

STATE OF NORTH CAROLINA
DIVISION OF HIGHWAYS

STATE PROJECT REFERENCE NO.	SHEET NO.
B - 3 1 8 7	TCP-1

**PLAN FOR PROPOSED
TRAFFIC CONTROL, MARKING & DELINEATION
HAYWOOD COUNTY**

B - 3 1 8 7

ROADWAY STANDARD DRAWINGS

THE FOLLOWING ROADWAY STANDARDS AS APPEAR IN "ROADWAY STANDARD DRAWINGS" - PROJECT SERVICES UNIT - N.C. DEPARTMENT OF TRANSPORTATION - RALEIGH, N.C., DATED JULY 2006 ARE APPLICABLE TO THIS PROJECT AND BY REFERENCE HEREBY ARE CONSIDERED A PART OF THESE PLANS:

STD. NO.	TITLE
1101.02	TEMPORARY LANE CLOSURES
1101.03	TEMPORARY ROAD CLOSURES
1101.04	TEMPORARY SHOULDER CLOSURES
1101.05	WORK ZONE VEHICLE ACCESSES
1101.06	WARNING SIGNS FOR BLASTING ZONES
1101.11	TRAFFIC CONTROL DESIGN TABLES
1110.01	STATIONARY WORK ZONE SIGNS
1110.02	PORTABLE WORK ZONE SIGNS
1130.01	DRUMS
1135.01	CONES
1145.01	BARRICADES
1150.01	FLAGGING DEVICES
1180.01	SKINNY-DRUM
1205.01	PAVEMENT MARKINGS - LINE TYPES & OFFSETS
1205.02	PAVEMENT MARKINGS - 2 LANE & MULTILANE ROADWAYS
1205.04	PAVEMENT MARKINGS - INTERSECTIONS
1205.12	PAVEMENT MARKINGS - BRIDGES
1250.01	PAVEMENT MARKER SPACING
1251.01	RAISED PAVEMENT MARKERS - (TEMPORARY & PERMANENT)
1261.01	GUARDRAIL & BARRIER DELINEATOR SPACING
1261.02	GUARDRAIL & BARRIER DELINEATOR TYPES

INDEX OF SHEETS

SHEET NO.	TITLE
TCP-1	LIST OF APPLICABLE ROADWAY STANDARD DRAWINGS, LEGEND AND INDEX OF SHEETS
TCP-2	GENERAL NOTES
TCP-3	PHASING
TCP-4	PHASE I DETAILS
TCP-5	PHASE II DETAILS
TCP-6	PHASE III DETAILS
TCP-7	DETOUR ROUTE
TCP-8	ADVANCE WORK ZONE WARNING SIGNS
SD-1	SPECIAL SIGN DETAILS

LEGEND

- GENERAL**
- DIRECTION OF TRAFFIC FLOW
 - NORTH ARROW
 - PROPOSED PVMT. EXIST. PVMT.
 - WORK AREA
 - REMOVAL OF EXISTING PAVEMENT
- TRAFFIC CONTROL DEVICES**
- TYPE III BARRICADE
 - CONE
 - DRUM SKINNY DRUM
 - FLASHING ARROW PANEL (TYPE C)
 - STATIONARY SIGN
 - PORTABLE SIGN
 - STATIONARY OR PORTABLE SIGN
 - CRASH CUSHION
 - CHANGEABLE MESSAGE SIGN
 - TRUCK MOUNTED IMPACT ATTENUATOR (TMIA)
 - POLICE
 - FLAGGER

TIP PROJECT:

I:\AUG-2009 08:05 [dot] 091011\proj\traffic\trafficcontrol\tcp\b-3187-tc-tcp-1.dgn
KBP:3/24/09 AT WZ:12/31/09

APPROVED: _____ DATE: _____	PLAN PREPARED BY: N.C.D.O.T. WORK ZONE TRAFFIC CONTROL UNIT
SEAL 	J. S. BOURNE, P.E. <i>J.S.B.</i> TRAFFIC CONTROL ENGINEER
	G. L. GETTIER, P.E. <i>G.L.G.</i> TRAFFIC CONTROL PROJECT ENGINEER
	JESSE GILSTRAP <i>J.G.</i> TRAFFIC CONTROL PROJECT DESIGN ENGINEER
	KEN BROADWELL <i>K.B.</i> TRAFFIC CONTROL DESIGN ENGINEER / TECHNICIAN

GENERAL NOTES

CHANGES MAY BE REQUIRED WHEN PHYSICAL DIMENSIONS IN THE DETAIL DRAWINGS, STANDARD DETAILS AND ROADWAY DETAILS ARE NOT ATTAINABLE TO MEET FIELD CONDITIONS, OR RESULT IN DUPLICATE, OR UNDESIRED OVERLAPPING OF DEVICES. MODIFICATION MAY INCLUDE: MOVING, SUPPLEMENTING, COVERING OR REMOVAL OF DEVICES, AS DIRECTED BY THE ENGINEER.

THE FOLLOWING GENERAL NOTES APPLY AT ALL TIMES FOR THE DURATION OF THE CONSTRUCTION PROJECT EXCEPT WHEN OTHERWISE NOTED IN THE PLAN OR AS DIRECTED BY THE ENGINEER.

TIME RESTRICTIONS

A) DO NOT CLOSE ROADS AS FOLLOWS:

ROAD NAME	DAY AND TIME RESTRICTIONS
1. -Y1- LAKE LOGAN ROAD	MON. 2PM - TUES. 9AM TUES. 2PM - WED. 9AM WED. 2PM - THURS. 9AM THURS. 2PM - MON. 9AM

B) DO NOT STOP TRAFFIC AS FOLLOWS:

ROAD NAME	DAY AND TIME RESTRICTIONS	DURATION AND OPERATION
1. -Y1- LAKE LOGAN ROAD	MON. 2PM - TUES. 9AM TUES. 2PM - WED. 9AM WED. 2PM - THURS. 9AM THURS. 2PM - MON. 9AM	30 MIN. MAX. FOR ROCK BLASTING, EXISTING BRIDGE REMOVAL/INSTALLATIONS

C) DO NOT CONDUCT ANY HAULING OPERATIONS AGAINST THE FLOW OF TRAFFIC OF AN OPEN TRAVELWAY UNLESS THE HAULING OPERATION IS PROTECTED BY BARRIER OR GUARDRAIL OR AS DIRECTED BY THE ENGINEER.

