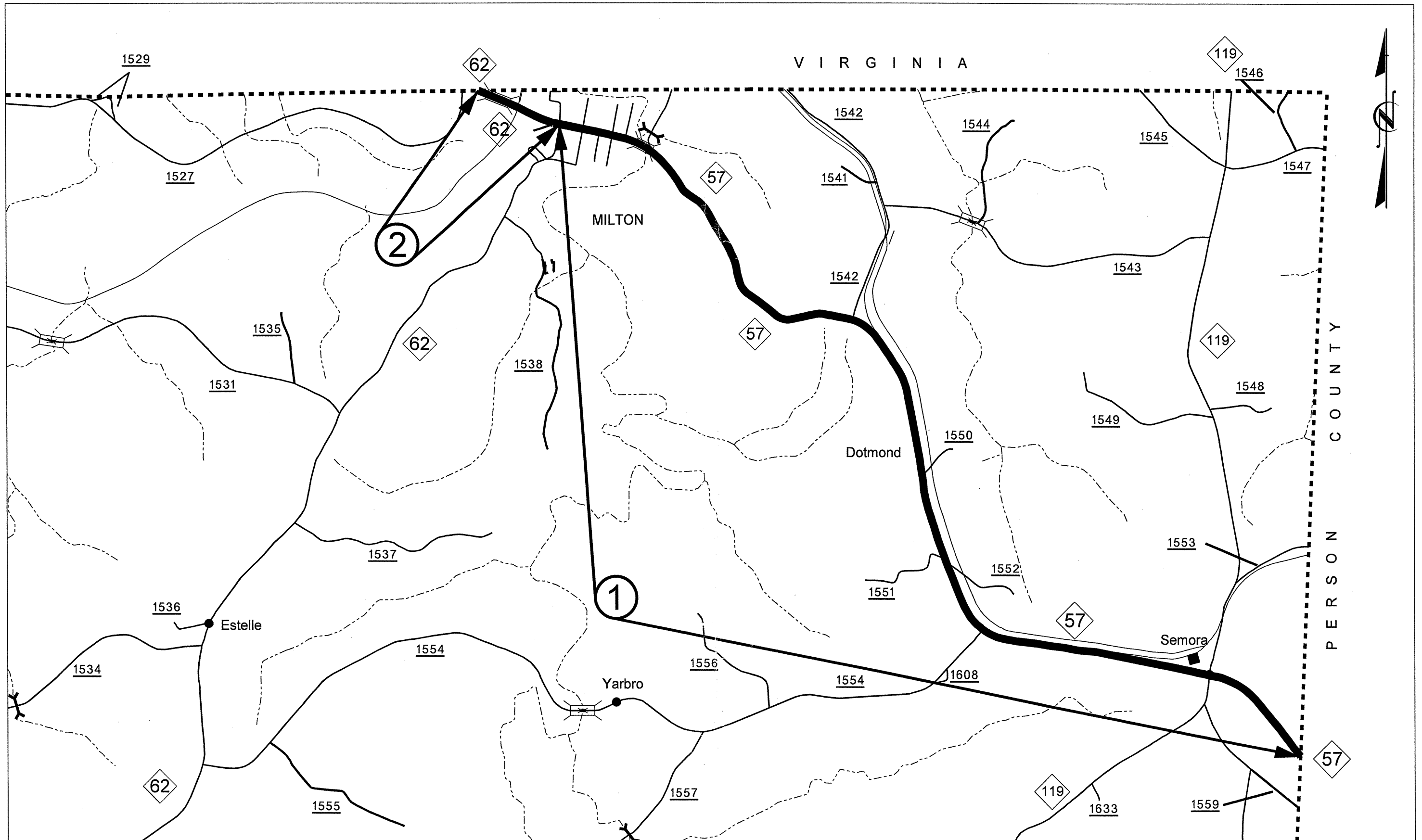


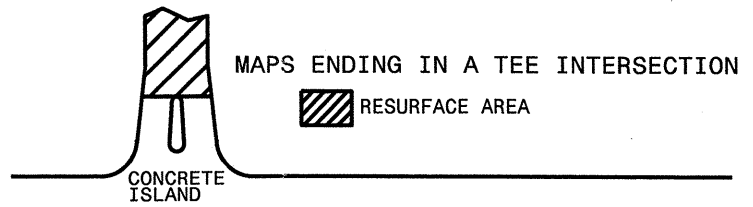
# CASWELL COUNTY

STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
N.C.	7CR.10171.29	1	
F.A. PROJ. NO.			



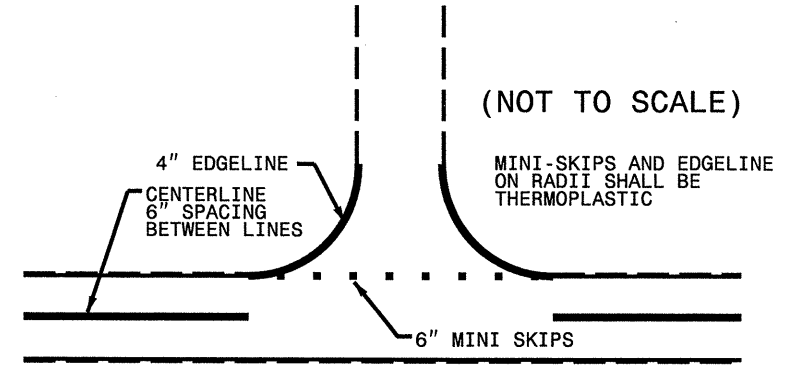
STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
N.C.	7CR.10171.29	2	

**PAVING DETAIL 1  
MAIN LINE IS NOT BEING RESURFACED**

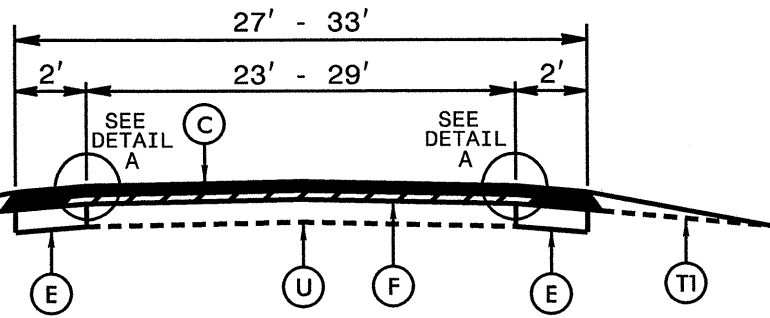


MAPS ENDING IN A TEE INTERSECTION  
RESURFACE AREA

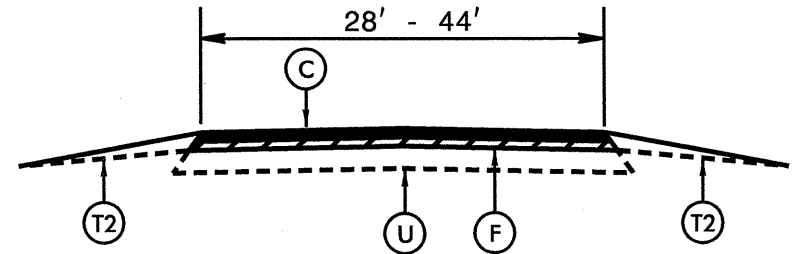
**TO BE USED AT ALL  
NON-SIGNALIZED INTERSECTIONS**



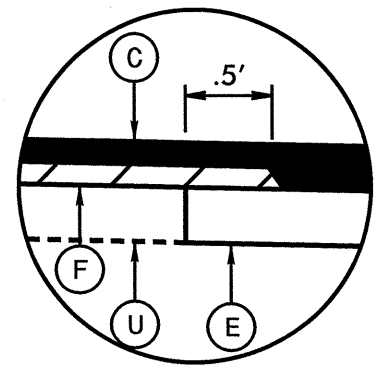
NOTE: MINI-SKIPS SHALL BE PLACED ON A 10' CYCLE, CONTAINING AN 8' AND 2' SKIP, THE WIDTH OF THE SKIP SHALL BE 6".



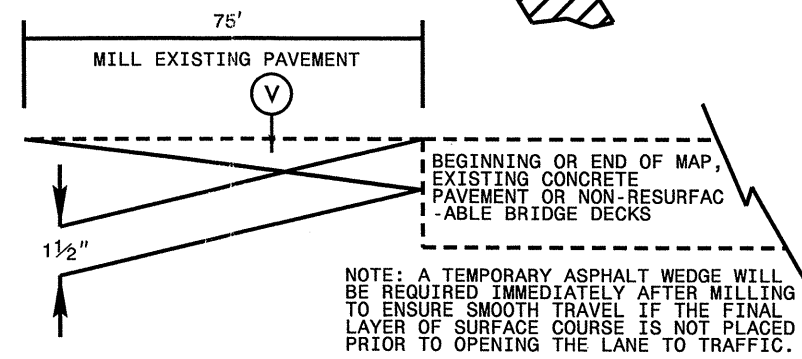
**TYPICAL SECTION NO. 1**  
TO BE USED ON MAP 1  
MAP 1: STA. 0+00 TO STA. 281+20



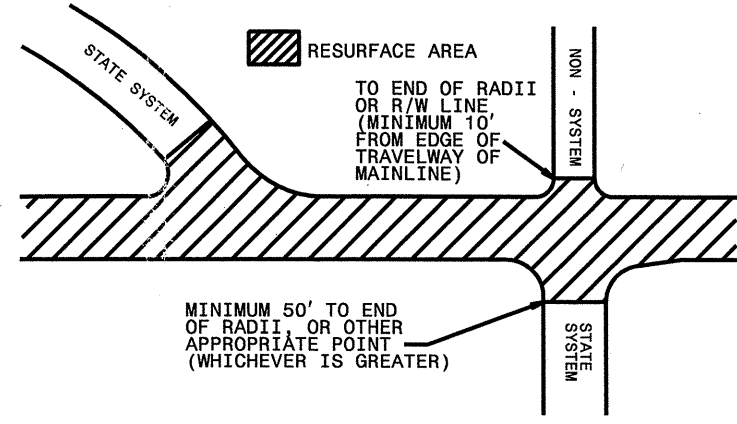
**TYPICAL SECTION NO. 2**  
TO BE USED ON MAPS 1 AND 2  
MAP 1: STA. 281+20 TO STA. 284+55  
STA. 293+65 TO STA. 295+35



