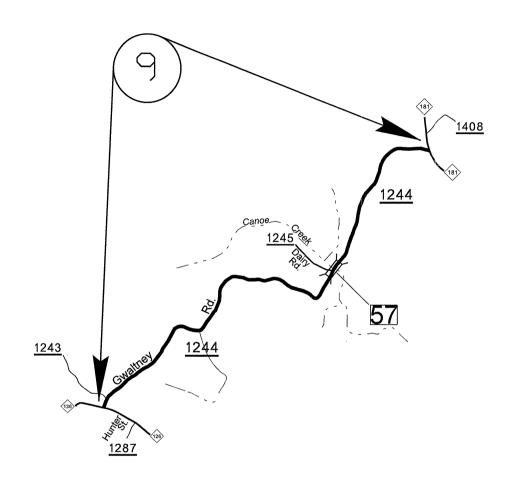
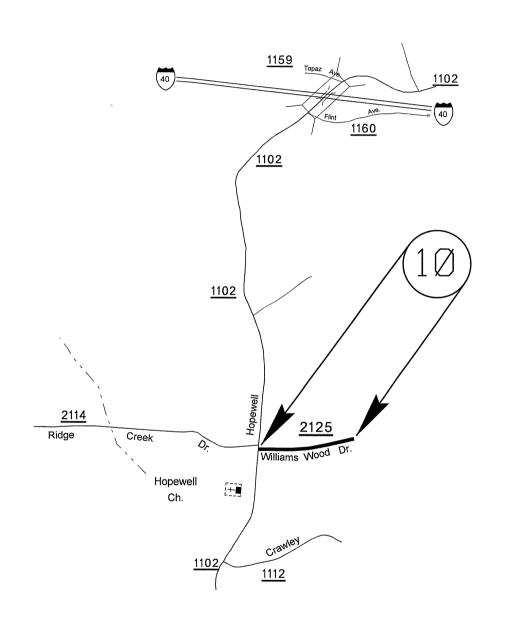
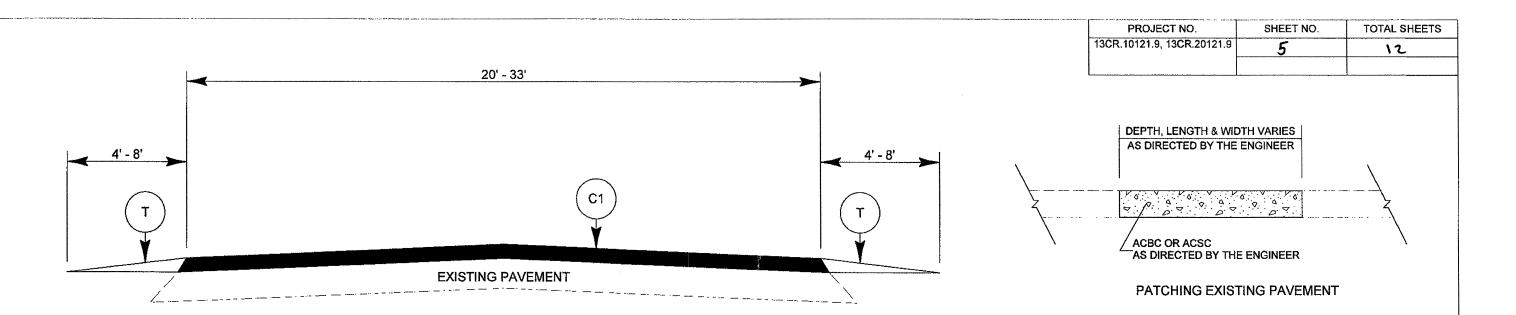


