

# PLAN FOR PROPOSED TRAFFIC CONTROL, MARKING & DELINEATION CUMBERLAND COUNTY

## ROADWAY STANDARD DRAWINGS

THE FOLLOWING ROADWAY STANDARDS AS APPEAR IN "ROADWAY STANDARD DRAWINGS"- PROJECT SERVICES UNIT-N.C. DEPARTMENT OF TRANSPORTATION-RALEIGH, N.C., DATED JULY 2006 ARE APPLICABLE TO THIS PROJECT AND BY REFERENCE HEREBY ARE CONSIDERED A PART OF THESE PLANS:

STD. NO.	TITLE
1101.02	TEMPORARY LANE CLOSURES
1101.05	WORK ZONE VEHICLE ACCESSES
1101.11	TRAFFIC CONTROL DESIGN TABLES
1110.02	PORTABLE WORK ZONE SIGNS
1115.01	FLASHING ARROW PANELS
1130.01	DRUM
1135.01	CONES
1150.01	FLAGGING DEVICES
1165.01	TRUCK MOUNTED IMPACT ATTENUATOR
1205.01	PAVEMENT MARKINGS - LINE TYPES & OFFSETS
1180.01	SKINNY-DRUM
1205.02	PAVEMENT MARKINGS - 2 LANE & MULTILANE ROADWAYS
1205.04	PAVEMENT MARKINGS - INTERSECTIONS
1205.05	PAVEMENT MARKINGS - TURN LANES
1205.07	PAVEMENT MARKINGS - PEDESTRIAN CROSSWALKS
1205.08	PAVEMENT MARKINGS - SYMBOLS & WORD MESSAGES
1205.10	PAVEMENT MARKINGS - SCHOOL AREAS
1250.01	PAVEMENT MARKER SPACING
1251.01	RAISED PAVEMENT MARKERS (TEMPORARY & PERMANENT)

## INDEX OF SHEETS

SHEET NO.	TITLE
TCP-1	LIST OF APPLICABLE ROADWAY STANDARD DRAWINGS, LEGEND AND INDEX OF SHEETS
TCP-2	PROJECT NOTES
TCP-3	PHASING NOTES
TCP-4,5,6	CONSTRUCTION PHASING
TCP-7,8	ADVANCED WARNING SIGNS
PM-1 THRU 8	PAVEMENT MARKING PLANS

## TEMP. PAVEMENT MARKING SCHEDULE

SYMBOL	DESCRIPTION	QUANTITY BREAKDOWN
PAVEMENT MARKING LINES		
PA	WHITE EDGELINE (2X)	
PC	10 FT WHITE SKIP (2X)	
PD	2 FT WHITE MINISKIP (2X)	
PE	WHITE SOLID LANE LINE (2X)	
PAVEMENT MARKING SYMBOLS		
QA	LEFT TURN ARROW (2X)	
QB	HIGHT TURN ARROW (2X)	
QC	STRAIGHT ARROW (2X)	
PAVEMENT MARKING REMOVAL		
	REMOVAL OF PAVEMENT MARKING	
	REMOVAL OF SYMBOLS	

NOTE: FOR EACH PAINT PAVEMENT MARKING ITEM, 2X IMPLIES TWO APPLICATIONS.

## LEGEND

- GENERAL**
- DIRECTION OF TRAFFIC FLOW
  - NORTH ARROW
  - PROPOSED PVMT. EXIST. PVMT.
  - WORK AREA
  - REMOVAL OF EXISTING PAVEMENT
- TRAFFIC CONTROL DEVICES**
- TYPE I BARRICADE
  - TYPE II BARRICADE
  - TYPE III BARRICADE
  - CONE
  - DRUM SKINNY DRUM
  - FLASHING ARROW PANEL (TYPE C)
  - STATIONARY SIGN
  - PORTABLE SIGN
  - STATIONARY OR PORTABLE SIGN
  - CRASH CUSHION
  - CHANGEABLE MESSAGE SIGN
  - TRUCK MOUNTED IMPACT ATTENUATOR (TMIA)
  - POLICE
  - FLAGGER
- PAVEMENT MARKINGS**
- CRYSTAL/CRYSTAL PAVEMENT MARKER
  - YELLOW/YELLOW PAVEMENT MARKER
  - CRYSTAL/RED PAVEMENT MARKER
  - PAVEMENT MARKING SYMBOLS

APPROVED: _____ DATE: _____		<b>TITLE SHEET</b>	
	SCALE:		REVISIONS
	DATE: 01/09		01/10
	DWG. BY: I.DERNEY		
	DESIGN BY: I.DERNEY		
REVIEWED BY:		CARD FILE:	

# GENERAL NOTES

CHANGES MAY BE REQUIRED WHEN PHYSICAL DIMENSIONS IN THE DETAIL DRAWINGS, STANDARD DETAILS AND ROADWAY DETAILS ARE NOT ATTAINABLE TO MEET FIELD CONDITIONS, OR RESULT IN DUPLICATE, OR UNDESIRED OVERLAPPING OF DEVICES. MODIFICATION MAY INCLUDE: MOVING, SUPPLEMENTING, COVERING OR REMOVAL OF DEVICES, AS DIRECTED BY THE ENGINEER.

THE FOLLOWING GENERAL NOTES APPLY AT ALL TIMES FOR THE DURATION OF THE CONSTRUCTION PROJECT, EXCEPT WHEN OTHERWISE NOTED IN THE PLAN, OR DIRECTED BY THE ENGINEER.

## TIME RESTRICTIONS

### A) DO NOT CLOSE OR NARROW TRAVEL LANES AS FOLLOWS:

ROAD NAME	DAY AND TIME RESTRICTIONS
RAMSEY ST	MONDAY THRU FRIDAY: 7 TO 9 AM AND 4 TO 6 PM

### B) DO NOT CLOSE OR NARROW TRAVEL LANES DURING HOLIDAYS AND SPECIAL EVENTS AS FOLLOWS:

ROAD NAME
RAMSEY ST

### HOLIDAY

- FOR ANY UNEXPECTED OCCURRENCE THAT CREATES UNUSUALLY HIGH TRAFFIC VOLUMES, AS DIRECTED BY THE ENGINEER.
- FOR NEW YEAR'S, BETWEEN THE HOURS OF 4:00 P.M. DECEMBER 31st TO 9:00 A.M. JANUARY 2ND. IF NEW YEAR'S DAY IS ON A FRIDAY, SATURDAY, SUNDAY, OR MONDAY THEN UNTIL 9:00 A.M. THE FOLLOWING TUESDAY.
- FOR EASTER, BETWEEN THE HOURS OF 4:00 P.M. THURSDAY AND 9:00 A.M. MONDAY.
- FOR MEMORIAL DAY, BETWEEN THE HOURS OF 4:00 P.M. FRIDAY TO 9:00 A.M. TUESDAY.
- FOR INDEPENDENCE DAY, BETWEEN THE HOURS OF 4:00 P.M. THE DAY BEFORE INDEPENDENCE DAY AND 9:00 A.M. THE DAY AFTER INDEPENDENCE DAY.