**DETAIL A**

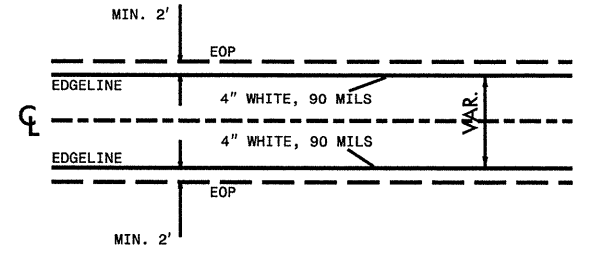


**PAVING DETAIL 2  
MAIN LINE IS BEING RESURFACED**  
NOTE: NON-SYSTEM (CITY STREET, PRIVATE DRIVE, SCHOOL BUS DRIVE)



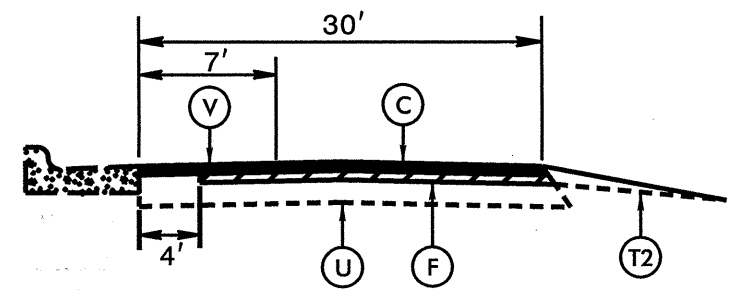
MINIMUM 50' TO END OF RADI, OR OTHER APPROPRIATE POINT (WHICHEVER IS GREATER)

**STRIPING DETAIL 1  
GENERAL STRIPING DETAIL FOR ENTIRE PROJECT**

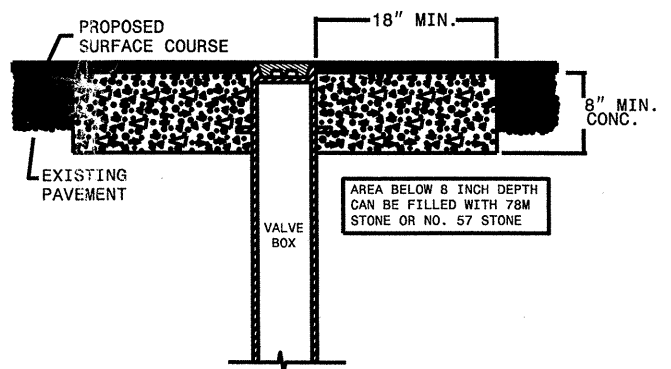


NOTE:  
1. TO BE USED IN CONJUNCTION WITH TYPICAL SECTION NO. 1  
2. USE IN CONJUNCTION WITH THE EXISTING PAVEMENT MARKINGS TO ESTABLISH THE STRIPING.  
3. USE IN CONJUNCTION WITH THE NCDOT STANDARD DRAWINGS.

**TYPICAL SECTION NO. 3**  
TO BE USED ON MAP 1  
STA. 284+55 TO STA. 293+65

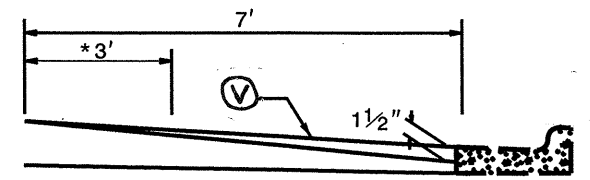


**STANDARD CONCRETE ENCASUREMENT FOR MANHOLE & VALVE CASTINGS IN PAVEMENT  
DETAIL DRAWING NO. 858.01**



USE RAPID SET GROUT, MORTAR, OR CONCRETE CLASS B CONCRETE MAY BE USED WHEN ADJUSTMENTS ARE NOT IN THE TRAVEL LANE.

**MILLING DETAIL 1**



PROFILE MILLING 0 - 1 1/2"  
\* IF #67 STONE IS INVOLVED, OVERLAP 3'.  
PROFILE MILL EXISTING ASPHALT PAVEMENT 1 1/2" AT LOCATIONS AS DIRECTED BY THE ENGINEER.  
NOTE: TO BE USED IN CONJUNCTION WITH:  
TS. NO. 3 ON MAP 1 STA. 284+75 TO STA. 293+65 LT

**PAVEMENT SCHEDULE**

C	PROP. APPROX. 1 1/2" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
E	PROP. APPROX. 7" ASPHALT CONCRETE BASE COURSE, TYPE B25.0B, AT AN AVERAGE RATE OF 399 LBS. PER SQ. YD. IN EACH OF TWO LAYERS
F	AST MAT COAT #67 STONE
U	EXISTING PAVEMENT.
T1	SHOULDER RECONSTRUCTION, AS DIRECTED BY THE ENGINEER.
T2	INCIDENTAL STONE BASE IN LOW SHOULDER AREAS, AS DIRECTED BY THE ENGINEER.
V	0 - 1 1/2" MILLING

STANDARD DRAWING

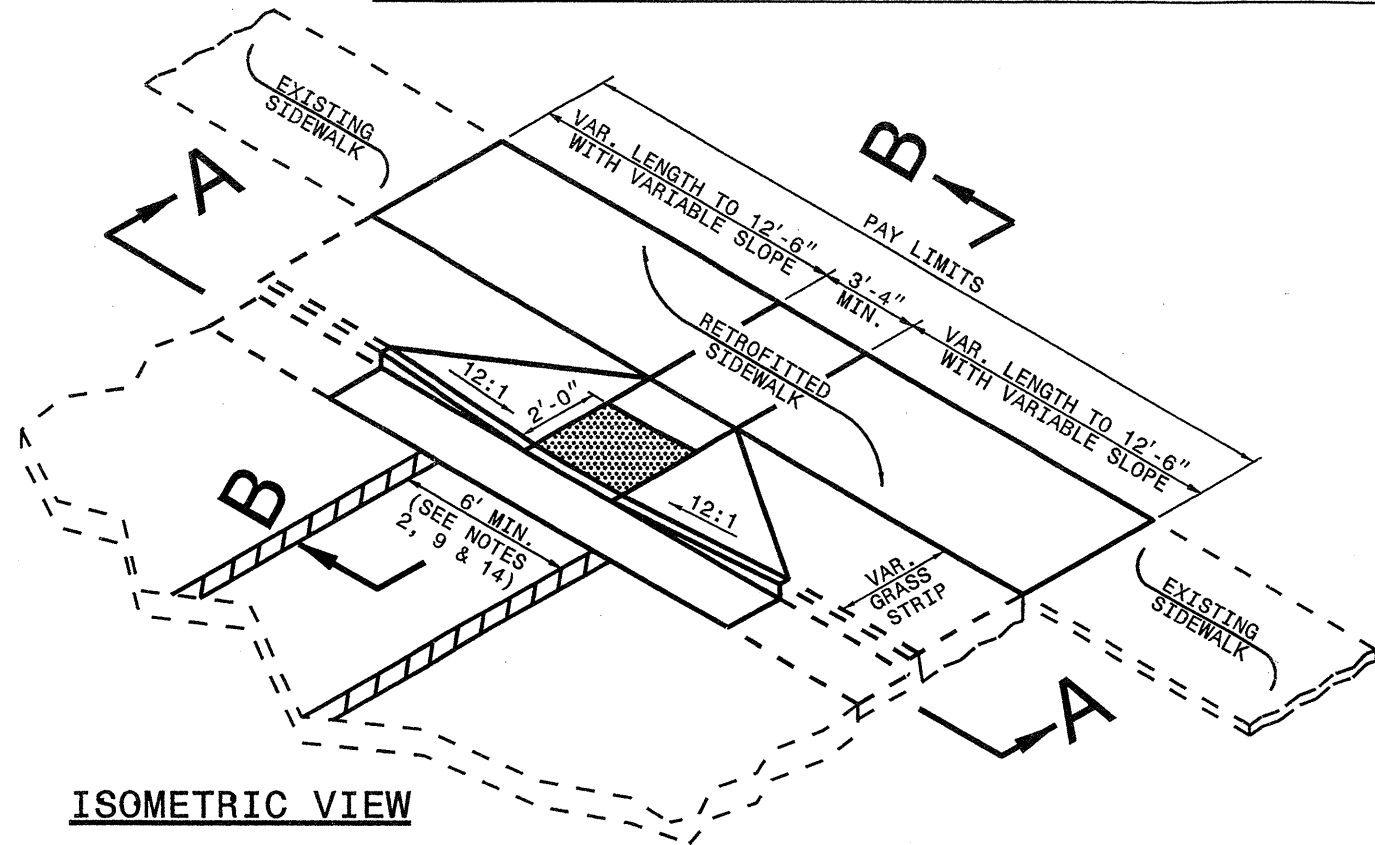
STATE OF NORTH CAROLINA  
DEPT. OF TRANSPORTATION  
DIVISION OF HIGHWAYS  
RALEIGH, N.C.