LANE AND SHOULDER CLOSURE REQUIREMENTS

- D) REMOVE LANE CLOSURE DEVICES FROM THE LANE WHEN WORK IS NOT BEING PERFORMED BEHIND THE LANE CLOSURE OR WHEN A LANE CLOSURE IS NO LONGER NEEDED, OR AS DIRECTED BY THE ENGINEER.
- E) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN 15 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN SHOULDER USING ROADWAY STANDARD DRAWING NO. 1101.04 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL OR A LANE CLOSURE IS INSTALLED.
- F) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING ON THE SHOULDER ADJACENT TO AN UNDIVIDED FACILITY AND WITHIN 5 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN TRAVEL LANE USING ROADWAY STANDARD DRAWING NO. 1101.02 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.
- WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING ON THE SHOULDER ADJACENT TO A DIVIDED FACILITY AND WITHIN 10 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN TRAVEL LANE USING ROADWAY STANDARD DRAWING NO. 1101.02 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.
- G) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN A LANE OF TRAVEL OF AN UNDIVIDED OR DIVIDED FACILITY, CLOSE THE LANE ACCORDING TO THE TRAFFIC CONTROL PLANS, ROADWAY STANDARD DRAWINGS OR AS DIRECTED BY THE ENGINEER. CONDUCT THE WORK SO THAT ALL PERSONNEL AND/OR EQUIPMENT REMAIN WITHIN THE CLOSED TRAVEL LANE.
- H) DO NOT WORK SIMULTANEOUSLY WITHIN 15 FT ON BOTH SIDES OF AN OPEN TRAVELWAY, RAMP OR LOOP WITHIN THE SAME LOCATION UNLESS PROTECTED WITH GUARDRAIL OR BARRIER.
- I) PROVIDE TRAFFIC CONTROL FOR APPROPRIATE LANE CLOSURES FOR SURVEYING DONE BY THE DEPARTMENT.

PAVEMENT EDGE DROP OFF REQUIREMENTS

- J) BACKFILL AT A 6:1 SLOPE UP TO THE EDGE AND ELEVATION OF EXISTING PAVEMENT IN AREAS ADJACENT TO AN OPENED TRAVEL LANE THAT HAS A DROP-OFF AS FOLLOWS:
- BACKFILL DROP-OFFS THAT EXCEED 2 INCHES ON ROADWAYS WITH POSTED SPEED LIMITS OF 45 MPH OR GREATER.
- BACKFILL DROP-OFFS THAT EXCEED 3 INCHES ON ROADWAYS WITH POSTED SPEED LIMITS LESS THAN 45 MPH.
- BACKFILL WITH SUITABLE COMPACTED MATERIAL, AS APPROVED BY THE ENGINEER, AT NO EXPENSE TO THE DEPARTMENT.
- K) DO NOT EXCEED A DIFFERENCE OF 2 INCHES IN ELEVATION BETWEEN OPEN LANES OF TRAFFIC FOR NOMINAL LIFTS OF 1.5 INCHES. INSTALL ADVANCE WARNING "UNEVEN LANES" SIGNS (W8-11) 350 FEET IN ADVANCE AND A MINIMUM OF ONCE EVERY HALF MILE THROUGHOUT THE UNEVEN AREA.

TRAFFIC PATTERN ALTERATIONS

- L) NOTIFY THE ENGINEER TWENTY ONE (21) CALENDAR DAYS PRIOR TO ANY TRAFFIC PATTERN ALTERATION.

SIGNING

- M) INSTALL ADVANCE WORK ZONE WARNING SIGNS WHEN WORK IS WITHIN 40 FT FROM THE EDGE OF TRAVEL LANE AND NO MORE THAN THREE (3) DAYS PRIOR TO THE BEGINNING OF CONSTRUCTION.
- N) STATE FORCES WILL BE RESPONSIBLE FOR PERMANENT SIGNING.
- O) PROVIDE DETOUR SIGNING WITH IN AND OFF THE PROJECT LIMITS.
- P) COVER OR REMOVE ALL DETOUR SIGNS WITH IN AND OFF THE PROJECT LIMITS WHEN A DETOUR IS NOT IN OPERATION.
- Q) ENSURE ALL NECESSARY SIGNING IS IN PLACE PRIOR TO ALTERING ANY TRAFFIC PATTERN.
- R) INSTALL BLACK ON ORANGE "DIP" SIGNS (W8-2) AND/OR "BUMP" SIGNS (W8-1) 350 FEET IN ADVANCE OF THE UNEVEN AREA, OR AS DIRECTED BY THE ENGINEER.

TRAFFIC BARRIER

- S) INSTALL TEMPORARY BARRIER ACCORDING TO THE TRAFFIC CONTROL PLANS A MAXIMUM OF TWO (2) WEEKS PRIOR TO BEGINNING WORK IN ANY LOCATION. ONCE TEMPORARY BARRIER IS INSTALLED AT ANY LOCATION, PROCEED IN A CONTINUOUS MANNER TO COMPLETE THE PROPOSED WORK IN THAT LOCATION UNLESS OTHERWISE STATED IN THE TRAFFIC CONTROL PLANS OR AS DIRECTED BY THE ENGINEER.
- DO NOT PLACE BARRIER DIRECTLY ON ANY SURFACE OTHER THAN ASPHALT OR CONCRETE.
- ONCE TEMPORARY BARRIER IS INSTALLED AT ANY LOCATION AND NO WORK IS PERFORMED BEHIND THE TEMPORARY BARRIER FOR A PERIOD LONGER THAN TWO (2) MONTHS, REMOVE/RESET TEMPORARY BARRIER AT NO COST TO THE DEPARTMENT UNLESS OTHERWISE STATED IN THE TRAFFIC CONTROL PLANS, TEMPORARY BARRIER IS PROTECTING A HAZARD, OR AS DIRECTED BY THE ENGINEER.
- INSTALL TEMPORARY BARRIER WITH THE TRAFFIC FLOW, BEGINNING WITH THE UPSTREAM SIDE OF TRAFFIC. REMOVE TEMPORARY BARRIER AGAINST THE TRAFFIC FLOW, BEGINNING WITH THE DOWNSTREAM SIDE OF TRAFFIC.
- INSTALL AND SPACE DRUMS NO GREATER THAN TWICE THE POSTED SPEED LIMIT (MPH) TO CLOSE OR KEEP THE SECTION OF THE ROADWAY CLOSED UNTIL THE TEMPORARY BARRIER CAN BE PLACED OR AFTER THE TEMPORARY BARRIER IS REMOVED.

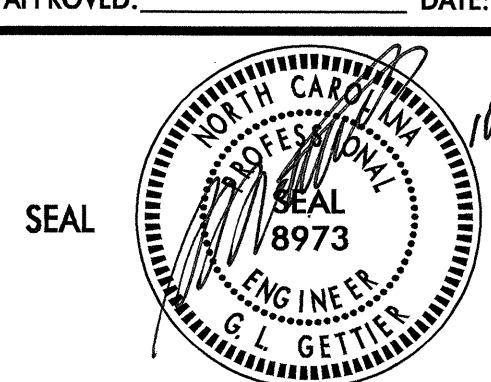
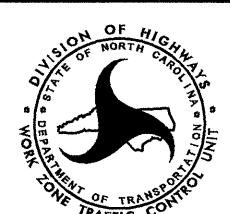
TRAFFIC CONTROL DEVICES

- T) SPACE CHANNELIZING DEVICES IN WORK AREAS NO GREATER THAN TWICE THE POSTED SPEED LIMIT (MPH), EXCEPT 10 FT ON-CENTER IN RADII, AND 3 FT OFF THE EDGE OF AN OPEN TRAVELWAY, WHEN LANE CLOSURES ARE NOT IN EFFECT. WHEN SKINNY DRUMS ARE ALLOWED REFER TO SECTION 1180 OF STANDARD SPECIFICATIONS FOR ROADS AND STRUCTURES OR AS SHOWN IN THE PLANS.
- U) PLACE TYPE III BARRICADES WITH "ROAD CLOSED" SIGN R11-2 ATTACHED OF SUFFICIENT LENGTH TO CLOSE ENTIRE ROADWAY.

