

STATE OF NORTH CAROLINA
DIVISION OF HIGHWAYS

STATE PROJECT REFERENCE NO.	SHEET NO.
R-5002B	TCP-1

**PLAN FOR PROPOSED
TRAFFIC CONTROL, MARKING & DELINEATION
McDOWELL COUNTY**

ROADWAY STANDARD DRAWINGS

THE FOLLOWING ROADWAY STANDARDS AS APPEAR IN "ROADWAY STANDARD DRAWINGS" - PROJECT SERVICES UNIT-N.C. DEPARTMENT OF TRANSPORTATION-RALEIGH, N.C., DATED JULY 2006 ARE APPLICABLE TO THIS PROJECT AND BY REFERENCE HEREBY ARE CONSIDERED A PART OF THESE PLANS:

STD. NO.	TITLE
1101.02	TEMPORARY LANE CLOSURES
1101.05	WORK ZONE VEHICLE ACCESSES
1101.06	WARNING SIGNS FOR BLASTING ZONES
1101.11	TRAFFIC CONTROL DESIGN TABLES
1110.01	STATIONARY WORK ZONE SIGNS
1110.02	PORTABLE WORK ZONE SIGNS
1130.01	DRUM
1135.01	CONES
1145.01	BARRICADES
1150.01	FLAGGING DEVICES
1160.01	TEMPORARY CRASH CUSHION
1165.01	TRUCK MOUNTED IMPACT ATTENUATOR
1170.01	PORTABLE CONCRETE BARRIER
1180.01	SKINNY-DRUM
1205.01	PAVEMENT MARKINGS - LINE TYPES & OFFSETS
1205.02	PAVEMENT MARKINGS - 2 LANE & MULTILANE ROADWAYS
1250.01	PAVEMENT MARKER SPACING
1251.01	RAISED PAVEMENT MARKERS (TEMPORARY & PERMANENT)
1253.01	SNOWPLOWABLE RAISED PAVEMENT MARKERS
1261.01	GUARDRAIL & BARRIER DELINEATOR SPACING
1261.02	GUARDRAIL & BARRIER DELINEATOR TYPES
1262.01	GUARDRAIL END DELINEATION

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TCP-8	WORK ZONE WARNING SIGNS
PM-1	TEMP AND FINAL PAVEMENT MARKING SCHEDULE
PM-2 THRU PM-3	FINAL PAVEMENT MARKINGS

LEGEND

GENERAL

- DIRECTION OF TRAFFIC FLOW
- NORTH ARROW
- PROPOSED PVMT. EXIST. PVMT.
- WORK AREA
- REMOVAL OF EXISTING PAVEMENT

TRAFFIC CONTROL DEVICES

- TYPE I BARRICADE
- TYPE II BARRICADE
- TYPE III BARRICADE
- CONE
- DRUM SKINNY DRUM
- FLASHING ARROW PANEL (TYPE C)
- STATIONARY SIGN
- PORTABLE SIGN
- STATIONARY OR PORTABLE SIGN
- CRASH CUSHION
- CHANGEABLE MESSAGE SIGN
- TRUCK MOUNTED IMPACT ATTENUATOR (TMIA)
- POLICE
- FLAGGER

PAVEMENT MARKINGS

- CRYSTAL/CRYSTAL PAVEMENT MARKER
- YELLOW/YELLOW PAVEMENT MARKER
- CRYSTAL/RED PAVEMENT MARKER
- PAVEMENT MARKING SYMBOLS

TIP PROJECT: R-5002B

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PLAN REVIEWED BY: N.C.D.O.T. WORK ZONE TRAFFIC CONTROL UNIT	APPROVED: <i>Kenneth Arthur Wilson</i> DATE: 6-5-09	PLAN PREPARED BY: N.C.D.O.T. DIVISION 13 DDC UNIT
_____ TRAFFIC CONTROL ENGINEER		K. A. WILSON, PE PROJECT ENGINEER
_____ TRAFFIC CONTROL PROJECT ENGINEER		M. K. PENLAND DIVISION DESIGN ENGINEER
_____ TRAFFIC CONTROL PROJECT DESIGN ENGINEER		J. P. FREEMAN DESIGN TECHNICIAN
_____ TRAFFIC CONTROL DESIGN ENGINEER / TECHNICIAN		

PROJECT NOTES

PROJ. REFERENCE NO. R-5002B	SHEET NO. TCP-2
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GENERAL NOTES

CHANGES MAY BE REQUIRED WHEN PHYSICAL DIMENSIONS IN THE DETAIL DRAWINGS STANDARD DETAILS AND ROADWAY DETAILS ARE NOT ATTAINABLE TO MEET FIELD CONDITIONS, OR RESULT IN DUPLICATE, OR UNDESIRE OVERLAPPING OF DEVICES. MODIFICATION MAY INCLUDE: MOVING, SUPPLEMENTING, COVERING OR REMOVAL OF DEVICES, AS DIRECTED BY THE ENGINEER.