ENGLISH DETAIL DRAWING FOR  
**WHEELCHAIR RAMP**  
EXISTING CURB AND GUTTER

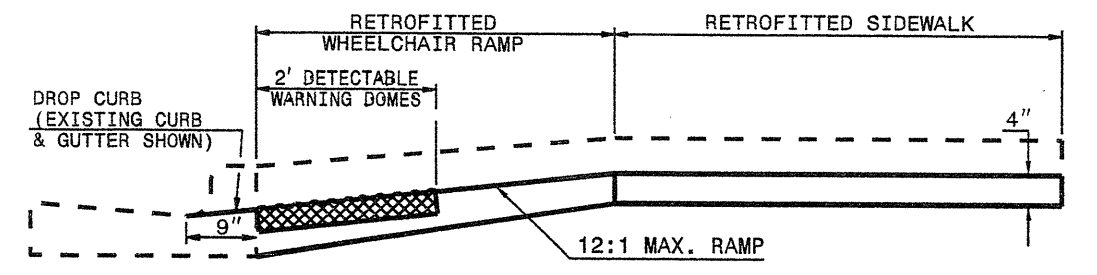
STATE OF NORTH CAROLINA  
DEPT. OF TRANSPORTATION  
DIVISION OF HIGHWAYS  
RALEIGH, N.C.

ENGLISH DETAIL DRAWING FOR  
**WHEELCHAIR RAMP**  
EXISTING CURB AND GUTTER

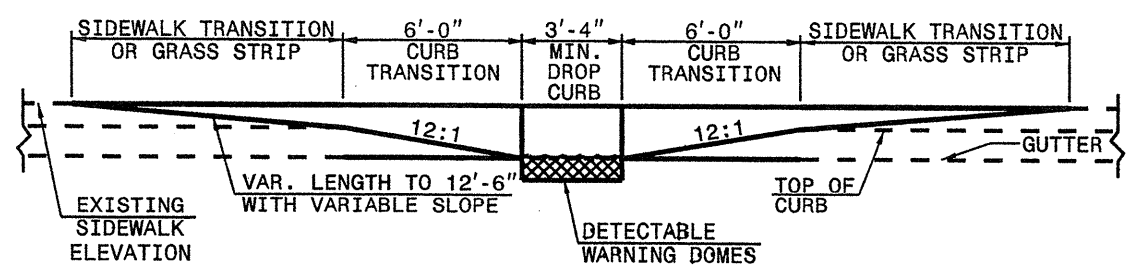
**WHEELCHAIR RAMP AND EXISTING SIDEWALK WITH GRASS STRIP**



**ISOMETRIC VIEW**

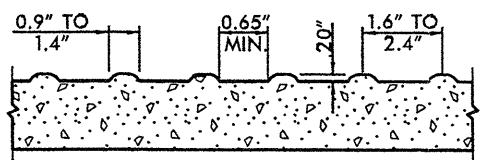
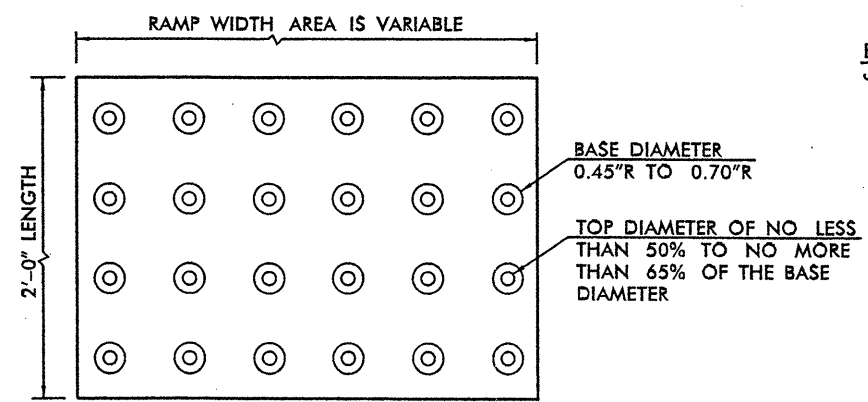


**SECTION B-B**

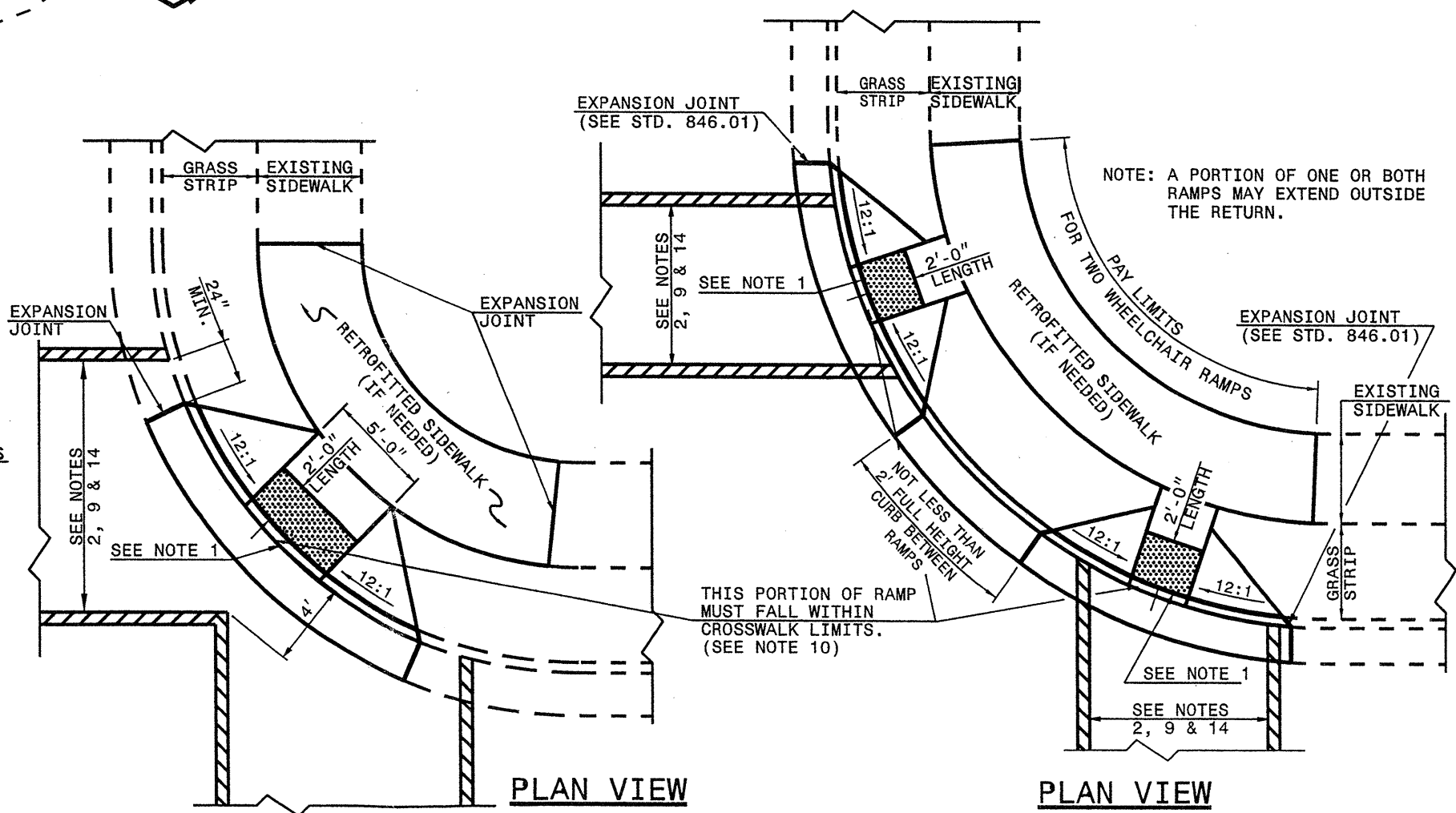


**SECTION A-A**

- NOTES:
1. DETECTABLE WARNING DOMES SHALL COVER 2'-0" LENGTH AND FULL WIDTH OF THE RAMP FLOOR AS SHOWN ON THE DETAILS.
  2. OBTAIN 70% CONTRAST VISIBILITY WITH ADJOINING SURFACE, EITHER LIGHT-ON-DARK, OR DARK-ON-LIGHT SEQUENCE COVERING THE ENTIRE RAMP.



**DETECTABLE WARNING DOMES**



**PLAN VIEW**

DIAGONAL RAMP  
MAX. 25' RADII  
(60" MIN. FLOOR WIDTH)

**PLAN VIEW**

DUAL RAMPS  
ANY RADII  
(40" MIN. FLOOR WIDTH)