PROJECT NO.	SHEET NO.	TOTAL SHEETS
13CR.10121.9	4	12
13CR.20121.9		

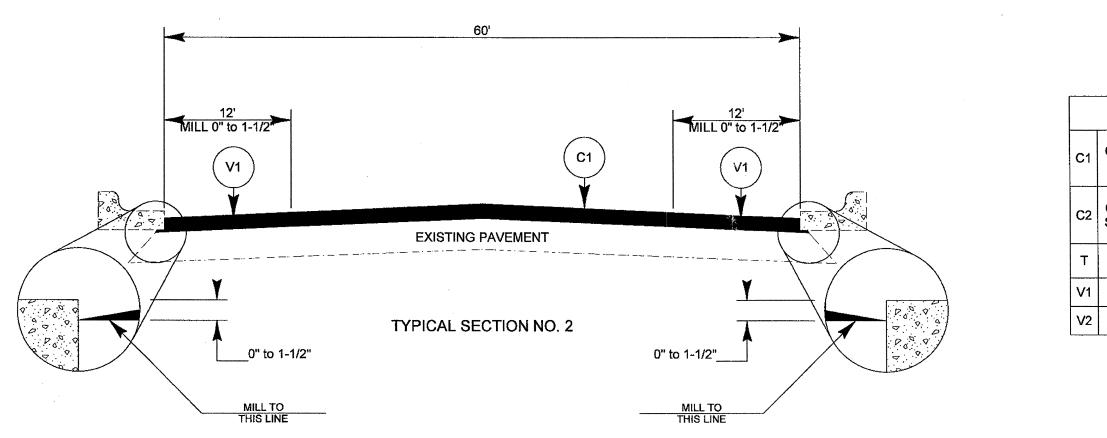




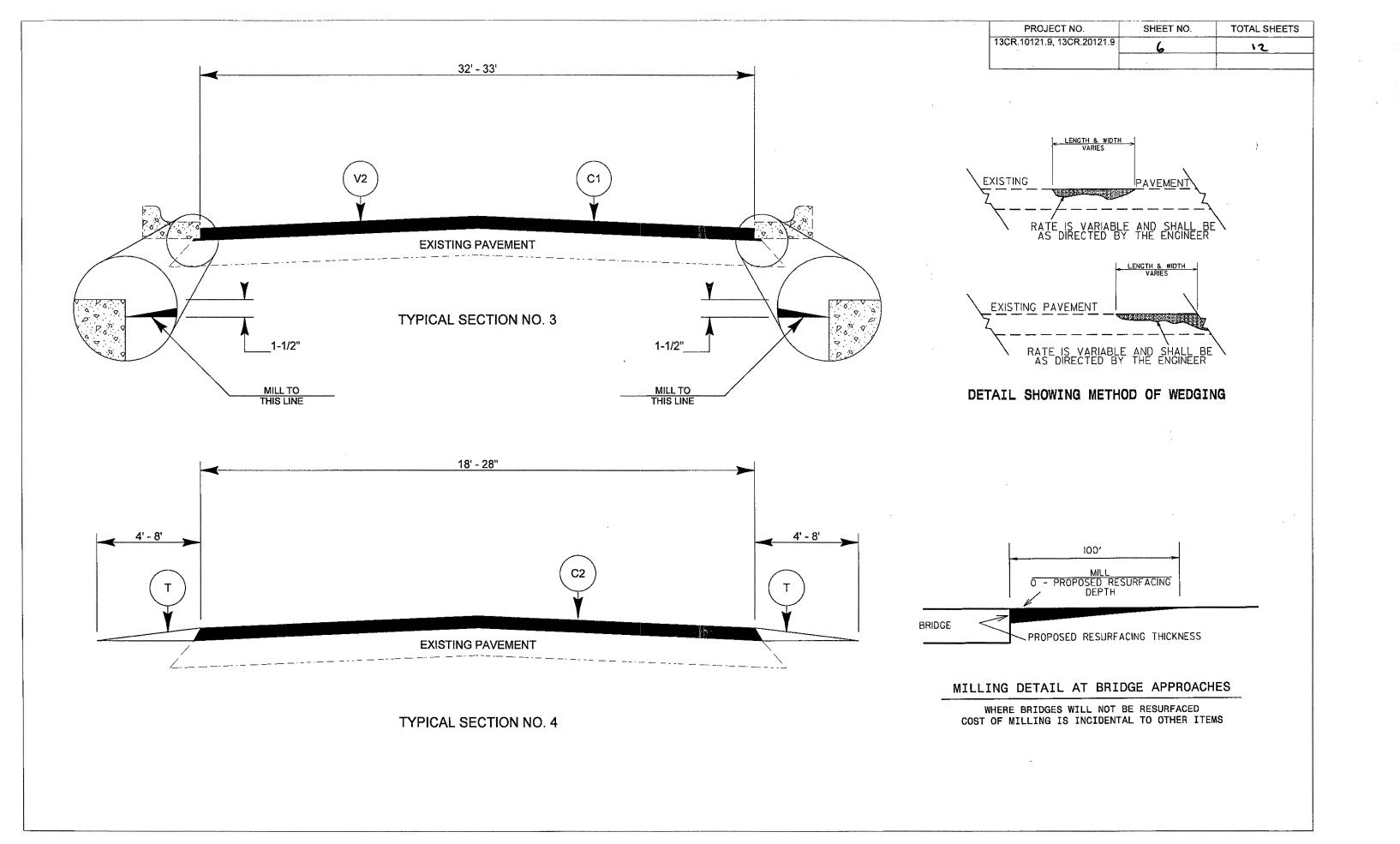
BURKE COUNTY



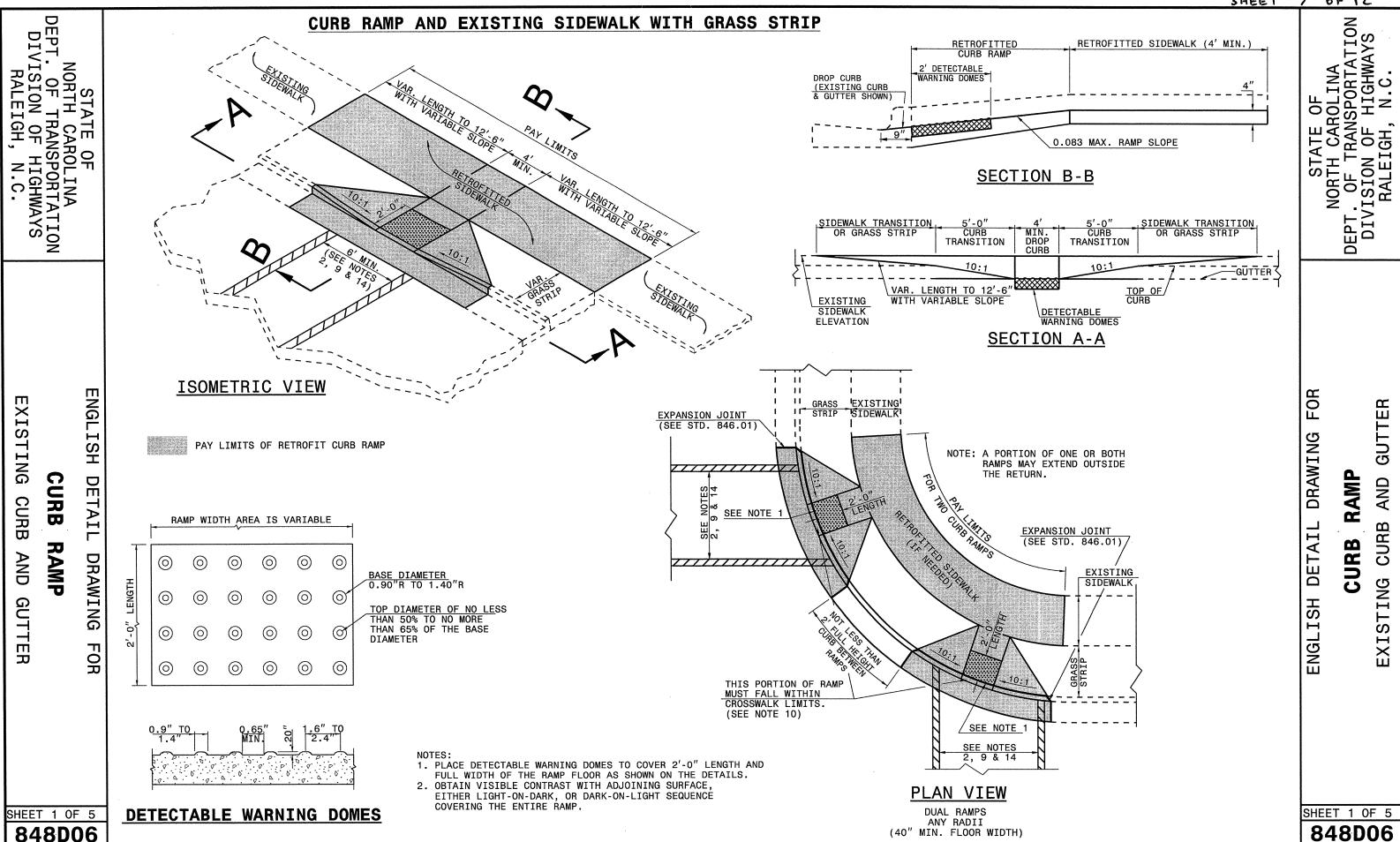
TYPICAL SECTION NO. 1



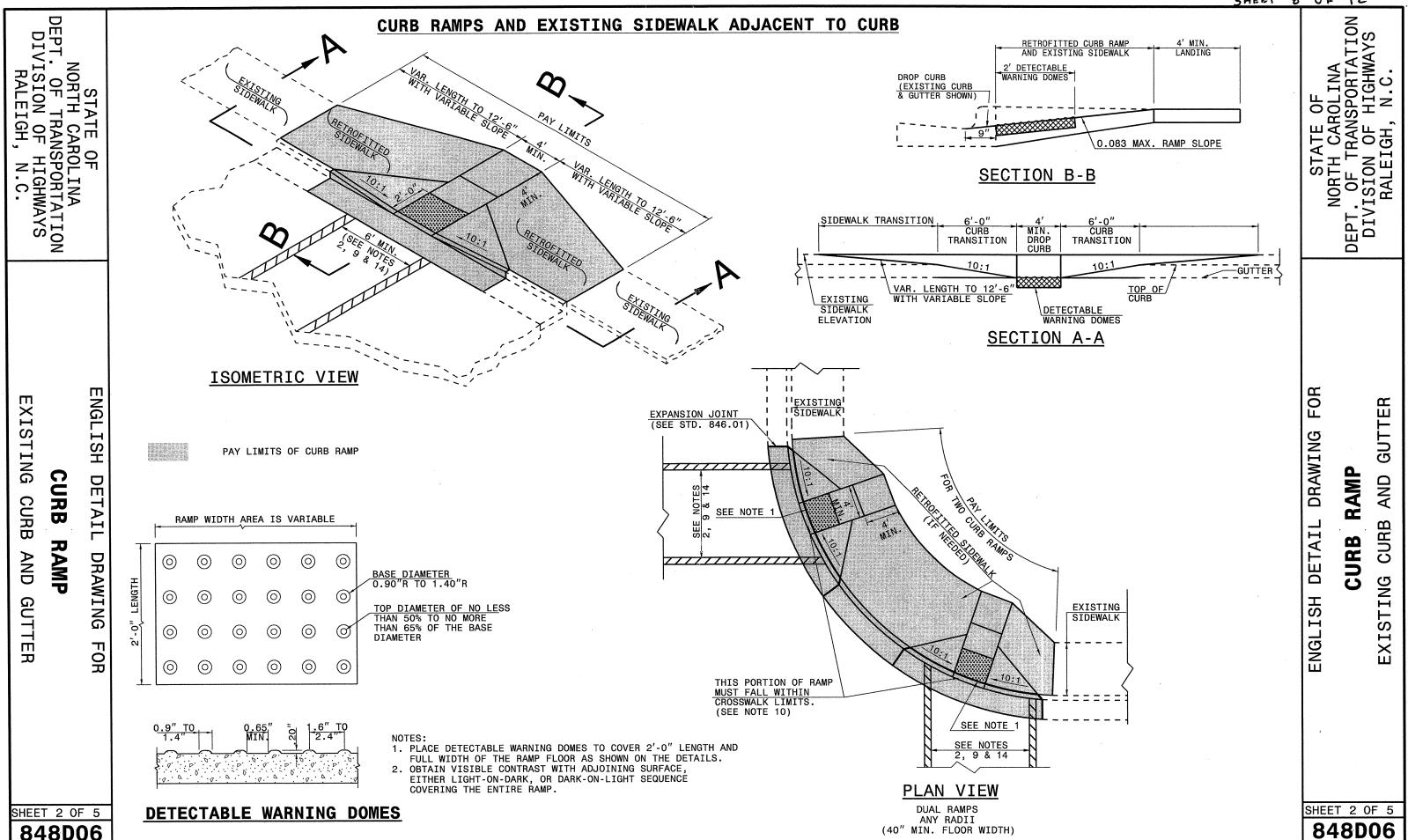
PAVEMENT SCHEDULE								
C1	PROP. APPROX. 1-1/2" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YARD							
C2	PROP. APPROX. 1-1/2" ASPHALT CONCRETE SURFACE COURSE, TYPE SF9.5A, AT AN AVERAGE RATE OF 165 LBS. PER SQ. YARD							
Т	SHOULDER RECONSTRUCTION							
V1	MILLING ASPHALT PAVEMENT, 0" TO 1-1/2" DEPTH							
V2	MILLING ASPHALT PAVEMENT, 1-1/2" DEPTH							