PAVEMENT MARKINGS AND MARKERS

- V) INSTALL PAVEMENT MARKINGS AND PAVEMENT MARKERS ON THE FINAL SURFACE AS DESCRIBED ON THE PAVEMENT MARKING PLANS.
- W) INSTALL TEMPORARY PAVEMENT MARKINGS AND TEMPORARY PAVEMENT MARKERS ON INTERIM LAYERS OF PAVEMENT AS FOLLOWS:
- | ROAD NAME | MARKING | MARKER |
|--------------|---------|------------------|
| 1. ALL ROADS | PAINT | TEMPORARY RAISED |
- X) PLACE ONE APPLICATION OF PAINT FOR TEMPORARY TRAFFIC PATTERNS. PLACE A SECOND APPLICATION OF PAINT SIX (6) MONTHS AFTER THE INITIAL APPLICATION AND EVERY SIX MONTHS AS DIRECTED BY THE ENGINEER.
- Y) TIE PROPOSED PAVEMENT MARKING LINES TO EXISTING PAVEMENT MARKING LINES.
- Z) REMOVE/REPLACE ANY CONFLICTING/DAMAGED PAVEMENT MARKINGS AND MARKERS BY THE END OF EACH DAY'S OPERATION.

I:\AUG-2009 08:02
 \dot\dfs\opt\01\p\p\projects-b\3187\traffic\trafficcontrol\tcp-b-3187_tc_tcp-tcp-l.dgn
 KDP oadwell AT 12/10/2009 15:55

APPROVED: _____ DATE: _____	GENERAL NOTES	
	SCALE: NONE	
	DATE: MAY 09	
	DWG. BY: KPB	
	DESIGN BY: KPB	
REVIEWED BY: JWG	REVISIONS	

PROJ. REFERENCE NO.	SHEET NO.
B-3187	TCP-3

PHASING

PHASE I

- STEP 1: - CONTRACTOR SHALL PLACE ADVANCE WORK ZONE WARNING SIGNS ON SR 1112 CHAMBERS FARM LANE (-L-/-Y3-), SR1111 LAKE LOGAN ROAD (-Y1-) AND MILL DAM LANE (-Y2-) AS SHOWN ON SHEET TCP-8.
- STEP 2: - CONTRACTOR SHALL:
- PLACE DRUMS AND TYPE III BARRICADES AT STA. 12+10 +/- -L- AND MAY BEGIN CONSTRUCTION OF THE NEW BRIDGE AWAY FROM TRAFFIC ON THE EAST SIDE OF THE RIVER (SEE SHEET TCP-4 AND THE CONSTRUCTION PLANS).
 - CONSTRUCT AS MUCH AS POSSIBLE OF THE -Y1- PROPOSED ROADWAY AWAY FROM TRAFFIC UP TO BUT NOT INCLUDING THE FINAL LAYER OF SURFACE COURSE FROM STA. 15+14 +/- -Y1- TO STA. 20+48 +/- -Y1- (SEE SHEET TCP-4 AND THE CONSTRUCTION PLANS).
 - USING LANE CLOSURES PER ROADWAY STANDARD DRAWING NO. 1101.02 SHEET 1 OF 9, CONSTRUCT THE -Y1- PROPOSED ROADWAY UP TO THE EDGE AND ELEVATION OF EXISTING FROM STA. 11+47 -Y1- TO STA. 15+14 -Y1- AND FROM STA. 20+48 +/- -Y1- TO STA. 23+50 +/- -Y1- (SEE SHEET TCP-4 AND THE CONSTRUCTION PLANS). NOTE: AS DIRECTED BY THE ENGINEER THE CONTRACTOR MAY USE ROADWAY STANDARD DRAWING NO. 1101.06 SHEET 1 OF 1 OR ROADWAY STANDARD DRAWING NO. 1101.03 SHEET 1 OF 9 AND SHEET TCP-7 IF BLASTING IS REQUIRED TO EXCAVATE ROCK.
 - AS DIRECTED BY THE ENGINEER, USE ROADWAY STANDARD DRAWING NO. 1101.02 SHEET 1 OF 9, CONSTRUCT THE PROPOSED -L-, -Y2- AND -Y3- ROADWAYS EAST OF THE RIVER, USING ABC AND DRUMS TO MAINTAIN TRAFFIC/ACCESS TO ROADWAYS AND DRIVES UNTIL A SUITABLE ELEVATION IS OBTAINED AND SHIFT TRAFFIC TO THE PROPOSED TRAFFIC PATTERN (SEE SHEET TCP-4 AND THE CONSTRUCTION PLANS).

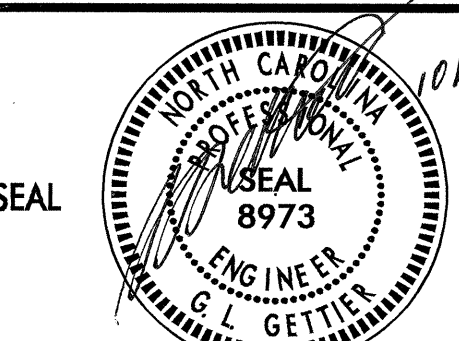
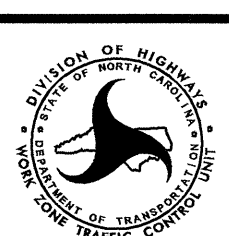
PHASE II

