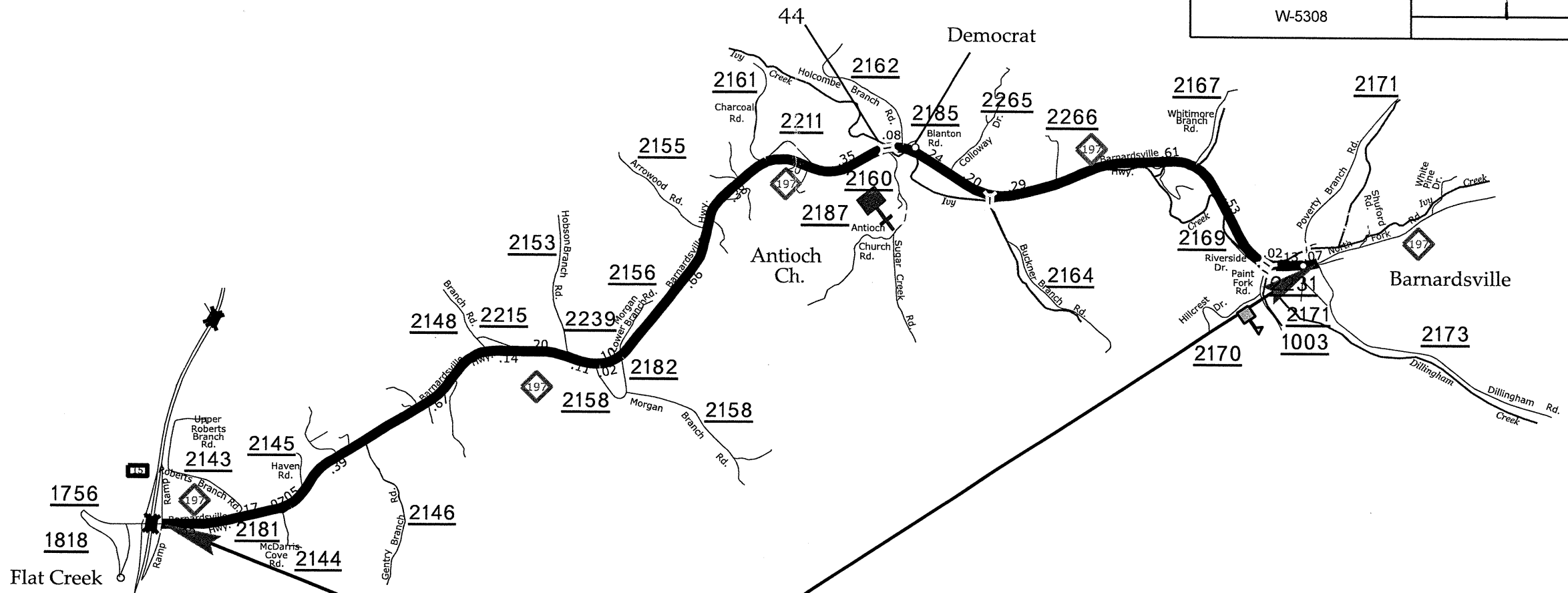
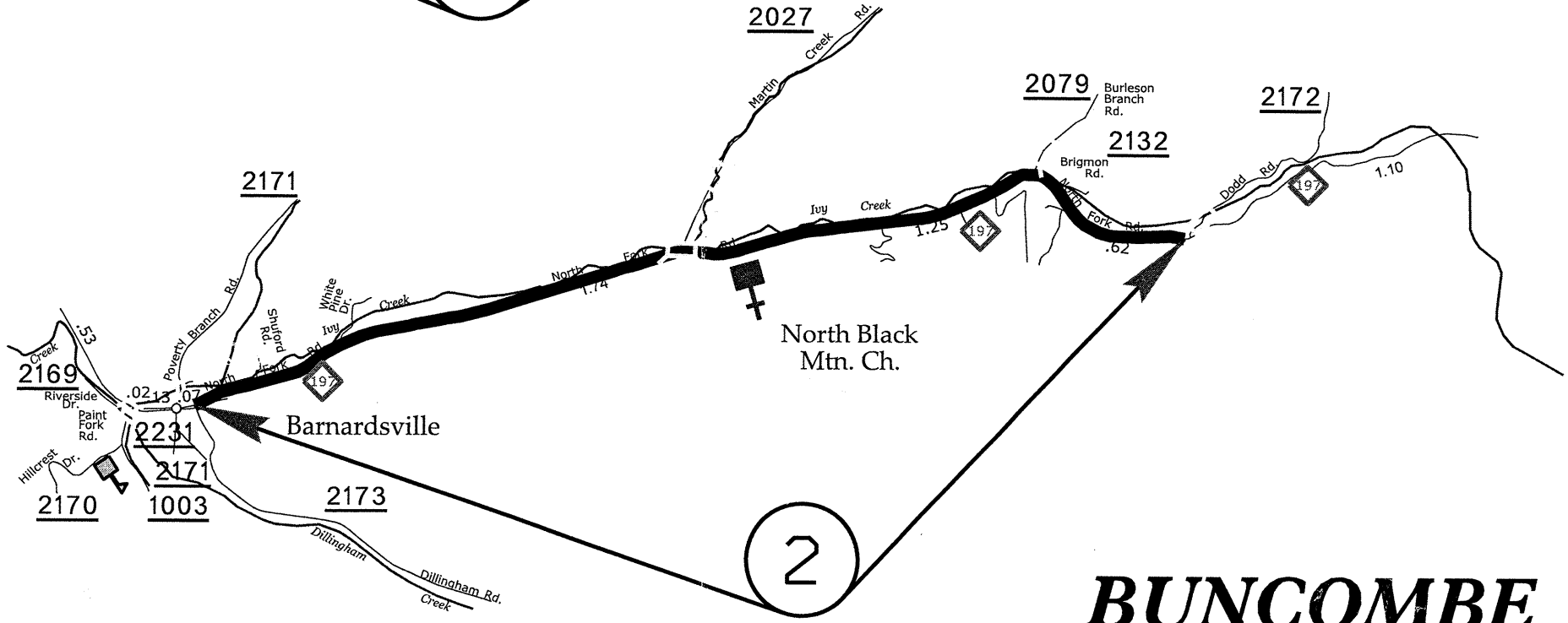


