 LBS. SQ. YD.

PROJECT NO.	SHEET NO.	TOTAL NO.
4CR.10511.21	6	

SUMMARY OF QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	FINAL SURFACE TESTING REQUIRED	LENGTH MI	WIDTH FT	1.5" MILLING SY	INCIDENTAL MILLING SY	SURFACE COURSE, S9.5B TONS	SURFACE COURSE, S9.5C TONS	ASPHALT BINDER FOR PLANT MIX TONS	ADJ. OF MANHOLES EA	ADJ. OF METER OR VALVE BOX EA
4CR.10511.21	Johnston	1	US 301 N CHURCH STREET	POINT PAST NC 222 - WILSON CL.	1	NO	0.586	60	25,407			2,099	124	12	6
4CR.10511.21	Johnston	2	US 301 BRIGHTLEAF	S SIDE OF E WELLONS INTERSECTION - JOINT PAST NORTH STREET	1	NO	1.334	55	46,912			4,089	241	24	19
4CR.10511.21	Johnston	3	US 301 BRIGHTLEAF	S SIDE OF E WELLONS INTERSECTION - JOINT AT I-95 BRIDGE	2	NO	3.7	29	50,943			6,519	385	13	
4CR.10511.21	Johnston	4	US 70B MARKET STREET	JOINT BEFORE US 301 - BEGINNING OF FRONT STREET INTERSECTION	1	NO	0.57	50.5	16,901			1,437	85	22	3
4CR.10511.21	Johnston	5	NC 96	NC50 JOINT- PAINT LINE PAST SR 1139	3	NO	1.273	21			2,025		122		
4CR.10511.21	Johnston	6	NC 210	HARNETT COUNTY LINE - START OF NC 50 INTERSECTION	3	NO	6.403	24.5			8,845		531		
4CR.10511.21	Johnston	7	US 701	JOINT AT SR 1178 TO SR 1148	3	NO	2.517	27		20	3,455		207		
TOTAL FOR PROJ NO. 4CR.10511.21							16.383		140,163	20	14,325	14,144	1,695	71	28
GRAND TOTAL							16.383		140,163	20	14,325	14,144	1,695	71	28

THERMOPLASTIC AND PAINT QUANTITIES

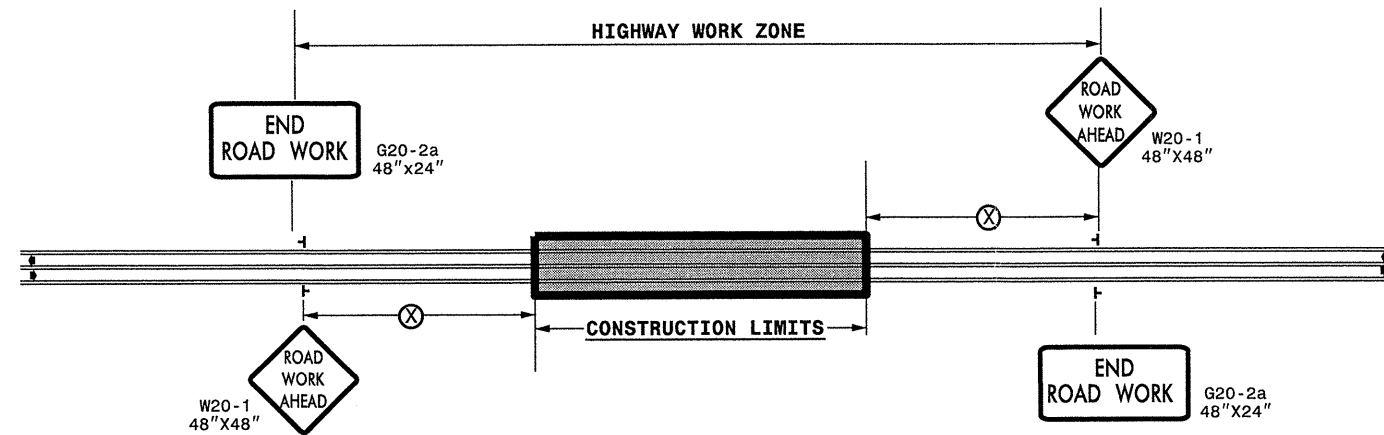
PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	LENGTH	WIDTH	4815000000-E		4835000000-E	4840000000-N	4845000000-N						
							6" YELLOW PAINT LF	6" WHITE PAINT LF	24" WHITE PAINT LF	PAINT MSG SCHOOL EA	PAINT LT ARROW EA	PAINT STR ARROW EA	PAINT STR & RT ARROW EA	PAINT RT ARROW EA	PAINT STR & LT ARROW EA		
4CR.10511.21	Johnston	1	US 301 N CHURCH STREET	POINT PAST NC 222 - WILSON CL.	0.586	60	12,376	12,376	36		17	2	2				
4CR.10511.21	Johnston	2	US 301 BRIGHTLEAF	S SIDE OF E WELLONS INTERSECTION - JOINT PAST NORTH STREET	1.334	55	28,174	15,154	360		46	21	17	5	3		
4CR.10511.21	Johnston	3	US 301 BRIGHTLEAF	S SIDE OF E WELLONS INTERSECTION - JOINT AT I-95 BRIDGE	3.7	29	42,176	57,276	36		102		1	1			
4CR.10511.21	Johnston	4	US 70B MARKET STREET	JOINT BEFORE US 301 - BEGINNING OF FRONT STREET INTERSECTION	0.57	50.5	6,024	12,494	240		6	13	22	3	12		
4CR.10511.21	Johnston	5	NC 96	NC50 JOINT- PAINT LINE PAST SR 1139	1.273	21	16,804	18,554	100	12	7		4				
4CR.10511.21	Johnston	6	NC 210	HARNETT COUNTY LINE - START OF NC 50 INTERSECTION	6.403	24.5	84,520	75,296	96		20	1	4	2			
4CR.10511.21	Johnston	7	US 701	JOINT AT SR 1178 TO SR 1148	2.517	27	26,580	26,580									
TOTAL FOR PROJ NO. 4CR.10511.21							16.383		216,654	217,730	868	12	198	37	46	15	15
									434,384				311				
GRAND TOTAL							16.383		216,654	217,730	868	12	198	37	46	15	15
									434,384				311				