IF INDEPENDENCE DAY IS ON A FRIDAY, SATURDAY, SUNDAY OR MONDAY THEN BETWEEN THE HOURS OF 4:00 P.M. THE THURSDAY BEFORE INDEPENDENCE DAY AND 9:00 A.M. THE TUESDAY AFTER INDEPENDENCE DAY.

- FOR LABOR DAY, BETWEEN THE HOURS OF 4:00 P.M. FRIDAY AND 9:00 A.M. TUESDAY.
- FOR THANKSGIVING DAY, BETWEEN THE HOURS OF 4:00 P.M. TUESDAY TO 9:00 A.M. MONDAY.
- FOR CHRISTMAS, BETWEEN THE HOURS OF 4:00 P.M. THE FRIDAY BEFORE THE WEEK OF CHRISTMAS DAY AND 9:00 A.M. THE FOLLOWING TUESDAY AFTER THE WEEK OF CHRISTMAS.

### C) DO NOT CLOSE ROADS AS FOLLOWS:

ROAD NAME	DAY AND TIME RESTRICTIONS
RAMSEY ST	ANY TIME

### D) DO NOT CONDUCT ANY HAULING OPERATIONS AGAINST THE FLOW OF TRAFFIC OF AN OPEN TRAVELWAY UNLESS THE HAULING OPERATION IS PROTECTED BY BARRIER OR GUARDRAIL OR AS DIRECTED BY THE ENGINEER.

## LANE AND SHOULDER CLOSURE REQUIREMENTS

- E) REMOVE LANE CLOSURE DEVICES FROM THE LANE WHEN WORK IS NOT BEING PERFORMED BEHIND THE LANE CLOSURE OR WHEN A LANE CLOSURE IS NO LONGER NEEDED, OR AS DIRECTED BY THE ENGINEER.
- F) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING ON THE SHOULDER ADJACENT TO AN UNDIVIDED FACILITY AND WITHIN 5 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN TRAVEL LANE USING ROADWAY STANDARD DRAWING NO. 1101.02 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.

WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING ON THE SHOULDER ADJACENT TO A DIVIDED FACILITY AND WITHIN 10 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN TRAVEL LANE USING ROADWAY STANDARD DRAWING NO. 1101.02 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.

- G) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN A LANE OF TRAVEL OF AN UNDIVIDED OR DIVIDED FACILITY, CLOSE THE LANE ACCORDING TO THE TRAFFIC CONTROL PLANS, ROADWAY STANDARD DRAWING OR AS DIRECTED BY THE ENGINEER. CONDUCT THE WORK SO THAT ALL PERSONNEL AND/OR EQUIPMENT REMAIN WITHIN THE CLOSED TRAVEL LANE.

- H) DO NOT WORK SIMULTANEOUSLY WITHIN 15 FT ON BOTH SIDES OF AN OPEN TRAVELWAY, RAMP OR LOOP WITHIN THE SAME LOCATION UNLESS PROTECTED WITH GUARDRAIL OR BARRIER.

- I) DO NOT INSTALL MORE THAN 2500 FT OF LANE CLOSURE ON RAMSEY ST MEASURED FROM THE BEGINNING OF THE MERGE TAPER TO THE END OF THE LANE CLOSURE.

- J) DO NOT INSTALL MORE THAN 1 SIMULTANEOUS LANE CLOSURES, IN ANY ONE DIRECTION, ON RAMSEY ST.

## PAVEMENT EDGE DROP OFF REQUIREMENTS

- K) BACKFILL AT A 6:1 SLOPE UP TO THE EDGE AND ELEVATION OF EXISTING PAVEMENT IN AREAS ADJACENT TO AN OPENED TRAVEL LANE THAT HAS AN EDGE OF PAVEMENT DROP-OFF AS FOLLOWS:

BACKFILL DROP-OFFS THAT EXCEED 2 INCHES ON ROADWAYS WITH POSTED SPEED LIMITS OF 45 MPH OR GREATER.

BACKFILL DROP-OFFS THAT EXCEED 3 INCHES ON ROADWAYS WITH POSTED SPEED LIMITS LESS THAN 45 MPH.

BACKFILL WITH SUITABLE COMPACTED MATERIAL, AS APPROVED BY THE ENGINEER, AT NO EXPENSE TO THE DEPARTMENT.

- L) DO NOT EXCEED A DIFFERENCE OF 2 INCHES IN ELEVATION BETWEEN OPEN LANES OF TRAFFIC FOR NOMINAL LIFTS OF 1.5 INCHES. INSTALL ADVANCE WARNING "UNEVEN LANES" SIGNS (W8-11) 350 FT IN ADVANCE AND A MINIMUM OF EVERY HALF MILE THROUGHOUT THE UNEVEN AREA.

## TRAFFIC PATTERN ALTERATIONS

- M) NOTIFY THE ENGINEER TWENTY ONE (21) CALENDAR DAYS PRIOR TO ANY TRAFFIC PATTERN ALTERATION.

## SIGNING

- N) INSTALL ADVANCE WORK ZONE WARNING SIGNS WHEN WORK IS WITHIN 40 FT FROM THE EDGE OF TRAVEL LANE AND NO MORE THAN THREE (3) DAYS PRIOR TO THE BEGINNING OF CONSTRUCTION.
- O) PROVIDE PERMANENT SIGNING.
- P) ENSURE ALL NECESSARY SIGNING IS IN PLACE PRIOR TO ALTERING ANY TRAFFIC PATTERN.
- Q) INSTALL BLACK ON ORANGE "DIP" SIGNS (W8-2) AND/OR "BUMP" SIGNS (W8-1) 500 IN ADVANCE OF THE UNEVEN AREA, OR AS DIRECTED BY THE ENGINEER.

## TRAFFIC CONTROL DEVICES

- R) SPACE CHANNELIZING DEVICES IN WORK AREAS NO GREATER IN FEET THAN TWICE THE POSTED SPEED LIMIT (MPH), EXCEPT 10 FT ON-CENTER IN RADII, AND 3 FT OFF THE EDGE OF AN OPEN TRAVELWAY, WHEN LANE CLOSURES ARE NOT IN EFFECT. WHEN SKINNY DRUMS ARE ALLOWED, REFER TO SECTION 1180 OF STANDARD SPECIFICATIONS FOR ROADS AND STRUCTURES OR AS SHOWN IN THE PLANS.
- S) PLACE ADDITIONAL SETS OF THREE CHANNELIZING DEVICES PERPENDICULAR TO THE EDGE OF TRAVELWAY ON 350 FT CENTERS WHEN UNOPENED LANES ARE CLOSED TO TRAFFIC.