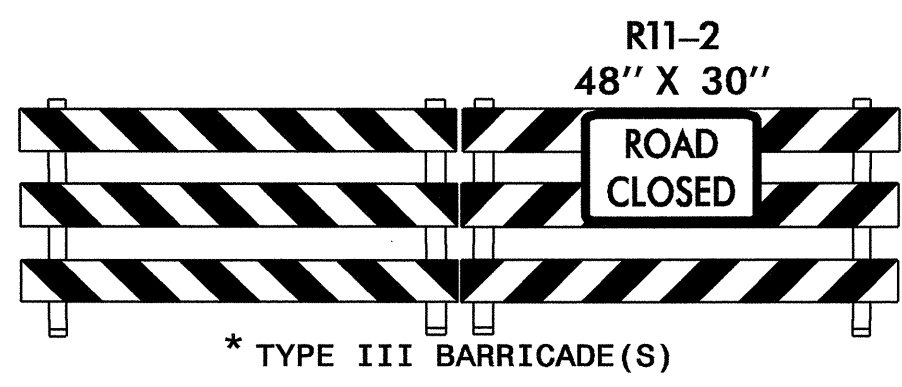
- STEP 1: - CONTRACTOR SHALL:
- AS DIRECTED BY THE ENGINEER, USING ROADWAY STANDARD DRAWING NO. 1101.02 SHEET 1 OF 9 AND/OR ROADWAY STANDARD DRAWING NO. 1101.03 SHEET 1 OF 9 AND SHEET TCP-7 CONSTRUCT THE -Y1- TIE INS FROM STA. 11+47 -Y1- TO STA. 15+14 +/- -Y1- AND FROM STA. 20+48 +/- -Y1- TO STA. 23+50 +/- -Y1- USING PAVING AND WEDGING OPERATIONS UP TO BUT NOT INCLUDING THE FINAL LAYER OF SURFACE COURSE, THEN INSTALL TEMPORARY PAVEMENT MARKINGS (PAINT) AND TEMPORARY RAISED PAVEMENT MARKERS, THEN OPEN -Y1- TO THE FINAL TRAFFIC PATTERN AND INSTALL DRUMS AND TYPE III BARRICADES (SEE SHEET TCP-5 AND THE CONSTRUCTION PLANS).
 - USING ROADWAY STANDARD DRAWING NO. 1101.02 SHEET 1 OF 9, CONSTRUCT UP TO BUT NOT INCLUDING THE FINAL LAYER OF SURFACE COURSE FROM STA. 12+40 +/- -L- TO STA. 15+25 -L-, -Y2- AND -Y3-, THEN INSTALL TEMPORARY PAVEMENT MARKINGS (PAINT) AND TEMPORARY RAISED PAVEMENT MARKERS AS SHOWN ON SHEET TCP-5 (SEE CONSTRUCTION PLANS).
- STEP 2: - CONTRACTOR SHALL:
- CONSTRUCT THE NEW BRIDGE AS MUCH AS POSSIBLE AWAY FROM TRAFFIC AND AS DIRECTED BY THE ENGINEER USING SHORT TERM ROAD CLOSURES AND DETOUR AS SHOWN ON SHEET TCP-7 TO SET BRIDGE GIRDERS (SEE SHEET TCP-5 AND THE CONSTRUCTION PLANS).