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WBS ELEMENT: 4CR.10511.21

PROJ. REFERENCE NO. SEE TO THE LEFT	SHEET NO. TCP-1
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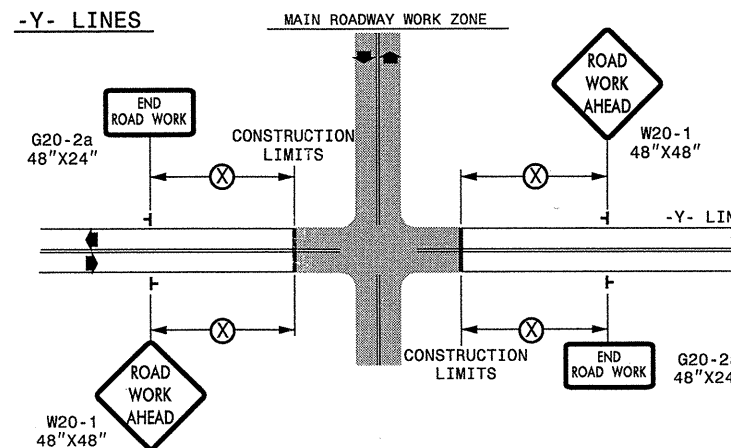
TWO-WAY UNDIVIDED ** (L-LINES)



POSTED SPEED LIMIT (M.P.H.)	RECOMMENDED MINIMUM SIGN SPACING
≤ 50	500'
≥ 55	1000'

STATE OF NORTH CAROLINA
 DEPT. OF TRANSPORTATION
 DIVISION OF HIGHWAYS
 RALEIGH, N.C.

ROADWAYS INTERSECTING ALONG 2 WAY UNDIVIDED WORK ZONE (Y-LINES)



DETAIL DRAWING FOR
 TWO-WAY UNDIVIDED
 WORK ZONE WARNING SIGNS

GENERAL NOTES

- USE FLUORESCENT ORANGE SHEETING (TYPE VII OR HIGHER) ON ALL ADVANCED WORK ZONE SIGNS.
- DO NOT INSTALL ADVANCE WARNING SIGNS MORE THAN 3 DAYS PRIOR TO BEGINNING OF WORK.
- SIGNS SHOWN ARE REQUIRED FOR WORK ZONES THAT WILL REMAIN IN EFFECT OVERNIGHT. FOR SHORT-TERM DAILY MAINTENANCE TYPE OPERATIONS, THIS SIGNING APPLICATION IS OPTIONAL; MAY USE ONLY APPLICABLE ROADWAY STANDARD DRAWINGS INSTEAD. HOWEVER, IF THIS SIGNING APPLICATION IS USED, SIGNS MAY BE PORTABLE MOUNTED.
- ALL SIGN SPACING DIMENSIONS ARE APPROXIMATE, FIELD ADJUST AS NECESSARY OR AS DIRECTED.
- USE 3LB STEEL U-CHANNEL POST OR 4" X 4" WOOD POST FOR ALL WORK ZONE SIGNS. 3LB STEEL U-CHANNEL POSTS MUST MEET THE REQUIREMENTS OF STANDARD SPECIFICATION SECTION 1094-1(B), MAY BE GALVANIZED STEEL, OR MAY BE PAINTED GREEN BY THE POST MANUFACTURER. SQUARE STEEL TUBING POSTS HAVING EQUIVALENT STRENGTH OF THE 3 LB STEEL U-CHANNEL POST ARE ALSO ACCEPTABLE FOR USE. ERECT SIGNS PER ROADWAY STANDARD DRAWING 1110.01. PAYMENT FOR WOOD POSTS, 3LB STEEL U-CHANNEL AND SQUARE STEEL TUBING POSTS WITH SIGNS WILL BE MADE ACCORDING TO STANDARD SPECIFICATION "WORK ZONE SIGNS" SECTION 1110.
- WHEN NECESSARY, USE SPLICING IN ACCORDANCE WITH ROADWAY STANDARD DRAWING NO. 1110.01. REMOVE ENTIRE POST WHEN REMOVING SIGNS WITH SPLICED POSTS.
- DO NOT BACK BRACE SIGN SUPPORTS.
- ** TWO-WAY UNDIVIDED ADVANCE WARNING SIGN CONFIGURATION MAY BE USED ON URBAN MULTI-LANE FACILITIES WHERE CONDITIONS LIMIT THE USE OF DUAL MOUNTED SIGNS AS DETERMINED BY THE ENGINEER.