THE FOLLOWING GENERAL NOTES APPLY AT ALL TIMES FOR THE DURATION OF THE CONSTRUCTION PROJECT, EXCEPT WHEN OTHERWISE NOTED IN THE PLAN, OR DIRECTED BY THE ENGINEER.

TIME RESTRICTIONS

A) DO NOT CLOSE OR NARROW TRAVEL LANES AS FOLLOWS:

ROAD NAME	DAY AND TIME RESTRICTIONS
NC 226	6:00 AM - 8:00 AM AND 3:00 PM - 6:00 PM MONDAY THRU FRIDAY

B) DO NOT CLOSE OR NARROW TRAVEL LANES DURING HOLIDAYS AND SPECIAL EVENTS AS FOLLOWS:

ROAD NAME
NC 226

HOLIDAY

1. FOR ANY UNEXPECTED OCCURRENCE THAT CREATES UNUSUALLY HIGH TRAFFIC VOLUMES, AS DIRECTED BY THE ENGINEER.
2. FOR NEW YEAR'S, BETWEEN THE HOURS OF 3:00 P.M. DECEMBER 31st TO 8:00 A.M. JANUARY 2ND. IF NEW YEAR'S DAY IS ON A FRIDAY, SATURDAY, SUNDAY, OR MONDAY THEN UNTIL 8:00 A.M. THE FOLLOWING TUESDAY.
3. FOR EASTER, BETWEEN THE HOURS OF 3:00 P.M. THURSDAY AND 8:00 A.M. MONDAY.
4. FOR MEMORIAL DAY, BETWEEN THE HOURS OF 3:00 P.M. FRIDAY TO 8:00 A.M. TUESDAY.
5. FOR INDEPENDENCE DAY, BETWEEN THE HOURS OF 3:00 P.M. THE DAY BEFORE INDEPENDENCE DAY AND 8:00 A.M. THE DAY AFTER INDEPENDENCE DAY.

IF INDEPENDENCE DAY IS ON A FRIDAY, SATURDAY, SUNDAY OR MONDAY THEN BETWEEN THE HOURS OF 3:00 P.M. THE THURSDAY BEFORE INDEPENDENCE DAY AND 8:00 A.M. THE TUESDAY AFTER INDEPENDENCE DAY.
6. FOR LABOR DAY, BETWEEN THE HOURS OF 3:00 P.M. FRIDAY AND 8:00 A.M. TUESDAY.
7. FOR THANKSGIVING DAY, BETWEEN THE HOURS OF 3:00 P.M. TUESDAY TO 8:00 A.M. MONDAY.
8. FOR CHRISTMAS, BETWEEN THE HOURS OF 3:00 P.M. THE FRIDAY BEFORE THE WEEK OF CHRISTMAS DAY AND 8:00 A.M. THE FOLLOWING TUESDAY AFTER THE WEEK OF CHRISTMAS.

C) DO NOT STOP TRAFFIC AS FOLLOWS:

ROAD NAME	DAY AND TIME RESTRICTIONS	DURATION AND OPERATION
NC 226	MONDAY - SUNDAY	FOR MORE THAN 15 MINUTES

LANE AND SHOULDER CLOSURE REQUIREMENTS

- D) REMOVE LANE CLOSURE DEVICES FROM THE LANE WHEN WORK IS NOT BEING PERFORMED BEHIND THE LANE CLOSURE OR WHEN A LANE CLOSURE IS NO LONGER NEEDED, OR AS DIRECTED BY THE ENGINEER.
- E) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING ON THE SHOULDER ADJACENT TO AN UNDIVIDED FACILITY AND WITHIN 5 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN TRAVEL LANE USING ROADWAY STANDARD DRAWING NO. 1101.02 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.

WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING ON THE SHOULDER ADJACENT TO A DIVIDED FACILITY AND WITHIN 10 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN TRAVEL LANE USING ROADWAY STANDARD DRAWING NO. 1101.02 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.

- F) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN A LANE OF TRAVEL OF AN UNDIVIDED OR DIVIDED FACILITY, CLOSE THE LANE ACCORDING TO THE TRAFFIC CONTROL PLANS, ROADWAY STANDARD DRAWINGS OR AS DIRECTED BY THE ENGINEER. CONDUCT THE WORK SO THAT ALL PERSONNEL AND/OR EQUIPMENT REMAIN WITHIN THE CLOSED TRAVEL LANE.
- G) DO NOT WORK SIMULTANEOUSLY WITHIN 15 FT ON BOTH SIDES OF AN OPEN TRAVELWAY RAMP OR LOOP WITHIN THE SAME LOCATION UNLESS PROTECTED WITH GUARDRAIL OR BARRIER.

PAVEMENT EDGE DROP OFF REQUIREMENTS

- H) BACKFILL AT A 6:1 SLOPE UP TO THE EDGE AND ELEVATION OF EXISTING PAVEMENT IN AREAS ADJACENT TO AN OPENED TRAVEL LANE THAT HAS AN EDGE OF PAVEMENT DROP-OFF AS FOLLOWS:

BACKFILL DROP-OFFS THAT EXCEED 2 INCHES ON ROADWAYS WITH POSTED SPEED LIMITS OF 45 MPH OR GREATER.