 - USING ROADWAY STANDARD DRAWING NO. 1101.02 SHEET 1 OF 9, CONSTRUCT THE -L- TIE INS TO THE NEW BRIDGE FROM STA. 10+11 +/- -L- TO STA. 10+41 +/- -L- AND FROM STA. 12+20 +/- -L- TO STA. 12+40 +/- -L- (SEE SHEET TCP-5 AND THE CONSTRUCTION PLANS).

PHASE III

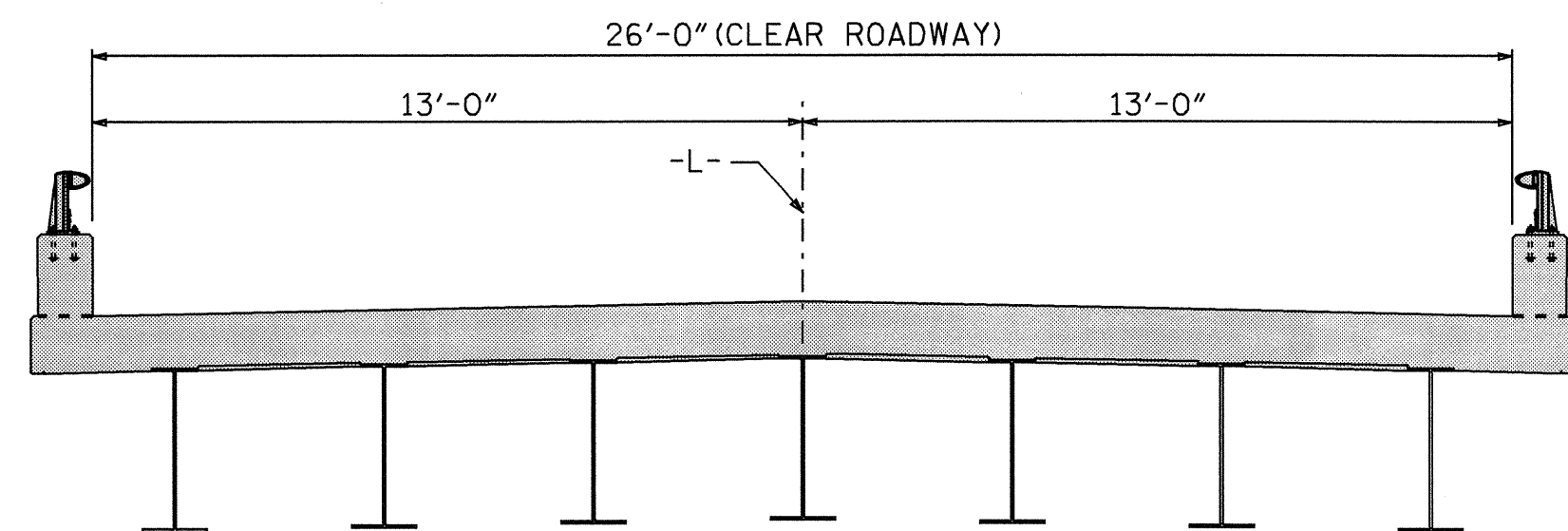
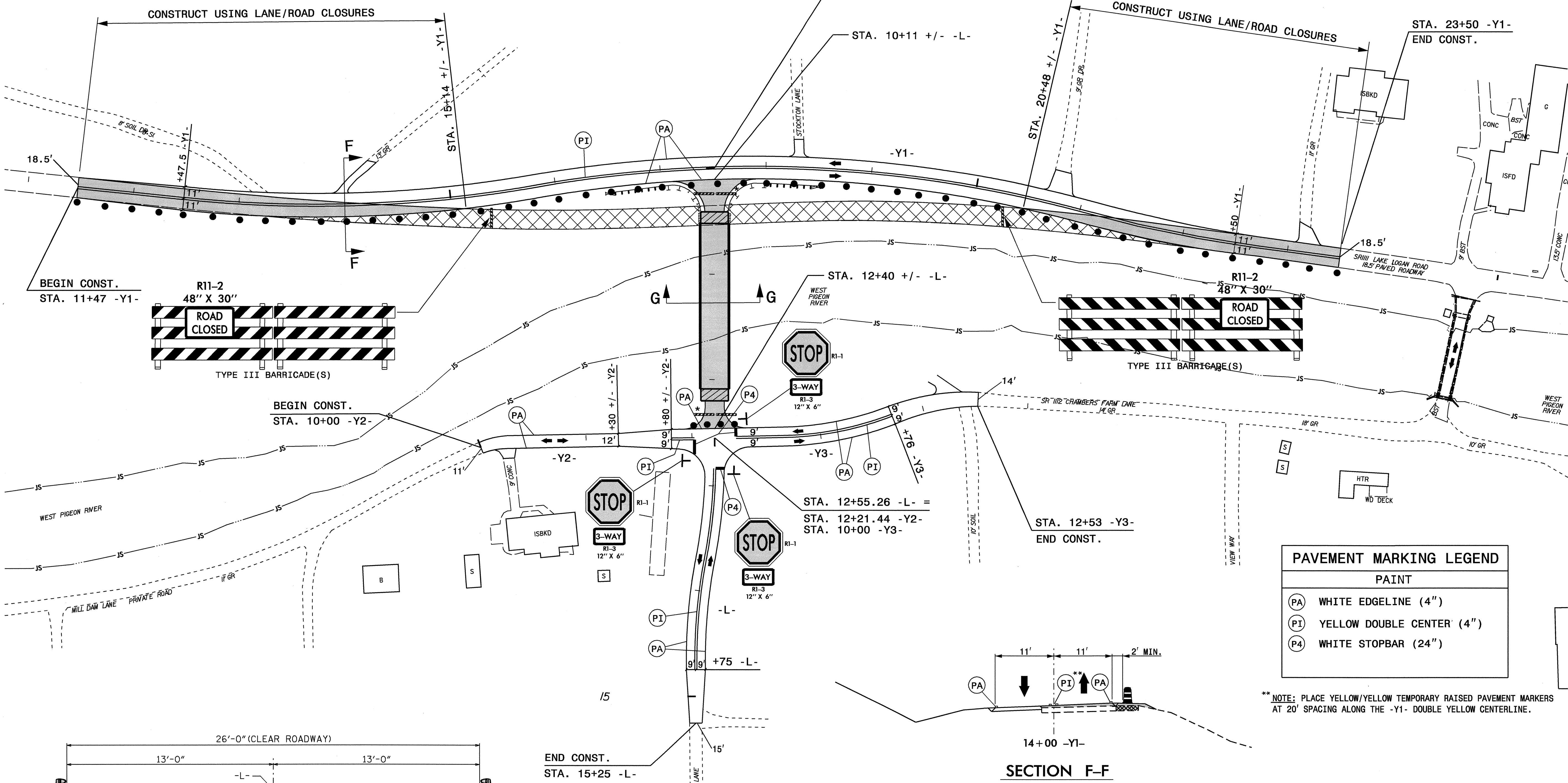
- STEP 1: - CONTRACTOR SHALL USING ROADWAY STANDARD DRAWING NO. 1101.02 SHEET 1 OF 9 PAVE THE FINAL LAYER OF SURFACE COURSE, INSTALL THE FINAL PAVEMENT MARKINGS, OPEN TO THE FINAL TRAFFIC PATTERN, THEN CLOSE THE EXISTING BRIDGE (SEE SHEET TCP-6, THE CONSTRUCTION PLANS AND THE FINAL PAVEMENT MARKING PLANS).
- STEP 2: - THE CONTRACTOR SHALL INSTALL DRUMS, WATER FILLED BARRIER AND SIGNAGE AT THE EXISTING BRIDGE AS SHOWN ON SHEET TCP-6, THEN AS DIRECTED BY THE ENGINEER USING ROADWAY STANDARD DRAWING NO. 1101.02 SHEET 1 OF 9 AND/OR ROADWAY STANDARD DRAWING NO. 1101.03 SHEET 1 OF 9 AND SHEET TCP-7 REMOVE THE EXISTING BRIDGE AND RE-INSTALL THE BRIDGE FOLLOWING REFURBISHMENT AND COMPLETE ANY REMAINING ROADWAY ITEMS THEN OPEN PROJECT TO THE FINAL TRAFFIC PATTERN (SEE CONSTRUCTION PLANS).
- STEP 3: - CONTRACTOR SHALL REMOVE ANY REMAINING TRAFFIC CONTROL DEVICES.