LEGEND

STATIONARY SIGN
 DIRECTION OF TRAFFIC FLOW

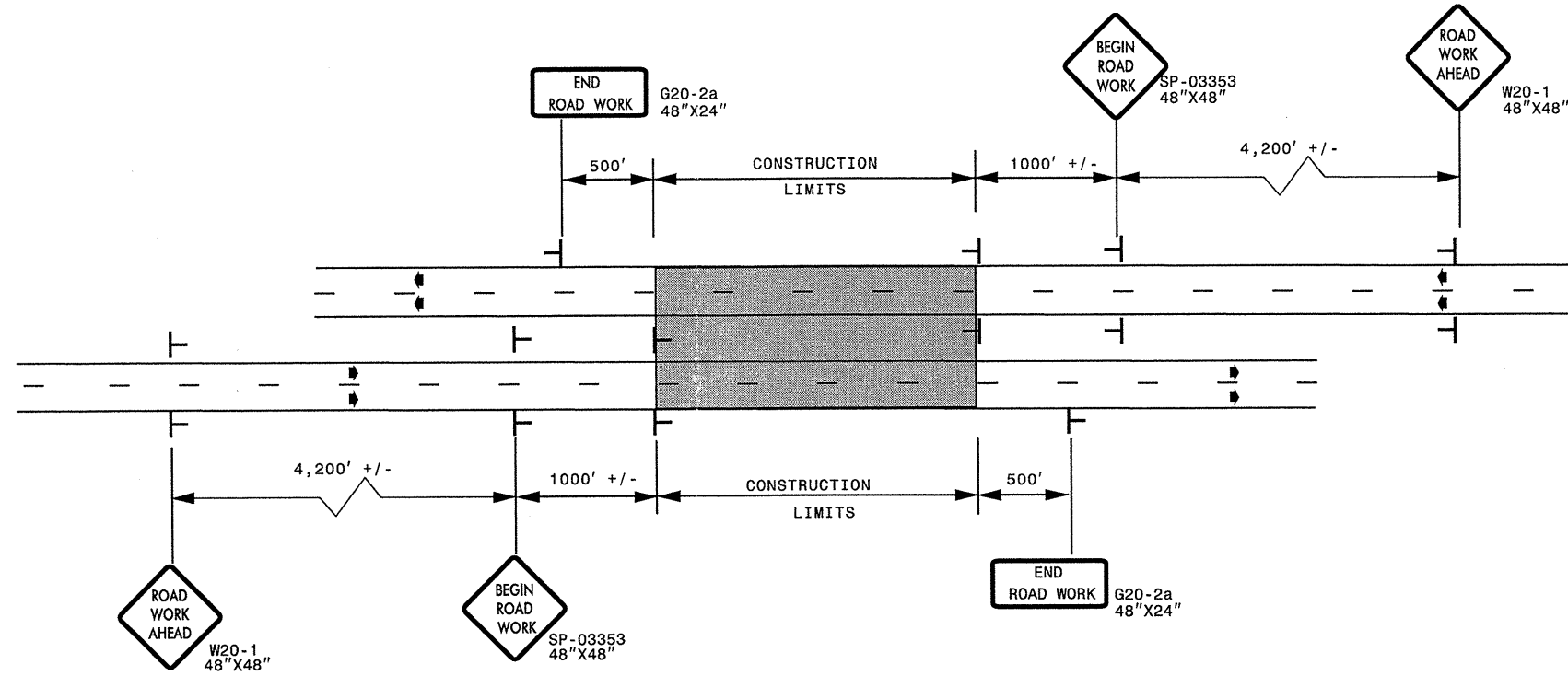
SHEET 1 OF 1

APPROVED: _____ DATE: _____	DETAIL DRAWING FOR TWO-WAY UNDIVIDED AND URBAN FREEWAYS ADVANCED WORK ZONE WARNING SIGNS		
SEAL 	SCALE: NONE		
	DATE: _____		REVISIONS
	DWG. BY: _____		7-98 10/01
	DESIGN BY: _____		10-98 03/04
REVIEWED BY: _____	01/01 11/04	CADD FILE	