SHEET 7 OF 12



SHEET 8 OF 12



SHEET 9 OF 12 STATE OF
NORTH CAROLINA
DEPT. OF TRANSPORTATION
DIVISION OF HIGHWAYS
RALEIGH, N.C. NORTH CAROLINA OF TRANSPORTATION ISION OF HIGHWAYS RALEIGH, N.C. RETROFITTING DETECTABLE WARNING DOMES ONTO EXISTING CURB RAMP PROPOSED 2' DETECTABLE
WARNING DOMES 0 STATE NORTH CAF SECTION B-B DEPT. DIVI MIN. DROP CURB -GUTTER 7 PROPOSED 2' DETECTABLE WARNING DOMES SIDEWALK ELEVATION PROPOSED DETECTABLE
WARNING DOMES SECTION A-A ISOMETRIC VIEW **ENGL** FOR EXISTING EXISTING SIDEWALK GUTTER PAY LIMITS OF RETROFIT CURB RAMP HSI DRAWING RAMP CURB DETAI AND **CURB** SEE NOTE 1 RAMP WIDTH AREA IS VARIABLE CURB DETAIL CURB RAMP AND DRAWING 0 0 STING 0 GUTTER ISH TOP DIAMETER OF NO LESS THAN 50% TO NO MORE THAN 65% OF THE BASE DIAMETER 0 ENGL FOR THIS PORTION OF RAMP MUST FALL WITHIN CROSSWALK LIMITS. (SEE NOTE 10) SEE NOTE 1. PLACE DETECTABLE WARNING DOMES TO COVER 2'-0" LENGTH AND FULL WIDTH OF THE RAMP FLOOR AS SHOWN ON THE DETAILS. OBTAIN VISIBLE CONTRAST WITH ADJOINING SURFACE, EITHER LIGHT-ON-DARK, OR DARK-ON-LIGHT SEQUENCE COVERING THE ENTIRE RAMP. PLAN VIEW DETECTABLE WARNING DOMES DUAL RAMPS SHEET 3 OF 5 SHEET 3 OF 5 ANY RADII 848D06 848D06 (40" MIN. FLOOR WIDTH)

ATION STATE NORTH CAF DEPT. DIVI

> GUTTER AND CURB

RAMP CURB EXISTING

DRAWING

DETAIL **ENGLISH** 

FOR

ALLOWABLE LOCATIONS DUAL RAMP RADII.....ANY

SIDEWALK

4'-0" (TYP)

SIDEWALK

18 1 1 111 1 1

111 1 1 18 1 1

6' MIN. CROSSWALK

DETAIL SHOWING TYPICAL LOCATION OF CURB

RAMPS, PEDESTRIAN CROSSWALKS AND STOP LINES

1 1

SEE NOTE 12

1 1 11

SĪDEWĀLK

STOP LINE

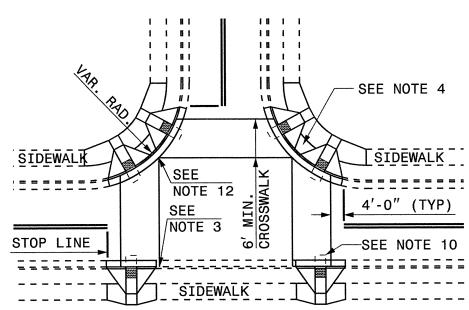
SIDEWALK

SEE NOTE 4

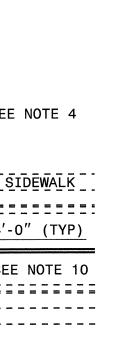
SEE NOTE 10-

SHEET 4 OF 5 848D06

## CURB RAMPS AND EXISTING SIDEWALK



DETAIL SHOWING TYPICAL LOCATION OF CURB RAMPS, PEDESTRIAN CROSSWALKS AND STOP LINES FOR TEE INTERSECTIONS



RESURFACING PROJECTS

PROPOSED CURB RAMP W/ LANDING FOR RESURFACING PROJECTS

EXISTING SIDEWALK

SHEET 4 OF 5

STATE OF
NORTH CAROLINA
DEPT. OF TRANSPORTATION
DIVISION OF HIGHWAYS
RALEIGH, N.C.

**ENGLISH** 

DETAIL

DRAWING

FOR

EXISTING

CURB

AND

GUTTER

CURB

RAMP

848D06

SPORTATION HIGHWAYS N.C.

STATE OF NORTH CAROL DEPT. OF TRANSPO DIVISION OF HI RALEIGH, N

STATE OF
NORTH CAROLINA
EPT. OF TRANSPORTA
DIVISION OF HIGHW

CURB

RAMP

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NGLISH DETAIL **CURB** AND DRAWING Ш FOR