I:\AUG-2009 08:01... \dot\dfs\opt\... \kdp\odgwell AT WZ1023745

APPROVED: 	DATE: 10/26/09	<h2>PHASING</h2>	
SCALE: NONE	DATE: MAY 09		REVISIONS
DWG. BY: KPB	DESIGN BY: KPB		
REVIEWED BY: JWG			



STA. 10+00 -L- =
STA. 17+51.35 -Y1-



SECTION G-G

PAVEMENT MARKING LEGEND	
PAINT	
(PA)	WHITE EDGLINE (4")
(PI)	YELLOW DOUBLE CENTER (4")
(P4)	WHITE STOPBAR (24")

** NOTE: PLACE YELLOW/YELLOW TEMPORARY RAISED PAVEMENT MARKERS AT 20' SPACING ALONG THE -Y1- DOUBLE YELLOW CENTERLINE.

APPROVED: _____ DATE: _____
SEAL
G. L. GETTNER
ENGINEER
8973

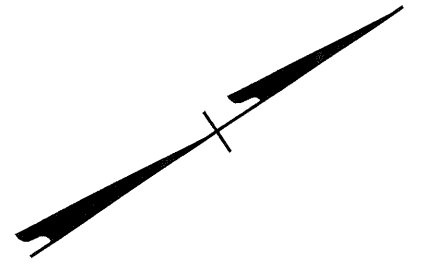
PHASE II DETAILS

SCALE: NONE
DATE: APR. 09
DWG. BY: KPB
DESIGN BY: KPB
REVIEWED BY: JWG

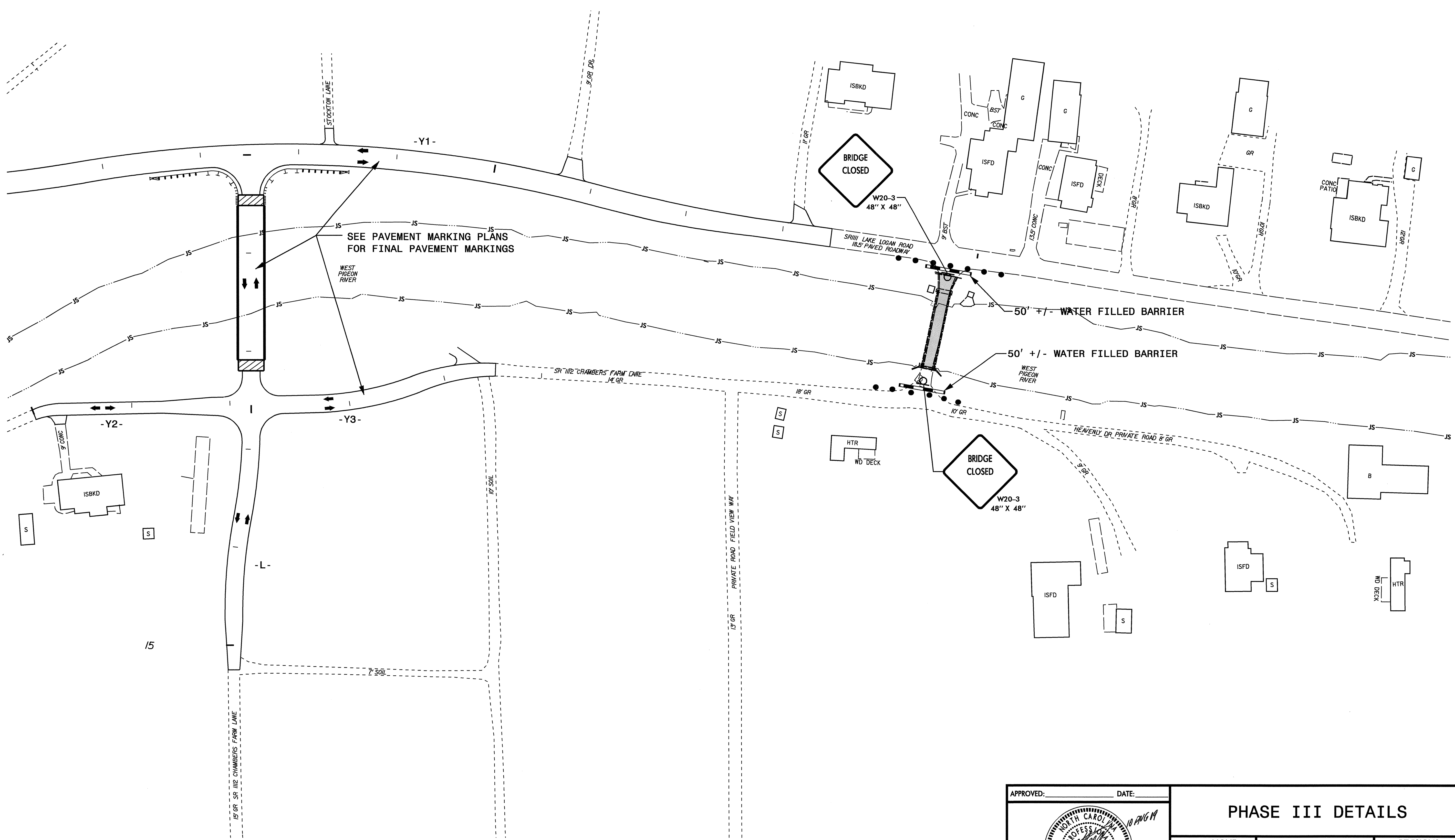


REVISIONS	

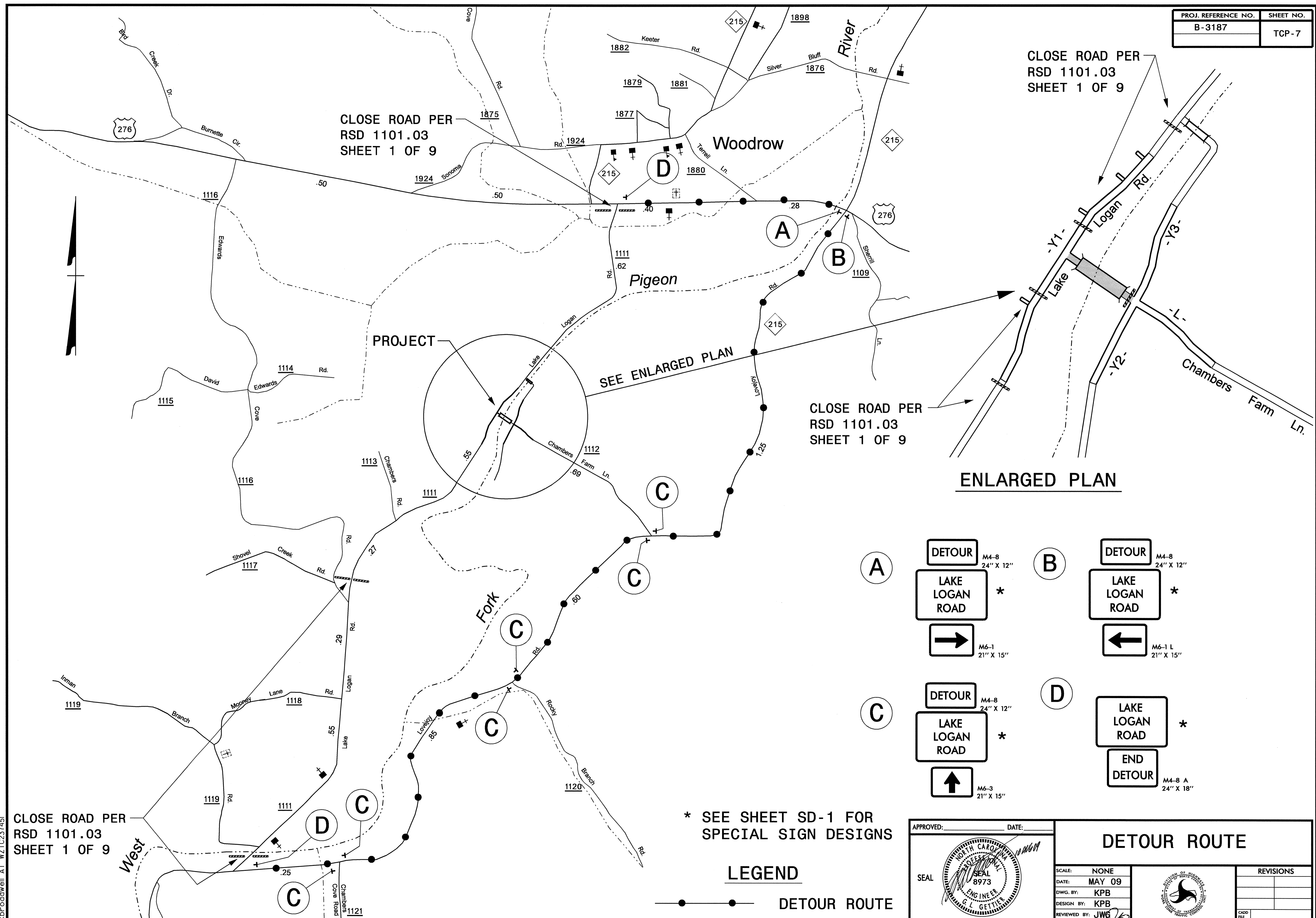
10-AUG-2009 08:01
\\dot\dfs\proj\traffic\trafficcontrol\tcp\B-3187.tc_tcp-5.dgn
kbr\oadwell AT WZTC2745



I:\0-AUG-2009 08:00
 \dot\dfs\p\proj\traffic\trafficcontrol\tcp\B-3187_tcp_tcp-6.dgn
 kbroadwell AT WZTC23745



APPROVED:	DATE:	PHASE III DETAILS									
SCALE:	NONE		<table border="1"> <tr><th colspan="2">REVISIONS</th></tr> <tr><td> </td><td> </td></tr> <tr><td> </td><td> </td></tr> <tr><td> </td><td> </td></tr> </table>	REVISIONS							
REVISIONS											
DATE:	APR. 09										
DWG. BY:	KPB										
DESIGN BY:	KPB										
REVIEWED BY:	JWG	<table border="1"> <tr><td> </td><td> </td></tr> <tr><td> </td><td> </td></tr> </table>									