ADVANCED WORK ZONE WARNING SIGNING FOR FREEWAYS (4 LANES OR GREATER) WBS ELEMENT: 4CR.10511.21

PROJ. REFERENCE NO.	SHEET NO.
SEE TO THE LEFT	TCP-2

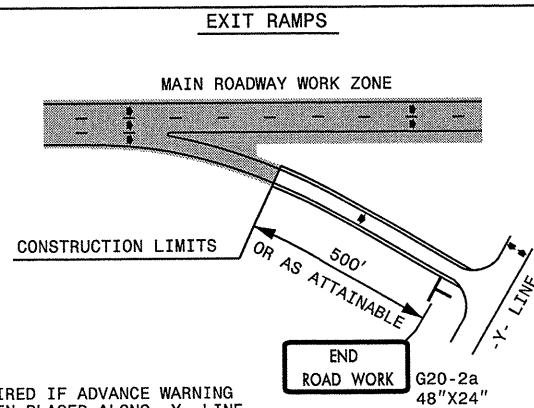
DETAIL A



LEGEND	
—	STATIONARY SIGN
→	DIRECTION OF TRAFFIC FLOW

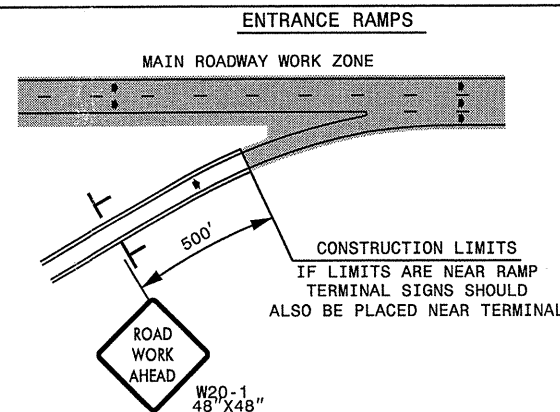
* USE THE "\$250 SPEEDING PENALTY" SIGN, SPEED LIMIT SIGN, AND ORANGE PANEL; ONLY WHEN A "\$250 SPEEDING PENALTY" ORDINANCE HAS BEEN ISSUED BY THE REGIONAL TRAFFIC ENGINEER.

DETAIL B



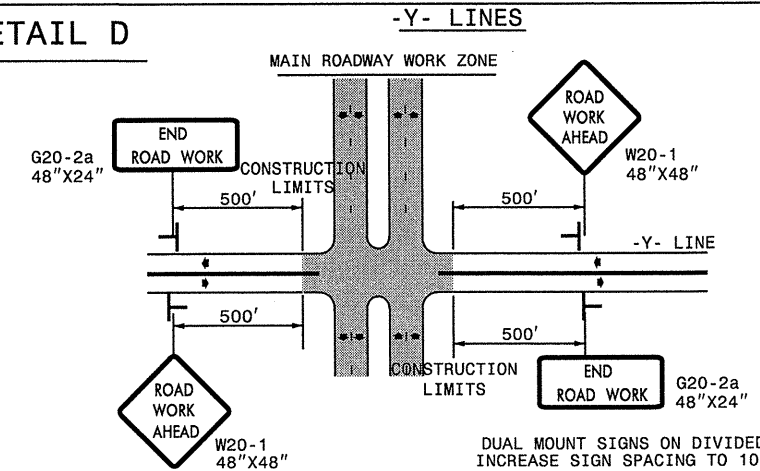
NOTE: SIGN NOT REQUIRED IF ADVANCE WARNING SIGNS HAVE BEEN PLACED ALONG -Y- LINE THAT RAMP INTERSECTS. IF CONSTRUCTION LIMITS ARE AT END OF RAMP, PLACE SIGN AT END OF RAMP.

DETAIL C



CONSTRUCTION LIMITS IF LIMITS ARE NEAR RAMP TERMINAL SIGNS SHOULD ALSO BE PLACED NEAR TERMINAL

DETAIL D



DUAL MOUNT SIGNS ON DIVIDED HIGHWAYS AND INCREASE SIGN SPACING TO 1000'+/-.

GENERAL NOTES

- USE FLUORESCENT ORANGE SHEETING (TYPE VII OR HIGHER) ON ALL ADVANCED WORK ZONE SIGNS.
- DO NOT INSTALL ADVANCE WARNING SIGNS MORE THAN 3 DAYS PRIOR TO BEGINNING OF WORK.
- SIGNS SHOWN ARE REQUIRED FOR WORK ZONES THAT WILL REMAIN IN EFFECT OVERNIGHT. FOR SHORT-TERM DAILY MAINTENANCE TYPE OPERATIONS, THIS SIGNING APPLICATION IS OPTIONAL; MAY USE ONLY APPLICABLE ROADWAY STANDARD DRAWINGS INSTEAD. HOWEVER, IF THIS SIGNING APPLICATION IS USED, SIGNS MAY BE PORTABLE MOUNTED.
- ALL SIGN SPACING DIMENSIONS ARE APPROXIMATE, FIELD ADJUST AS NECESSARY OR AS DIRECTED.
- USE 3LB STEEL U-CHANNEL POST OR 4" X 4" WOOD POST FOR ALL WORK ZONE SIGNS. 3LB STEEL U-CHANNEL POSTS MUST MEET THE REQUIREMENTS OF STANDARD SPECIFICATION SECTION 1094-1(B), MAY BE GALVANIZED STEEL, OR MAY BE PAINTED GREEN BY THE POST MANUFACTURER. SQUARE STEEL TUBING POSTS HAVING EQUIVALENT STRENGTH OF THE 3 LB STEEL U-CHANNEL POST ARE ALSO ACCEPTABLE FOR USE. ERECT SIGNS PER ROADWAY STANDARD DRAWING 1110.01. PAYMENT FOR WOOD POSTS, 3LB STEEL U-CHANNEL AND SQUARE STEEL TUBING POSTS WITH SIGNS WILL BE MADE ACCORDING TO STANDARD SPECIFICATION "WORK ZONE SIGNS" SECTION 1110.
- WHEN NECESSARY, USE SPLICING IN ACCORDANCE WITH ROADWAY STANDARD DRAWING NO. 1110.01. REMOVE ENTIRE POST WHEN REMOVING SIGNS WITH SPLICED POSTS.
- DO NOT BACK BRACE SIGN SUPPORTS.