PROJECT NO.	SHEET NO.	TOTAL SHEETS
W-5308		



1

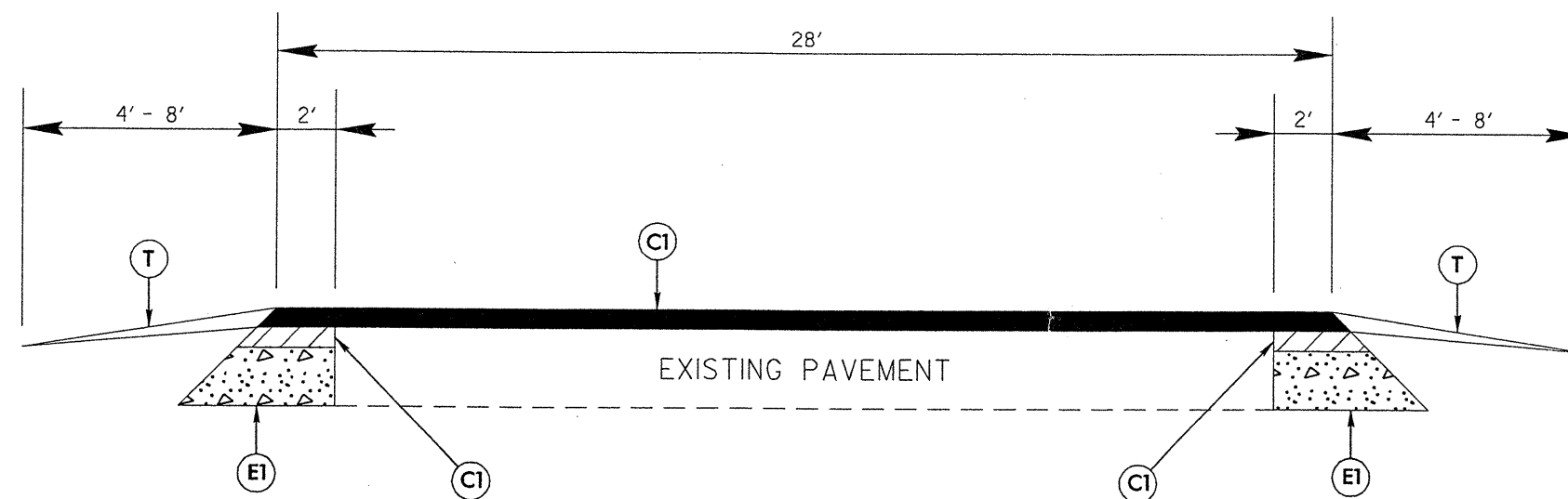


2

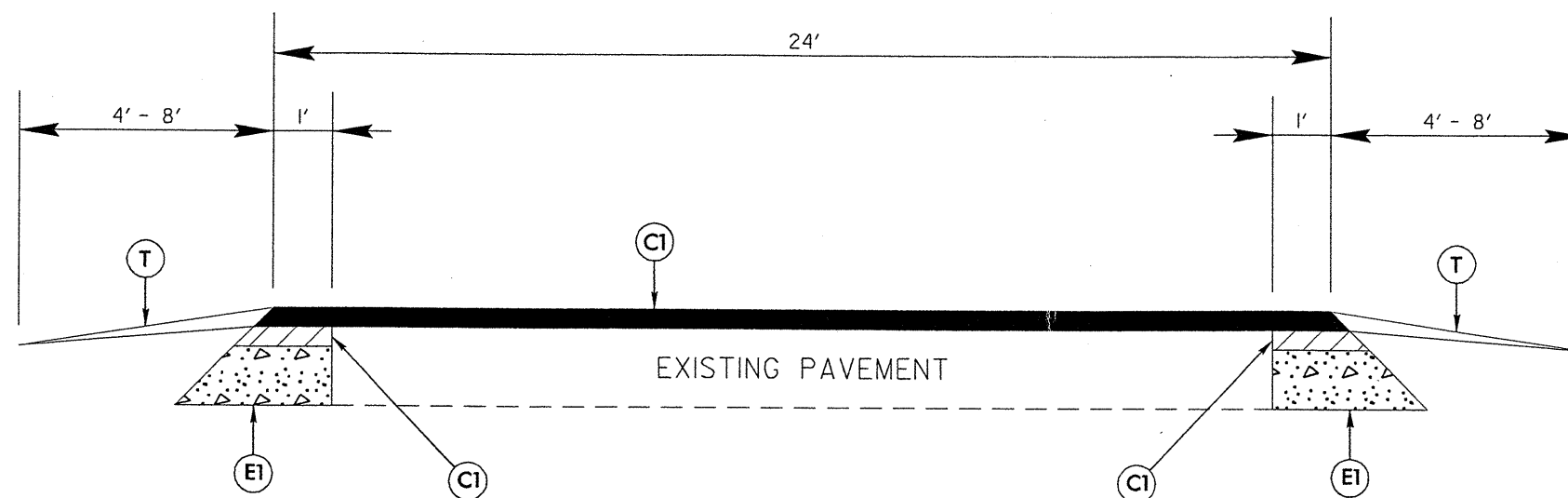
**BUNCOMBE COUNTY**



PROJECT NO.	SHEET NO.	TOTAL SHEETS
W-5308	2	



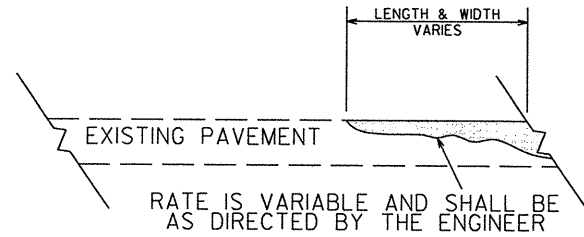
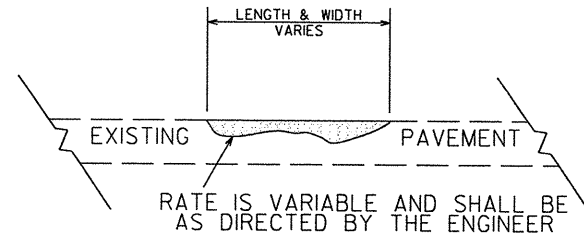
TYPICAL SECTION NO. 1



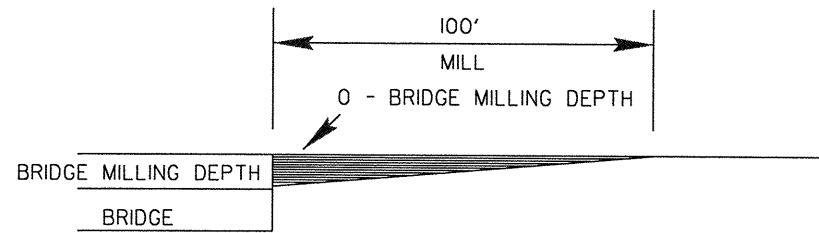
TYPICAL SECTION NO. 2

PAVEMENT SCHEDULE	
C1	PROP. APPROX. 1-1/2" ASPHALT CONC SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
E1	PROP. APPROX. 6" ASPHALT CONC BASE COURSE, TYPE B25.0B, AT AN AVERAGE RATE OF 342 LBS. PER SQ. YD. IN EACH OF TWO LAYERS
T	SHOULDER RECONSTRUCTION