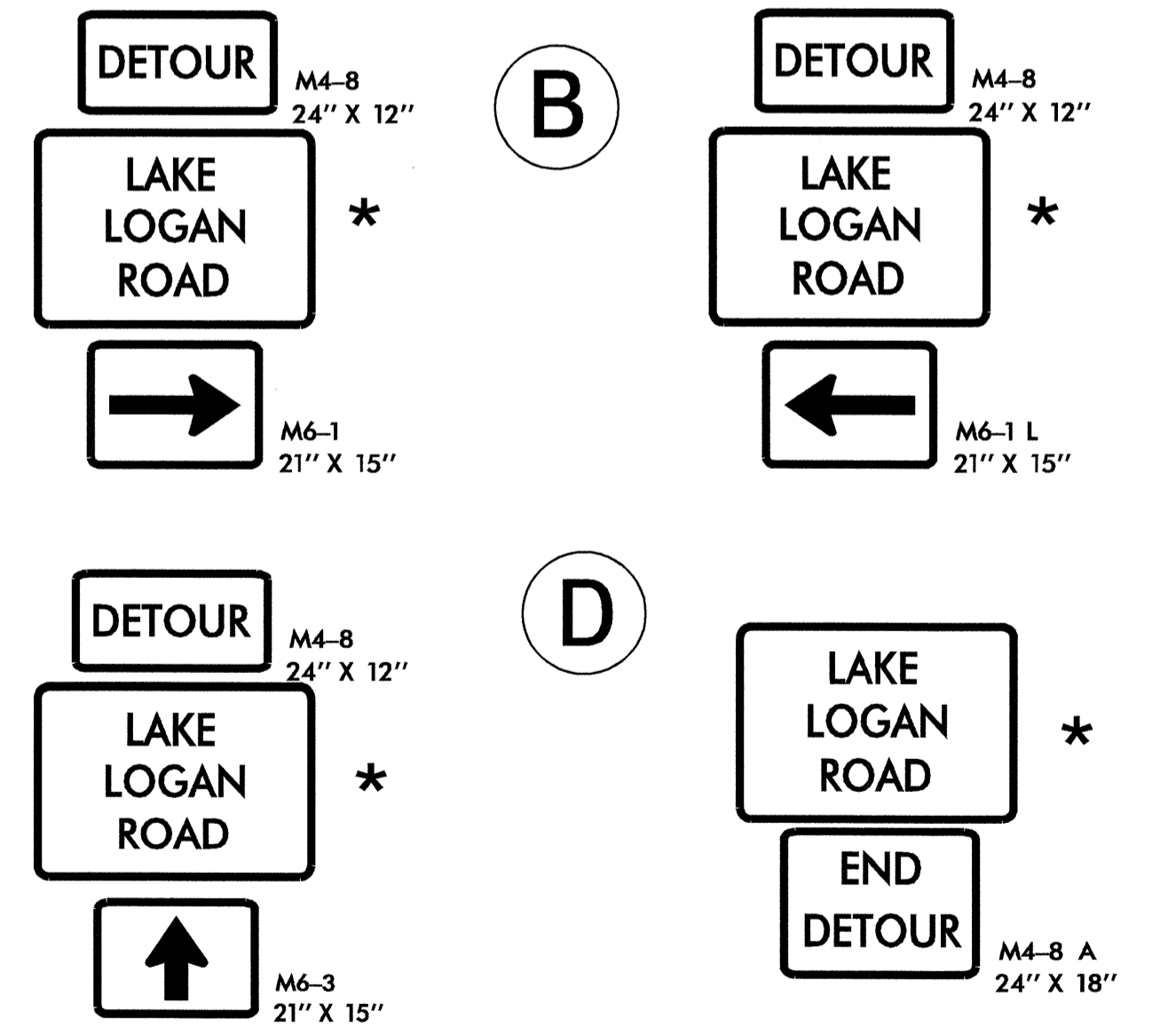
CLOSE ROAD PER
RSD 1101.03
SHEET 1 OF 9

CLOSE ROAD PER
RSD 1101.03
SHEET 1 OF 9

CLOSE ROAD PER
RSD 1101.03
SHEET 1 OF 9

CLOSE ROAD PER
RSD 1101.03
SHEET 1 OF 9

ENLARGED PLAN



* SEE SHEET SD-1 FOR SPECIAL SIGN DESIGNS

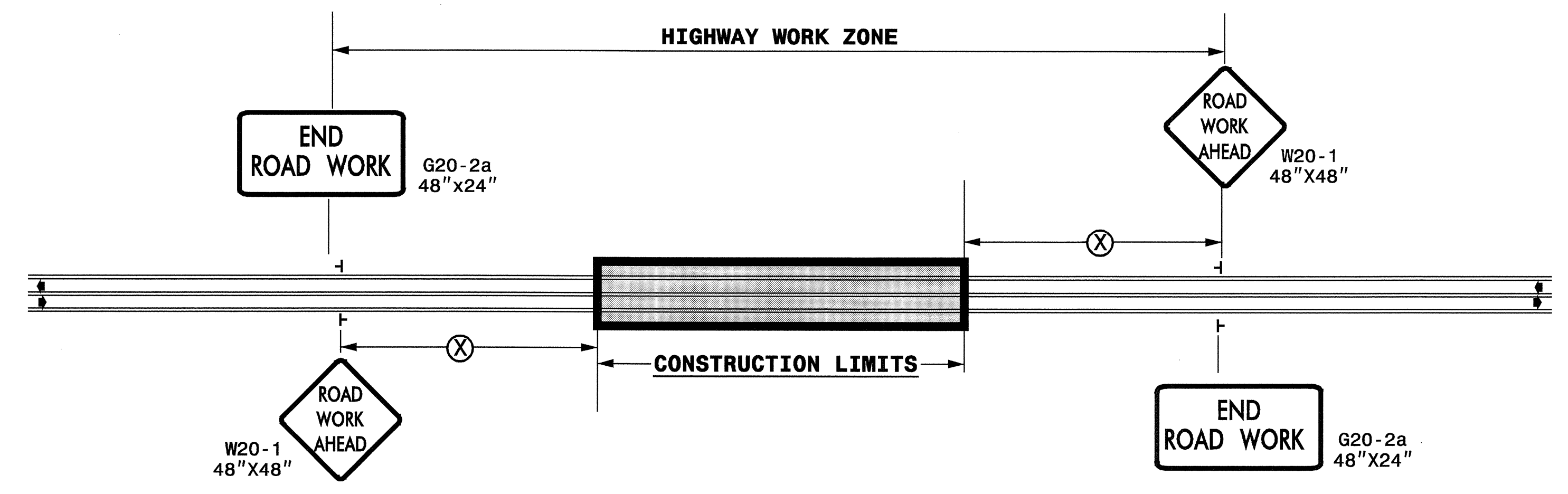
LEGEND



APPROVED: _____ DATE: _____	DETOUR ROUTE	
SCALE: NONE		REVISIONS
DATE: MAY 09		
DWG. BY: KPB		
DESIGN BY: KPB		
REVIEWED BY: JWG		CADD FILE

I:\AUG-2009-08\001\proj\Traffic\TrafficControl\top\haywood2.dgn
 K:\Roadwell_AT\WZ\237451