## PAVEMENT MARKINGS AND MARKERS

- T) INSTALL PAVEMENT MARKINGS AND PAVEMENT MARKERS ON THE FINAL SURFACE AS FOLLOWS:

ROAD NAME	MARKING	MARKER
RAMSEY ST	THERMOPLASTIC	RAISED

- U) INSTALL TEMPORARY PAVEMENT MARKINGS AND TEMPORARY PAVEMENT MARKERS ON INTERIM LAYERS OF PAVEMENT AS FOLLOWS:

ROAD NAME	MARKING	MARKER
RAMSEY ST	PAINT	N/A

- V) PLACE ONE APPLICATION OF PAINT FOR TEMPORARY TRAFFIC PATTERNS. PLACE A SECOND APPLICATION OF PAINT SIX (6) MONTHS AFTER THE INITIAL APPLICATION AND EVERY SIX MONTHS AS DIRECTED BY THE ENGINEER.

- W) TIE PROPOSED PAVEMENT MARKING LINES TO EXISTING PAVEMENT MARKING LINES.

- X) REMOVE/REPLACE ANY CONFLICTING/DAMAGED PAVEMENT MARKINGS AND MARKERS BY THE END OF EACH DAY'S OPERATION.

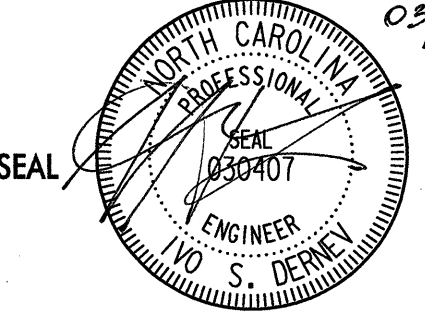
- Y) TRACE THE PROPOSED MONOLITHIC ISLAND LOCATIONS WITH PROPER COLOR PAVEMENT MARKINGS PRIOR TO INSTALLATION. PLACE DRUMS TO DELINEATE ANY PROPOSED MONOLITHIC ISLANDS BEFORE INSTALLATION

## TEMPORARY / FINAL SIGNALS

- Z) NOTIFY THE ENGINEER TWO (2) MONTHS BEFORE A TRAFFIC SIGNAL INSTALLATION BY OTHERS IS REQUIRED.

## MISCELLANEOUS

- AA) ALL WHEELCHAIR RAMP LOCATIONS SHALL BE DERIVED FROM STATIONING SHOWN ON PAVEMENT MARKING PLANS OR AS DIRECTED BY THE ENGINEER IN COORDINATION WITH THE SIGNING AND DELINEATION UNIT.
- BB) CONTRACTOR SHALL MAINTAIN SIDEWALK ACCESS AT ALL TIMES. CONTRACTOR SHALL BE RESPONSIBLE TO PROVIDE TEMPORARY SIDEWALKS (CONCRETE, ASPHALT, OR OTHER SUITABLE MATERIAL AS APPROVED BY THE ENGINEER) AT ALL LOCATIONS WHERE THE OPEN PEDESTRIAN TRAVELWAY HAS BEEN REMOVED FOR CONSTRUCTION OPERATIONS (UTILITIES, DRAINAGE, ETC.).
- CC) LAW ENFORCEMENT MAY BE USED TO MAINTAIN TRAFFIC THROUGH THE WORK AREA AND/OR INTERSECTIONS, AS DIRECTED BY THE ENGINEER.

APPROVED:	DATE:	<b>PROJECT NOTES</b>	
	02/12/10		
SCALE:	01/09	REVISIONS	
DWG. BY:	I. DERNEV	01/10	
DESIGN BY:	I. DERNEV		
REVIEWED BY:			

# PHASING NOTES

PROJ. REFERENCE NO.	SHEET NO.
W-5000	TCP-3

INSTALL ADVANCE WORKZONE SIGNS. USE NCDOT ROADWAY STANDARD DRAWING 1101.02 (TEMPORARY LANE CLOSURE) AND WHILE MAINTAINING TRAFFIC, CONSTRUCT:

**STEP 1. PROPOSED BULB-OUTS AND LANE WIDENINGS ON RAMSEY ST.**

**SECTION 1**

- \* AT LAW RD INTERSECTION\* STA. 11.00+/-
- \* ACROSS FROM SHOPPING CENTER NORTH ENTRANCE - STA. 28.00+/-
- \* ACROSS FROM LOWE\*S SOUTH ENTRANCE - STA. 36.00+/-
- \* AT ANDOVER RD INTERSECTION\* STA. 44.00+/-
- \* AT STA. 56.00 +/-
- \* AT WAL-MART ENTRANCE - STA. 67.00+/-

**SECTION 2**

- \* ACROSS FROM WALMART ENTRANCE - STA. 69.+00+/-
- \* INTERSECTION WITH MEADOWCROFT DR - STA. 78.00+/-
- \* INTERSECTION WITH TREETOP DR - STA. 84.00+/- AND STA. 85.00+/-
- \* INTERSECTION WITH COLLEGE CENTER DR - STA. 92.00+/-
- \* INTERSECTION WITH FERNWOOD DR AND LOWDERMILK RD - STA. 99.00+/- AND STA. 100.00+/-
- \* ACROSS FROM TALLSTONE DR - STA. 112.00+/-
- \* INTERSECTION WITH ALOHA DR - STA. 125.00 +/-

**SECTION 3**

- \* ACROSS FROM RIDGEWAY DR - STA. 130.00+/-
- \* INTERSECTION WITH LOFTON DR - STA. 140.00+/-
- \* INTERSECTION WITH KINGS CREEK DR - STA. 149.00+/-
- \* INTERSECTION WITH SHAWCROFT RD - STA. 162.00+/-

**STEP 2. PROPOSED MONOLITHIC CONCRETE ISLAND AND MEDIAN CURB AND GUTTER.**

**SECTION 1**

- \* STA. 10.27+/- (LAW RD INTERSECTION) TO STA. 26.58+/- (SHOPPING CENTER NORTH ENTRANCE) - 1631 FEET (0.3 MILES)
- \* STA. 27.71+/- (SHOPPING CENTER NORTH ENTRANCE) TO STA. 34.58+/- (LOWE\*S SOUTH ENTRANCE) - 678 FEET (0.13 MILES)
- \* STA. 35.57+/- (LOWE\*S SOUTH ENTRANCE) TO STA. 43.24+/- (MCARTHUR RD INTERSECTION) - 767 FEET
- \* STA. 44.05+/- (MCARTHUR RD INTERSECTION) TO STA. 55.94+/- - 1189 FEET
- STA. 56.37+/- TO STA. 67.60+/- (WAL-MART ENTRANCE) - 1123 FEET