PROJECT NO.	SHEET NO.	TOTAL SHEETS
W-5308	3	

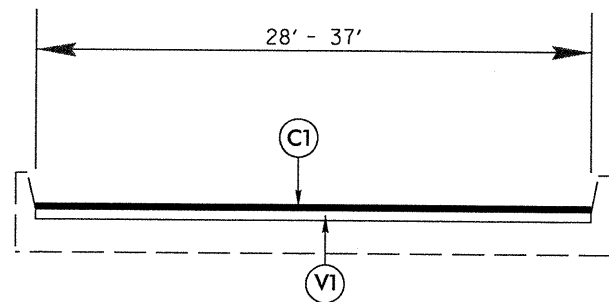


DETAIL SHOWING METHOD OF PATCHING



MILLING DETAIL AT BRIDGE APPROACHES  
WHERE BRIDGES WILL BE MILLED THEN RESURFACED  
COST OF MILLING APPROACHES IS INCIDENTAL TO OTHER ITEMS

**BRIDGE**



BRIDGE DETAIL

BRIDGE NUMBER 44 & BRIDGE NUMBER 78  
MILL 1-1/2" OFF EXISTING PAVEMENT ON BRIDGE  
THEN RESURFACE BRIDGE DECK WITH 1-1/2" OF S9.5B  
SEE MAPS FOR BRIDGE LOCATION

PAVEMENT SCHEDULE	
C1	PROP. APPROX. 1-1/2" ASPHALT CONC SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
VI	MILLING ASPHALT PAVEMENT, 1-1/2" DEPTH