APPROVED: _____ DATE: _____	ADVANCED WORK ZONE WARNING SIGNS FOR FREEWAYS (4 LANES OR GREATER)	
SEAL	SCALE: NONE	REVISIONS
	DATE: _____	03/04
	DESIGN BY: _____	_____
	REVIEWED BY: _____	_____

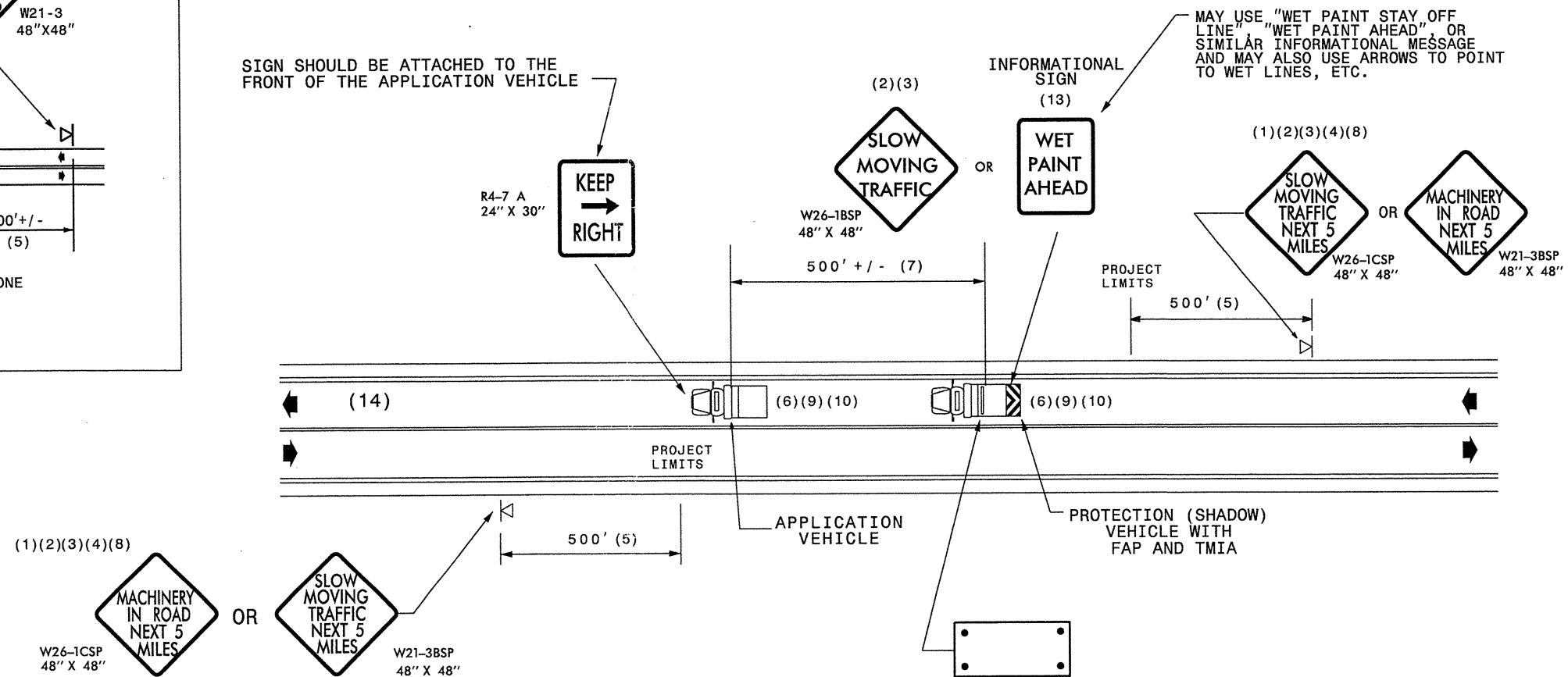
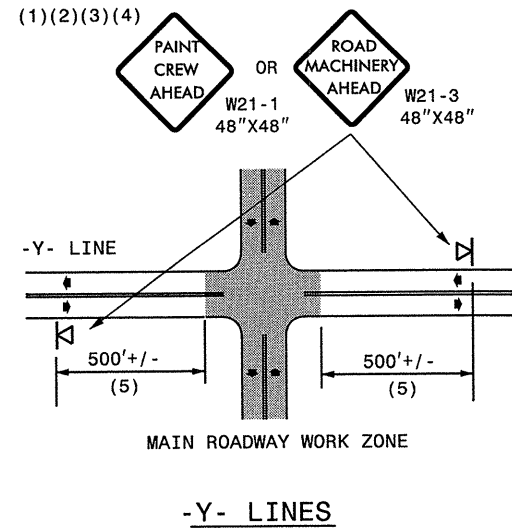
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GENERAL NOTES