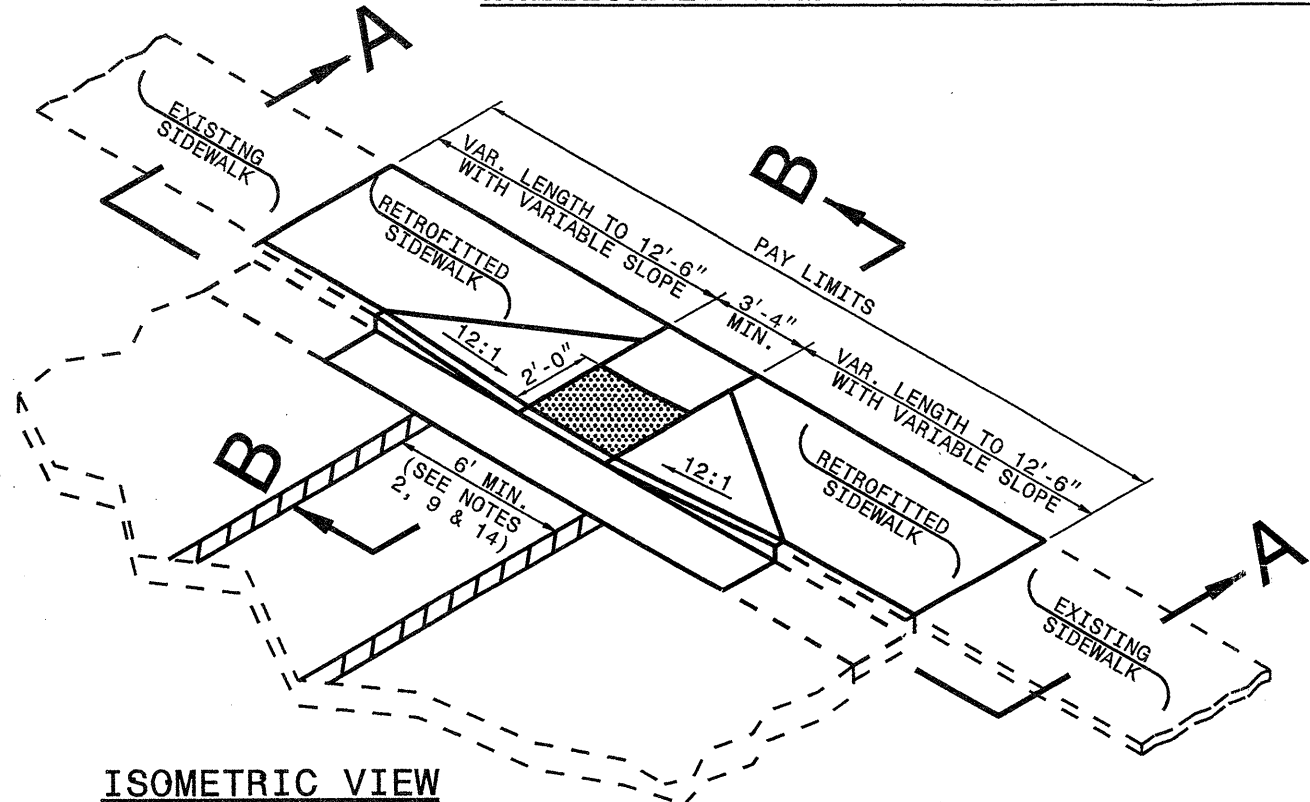
STATE OF NORTH CAROLINA  
DEPT. OF TRANSPORTATION  
DIVISION OF HIGHWAYS  
RALEIGH, N.C.

ENGLISH DETAIL DRAWING FOR  
**WHEELCHAIR RAMP**  
EXISTING CURB AND GUTTER

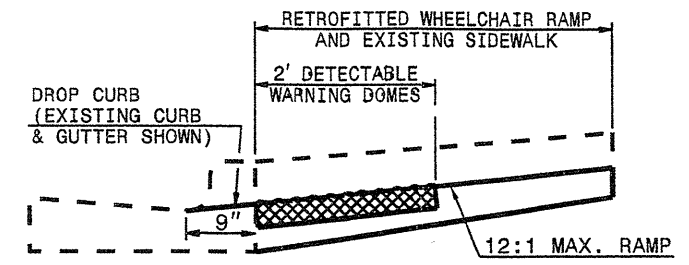
STATE OF NORTH CAROLINA  
DEPT. OF TRANSPORTATION  
DIVISION OF HIGHWAYS  
RALEIGH, N.C.

ENGLISH DETAIL DRAWING FOR  
**WHEELCHAIR RAMP**  
EXISTING CURB AND GUTTER

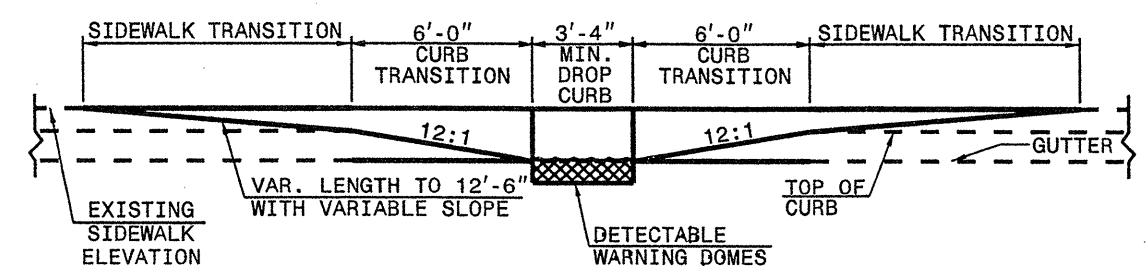
**WHEELCHAIR RAMP AND EXISTING SIDEWALK ADJACENT TO CURB**



**ISOMETRIC VIEW**

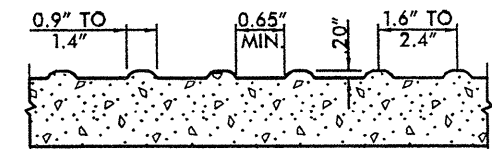
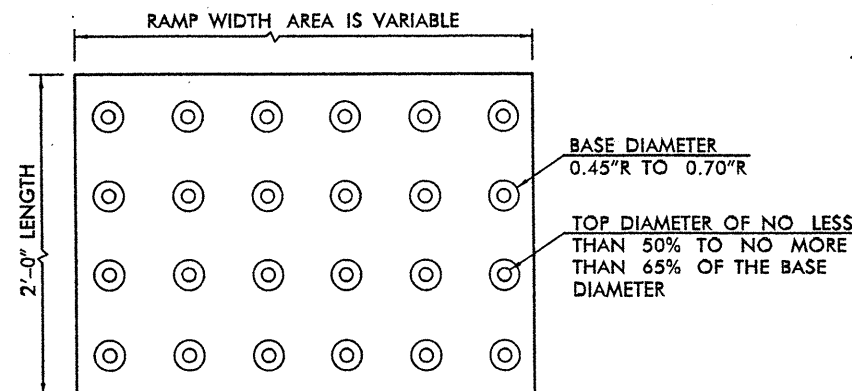


**SECTION B-B**

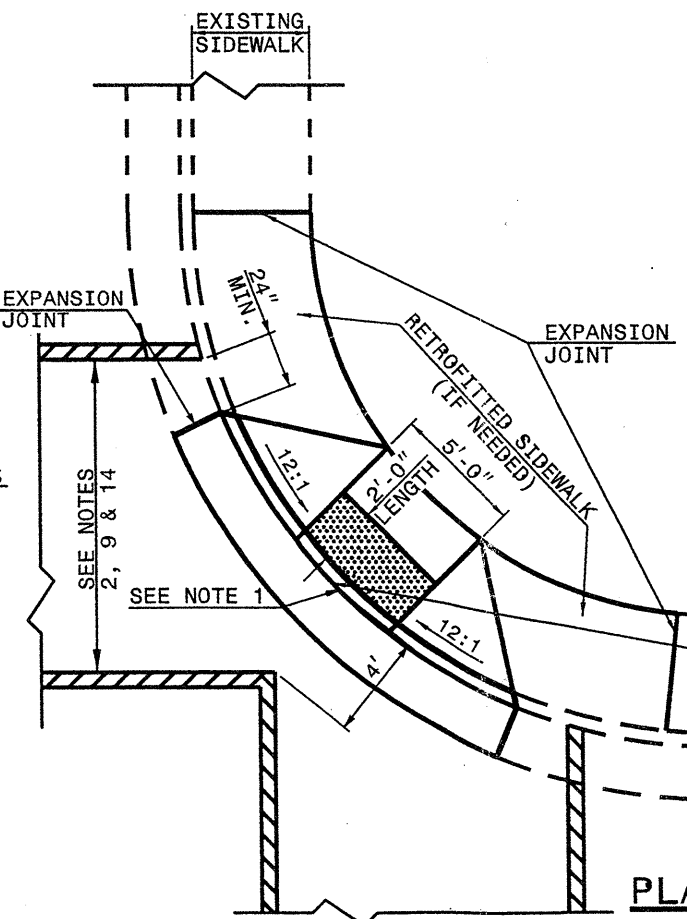


**SECTION A-A**

- NOTES:
1. DETECTABLE WARNING DOMES SHALL COVER 2'-0" LENGTH AND FULL WIDTH OF THE RAMP FLOOR AS SHOWN ON THE DETAILS.
  2. OBTAIN 70% CONTRAST VISIBILITY WITH ADJOINING SURFACE, EITHER LIGHT-ON-DARK, OR DARK-ON-LIGHT SEQUENCE COVERING THE ENTIRE RAMP.