PROJECT NO.	SHEET NO.	TOTAL NO.
W-5308	4	

### SUMMARY OF QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	FINAL SURFACE TESTING REQUIRED	LENGTH MI	WIDTH FT	INCIDENTAL STONE BASE TON	SHOULDER RECONSTRUCTION SMI	MILLING ASPHALT PAVEMENT, 1½" DEPTH SY	ASPHALT CONC BASE COURSE, TYPE B25.0B TON	ASPHALT CONC SURFACE COURSE, TYPE S9.5B TON	ASPHALT BINDER FOR PLANT MIX TON	PATCHING EXISTING PAVEMENT TON	TEMPORARY SILT FENCE LF	WATTLE LF	SEEDING & MULCHING AC
W-5308	Buncombe	1	NC 197	FROM US 19/23 (RAMP) TO SR 2173 (DILLINGHAM ROAD)	1	YES	6	28	300.00	12.00	720	5,959	10,490	892	120	90.00	30.00	3.70
W-5308	Buncombe	2	NC 197	FROM SR 2173 (DILLINGHAM ROAD) TO A POINT 0.1 MILES WEST OF SR 2172 (DODD ROAD)	2	YES	3.5	24	175.00	7.00		1,931	4,980	384	70	1,690.00	310.00	2.20
<b>TOTAL FOR PROJ NO. 46132.3.1</b>							<b>9.5</b>		<b>475.00</b>	<b>19.00</b>	<b>720</b>	<b>7,890</b>	<b>15,470</b>	<b>1,276</b>	<b>190</b>	<b>1,780.00</b>	<b>340.00</b>	<b>5.90</b>
<b>GRAND TOTAL</b>							<b>9.5</b>		<b>475.00</b>	<b>19.00</b>	<b>720</b>	<b>7,890</b>	<b>15,470</b>	<b>1,276</b>	<b>190</b>	<b>1,780.00</b>	<b>340.00</b>	<b>5.90</b>

### THERMOPLASTIC AND PAINT QUANTITIES

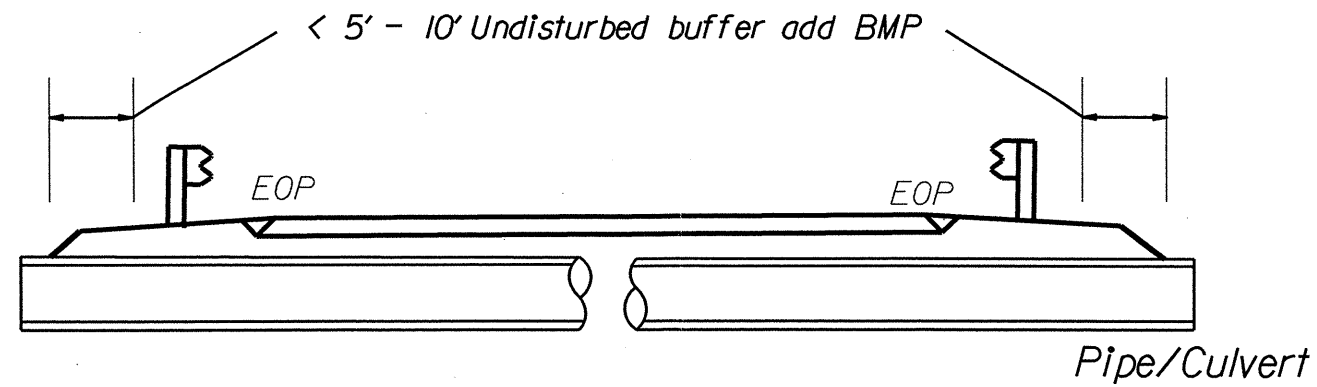
PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	LENGTH	WIDTH	481000000-E	484700000-E	484700000-E	490500000-N
							PAINT PAVEMENT MARKING LINES (4" WHITE) LF	POLYUREA PAVEMENT MARKING LINES (4", HIGHLY REFLECTIVE) WHITE LF	POLYUREA PAVEMENT MARKING LINES (4", HIGHLY REFLECTIVE) YELLOW LF	SNOWPLOWABLE PAVEMENT MARKERS EA
W-5308	Buncombe	1	NC 197	FROM US 19/23 (RAMP) TO SR 2173 (DILLINGHAM ROAD)	6	28	63,360	63,360	63,360	396
W-5308	Buncombe	2	NC 197	FROM SR 2173 (DILLINGHAM ROAD) TO A POINT 0.1 MILES WEST OF SR 2172 (DODD ROAD)	3.5	24	36,960	36,960	36,960	231
<b>TOTAL FOR PROJ NO. 46132.3.1</b>					<b>9.5</b>		<b>100,320</b>	<b>100,320</b>	<b>100,320</b>	<b>627</b>
<b>GRAND TOTAL</b>					<b>9.5</b>		<b>100,320</b>	<b>200,640</b>	<b>200,640</b>	<b>627</b>

NOTES: Less than 5' - 10' undisturbed buffer from ROW, ditchline, water feature, or drainage inlet, add BMP.