- (1) THE FOLLOWING OPTIONS MAY BE USED FOR ADVANCE WARNING SIGNS:
 - A. TRUCK MOUNTED SIGNS
 - B. TRUCK MOUNTED CHANGEABLE MESSAGE SIGN (CMS)
 - C. GROUND MOUNTED ADVANCE WARNING SIGNS (MUST CIRCLE TO PICK UP SIGNS)
 - D. GROUND MOUNTED CHANGEABLE MESSAGE SIGN (CMS) (MUST USE CIRCLE TO PICK UP SIGNS)
- (2) ALL ADVANCE WARNING SIGNS MUST BE 48" X 48" WITH FLUORESCENT ORANGE TYPE VII, VIII OR IX SHEETING. IF SPACE LIMITATIONS ON SHOULDER PROHIBIT A 48" X 48" SIGN, A SMALLER SIGN CAN BE USED WITH APPROVAL FROM ENGINEER.
- (3) SIGNS ON VEHICLES SHOULD BE MOUNTED A MINIMUM OF ONE (1) FOOT FROM THE GROUND AND SHOULD NOT BLOCK THE MOTORIST'S SIGHT OF THE FLASHING ARROW PANEL AND/OR LIGHTBAR.
- (4) GROUND MOUNTED ADVANCED WARNING SIGNS SHOULD BE MOUNTED A MINIMUM OF ONE (1) FOOT FROM THE GROUND TO BOTTOM OF SIGN.
- (5) SIGN SPACING SHOULD BE ADJUSTED FOR HORIZONTAL AND VERTICAL CURVES, ETC. TO IMPROVE SIGHT DISTANCES.
- (6) ADDITIONAL VEHICLES SHOULD BE USED IN WORK CARAVAN TO FACILITATE DRYING OF PAVEMENT MARKING MATERIAL (TMIA'S ARE OPTIONAL ON THESE ADDITIONAL VEHICLES). HOWEVER, THE FIRST VEHICLE MOTORISTS SEE IN THE TRAVEL LANE SHALL HAVE A TMIA.
- (7) ADJUST DISTANCE AS NEEDED TO PREVENT MOTORISTS FROM ENTERING SPACE BETWEEN THE APPLICATION AND PROTECTION VEHICLE. DISTANCE CAN BE LENGTHENED TO ACCOMODATE SIGHT DISTANCE NEEDS.
- (8) ROUND UP MILEAGE TO NEXT WHOLE MILE. WORK ZONE SHOULD NOT EXCEED FIVE (5) MILES IN LENGTH.
- (9) RADIO COMMUNICATION BETWEEN VEHICLES IS REQUIRED.
- (10) USE OF A LIGHT BAR ON ALL VEHICLES IS PREFERRED, BUT A ROTATING BEACON MAY BE USED INSTEAD.
- (11) IF WORK IS PERFORMED AT NIGHT, THE WORK AREA MUST BE ILLUMINATED WITH MACHINE AND/OR TOWER LIGHTS AS APPROVED BY THE ENGINEER.
- (12) ALL TRAFFIC CONTROL DEVICES WILL BE CONSIDERED INCIDENTAL TO THE PAY ITEMS FOR PAVEMENT MARKING AND MARKERS.
- (13) INFORMATIONAL SIGNS SHOULD BE ACTIVITY SPECIFIC, i.e. "PAINT CREW IN ROAD". SIGNS MAY BE RECTANGULAR OR DIAMOND SHAPE. SIGN SIZE SHOULD BE BASED ON THE MOTORIST ABILITY TO RECOGNIZE SIGN WHEN TRAVELING FIVE (5) MILES ABOVE POSTED SPEED LIMIT.
- (14) IF A LEAD VEHICLE IS ADDED TO OPERATION, IT SHOULD HAVE THE SAME ADVANCE WARNING SIGNS AS THE APPLICATION VEHICLE SHOWN BELOW.