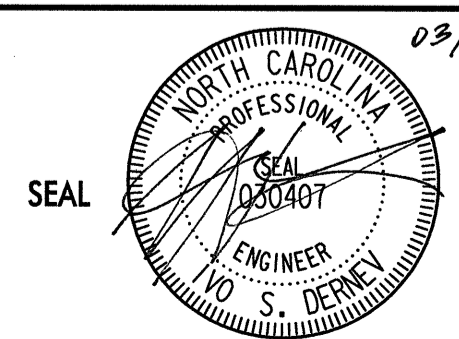
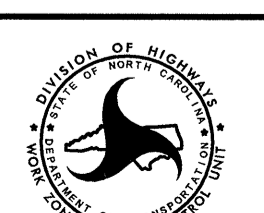
**SECTION 2**

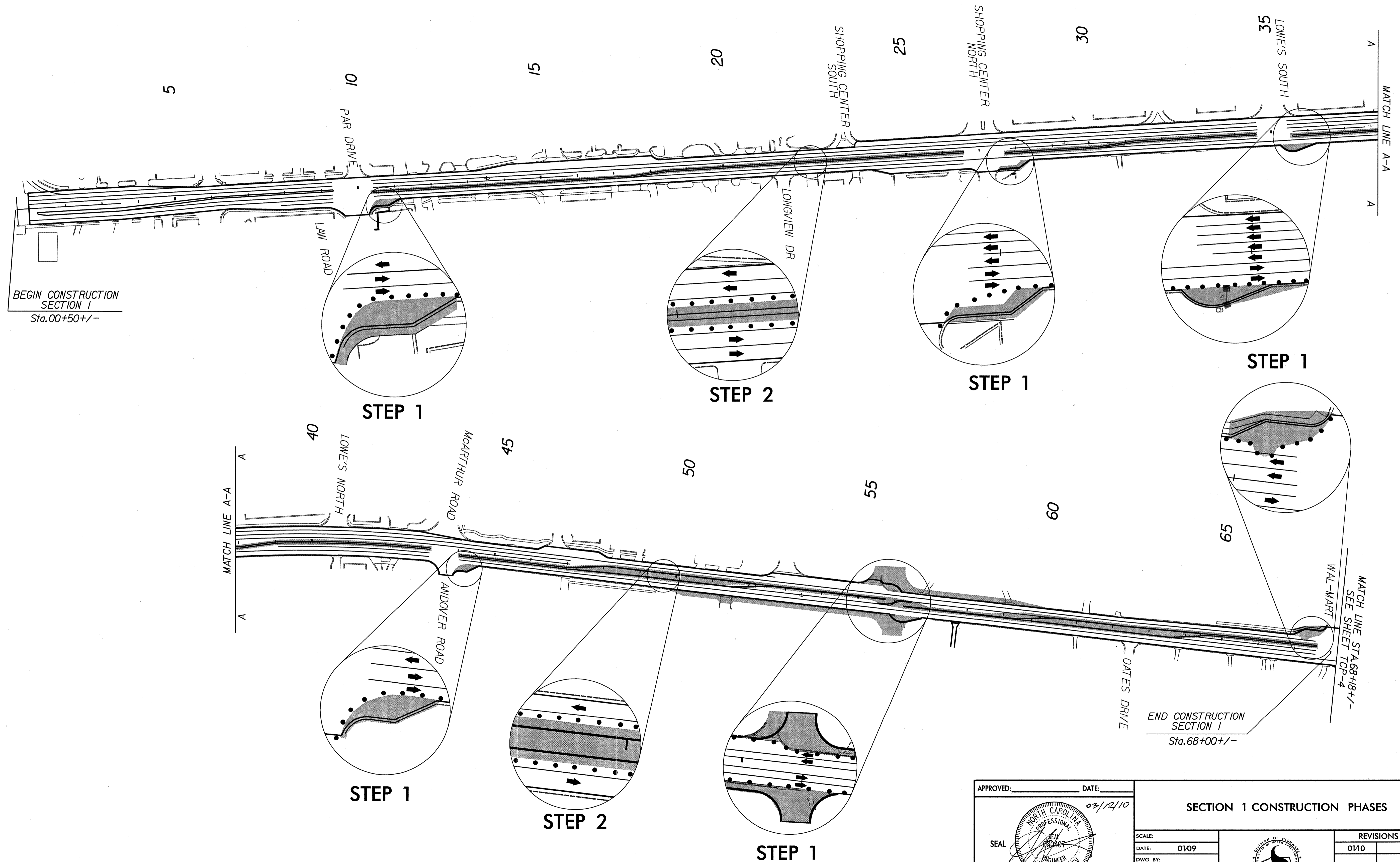
- \* STA. 68.79+/- (WAL-MART ENTRANCE) TO STA. 76.95+/- (MEADOWCROFT DR INTERSECTION) - 816 FEET
- \* STA. 77.77+/- (MEADOWCROFT DR INTERSECTION) TO STA. 84.31+/- (TREETOP DR INTERSECTION) - 654 FEET
- \* STA. 85.06+/- (TREETOP DR INTERSECTION DRAINAGE ISSUE) TO STA. 90.18+/- (STACY WEAVER DR INTERSECTION) - 512 FEET
- \* STA.91.50+/- (STACY WEAVER DR INTERSECTION) TO STA. 98.71+/- (LOWDERMILK RD INTERSECTION) - 721 FEET
- \* STA. 99.70+/- (LOWDERMILK RD INTERSECTION) TO STA. 111.62+/- (TALLSTONE DR INTERSECTION) - 1192 FEET
- \* STA. 112.06+/- (TALLSTONE DR) TO STA. 125.38+/- (KINLAW RD INTERSECTION) - 1332 FEET

**SECTION 3**

- \* STA. 126.03+/- (KINLAW RD INTERSECTION) TO STA. 128.83+/- (RIDGEWAY DR INTERSECTION) - 280 FEET
- \* STA. 129.75+/- (RIDGEWAY DR) TO STA. 136.00+/- (LONDON CT INTERSECTION) DO NOT COMPLETE ISLAND - 625 FEET
- \* STA. 136.70+/- (LONDON CT INTERSECTION) TO STA. 140.00+/- (LOFTON DR INTERSECTION) - 330 FEET
- \* STA. 140.00+/- (LONDON CT INTERSECTION) TO STA. 161.75+/- (SHAWCROFT RD INTERSECTION) - 2175 FEET
- \* STA. 162.46+/- (SHAWCROFT RD INTERSECTION) TO STA. 180.50+/- - 1804 FEET

**STEP 3. MILL AND RESURFACE TRAVEL LANES. PLACE/INSTALL FINAL PAVEMENT MARKINGS AND MARKERS**

APPROVED: _____ DATE: _____ 	<h2 style="margin: 0;">PHASING NOTES</h2>	<table border="1" style="width: 100%; border-collapse: collapse; font-size: 8px;"> <tr> <th colspan="2">REVISIONS</th> </tr> <tr> <td style="width: 50%;">01/10</td> <td style="width: 50%;"></td> </tr> <tr> <td> </td> <td> </td> </tr> <tr> <td> </td> <td> </td> </tr> </table>	REVISIONS		01/10					
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BEGIN CONSTRUCTION  
SECTION 1  
Sta. 00+50+/-