BMP Options: Wattle or Silt Fence

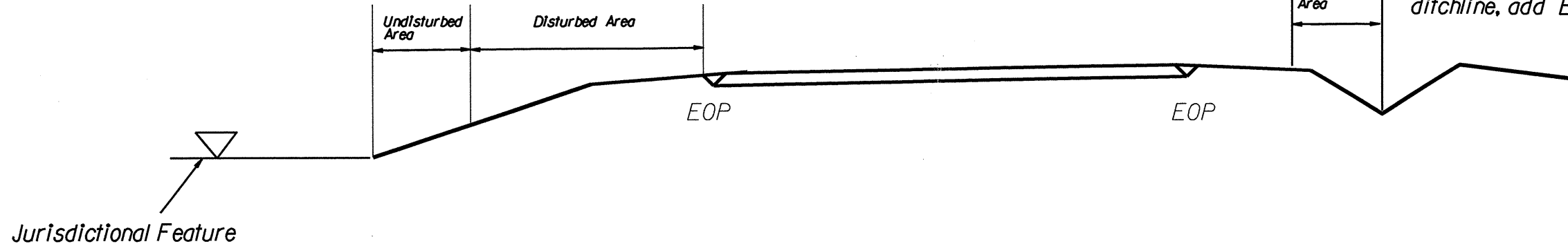
# EROSION CONTROL DETAIL

PROJECT REFERENCE NO. W-5308	SHEET NO. EC-1
ROADWAY DESIGN ENGINEER	HYDRAULICS ENGINEER

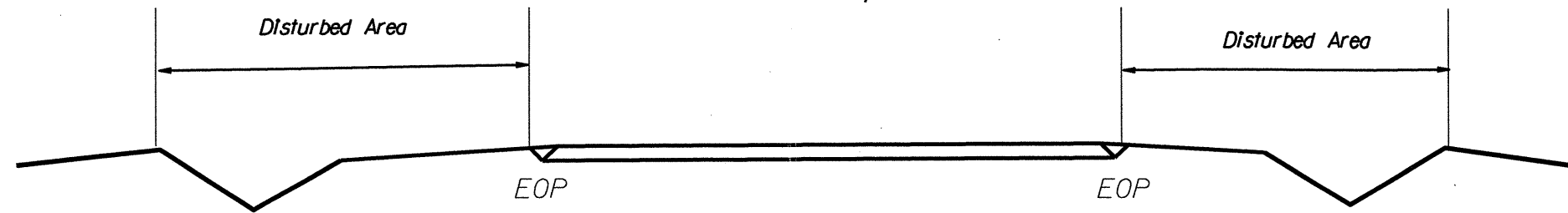


< 5' - 10' Undisturbed buffer from jurisdictional feature add BMP

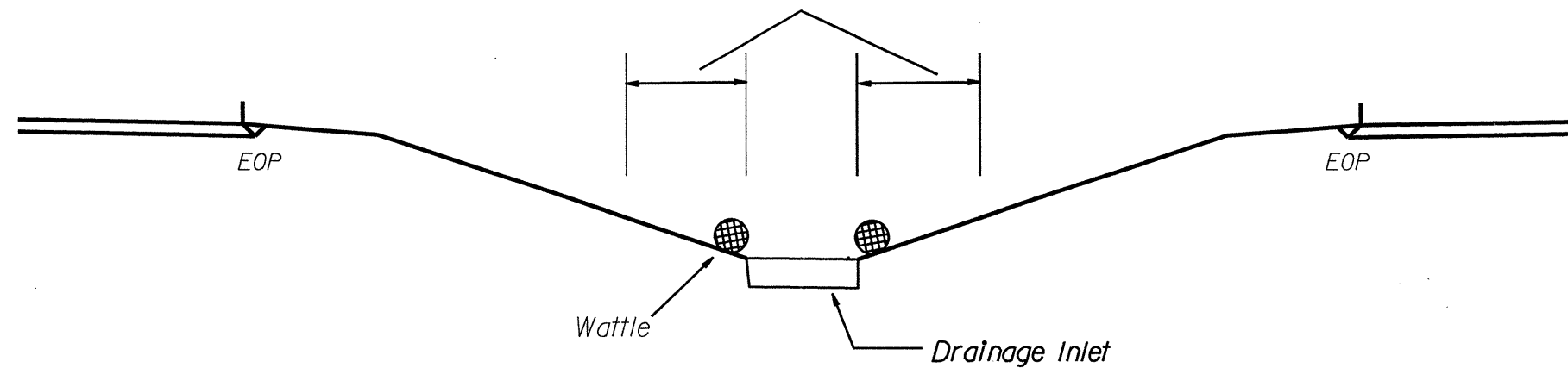
< 5' - 10' Undisturbed buffer from ditchline, add BMP



Use BMP's if shoulders and/or frontslopes and/or ditchline and/or backslopes are disturbed