LEGEND

-  PORTABLE SIGN. SIGNS MUST BE NCHRP-350 AND NCDOT APPROVED.
-  DIRECTION OF TRAFFIC FLOW
-  APPLICATION VEHICLE WITH LIGHT BAR
-  PROTECTION VEHICLE WITH TRUCK MOUNTED IMPACT ATTENUATOR (TMIA) AND LIGHT BAR (SEE ROADWAY STANDARD NO. 1165.01). TMIA MUST BE NCHRP-350 TEST LEVEL 3 (60+MPH) APPROVED.
-  FLASHING ARROW PANEL, TYPE "B" (60"X30" MIN.), "CAUTION MODE"



MOVING OPERATION CARAVAN

(OPERATIONS TRAVELING 3 MPH OR FASTER)
PLACING PAVEMENT MARKING OR MARKERS
ON TWO-LANE TWO-WAY ROADWAYS






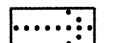

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IMPLEMENTATION DATE: 07/01/97
REVISED: 11/03/04

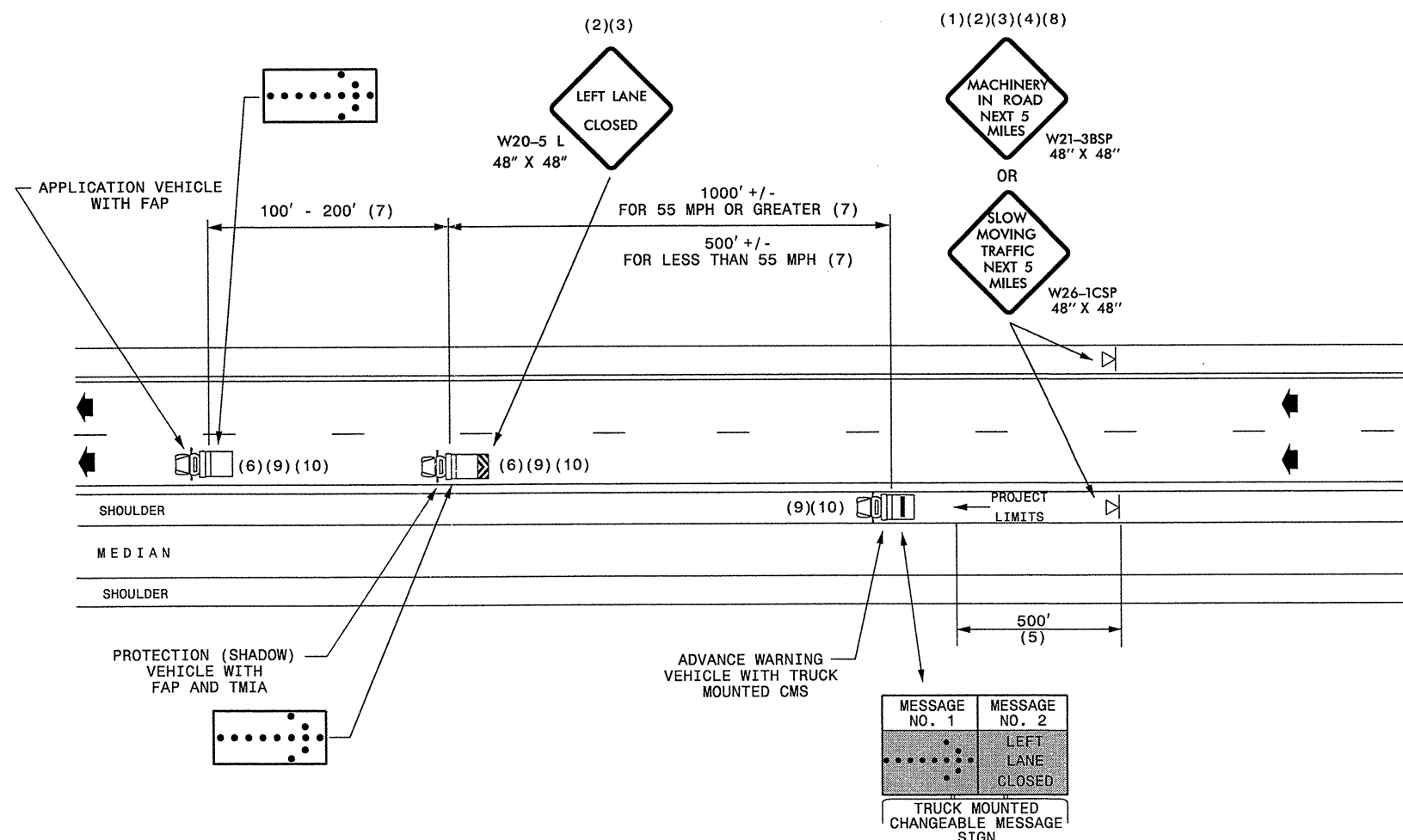
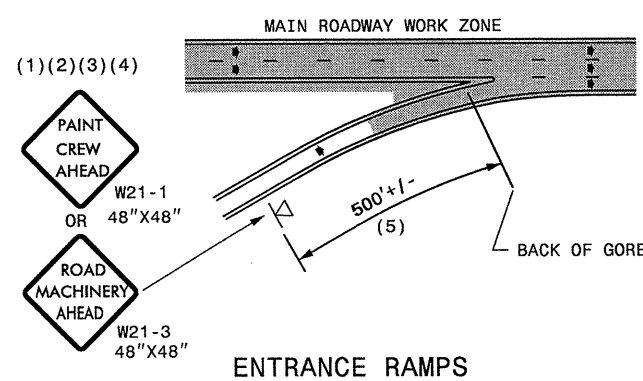
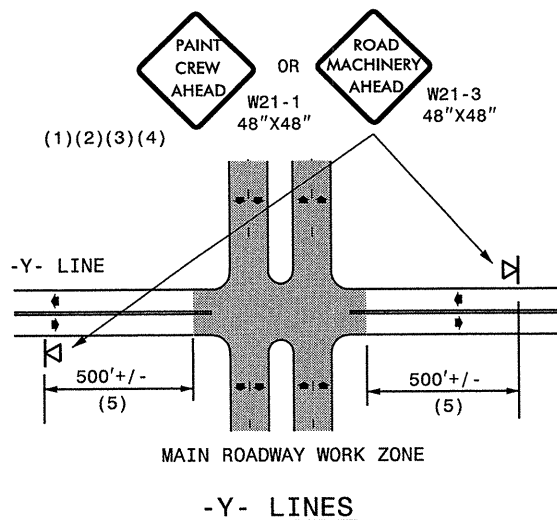
GENERAL NOTES