HEET 5 OF 5 848D06

### CURB RAMP AND EXISTING SIDEWALK

#### NOTES:

- 1. CONSTRUCT THE RAMP SURFACE TO BE STABLE, FIRM, AND SLIP RESISTANT. CONSTRUCT THE CURB RAMP TYPE AS SHOWN IN THE PAVEMENT MARKING PLANS OR AS DIRECTED BY THE ENGINEER.
- 2. LOCATE CURB RAMPS AND PLACE PEDESTRIAN CROSSWALK MARKINGS AS SHOWN IN THE PAVEMENT MARKING PLANS. WHEN FIELD ADJUSTMENTS REQUIRE MOVING CURB RAMPS OR MARKINGS AS SHOWN, CONTACT THE SIGNING AND DELINEATION UNIT OR LOCATE AS DIRECTED BY THE ENGINEER.
- 3. COORDINATE THE CURB RAMP AND THE PEDESTRIAN CROSSWALK MARKINGS SO A 4'x4' CLEAR SPACE AT THE BASE OF THE CURB RAMP WILL FALL WITHIN THE PEDESTRIAN CROSSWALK LINES.
- SET BACK DISTANCE FROM INSIDE CROSSWALK MARKING TO NEAREST EDGE OF TRAVEL LANE IS 4' MINIMUM.
- 5. REFER TO THE PAVEMENT MARKING PLANS FOR STOP BAR LOCATIONS AT SIGNALIZED INTERSECTIONS. IF A PAVEMENT MARKING PLAN IS NOT PROVIDED. CONTACT THE SIGNAL DESIGN SECTION FOR THE STOP BAR LOCATIONS OR LOCATE AS DIRECTED BY THE ENGINEER.
- TERMINATE PARKING A MINIMUM OF 20' BACK OF A PEDESTRIAN CROSSWALK.
- CONSTRUCT CURB RAMPS A MINIMUM OF 4' WIDE.
- CONSTRUCT THE RUNNING SLOPE OF THE RAMP 8.33% MAXIMUM.
- 9. ALLOWABLE CROSS SLOPE ON SIDEWALKS AND CURB RAMPS WILL BE 2% MAXIMUM.
- 10. CONSTRUCT THE SIDE FLARE SLOPE A MAXIMUM OF 10% MEASURED ALONG THE CURB LINE.
- 11. CONSTRUCT THE COUNTER SLOPE OF THE GUTTER OR STREET AT THE BASE OF THE CURB RAMP A MAXIMUM OF 5% AND MAINTAIN A SMOOTH TRANSITION.
- 12. CONSTRUCT LANDINGS FOR SIDEWALK A MINIMUM OF 4'x4' WITH A MAXIMUM SLOPE OF 2% IN ANY DIRECTION. CONSTRUCT LANDINGS FOR MEDIAN ISLANDS A MINIMUM OF 5'x5' WITH A MAXIMUM SLOPE OF 2% IN ANY DIRECTION.
- 13. TO USE A MEDIAN ISLAND AS A PEDESTRIAN REFUGE AREA, MEDIAN ISLANDS WILL BE A MINIMUM OF 6' WIDE. CONSTRUCT MEDIAN ISLANDS TO PROVIDE PASSAGE OVER OR THROUGHT THE ISLAND.
- 14. SMALL CHANNELIZATION ISLANDS THAT CAN NOT PROVIDE A 5'X5' LANDING AT THE TOP OF A RAMPS, WILL BE CUT THROUGH LEVEL WITH THE SURFACE STREET.
- 15. CURB RAMPS WITH RETURNED CURBS MAY BE USED ONLY WHERE PEDESTRIANS WOULD NOT NORMALLY WALK ACROSS THE RAMP. THE ADJACENT SURFACE IS PLANTING OR OTHER NON-WALKING SURFACE OR THE SIDE APPROACH IS SUBSTANTIALLY OBSTRUCTED.
- 16. PLACE A 1/2" EXPANSION JOINT WHERE THE CONCRETE CURB RAMP JOINS THE CURB AS SHOWN IN ROADWAY STANDARD DRAWING 848.01
- 17. PLACE ALL PEDESTRIAN PUSH BUTTON ACTUATORS AND CROSSING SIGNALS AS SHOWN IN THE PLANS OR AS SHOWN IN THE MUTCD.
- 18. CURB RAMPS THROUGH MEDIAN ISLANDS, SINGLE RAMPS AT DUAL CROSSWALKS OR LIMITED R/W SITUATIONS, WILL BE HANDLED BY SPECIAL DETAILS. CONTACT THE CONTRACT STANDARDS AND DEVELOPMENT UNIT FOR THE DETAILS OR FOR A SPECIAL DESIGN.

SHEET 5 OF 5

848D06

DETAIL ENGLISH

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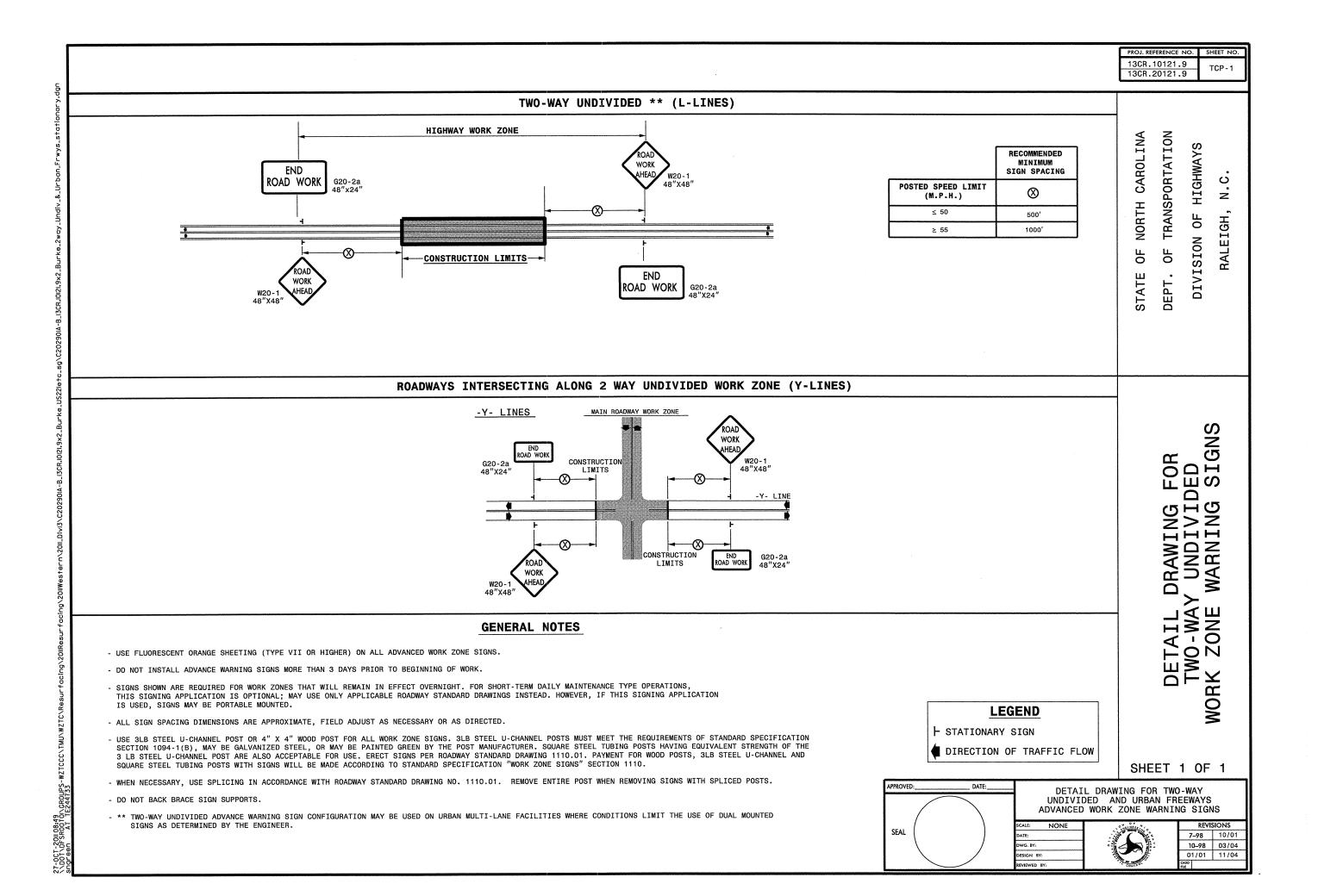
PROJECT NO.	SHEET NO.	TOTAL NO.
13CR.10121.9, 13CR.20121.9	12	12
1 '		