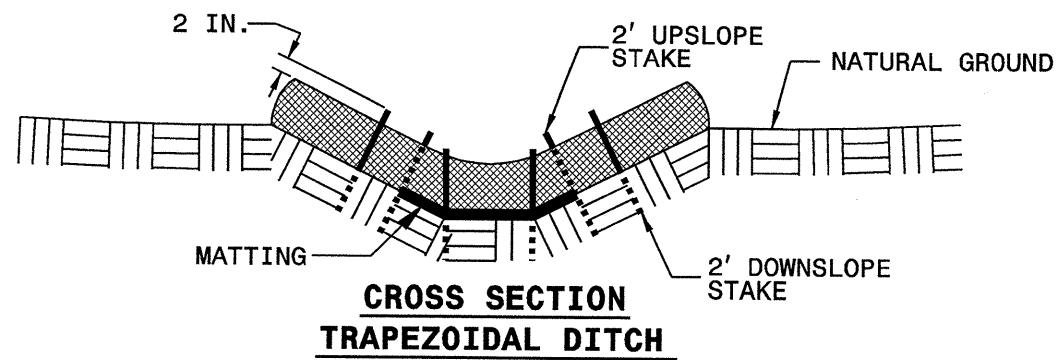
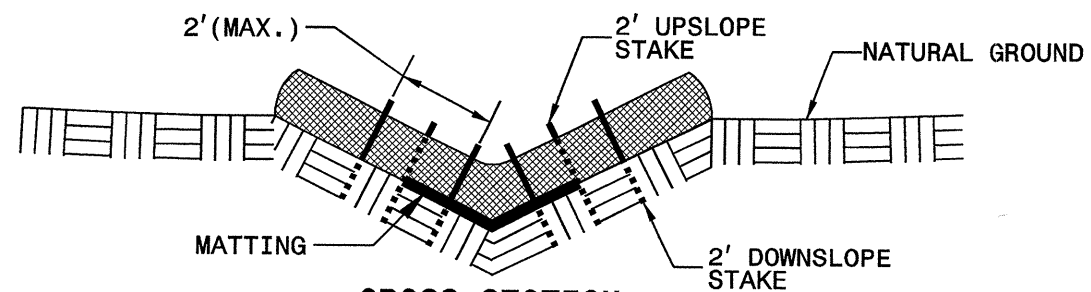
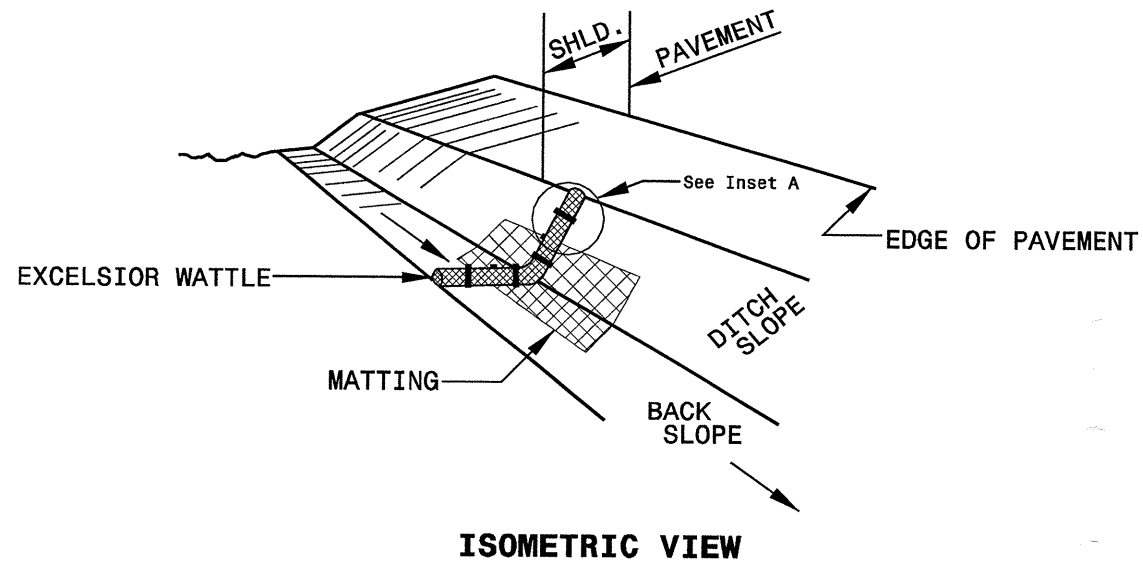
< 5' - 10' Undisturbed buffer from inlet, add wattle