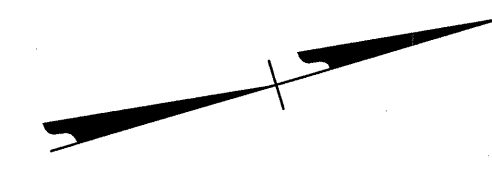
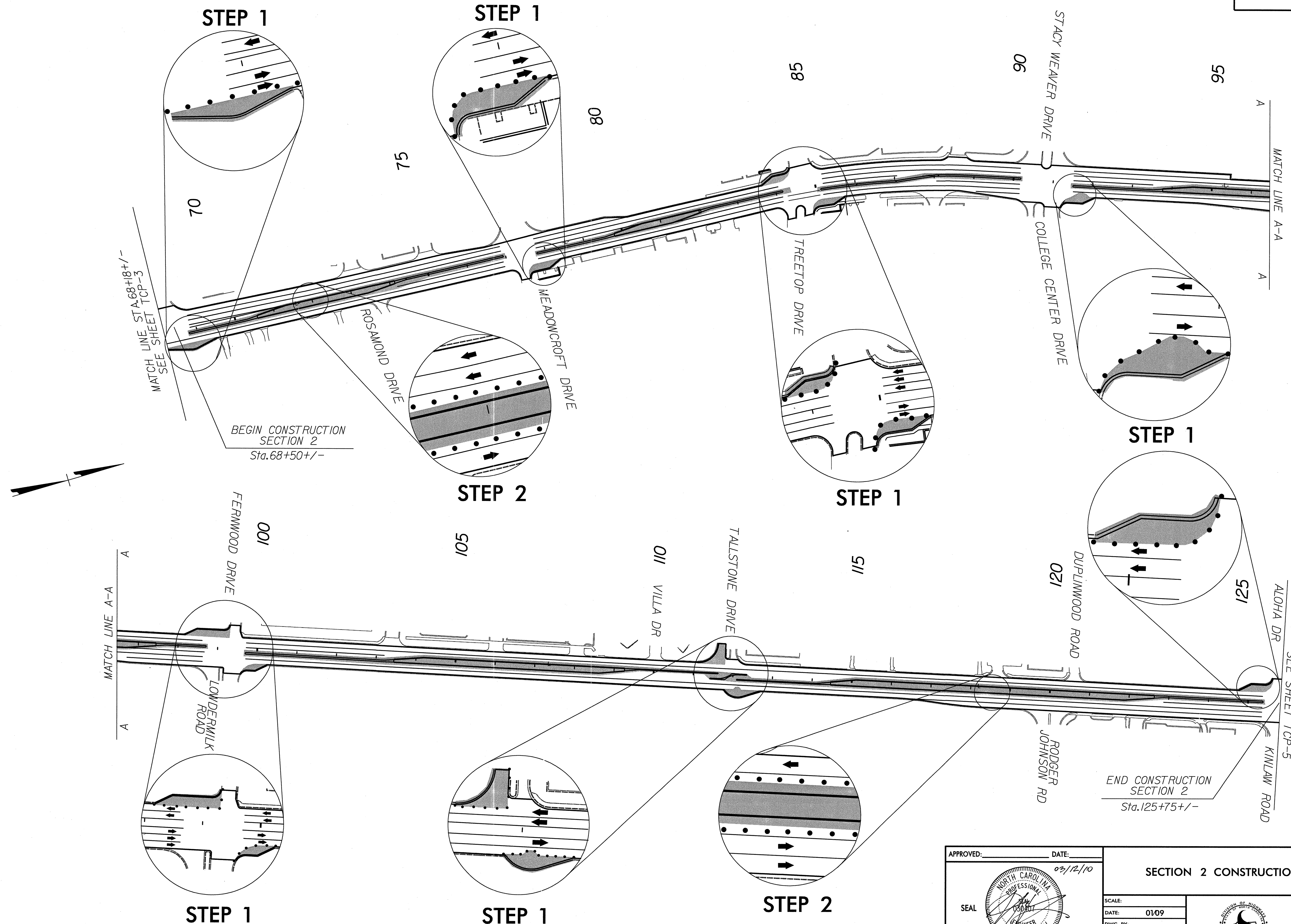
END CONSTRUCTION  
SECTION 1  
Sta. 68+00+/-

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DWG. BY:	DESIGN BY: I. DERNEY											
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MATCH LINE STA 68+48+/-  
SEE SHEET TCP-4