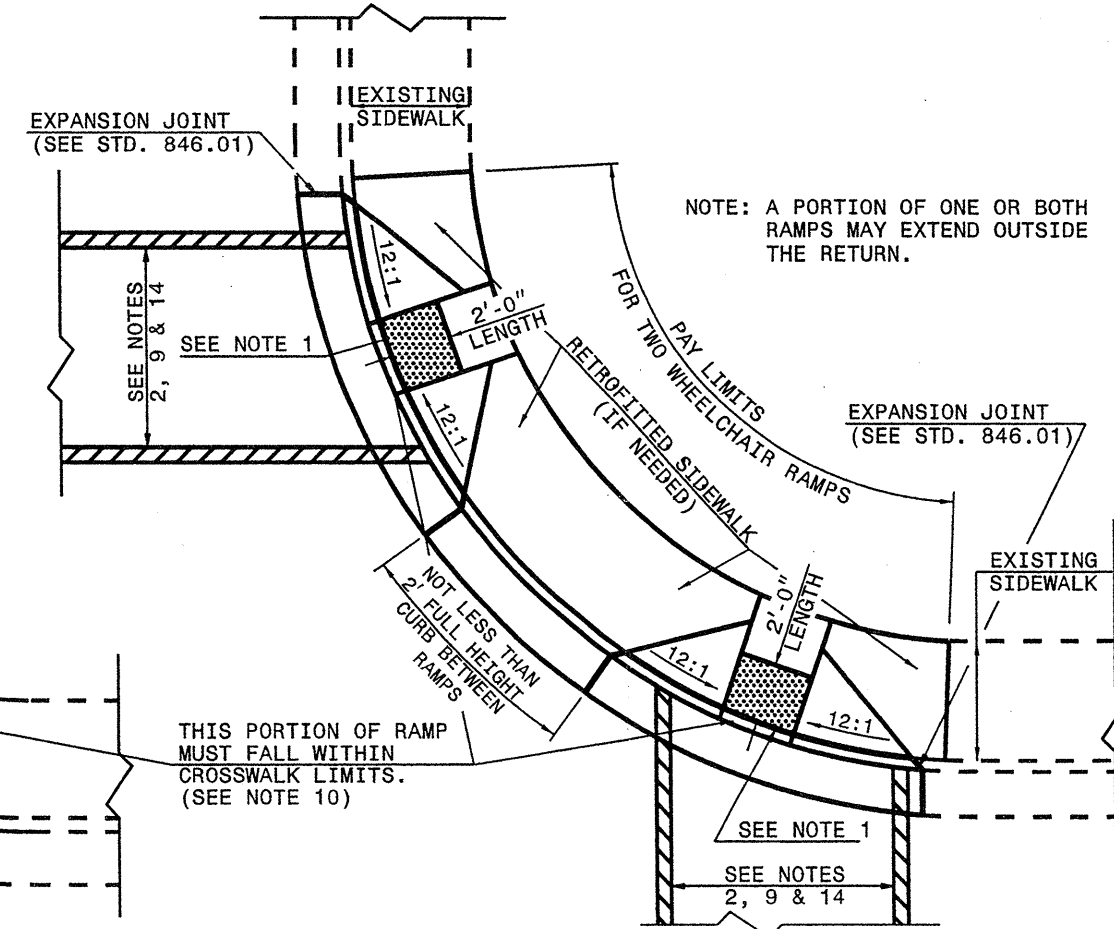


**DETECTABLE WARNING DOMES**



**PLAN VIEW**

DIAGONAL RAMP  
MAX. 25' RADII  
(60" MIN. FLOOR WIDTH)



**PLAN VIEW**

DUAL RAMPS  
ANY RADII  
(40" MIN. FLOOR WIDTH)

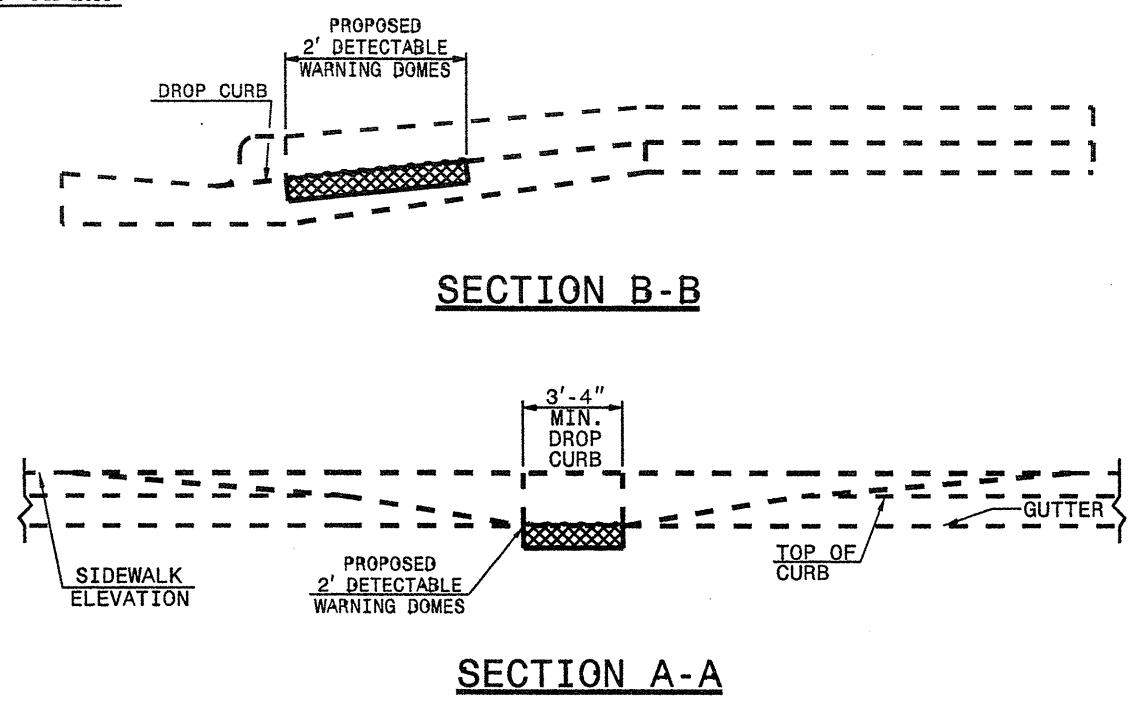
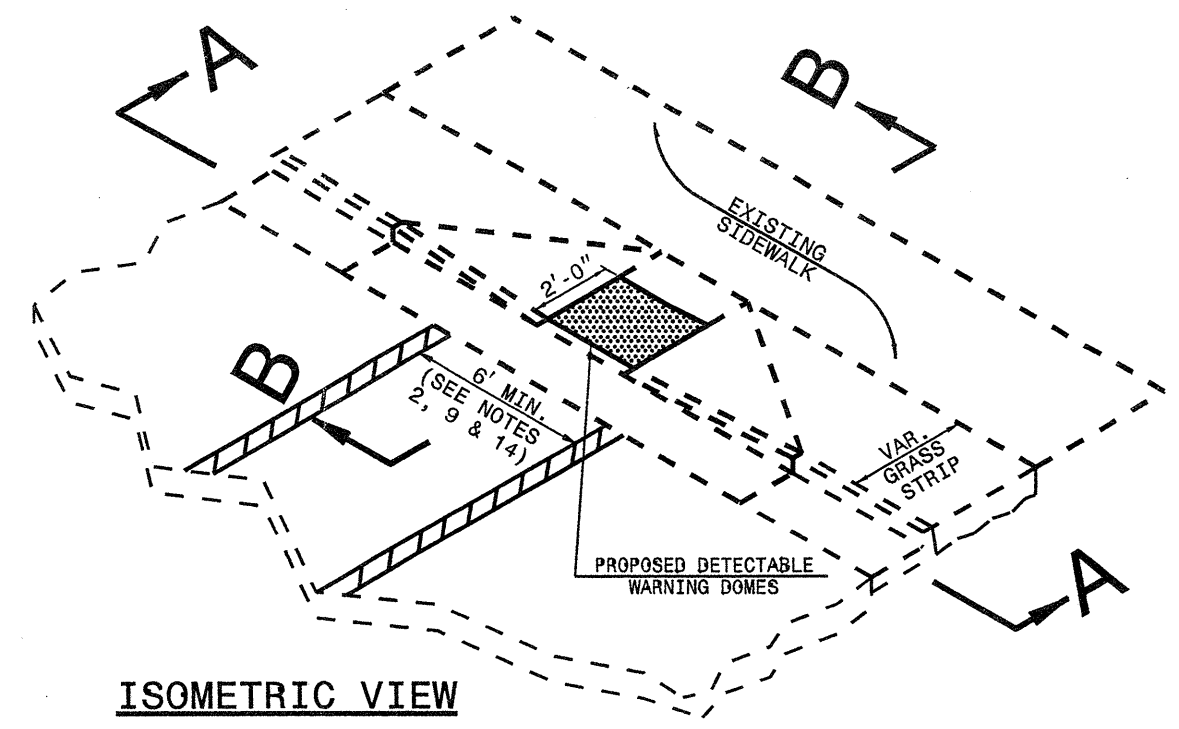
NOTE: A PORTION OF ONE OR BOTH RAMPS MAY EXTEND OUTSIDE THE RETURN.

THIS PORTION OF RAMP MUST FALL WITHIN CROSSWALK LIMITS. (SEE NOTE 10)