NOT TO SCALE

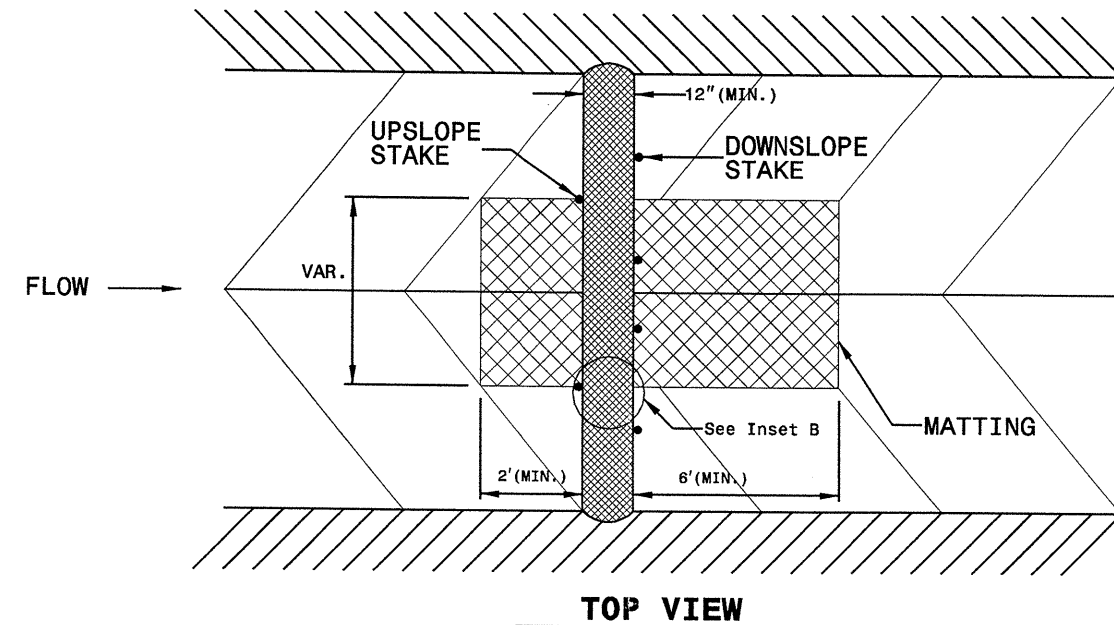
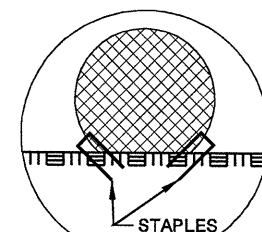
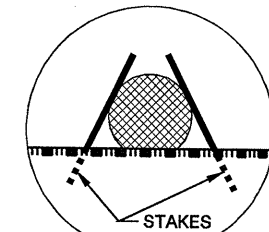
PROJECT REFERENCE NO. <b>W-3308</b>	SHEET NO. <b>EC-2</b>
HWY SHEET NO.	
ROADWAY DESIGN ENGINEER	HYDRAULICS ENGINEER

# WATTLE DETAIL

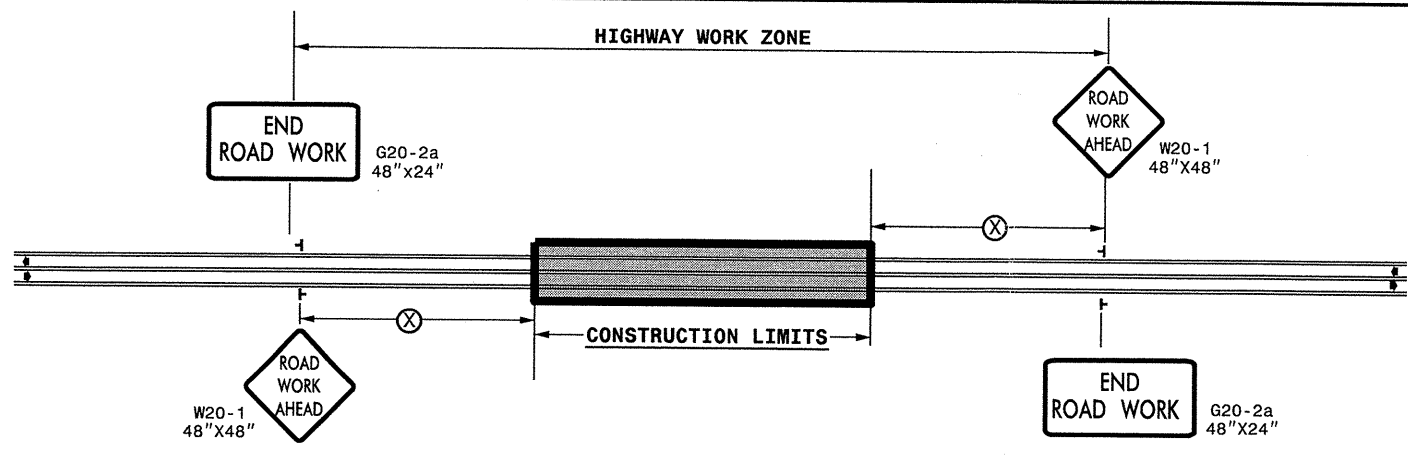


**NOTES:**

- USE MINIMUM 12 IN. DIAMETER EXCELSIOR WATTLE.
- USE 2 FT. WOODEN STAKES WITH A 2 IN. BY 2 IN. NOMINAL CROSS SECTION.
- ONLY** INSTALL WATTLE(S) TO A HEIGHT IN DITCH SO FLOW WILL NOT WASH AROUND WATTLE AND SCOUR DITCH SLOPES AND AS DIRECTED.
- INSTALL A MINIMUM OF 2 UPSLOPE STAKES AND 4 DOWNSLOPE STAKES AT AN ANGLE TO WEDGE WATTLE TO BOTTOM OF DITCH.
- PROVIDE STAPLES MADE OF 0.125 IN. DIAMETER STEEL WIRE FORMED INTO A U SHAPE NOT LESS THAN 12" IN LENGTH.
- INSTALL STAPLES APPROXIMATELY EVERY 1 LINEAR FOOT ON BOTH SIDES OF WATTLE AND AT EACH END TO SECURE IT TO THE SOIL.
- INSTALL MATTING IN ACCORDANCE WITH SECTION 1631 OF THE STANDARD SPECIFICATIONS.