MATCH LINE A-A

MATCH LINE A-A



MATCH LINE STA. 68+18 +/-  
SEE SHEET TCP-3

BEGIN CONSTRUCTION  
SECTION 2  
Sta. 68+50 +/-

END CONSTRUCTION  
SECTION 2  
Sta. 125+75 +/-

MATCH LINE A-A

MATCH LINE A-A

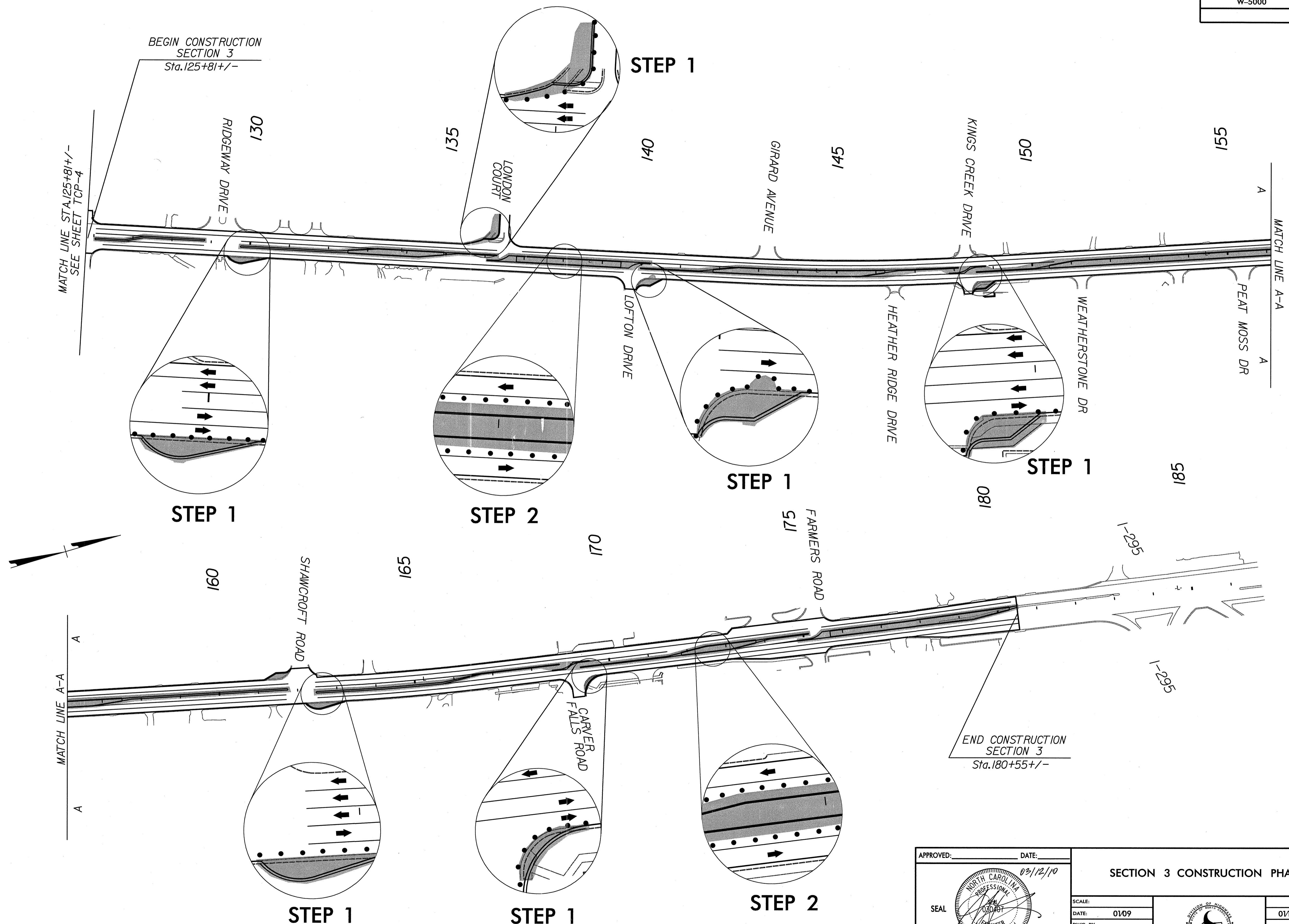
MATCH LINE STA. 125+81 +/-  
SEE SHEET TCP-5

APPROVED: \_\_\_\_\_ DATE: 02/12/10

SEAL

SEAL NOT VALID UNLESS SIGNED AND DATED

SECTION 2 CONSTRUCTION PHASES		REVISIONS	
SCALE:		01/10	
DATE:	01/09		
DWG. BY:			
DESIGN BY:	I. DERNEY		
REVIEWED BY:			

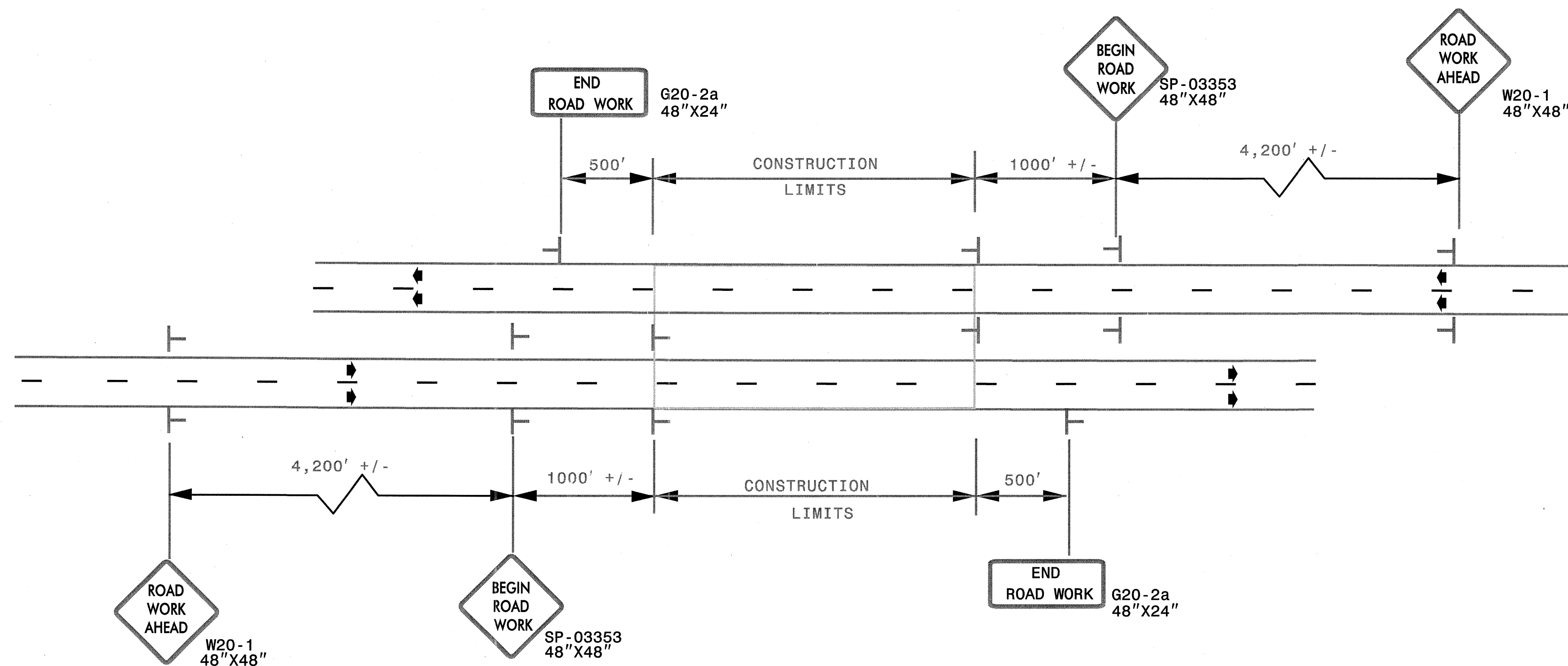


APPROVED:	DATE:	SECTION 3 CONSTRUCTION PHASES	
	03/12/10	SCALE:	REVISIONS
		DATE: 01/09	01/10
		DWG. BY:	
		DESIGN BY: I. DERNEY	
	REVIEWED BY:		