#### SUMMARY OF QUANTITIES

	SUMMAN OF QUANTITIES																						
PROJECT	COUN	NTY MA	AP RO	JTE	DESCRIPTION	TYP	FINAL	WMA REQUIRED	LENGTH	WIDTH	INCIDENTAL STONE	SHOULDER	MILLING ASPHALT	MILLING ASPHALT	ASPHALT CONC	ASPHALT CONC	ASPHALT BINDER	PATCHING	CONCRETE CURB	ADJUSTMENT OF	ADJUSTMENT	ADJUSTMENT	PORTABLE
			1	1			SURFACE	1			BASE	RECONSTRUCTION	PAVEMENT, 11/4"	PAVEMENT, 0" TO	SURFACE COURSE,	SURFACE COURSE,	FOR PLANT MIX	EXISTING	RAMPS	CATCH BASIN	OF MANHOLES	OF METER	LIGHTING
			-	- 1			TESTING						DEPTH	1½" DEPTH	TYPE S9.5B	TYPE SF9.5A		PAVEMENT				BOXES OR	
			- 1	l			REQUIRED															VALVE BOXES	
NO	1	N	0			NO			MI	FT	TON	SMI	SY	SY	TON	TON	TON	TON	EA	EA	EA	EA	ıs
	1				FROM MCDOWELL COUNTY LINE MP 0.00 TO AVERY COUNTY																		
13CR.10121.9	Burk	ke 1	US	221	LINE MP 0.06	1	NO		0.06	20	3.00	0.12			65		4	20					
	T				FROM 2-LANE SECTION MP 11.18 TO BRIDGE #25 OVER I-40																		
1	1	2	2 US	64	MP 12.02	2	NO	1	0.84	60				11,827	2,738		164	75			5		
	1	3	US	70	FROM NEW PAVEMENT MP 7.40 FOR 1.65 MILES MP 9.05	1	NO	YES	1.65	22	82.50	3.30			1,979		119	225					
	1	4	NC	114	FROM US 70 MP 1.74 TO SR 1531 MP 2.59	1,3	NO	YES	0.85	33	12.50	0.50	11,616		1,526		92	50	12	5	7	6	
	T	5	US 7	BUS	FROM US 64 MP 0.00 TO DIVIDE IN HWY MP 1.29	3	NO		1.29	32			24,218		2,246		135	50	16		8	1	*
TOTAL FO	R PROJ	NO. 130	R.10121	9					4.69		98.00	3.92	35,834	11,827	8,554	·	514	420	28	5	20	7	1
13CR.20121.9	Burk	ke 6	SR:	443	FROM US 70 MP 0.01 TO US 64/NC 18 MP 2.69	4	NO		2.68	28	134.00	5.36				4,013	269	550					
		7	SR	264	FROM SR 1266 MP 0.17 TO NC 181 MP 2.71	4	NO		2.54	18	127.00	5.08				2,451	164	400					
	1	8	SR:	712	FROM SR 1704 MP 0.9 TO BRIDGE #29 OVER I-40 MP 2.19	4	NO		1.29	20	64.50	2.58				1,382	93	300					
	T	9	SR:	244	FROM NC 126 MP 0.00 TO NC 181 MP 3.20	4	NO		3.2	18	160.00	6.40				3,088	207	150					
		10	0 SR :	125	FROM SR 1102 MP 0.00 TO END OF MAINTENANCE MP 0.24	4	NO		0.24	18	12.00	0.48				242	16	50					
TOTAL FO	R PROJ	NO. 130	R.20121	9		1			9.95		497.50	19.90				11,176	749	1,450	1	***************************************			
							***************************************									***************************************	······································			······		· · · · · · · · · · · · · · · · · · ·	l
	GRAND	D TOTAL		$\neg \tau$		T	1		14.64		595.50	23.82	35,834	11,827	8,554	11,176	1,263	1,870	28	5	20	7	1