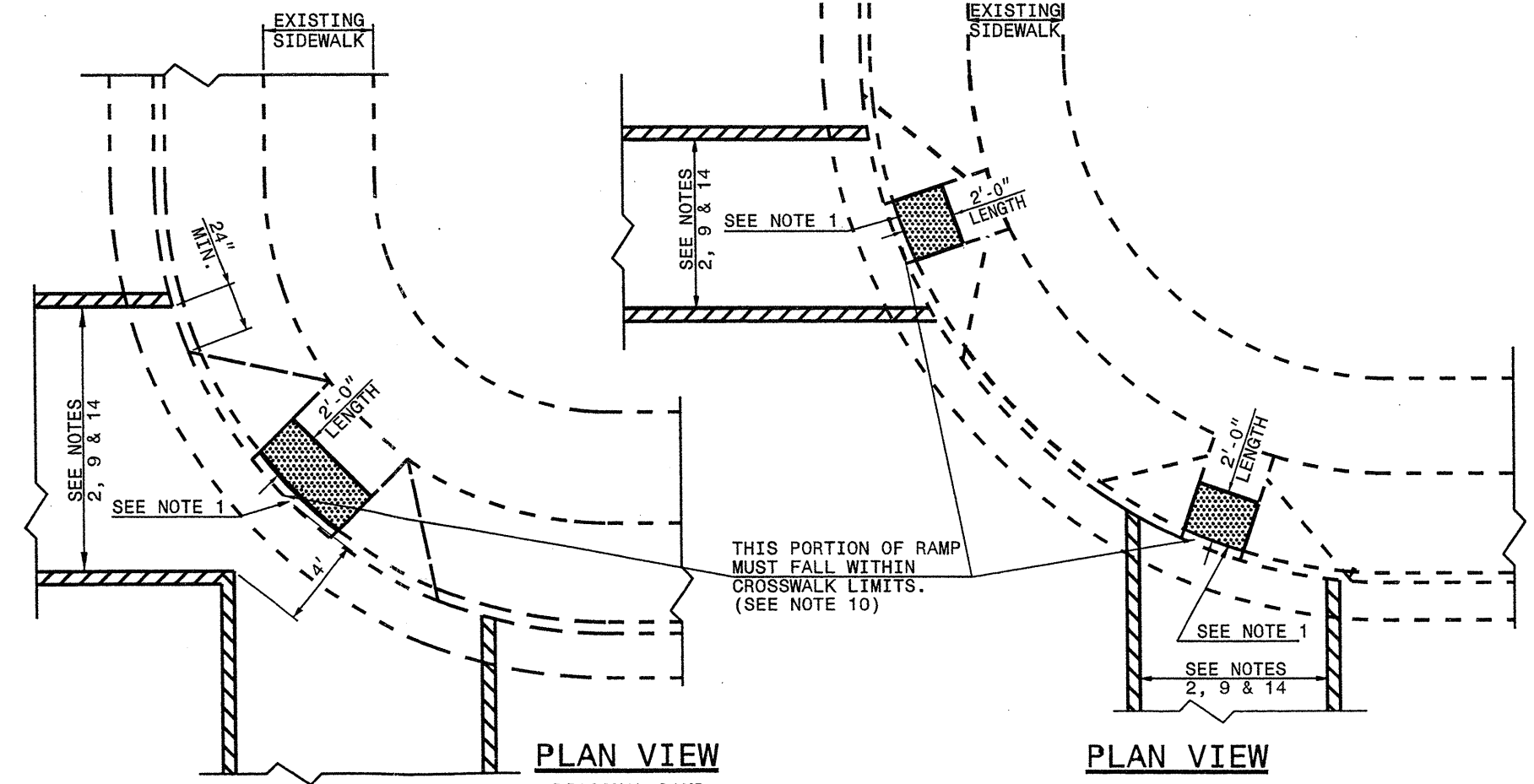
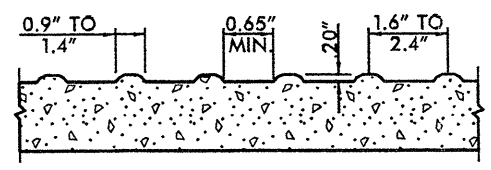
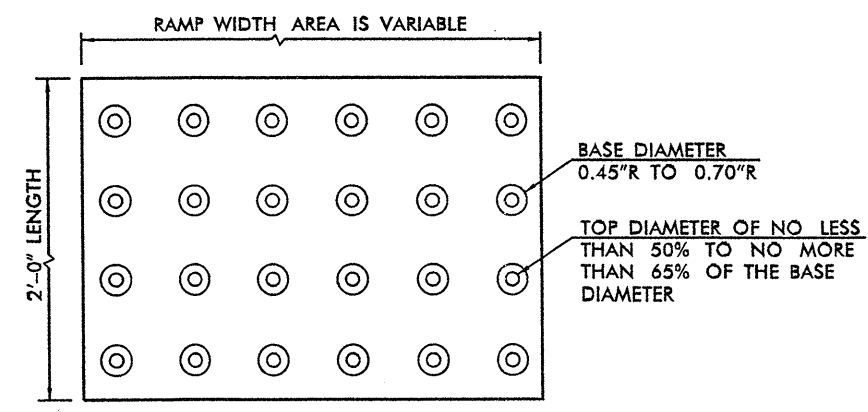
**RETROFITTING DETECTABLE WARNING DOMES ONTO EXISTING WHEELCHAIR RAMP**

STATE OF NORTH CAROLINA  
DEPT. OF TRANSPORTATION  
DIVISION OF HIGHWAYS  
RALEIGH, N.C.

STATE OF NORTH CAROLINA  
DEPT. OF TRANSPORTATION  
DIVISION OF HIGHWAYS  
RALEIGH, N.C.



- NOTES:
1. DETECTABLE WARNING DOMES SHALL COVER 2'-0" LENGTH AND FULL WIDTH OF THE RAMP FLOOR AS SHOWN ON THE DETAILS.
  2. OBTAIN 70% CONTRAST VISIBILITY WITH ADJOINING SURFACE, EITHER LIGHT-ON-DARK, OR DARK-ON-LIGHT SEQUENCE COVERING THE ENTIRE RAMP.



DIAGONAL RAMP  
MAX. 25' RADII  
(60" MIN. FLOOR WIDTH)

DUAL RAMP  
ANY RADII  
(40" MIN. FLOOR WIDTH)

ENGLISH DETAIL DRAWING FOR  
**WHEELCHAIR RAMP**  
EXISTING CURB AND GUTTER

ENGLISH DETAIL DRAWING FOR  
**WHEELCHAIR RAMP**  
EXISTING CURB AND GUTTER

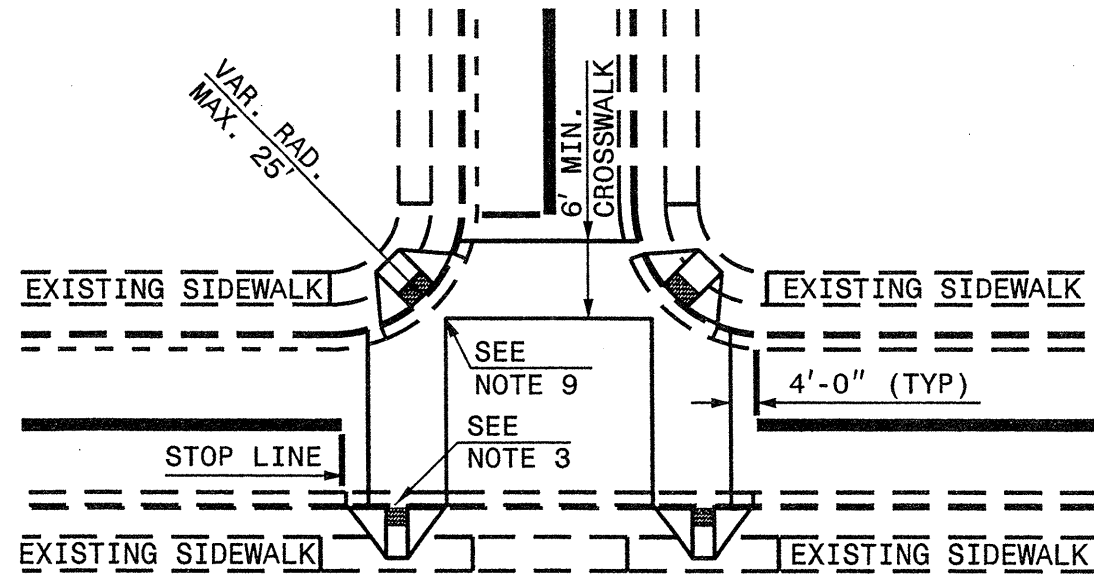
### WHEELCHAIR RAMP AND EXISTING SIDEWALK

STATE OF NORTH CAROLINA  
DEPT. OF TRANSPORTATION  
DIVISION OF HIGHWAYS  
RALEIGH, N.C.

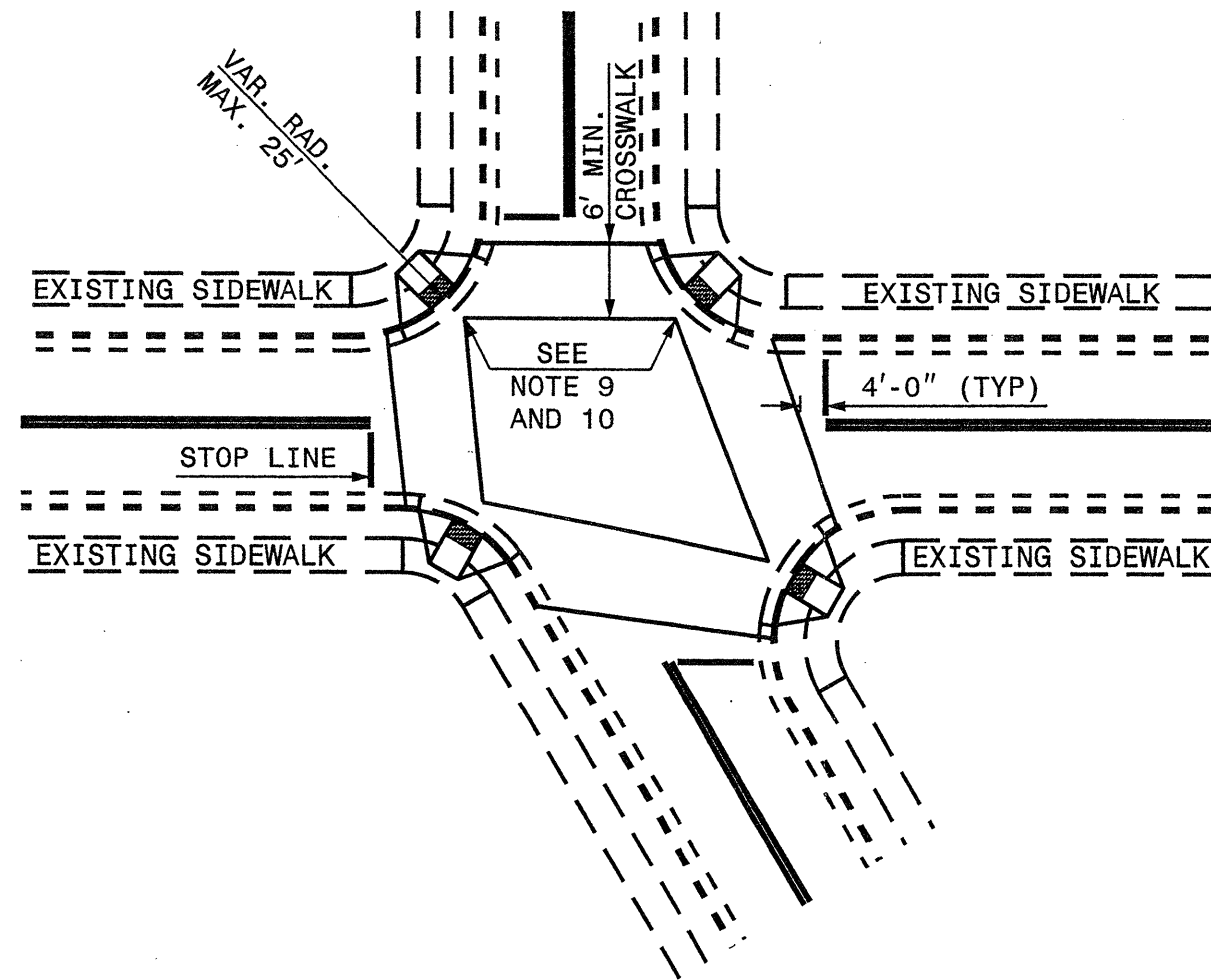
STATE OF NORTH CAROLINA  
DEPT. OF TRANSPORTATION  
DIVISION OF HIGHWAYS  
RALEIGH, N.C.