# ADVANCED WORK ZONE WARNING SIGNING FOR FREEWAYS (4 LANES OR GREATER)

PROJ. REFERENCE NO. W-5000	SHEET NO. TCP-7
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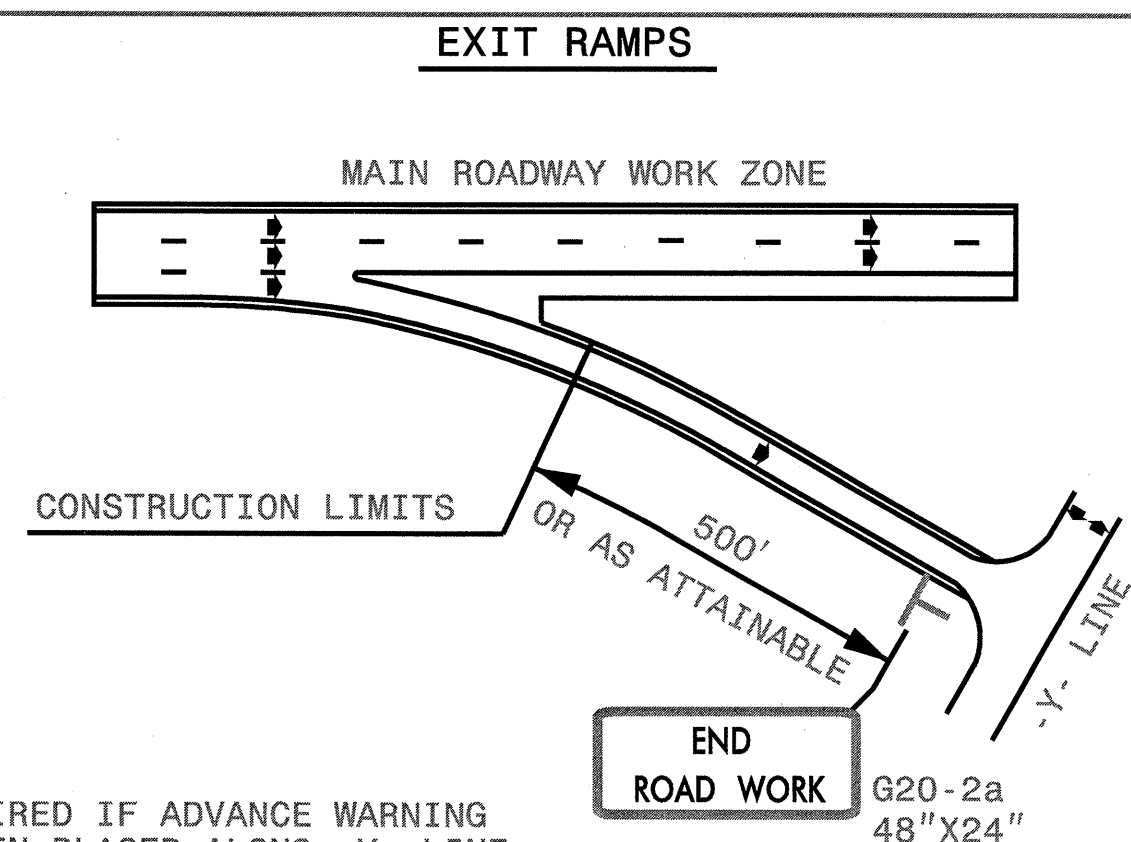
## DETAIL A



LEGEND	
	STATIONARY SIGN
▶	DIRECTION OF TRAFFIC FLOW

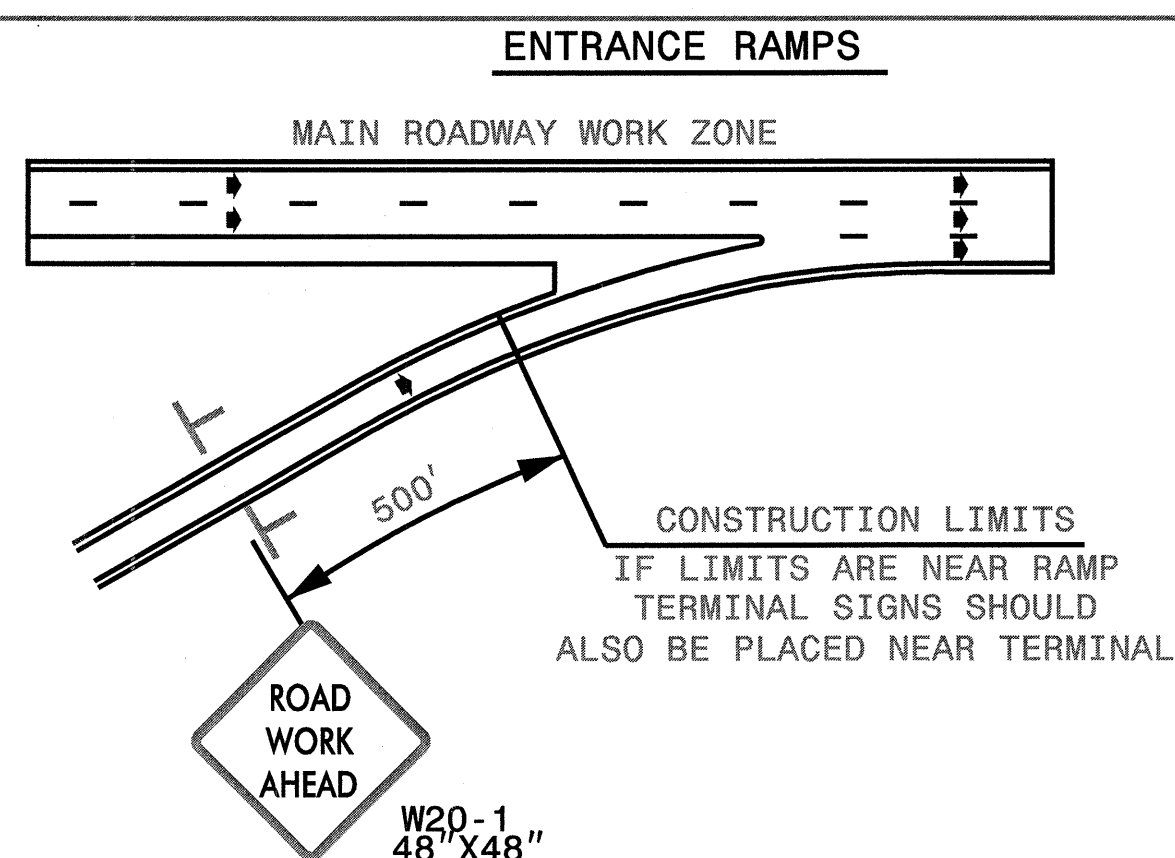
\* USE THE "\$250 SPEEDING PENALTY" SIGN, SPEED LIMIT SIGN, AND ORANGE PANEL; ONLY WHEN A "\$250 SPEEDING PENALTY" ORDINANCE HAS BEEN ISSUED BY THE REGIONAL TRAFFIC ENGINEER.