#### THERMOPLASTIC AND PAINT QUANTITIES

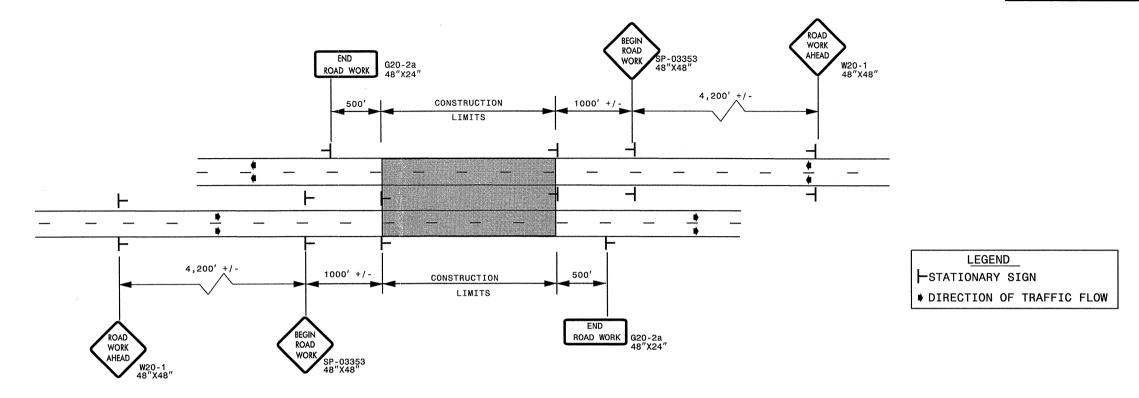
														-		<b>.</b>								·
		- 1	l				4685000000-E	46860	00000-E	4697000000-E	4705000000-E	4710000000-E		4721000000-E			47250	00000-E		481000	0000-E	4835000000-E	4845000000-N	4905000000-N
PROJECT	COUNTY M	/AP	ROUTE	DESCRIPTION	LENGTH	WIDTH	THERMOPLASTIC	THERMOPLASTIC	THERMOPLASTIC	THERMOPLASTIC	THERMOPLASTIC	THERMOPLASTIC	THERMOPLASTIC	THERMOPLASTIC	THERMOPLASTIC	THERMOPLASTIC	THERMOPLASTIC	THERMOPLASTIC	THERMOPLASTIC	PAINT	PAINT	PAINT	PAINT	SNOWPLOWABLE
			İ				PAVEMENT	PAVEMENT	PAVEMENT	PAVEMENT	PAVEMENT	PAVEMENT	PAVEMENT	PAVEMENT	PAVEMENT	PAVEMENT	PAVEMENT	PAVEMENT	PAVEMENT	PAVEMENT	PAVEMENT	PAVEMENT	PAVEMENT	PAVEMENT
			ŀ				MARKING LINES	MARKING LINES	MARKING LINES	MARKING LINES (8".	MARKING LINES	MARKING LINES	MARKING	MARKING	MARKING	MARKING SYMBOL	MARKING SYMBOL	MARKING SYMBOL	MARKING SYMBOL	MARKING LINES	MARKING LINES	MARKING LINES	MARKING	MARKERS
	1		l				(4", 90 MILS)	(4", 120 MILS)	(4", 120 MILS)	120 MILS) WHITE	(16", 120 MILS)	(24", 120 MILS)	CHARACTER (120	CHARACTER (120	CHARACTER (120	(90 MILS, LT	(90 MILS, RT	(90 MILS, STR	(90 MILS, STR & RT		(4") YELLOW		SYMBOL (LT	
		- 1	1				WHITE	YELLOW	WHITE	,	WHITE	,	MILS, ONLY)	MILS, RXR)	MILS, SCHOOL)	ARROW)	ARROW)	ARROW)	ARROW)	(,,,,,,,,,	(.,	(21)	ARROW)	1
			I				"""						, ,	,	,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,		1	""""						
NO		NO					LF	LF	LF	LF	LF	LF	EA	EA	EA	EA	EA	EA	EA	LF	LF	LF	EA	EA
				FROM MCDOWELL COUNTY LINE MP 0.00 TO AVERY COUNTY																				
13CR.10121.9	Burke	1	US 221	LINE MP 0.06	0.06	20	634	634																19
				FROM 2-LANE SECTION MP 11.18 TO BRIDGE #25 OVER I-40																				
		2	US 64	MP 12.02	0.84	60		11,088	2,950			200	8			32	6							222
		3	US 70	FROM NEW PAVEMENT MP 7.40 FOR 1.65 MILES MP 9.05	1.65	22	17,424	17,424																218
		4	NC 114	FROM US 70 MP 1.74 TO SR 1531 MP 2.59	0.85	33	2,640	9,200	122	278	90	187		. 4	12	2								113
		5 l	US 70 BUS	FROM US 64 MP 0.00 TO DIVIDE IN HWY MP 1.29	1.29	32		17,028	142	64		87				35		2	2					341
TOTAL FO	R PROJ NO. 13	2CD 10	0121.0		4.69		20,698	55,374	3,214	342	90	474	8	4	12	69	6	2	2					913
TOTAL FO	Y PROJ NO. 13		0121.9		<u> </u>	l	1	58	3,588					24				79						
13CR.20121.9	Purko	6	SD 1442	FROM US 70 MP 0.01 TO US 64/NC 18 MP 2.69	2.68	28	Т	T		T	<u> </u>			<u> </u>	T	Г	T	T	T	56,602	56,602	34	7	Τ
13CK.20121.5			SR 1264	FROM SR 1266 MP 0.17 TO NC 181 MP 2.71	2.54	18	<del> </del>												<u> </u>	53,645	53,645	34		
			SR 1712	FROM SR 1704 MP 0.9 TO BRIDGE #29 OVER I-40 MP 2.19	1.29	20	+												<b></b>	27,245	27,245	11		<del> </del>
			SR 1244	FROM NC 126 MP 0.00 TO NC 181 MP 3.20	3.2	18	1													67,584	67,584			<del> </del>
			SR 2125	FROM SR 1102 MP 0.00 TO END OF MAINTENANCE MP 0.24	0.24	18	-		<del> </del>									<del> </del>	<del> </del>	07,554	07,504			<u> </u>
	l	10 1	3K 2123	PROMISE 1102 INF 0.00 TO END OF MAINTENANCE INF 0.24	9.95	10	- <del> </del>		<del> </del>							<del> </del>	<del> </del>	<del> </del>	<del> </del>	205,076	205,076	45		
TOTAL FOR PROJ NO. 13CR.20121.9		0121.9		3.33	<del> </del>			<u> </u>	<u> </u>				L	L	<del> </del>	1	L	J		,152	43		<del> </del>	
						1	.1	l		1	L	L				L				410	,136		L	L
	CDAND TOTA		T		14.64	T T	20,698	55,374	3,214	342	90	474	8	4	12	69	6	2	2	205,076	205,076	45	2	913
GRAND TOTAL			1	1	1	50	3,588	1				24				70		010	,152			1		