- (1) THE FOLLOWING OPTIONS MAY BE USED FOR ADVANCE WARNING SIGNS:
 - A. TRUCK MOUNTED SIGNS
 - B. TRUCK MOUNTED CHANGEABLE MESSAGE SIGN (CMS)
 - C. GROUND MOUNTED ADVANCE WARNING SIGNS (MUST CIRCLE TO PICK UP SIGNS)
 - D. GROUND MOUNTED CHANGEABLE MESSAGE SIGN (CMS) (MUST USE CIRCLE TO PICK UP SIGNS)
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- (3) SIGNS ON VEHICLES SHOULD BE MOUNTED A MINIMUM OF ONE (1) FOOT FROM THE GROUND AND SHOULD NOT BLOCK THE MOTORIST'S SIGHT OF THE FLASHING ARROW PANEL AND/OR LIGHTBAR.
- (4) GROUND MOUNTED ADVANCED WARNING SIGNS SHOULD BE MOUNTED A MINIMUM OF FIVE (5) FEET FROM THE GROUND TO BOTTOM OF SIGN.
- (5) SIGN SPACING SHOULD BE ADJUSTED FOR HORIZONTAL AND VERTICAL CURVES, ETC. TO IMPROVE SIGHT DISTANCES.

- (6) ADDITIONAL VEHICLES SHOULD BE USED IN WORK CARAVAN TO FACILITATE DRYING OF PAVEMENT MARKING MATERIAL (TMIA'S ARE OPTIONAL ON THESE ADDITIONAL VEHICLES). HOWEVER, THE FIRST VEHICLE MOTORISTS SEE IN THE TRAVEL LANE SHALL HAVE A TMIA.
- (7) ADJUST DISTANCE AS NEEDED TO PREVENT MOTORISTS FROM ENTERING SPACE BETWEEN THE APPLICATION AND PROTECTION VEHICLE. DISTANCE CAN BE LENGTHENED TO ACCOMMODATE SIGHT DISTANCE NEEDS.
- (8) ROUND UP MILEAGE TO NEXT WHOLE MILE. WORK ZONE SHOULD NOT EXCEED FIVE (5) MILES IN LENGTH.
- (9) RADIO COMMUNICATION BETWEEN VEHICLES IS REQUIRED.
- (10) USE OF A LIGHT BAR ON ALL VEHICLES IS PREFERRED, BUT A ROTATING BEACON MAY BE USED INSTEAD.
- (11) IF WORK IS PERFORMED AT NIGHT, THE WORK AREA MUST BE ILLUMINATED WITH MACHINE AND/OR TOWER LIGHTS AS APPROVED BY THE ENGINEER.
- (12) ALL TRAFFIC CONTROL DEVICES WILL BE CONSIDERED INCIDENTAL TO THE PAY ITEMS FOR PAVEMENT MARKING AND MARKERS.

LEGEND

-  PORTABLE SIGN. SIGNS MUST BE NCHRP-350 AND NCDOT APPROVED.
-  DIRECTION OF TRAFFIC FLOW
-  APPLICATION VEHICLE WITH LIGHT BAR
-  PROTECTION VEHICLE WITH TRUCK MOUNTED IMPACT ATTENUATOR (TMIA) AND LIGHT BAR (SEE ROADWAY STANDARD NO. 1165.01). TMIA MUST BE NCHRP-350 TEST LEVEL 3 (60+MPH) APPROVED.
-  ADVANCE WARNING VEHICLE WITH TRUCK MOUNTED CHANGEABLE MESSAGE SIGN (CMS) AND LIGHT BAR. MESSAGE SIGN LETTER HEIGHT SHOULD BE A MINIMUM OF 10 INCHES.
-  FLASHING ARROW PANEL, TYPE "B" (60"X30" MIN.), APPROPRIATE DIRECTION INDICATED
-  CHANGEABLE MESSAGE SIGN



MOVING OPERATION CARAVAN

(OPERATIONS TRAVELING 3 MPH OR FASTER)
PLACING PAVEMENT MARKING OR MARKERS
ON NON-INTERSTATE MULTILANE DIVIDED ROADWAYS

DRAWING NUMBER 7
IMPLEMENTATION DATE: 07/01/97
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