## DETAIL B



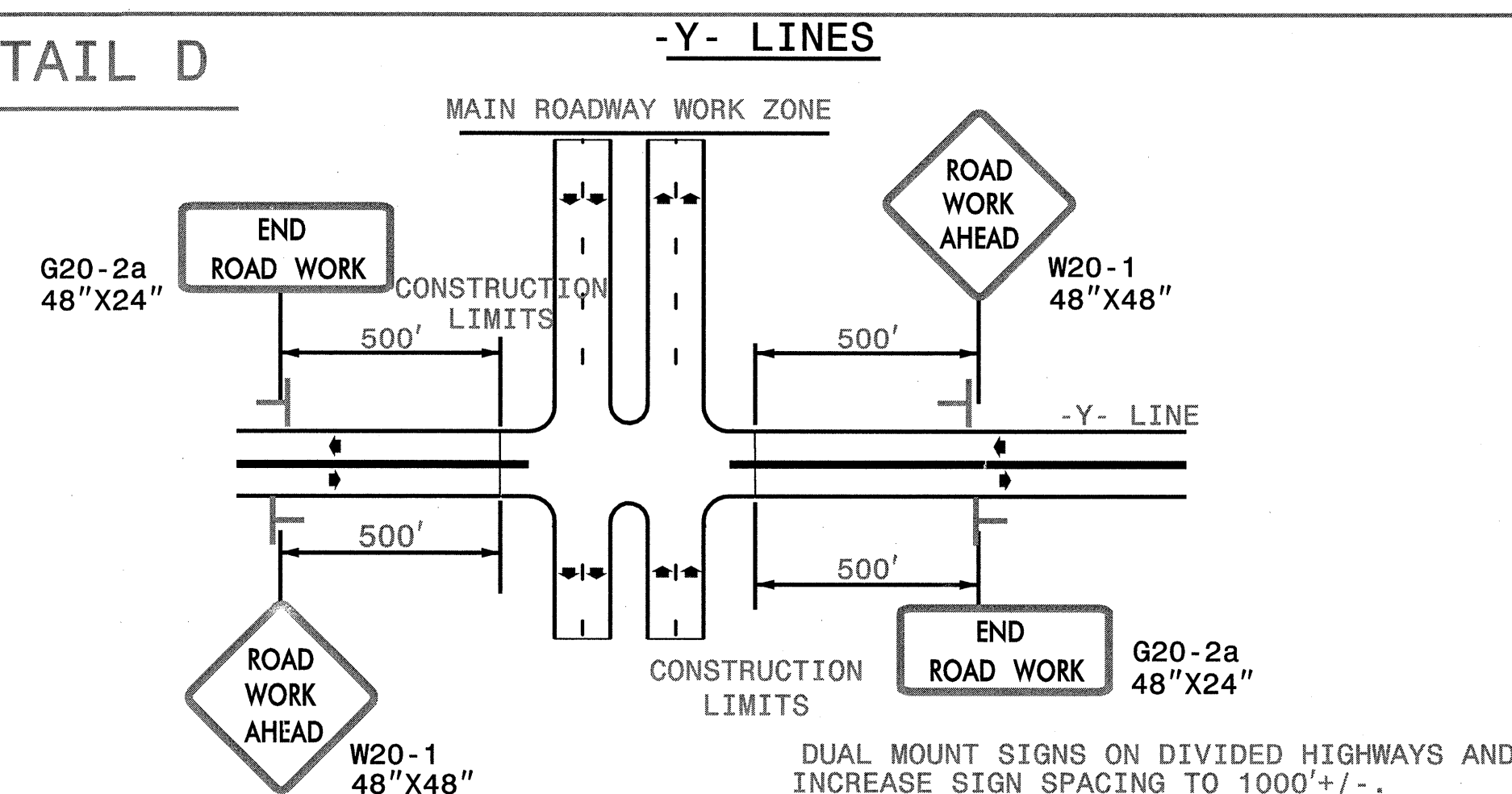
NOTE: SIGN NOT REQUIRED IF ADVANCE WARNING SIGNS HAVE BEEN PLACED ALONG -Y- LINE THAT RAMP INTERSECTS. IF CONSTRUCTION LIMITS ARE AT END OF RAMP, PLACE SIGN AT END OF RAMP.

## DETAIL C



CONSTRUCTION LIMITS IF LIMITS ARE NEAR RAMP TERMINAL SIGNS SHOULD ALSO BE PLACED NEAR TERMINAL

## DETAIL D



DUAL MOUNT SIGNS ON DIVIDED HIGHWAYS AND INCREASE SIGN SPACING TO 1000'+/-.

## GENERAL NOTES

- USE FLUORESCENT ORANGE SHEETING (TYPE VII OR HIGHER) ON ALL ADVANCED WORK ZONE SIGNS.
- DO NOT INSTALL ADVANCE WARNING SIGNS MORE THAN 3 DAYS PRIOR TO BEGINNING OF WORK.
- SIGNS SHOWN ARE REQUIRED FOR WORK ZONES THAT WILL REMAIN IN EFFECT OVERNIGHT. FOR SHORT-TERM DAILY MAINTENANCE TYPE OPERATIONS, THIS SIGNING APPLICATION IS OPTIONAL; MAY USE ONLY APPLICABLE ROADWAY STANDARD DRAWINGS INSTEAD. HOWEVER, IF THIS SIGNING APPLICATION IS USED, SIGNS MAY BE PORTABLE MOUNTED.
- ALL SIGN SPACING DIMENSIONS ARE APPROXIMATE, FIELD ADJUST AS NECESSARY OR AS DIRECTED.
- USE 3LB STEEL U-CHANNEL POST OR 4" X 4" WOOD POST FOR ALL WORK ZONE SIGNS. 3LB STEEL U-CHANNEL POSTS MUST MEET THE REQUIREMENTS OF STANDARD SPECIFICATION SECTION 1094-1(B), MAY BE GALVANIZED STEEL, OR MAY BE PAINTED GREEN BY THE POST MANUFACTURER. SQUARE STEEL TUBING POSTS HAVING EQUIVALENT STRENGTH OF THE 3 LB STEEL U-CHANNEL POST ARE ALSO ACCEPTABLE FOR USE. ERECT SIGNS PER ROADWAY STANDARD DRAWING 1110.01. PAYMENT FOR WOOD POSTS, 3LB STEEL U-CHANNEL AND SQUARE STEEL TUBING POSTS WITH SIGNS WILL BE MADE ACCORDING TO STANDARD SPECIFICATION "WORK ZONE SIGNS" SECTION 1110.
- WHEN NECESSARY, USE SPLICING IN ACCORDANCE WITH ROADWAY STANDARD DRAWING NO. 1110.01. REMOVE ENTIRE POST WHEN REMOVING SIGNS WITH SPLICED POSTS.
- DO NOT BACK BRACE SIGN SUPPORTS.

APPROVED: _____	DATE: 03/12/10	<b>ADVANCED WORK ZONE WARNING SIGNS FOR FREEWAYS (4 LANES OR GREATER)</b>		
	SCALE: _____			
	DATE: 01/09			REVISIONS
	DWG. BY: _____			01/10
	DESIGN BY: I. DERNEV			
REVIEWED BY: _____				