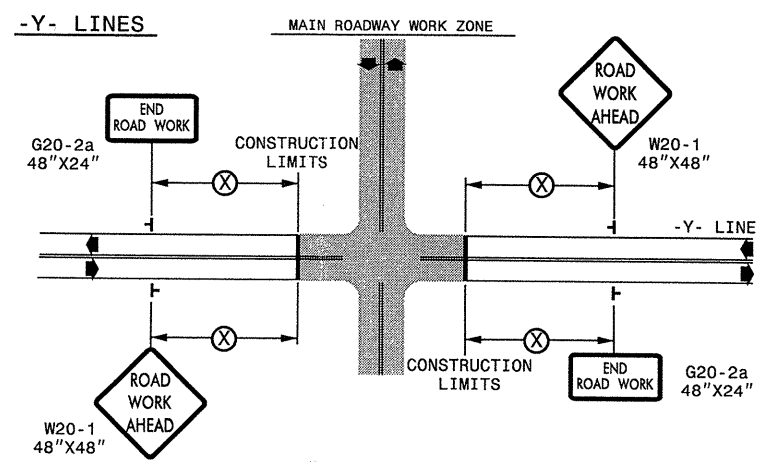
**TWO-WAY UNDIVIDED \*\* (L-LINES)**



POSTED SPEED LIMIT (M.P.H.)	RECOMMENDED MINIMUM SIGN SPACING
≤ 50	500'
≥ 55	1000'

STATE OF NORTH CAROLINA  
DEPT. OF TRANSPORTATION  
DIVISION OF HIGHWAYS  
RALEIGH, N.C.

**ROADWAYS INTERSECTING ALONG 2 WAY UNDIVIDED WORK ZONE (Y-LINES)**



**GENERAL NOTES**

- USE FLUORESCENT ORANGE SHEETING (TYPE VII OR HIGHER) ON ALL ADVANCED WORK ZONE SIGNS.
- DO NOT INSTALL ADVANCE WARNING SIGNS MORE THAN 3 DAYS PRIOR TO BEGINNING OF WORK.
- SIGNS SHOWN ARE REQUIRED FOR WORK ZONES THAT WILL REMAIN IN EFFECT OVERNIGHT. FOR SHORT-TERM DAILY MAINTENANCE TYPE OPERATIONS, THIS SIGNING APPLICATION IS OPTIONAL; MAY USE ONLY APPLICABLE ROADWAY STANDARD DRAWINGS INSTEAD. HOWEVER, IF THIS SIGNING APPLICATION IS USED, SIGNS MAY BE PORTABLE MOUNTED.
- ALL SIGN SPACING DIMENSIONS ARE APPROXIMATE, FIELD ADJUST AS NECESSARY OR AS DIRECTED.
- USE 3LB STEEL U-CHANNEL POST OR 4" X 4" WOOD POST FOR ALL WORK ZONE SIGNS. 3LB STEEL U-CHANNEL POSTS MUST MEET THE REQUIREMENTS OF STANDARD SPECIFICATION SECTION 1094-1(B), MAY BE GALVANIZED STEEL, OR MAY BE PAINTED GREEN BY THE POST MANUFACTURER. SQUARE STEEL TUBING POSTS HAVING EQUIVALENT STRENGTH OF THE 3 LB STEEL U-CHANNEL POST ARE ALSO ACCEPTABLE FOR USE. ERECT SIGNS PER ROADWAY STANDARD DRAWING 1110.01. PAYMENT FOR WOOD POSTS, 3LB STEEL U-CHANNEL AND SQUARE STEEL TUBING POSTS WITH SIGNS WILL BE MADE ACCORDING TO STANDARD SPECIFICATION "WORK ZONE SIGNS" SECTION 1110.
- WHEN NECESSARY, USE SPLICING IN ACCORDANCE WITH ROADWAY STANDARD DRAWING NO. 1110.01. REMOVE ENTIRE POST WHEN REMOVING SIGNS WITH SPLICED POSTS.
- DO NOT BACK BRACE SIGN SUPPORTS.
- \*\* TWO-WAY UNDIVIDED ADVANCE WARNING SIGN CONFIGURATION MAY BE USED ON URBAN MULTI-LANE FACILITIES WHERE CONDITIONS LIMIT THE USE OF DUAL MOUNTED SIGNS AS DETERMINED BY THE ENGINEER.

**LEGEND**

▬ STATIONARY SIGN

◀ DIRECTION OF TRAFFIC FLOW

DETAIL DRAWING FOR  
TWO-WAY UNDIVIDED  
WORK ZONE WARNING SIGNS

APPROVED: _____	DATE: _____	DETAIL DRAWING FOR TWO-WAY UNDIVIDED AND URBAN FREEWAYS ADVANCED WORK ZONE WARNING SIGNS	
SEAL	SCALE: NONE	REVISIONS	
	DATE: _____	7-98	10/01
	DWG. BY: _____	10-98	03/04
	DESIGN BY: _____	01/01	11/04
REVIEWED BY: _____			

I:\JUN-2016\26\DOT\DOTSR001\CONGROUPS-WZTC\TMU\WZTC\Resurfacing\2011\Western\2011\Div13\W-5308.c202\_Buncombe\_NCI97\W-5308.c202723\_Buncombe\_2way\_Undiv\_&\_Urban\_Frways\_stationary.dgn  
 snogreen AT 1E244733