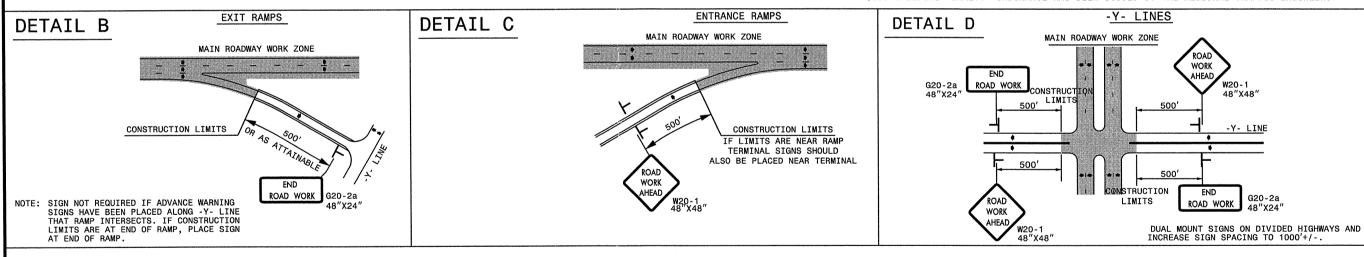
## ADVANCED WORK ZONE WARNING SIGNING FOR FREEWAYS (4 LANES OR GREATER)

TCP-2

DETAIL A

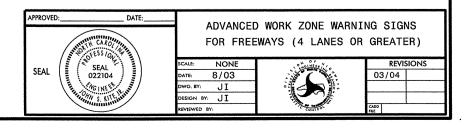


★ USE THE "\$250 SPEEDING PENALTY" SIGN, SPEED LIMIT SIGN, AND ORANGE PANEL; ONLY WHEN A "\$250 SPEEDING PENALTY" ORDINANCE HAS BEEN ISSUED BY THE REGIONAL TRAFFIC ENGINEER.

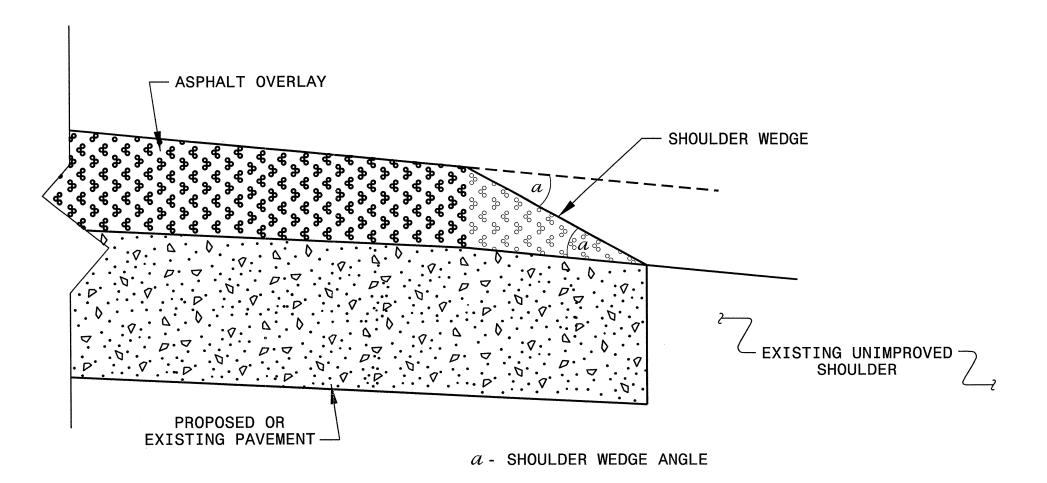


#### GENERAL NOTES

- USE FLUORESCENT ORANGE SHEETING (TYPE VII OR HIGHER) ON ALL ADVANCED WORK ZONE SIGNS.
- DO NOT INSTALL ADVANCE WARNING SIGNS MORE THAN 3 DAYS PRIOR TO BEGINNING OF WORK.
- SIGNS SHOWN ARE REQUIRED FOR WORK ZONES THAT WILL REMAIN IN EFFECT OVERNIGHT. FOR SHORT-TERM DAILY MAINTENANCE TYPE OPERATIONS, THIS SIGNING APPLICATION IS OPTIONAL; MAY USE ONLY APPLICABLE ROADWAY STANDARD DRAWINGS INSTEAD. HOWEVER, IF THIS SIGNING APPLICATION IS USED, SIGNS MAY BE PORTABLE MOUNTED.
- ALL SIGN SPACING DIMENSIONS ARE APPROXIMATE, FIELD ADJUST AS NECESSARY OR AS DIRECTED.
- USE 3LB STEEL U-CHANNEL POST OR 4" X 4" WOOD POST FOR ALL WORK ZONE SIGNS. 3LB STEEL U-CHANNEL POSTS MUST MEET THE REQUIREMENTS OF STANDARD SPECIFICATION SECTION 1094-1(B), MAY BE GALVANIZED STEEL, OR MAY BE PAINTED GREEN BY THE POST MANUFACTURER. SQUARE STEEL TUBING POSTS HAVING EQUIVALENT STRENGTH OF THE 3 LB STEEL U-CHANNEL POST ARE ALSO ACCEPTABLE FOR USE. ERECT SIGNS PER ROADWAY STANDARD DRAWING 1110.01. PAYMENT FOR WOOD POSTS, 3LB STEEL U-CHANNEL AND SQUARE STEEL TUBING POSTS WITH SIGNS WILL BE MADE ACCORDING TO STANDARD SPECIFICATION "WORK ZONE SIGNS" SECTION 1110.
- WHEN NECESSARY, USE SPLICING IN ACCORDANCE WITH ROADWAY STANDARD DRAWING NO. 1110.01. REMOVE ENTIRE POST WHEN REMOVING SIGNS WITH SPLICED POSTS.
- DO NOT BACK BRACE SIGN SUPPORTS.



PROJECT REFERENCE NO.



## SHOULDER WEDGE DETAIL

CONTRACT STANDARDS
AND DEVELOPMENT UNIT
Office 919-707-6950 FAX 919-250-4119

# SHOULDER WEDGE DETAIL

	ORIGINAL BY	/: T.SPELL	DATE:	7-19-11
ı	MODIFIED BY	/:	DATE:	
	CHECKED BY:	s:usr/details/sta	DATE:	
ı	FILE SPEC.	s:usr/details/sta	nd/shoulderwedged	etail.dgn

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