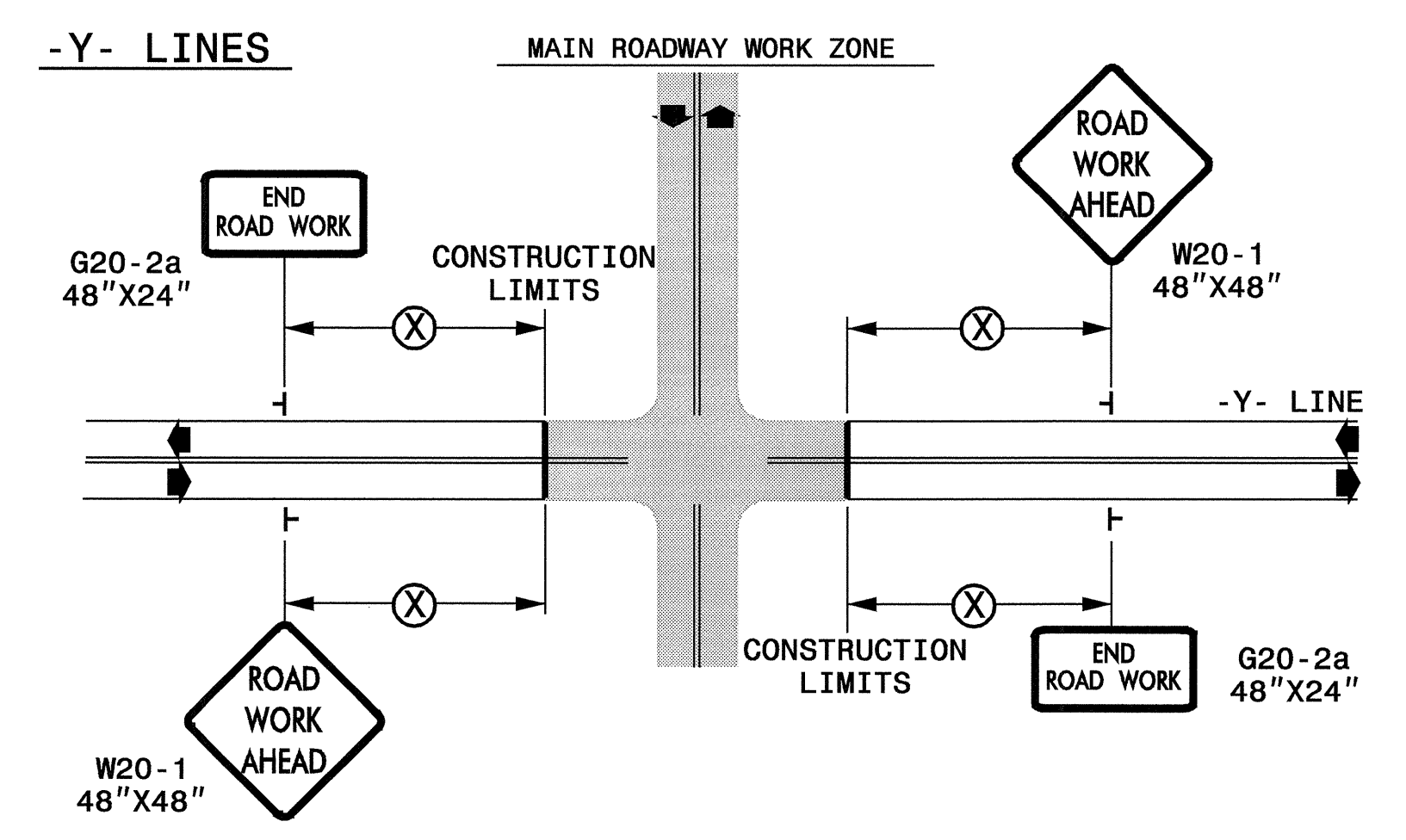
TWO-WAY UNDIVIDED ** (L-LINES)



POSTED SPEED LIMIT (M.P.H.)	RECOMMENDED MINIMUM SIGN SPACING
≤ 50	500'
≥ 55	1000'

STATE OF NORTH CAROLINA
DEPT. OF TRANSPORTATION
DIVISION OF HIGHWAYS
RALEIGH, N.C.

ROADWAYS INTERSECTING ALONG 2 WAY UNDIVIDED WORK ZONE (Y-LINES)



DETAIL DRAWING FOR
TWO-WAY UNDIVIDED
WORK ZONE WARNING SIGNS

GENERAL NOTES

- USE FLUORESCENT ORANGE SHEETING (TYPE VII OR HIGHER) ON ALL ADVANCED WORK ZONE SIGNS.
- DO NOT INSTALL ADVANCE WARNING SIGNS MORE THAN 3 DAYS PRIOR TO BEGINNING OF WORK.
- SIGNS SHOWN ARE REQUIRED FOR WORK ZONES THAT WILL REMAIN IN EFFECT OVERNIGHT. FOR SHORT-TERM DAILY MAINTENANCE TYPE OPERATIONS, THIS SIGNING APPLICATION IS OPTIONAL; MAY USE ONLY APPLICABLE ROADWAY STANDARD DRAWINGS INSTEAD. HOWEVER, IF THIS SIGNING APPLICATION IS USED, SIGNS MAY BE PORTABLE MOUNTED.
- ALL SIGN SPACING DIMENSIONS ARE APPROXIMATE, FIELD ADJUST AS NECESSARY OR AS DIRECTED.
- USE 3LB STEEL U-CHANNEL POST OR 4" X 4" WOOD POST FOR ALL WORK ZONE SIGNS. 3LB STEEL U-CHANNEL POSTS MUST MEET THE REQUIREMENTS OF STANDARD SPECIFICATION SECTION 1094-1(B), MAY BE GALVANIZED STEEL, OR MAY BE PAINTED GREEN BY THE POST MANUFACTURER. SQUARE STEEL TUBING POSTS HAVING EQUIVALENT STRENGTH OF THE 3 LB STEEL U-CHANNEL POST ARE ALSO ACCEPTABLE FOR USE. ERECT SIGNS PER ROADWAY STANDARD DRAWING 1110.01. PAYMENT FOR WOOD POSTS, 3LB STEEL U-CHANNEL AND SQUARE STEEL TUBING POSTS WITH SIGNS WILL BE MADE ACCORDING TO STANDARD SPECIFICATION "WORK ZONE SIGNS" SECTION 1110.
- WHEN NECESSARY, USE SPLICING IN ACCORDANCE WITH ROADWAY STANDARD DRAWING NO. 1110.01. REMOVE ENTIRE POST WHEN REMOVING SIGNS WITH SPLICED POSTS.
- DO NOT BACK BRACE SIGN SUPPORTS.
- ** TWO-WAY UNDIVIDED ADVANCE WARNING SIGN CONFIGURATION MAY BE USED ON URBAN MULTI-LANE FACILITIES WHERE CONDITIONS LIMIT THE USE OF DUAL MOUNTED SIGNS AS DETERMINED BY THE ENGINEER.

LEGEND

┆ STATIONARY SIGN

◀ DIRECTION OF TRAFFIC FLOW

SHEET 1 OF 1

APPROVED: _____	DATE: _____
DETAIL DRAWING FOR TWO-WAY UNDIVIDED AND URBAN FREEWAYS ADVANCED WORK ZONE WARNING SIGNS	
SCALE: NONE	REVISIONS
DATE: MAY 09	7-98 10/01
DWG. BY: KPB	10-98 03/04
DESIGN BY: KPB	01/01 11/04
REVIEWED BY: JWJ	CADD FILE

10-AUG-2009 08:00
 \\dot\dfs\p010\proj\trafficcontrol\top\2way_undiv.&_urban_fr_wys.dgn
 kbrodwell AT WZTC237451