ENGLISH DETAIL DRAWING FOR  
**WHEELCHAIR RAMP**  
EXISTING CURB AND GUTTER

ENGLISH DETAIL DRAWING FOR  
**WHEELCHAIR RAMP**  
EXISTING CURB AND GUTTER

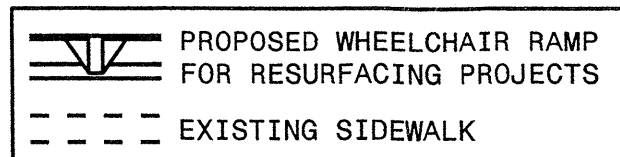


DETAIL SHOWING TYPICAL LOCATION OF WHEELCHAIR RAMPS, PEDESTRIAN CROSSWALKS AND STOP LINES FOR TEE INTERSECTIONS



DETAIL SHOWING TYPICAL LOCATION OF WHEELCHAIR RAMPS, PEDESTRIAN CROSSWALKS AND STOP LINES

#### RESURFACING PROJECTS



ALLOWABLE LOCATIONS  
DIAGONAL RAMP RADII...MAX. 25'

**WHEELCHAIR RAMP AND EXISTING SIDEWALK**

## NOTES:

1. CONSTRUCT THE WALKING SURFACE WITH SLIP RESISTANCE AND A 70% CONTRASTING COLOR TO THE SIDEWALK.
2. CROSSWALK WIDTHS AND CONFIGURATION VARY, BUT MUST CONFORM TO TRAFFIC DESIGN STANDARDS.
3. NORTH CAROLINA GENERAL STATUTE 136-44.14 REQUIRES THAT ALL STREET CURBS BEING CONSTRUCTED OR RECONSTRUCTED FOR MAINTENANCE PROCEDURES, TRAFFIC OPERATIONS, REPAIRS, CORRECTION OF UTILITIES OR ALTERED FOR ANY REASON AFTER SEPTEMBER 1, 1973 SHALL PROVIDE WHEELCHAIR RAMPS FOR THE PHYSICALLY DISABLED AT ALL INTERSECTIONS WHERE BOTH CURB AND GUTTER AND SIDEWALKS ARE PROVIDED AND AT OTHER POINTS OF PEDESTRIAN FLOW.

IN ADDITION, SECTION 228 OF THE 1973 FEDERAL AID HIGHWAY SAFETY ACT REQUIRES PROVISION OF CURB RAMPS ON ANY CURB CONSTRUCTION AFTER JULY 1, 1976 WHETHER A SIDEWALK IS PROPOSED INITIALLY OR IS PLANNED FOR A FUTURE DATE.

THE AMERICANS WITH DISABILITIES ACT (ADA) OF 1990 EXTENDS TO INDIVIDUALS WITH DISABILITIES, COMPREHENSIVE CIVIL RIGHTS PROTECTIONS SIMILAR TO THOSE PROVIDED TO PERSONS ON THE BASIS OF RACE, SEX, NATIONAL ORIGIN AND RELIGION UNDER THE CIVIL RIGHTS ACT OF 1964. THESE CURB RAMPS HAVE BEEN DESIGNED TO COMPLY WITH THE CURRENT ADA STANDARDS.

4. PROVIDE WHEELCHAIR RAMPS AT LOCATIONS AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER. LOCATE WHEELCHAIR RAMPS AS DIRECTED BY THE ENGINEER WHERE EXISTING LIGHT POLES, FIRE HYDRANTS, DROP INLETS, ETC. AFFECT PLACEMENT. WHERE TWO RAMPS ARE INSTALLED PLACE NOT LESS THAN 2 FEET OF FULL HEIGHT CURB BETWEEN THE RAMPS. PLACE DUAL RAMPS AS NEAR PERPENDICULAR TO THE TRAVEL LANE BEING CROSSED AS POSSIBLE.
5. DO NOT EXCEED 0.08 (12:1) SLOPE ON THE WHEELCHAIR RAMP IN RELATIONSHIP TO THE GRADE OF THE STREET.
6. CONSTRUCT WHEELCHAIR RAMPS 40" (3'-4") OR GREATER FOR DUAL RAMPS AND 60" (5'-0") OR GREATER FOR DIAGONAL RAMPS.
7. USE CLASS "B" CONCRETE WITH A SIDEWALK FINISH IN ORDER TO OBTAIN A ROUGH NON-SKID TYPE SURFACE.
8. PLACE A 1/2" EXPANSION JOINT WHERE THE CONCRETE WHEELCHAIR RAMP JOINS THE CURB AND AS SHOWN ON STD. DWG. 848.01.
9. PLACE THE INSIDE PEDESTRIAN CROSSWALK LINES NO CLOSER IN THE INTERSECTION BY BISECTING THE INTERSECTION RADII, WITH ALLOWANCE OF A 4' CLEAR ZONE IN THE VEHICULAR TRAVELWAY WHEN ONE RAMP IS INSTALLED. (SEE NOTE 14)
10. COORDINATE THE CURB CUT AND THE PEDESTRIAN CROSSWALK LINES SO THE FLOOR OF THE WHEELCHAIR RAMP WILL FALL WITHIN THE PEDESTRIAN CROSSWALK LINES. PLACE DIAGONAL RAMPS WITH FLARED SIDES SO 24" OF FULL HEIGHT CURB FALLS WITHIN THE CROSSWALK MARKINGS ON EACH SIDE OF THE FLARES.
11. CONSTRUCT THE PEDESTRIAN CROSSWALK A MINIMUM OF 6 FEET. A CROSSWALK WIDTH OF 10 FEET OR GREATER IS DESIRABLE.
12. USE STOP LINES, NORMALLY PERPENDICULAR TO THE LANE LINES, WHERE IT IS IMPORTANT TO INDICATE THE POINT BEHIND WHICH VEHICLES ARE REQUIRED TO STOP IN COMPLIANCE WITH A TRAFFIC SIGNAL, STOP SIGN OR OTHER LEGAL REQUIREMENT. AN UNUSUAL APPROACH SKEW MAY REQUIRE THE PLACEMENT OF THE STOP LINE TO BE PARALLEL TO THE INTERSECTING ROADWAY.
13. TERMINATE PARKING A MINIMUM OF 20 FEET BACK OF PEDESTRIAN CROSSWALK.
14. PLACE ALL PAVEMENT MARKINGS IN ACCORDANCE WITH THE LATEST EDITION OF THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) PUBLISHED BY THE FEDERAL HIGHWAY ADMINISTRATION AND THE NORTH CAROLINA SUPPLEMENT TO THE MUTCD.

ENGLISH DETAIL DRAWING FOR

**WHEELCHAIR RAMP**

EXISTING CURB AND GUTTER

SHEET 5 OF 5

848D06

STATE OF

NORTH CAROLINA

DEPT. OF TRANSPORTATION  
DIVISION OF HIGHWAYS

RALEIGH, N.C.

STATE OF

NORTH CAROLINA

DEPT. OF TRANSPORTATION  
DIVISION OF HIGHWAYS

RALEIGH, N.C.

ENGLISH DETAIL DRAWING FOR

**WHEELCHAIR RAMP**

EXISTING CURB AND GUTTER

SHEET 5 OF 5

848D06

PROJECT NO.	SHEET NO.	TOTAL NO.
7CR.10171.29		

### SUMMARY OF QUANTITIES

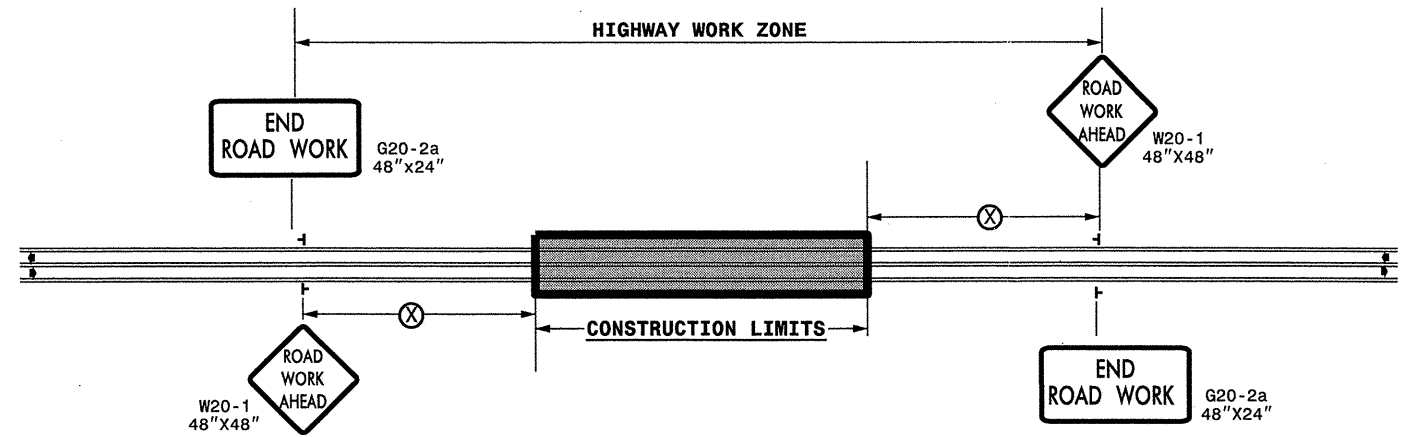
PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	FINAL SURFACE TESTING REQUIRED	LENGTH MI	WIDTH FT	BORROW EXCAVATION CY	INCIDENTAL STONE BASE TONS	SHOULDER RECONSTRUCTION SMI	MILLING ASPHALT PAVEMENT, 0" TO 1 1/2" DEPTH SY	INCIDENTAL MILLING SY	ASPHALT CONC BASE COURSE, TYPE B25.0B TONS	ASPHALT CONC SURFACE COURSE, TYPE S9.5B TONS	ASPHALT BINDER FOR PLANT MIX, GRADE PG 64-22 TONS	GENERIC PAVING ITEM - ASPHALT SURFACE TREATMENT, MAT COAT, #67M STONE SY	RETROFITTING EXISTING WHEELCHAIR RAMPS EA	ADJ. OF METER OR VALVE BOX EA	SEED & MULCHING AC	RESIDENTIAL SEEDING AC			
7CR.10171.29	Caswell	1	NC57	FROM PERSON COUNTY LINE TO NC 62	1	NO	5.11	23	20	302	10.51		192	5,482	7,182	667	68,951			1.91	1.91			
				Bridge # 15	1	NO	0.071	26																
					1	NO	0.134	24							200	144	206	19	1,887		2			
					1	NO	0.01	24-29								11	55	4	158		1			
					2,3	NO	0.067	29							31		116	7	1,140		3			
					3	NO	0.169	30							694		251	15	2,974		3			
					2	NO	0.007	28-30									10	1	119					
					2	NO	0.026	28										36	2	427				
		<b>TOTAL FOR MAP NO. 1</b>						<b>5.594</b>		<b>20</b>	<b>302</b>	<b>10.51</b>	<b>725</b>	<b>392</b>	<b>5,637</b>	<b>7,856</b>	<b>715</b>	<b>75,656</b>		<b>9</b>	<b>1.91</b>	<b>1.91</b>		
		2	NC 62	FROM NC 57 TO VIRGINIA STATE LINE	2	NO	0.022	30			109						125	7	387	1	1			
					2	NO	0.174	32									275	17	3,267					
					2	NO	0.022	38							317		41	2	491					
				Bridge # 63	2	NO	0.182	41																
					2	NO	0.084	35							292		145	9	1,725					
		<b>TOTAL FOR MAP NO. 2</b>						<b>0.484</b>			<b>109</b>			<b>609</b>		<b>586</b>	<b>35</b>	<b>5,870</b>	<b>1</b>	<b>1</b>				
		<b>TOTAL FOR PROJ NO. 7CR.10171.29</b>						<b>6.078</b>		<b>20</b>	<b>411</b>	<b>10.51</b>	<b>725</b>	<b>1,001</b>	<b>5,637</b>	<b>8,442</b>	<b>750</b>	<b>81,526</b>	<b>1</b>	<b>10</b>	<b>1.91</b>	<b>1.91</b>		
		<b>GRAND TOTAL</b>						<b>6.078</b>		<b>20</b>	<b>411</b>	<b>10.51</b>	<b>725</b>	<b>1,001</b>	<b>5,637</b>	<b>8,442</b>	<b>750</b>	<b>81,526</b>	<b>1</b>	<b>10</b>	<b>1.91</b>	<b>1.91</b>		

### THERMOPLASTIC AND PAINT QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	LENGTH	WIDTH	4685000000-E	4686000000-E	4690000000-E	4710000000-E	4721000000-E	4905000000-N	
							4" X 90 M WHITE THERMO LF	4" X 120 M YELLOW THERMO LF	6" X 120 M WHITE THERMO LF	24" X 120 M WHITE THERMO LF	THERMO MSG SPEED LIMIT 30 EA	SNOW PLOWABLE MARKERS EA	
7CR.10171.29	Caswell	1	NC57	FROM PERSON COUNTY LINE TO NC 62	5.594	23	59,070	52,010	250		12	424	
				<b>TOTAL FOR MAP NO. 1</b>				<b>5.594</b>		<b>59,070</b>	<b>52,010</b>	<b>250</b>	
		2	NC 62	FROM NC 57 TO VIRGINIA STATE	0.484	30	5,110	5,234		64	12	36	
				<b>TOTAL FOR MAP NO. 2</b>				<b>0.484</b>		<b>5,110</b>	<b>5,234</b>		<b>12</b>
<b>TOTAL FOR PROJ NO. 7CR.10171.29</b>				<b>6.078</b>		<b>64,180</b>	<b>57,244</b>	<b>250</b>	<b>64</b>	<b>24</b>	<b>460</b>		
<b>GRAND TOTAL</b>				<b>6.078</b>		<b>64,180</b>	<b>57,244</b>	<b>250</b>	<b>64</b>	<b>24</b>	<b>460</b>		



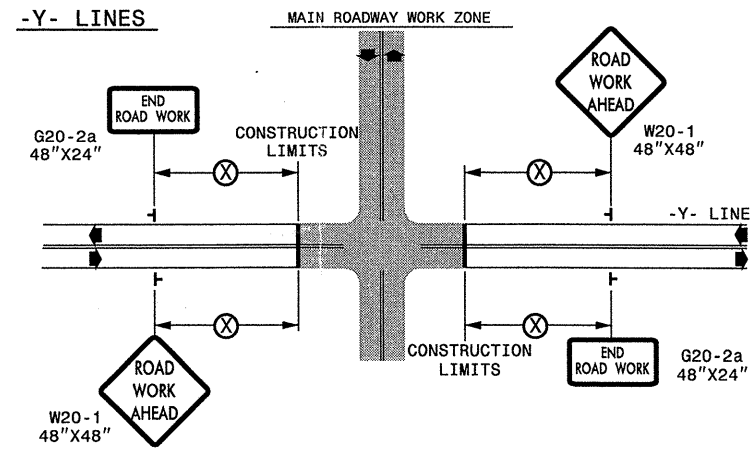
**TWO-WAY UNDIVIDED \*\* (L-LINES)**



POSTED SPEED LIMIT (M.P.H.)	RECOMMENDED MINIMUM SIGN SPACING
≤ 50	500'
≥ 55	1000'

STATE OF NORTH CAROLINA  
DEPT. OF TRANSPORTATION  
DIVISION OF HIGHWAYS  
RALEIGH, N.C.

**ROADWAYS INTERSECTING ALONG 2 WAY UNDIVIDED WORK ZONE (Y-LINES)**



DETAIL DRAWING FOR  
TWO-WAY UNDIVIDED  
WORK ZONE WARNING SIGNS

**GENERAL NOTES**

- USE FLUORESCENT ORANGE SHEETING (TYPE VII OR HIGHER) ON ALL ADVANCED WORK ZONE SIGNS.
- DO NOT INSTALL ADVANCE WARNING SIGNS MORE THAN 3 DAYS PRIOR TO BEGINNING OF WORK.
- SIGNS SHOWN ARE REQUIRED FOR WORK ZONES THAT WILL REMAIN IN EFFECT OVERNIGHT. FOR SHORT-TERM DAILY MAINTENANCE TYPE OPERATIONS, THIS SIGNING APPLICATION IS OPTIONAL; MAY USE ONLY APPLICABLE ROADWAY STANDARD DRAWINGS INSTEAD. HOWEVER, IF THIS SIGNING APPLICATION IS USED, SIGNS MAY BE PORTABLE MOUNTED.
- ALL SIGN SPACING DIMENSIONS ARE APPROXIMATE, FIELD ADJUST AS NECESSARY OR AS DIRECTED.
- USE 3LB STEEL U-CHANNEL POST OR 4" X 4" WOOD POST FOR ALL WORK ZONE SIGNS. 3LB STEEL U-CHANNEL POSTS MUST MEET THE REQUIREMENTS OF STANDARD SPECIFICATION SECTION 1094-1(B), MAY BE GALVANIZED STEEL, OR MAY BE PAINTED GREEN BY THE POST MANUFACTURER. SQUARE STEEL TUBING POSTS HAVING EQUIVALENT STRENGTH OF THE 3 LB STEEL U-CHANNEL POST ARE ALSO ACCEPTABLE FOR USE. ERECT SIGNS PER ROADWAY STANDARD DRAWING 1110.01. PAYMENT FOR WOOD POSTS, 3LB STEEL U-CHANNEL AND SQUARE STEEL TUBING POSTS WITH SIGNS WILL BE MADE ACCORDING TO STANDARD SPECIFICATION "WORK ZONE SIGNS" SECTION 1110.
- WHEN NECESSARY, USE SPLICING IN ACCORDANCE WITH ROADWAY STANDARD DRAWING NO. 1110.01. REMOVE ENTIRE POST WHEN REMOVING SIGNS WITH SPLICED POSTS.
- DO NOT BACK BRACE SIGN SUPPORTS.
- \*\* TWO-WAY UNDIVIDED ADVANCE WARNING SIGN CONFIGURATION MAY BE USED ON URBAN MULTI-LANE FACILITIES WHERE CONDITIONS LIMIT THE USE OF DUAL MOUNTED SIGNS AS DETERMINED BY THE ENGINEER.

**LEGEND**

┌ STATIONARY SIGN

◄ DIRECTION OF TRAFFIC FLOW

SHEET 1 OF 1

APPROVED: _____ DATE: _____	DETAIL DRAWING FOR TWO-WAY UNDIVIDED AND URBAN FREEWAYS ADVANCED WORK ZONE WARNING SIGNS	
SEAL		
SCALE: NONE	REVISIONS	
DATE: _____	7-98	10/01
DWG. BY: _____	10-98	03/04
DESIGN BY: _____	01/01	11/04
REVIEWED BY: _____		

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