BACKFILL DROP-OFFS THAT EXCEED 3 INCHES ON ROADWAYS WITH POSTED SPEED LIMITS LESS THAN 45 MPH.

BACKFILL WITH SUITABLE COMPACTED MATERIAL, AS APPROVED BY THE ENGINEER, AT NO EXPENSE TO THE DEPARTMENT.
- I) DO NOT EXCEED A DIFFERENCE OF 2 INCHES IN ELEVATION BETWEEN OPEN LANES OF TRAFFIC FOR NOMINAL LIFTS OF 1.5 INCHES. INSTALL ADVANCE WARNING "UNEVEN LANES" SIGNS (W8-11) 500 FT IN ADVANCE AND A MINIMUM OF EVERY HALF MILE THROUGHOUT THE UNEVEN AREA.

TRAFFIC PATTERN ALTERATIONS

- J) NOTIFY THE ENGINEER TWENTY ONE (21) CALENDAR DAYS PRIOR TO ANY TRAFFIC PATTERN ALTERATION.
- K) INSTALL ADVANCE WORK ZONE WARNING SIGNS WHEN WORK IS WITHIN 40 FT FROM THE EDGE OF TRAVEL LANE AND NO MORE THAN THREE (3) DAYS PRIOR TO THE BEGINNING OF CONSTRUCTION.
- L) STATE FORCES WILL BE RESPONSIBLE FOR PERMANENT SIGNING.
- M) ENSURE ALL NECESSARY SIGNING IS IN PLACE PRIOR TO ALTERING ANY TRAFFIC PATTERN.
- N) INSTALL BLACK ON ORANGE "DIP" SIGNS (W8-2) AND/OR "BUMP" SIGNS (W8-1) 500 FT IN ADVANCE OF THE UNEVEN AREA, OR AS DIRECTED BY THE ENGINEER.

TRAFFIC BARRIER

- O) INSTALL TEMPORARY BARRIER ACCORDING TO THE TRAFFIC CONTROL PLANS A MAXIMUM OF TWO (2) WEEKS PRIOR TO BEGINNING WORK IN ANY LOCATION. ONCE TEMPORARY BARRIER IS INSTALLED AT ANY LOCATION, PROCEED IN A CONTINUOUS MANNER TO COMPLETE THE PROPOSED WORK IN THAT LOCATION UNLESS OTHERWISE STATED IN THE TRAFFIC CONTROL PLANS OR AS DIRECTED BY THE ENGINEER.
- DO NOT PLACE TEMPORARY BARRIER DIRECTLY ON ANY SURFACE OTHER THAN ASPHALT OR CONCRETE.
- ONCE TEMPORARY BARRIER IS INSTALLED AT ANY LOCATION AND NO WORK IS PERFORMED BEHIND THE TEMPORARY BARRIER FOR A PERIOD LONGER THAN TWO (2) MONTHS, REMOVE/RESET TEMPORARY BARRIER AT NO COST TO THE DEPARTMENT UNLESS OTHERWISE STATED IN THE TRAFFIC CONTROL PLANS, TEMPORARY BARRIER IS PROTECTING A HAZARD, OR AS DIRECTED BY THE ENGINEER.
- INSTALL TEMPORARY BARRIER WITH THE TRAFFIC FLOW, BEGINNING WITH THE UPSTREAM SIDE OF TRAFFIC. REMOVE TEMPORARY BARRIER AGAINST THE TRAFFIC FLOW, BEGINNING WITH THE DOWNSTREAM SIDE OF TRAFFIC.

PROTECT THE APPROACH END OF PORTABLE CONCRETE BARRIER AT ALL TIMES DURING THE INSTALLATION AND REMOVAL OF THE BARRIER BY EITHER A TRUCK MOUNTED IMPACT ATTENUATOR (MAXIMUM 72 HOURS) OR A TEMPORARY CRASH CUSHION.

PROTECT THE APPROACH END OF PORTABLE CONCRETE BARRIER FROM ONCOMING TRAFFIC AT ALL TIMES BY A TEMPORARY CRASH CUSHION UNLESS THE APPROACH END OF PORTABLE CONCRETE BARRIER IS OFFSET FROM ONCOMING TRAFFIC.

TRAFFIC CONTROL DEVICES

- P) SPACE CHANNELIZING DEVICES IN WORK AREAS NO GREATER THAN TWICE THE POSTED SPEED LIMIT (MPH), EXCEPT 10 FT ON-CENTER IN RADII, AND 3 FT OFF THE EDGE OF AN OPEN TRAVELWAY, WHEN LANE CLOSURES ARE NOT IN EFFECT. WHEN SKINNY DRUMS ARE ALLOWED, REFER TO SECTION 1180 OF STANDARD SPECIFICATIONS FOR ROADS AND STRUCTURES OR AS SHOWN IN THE PLANS.

- Q) PLACE TYPE III BARRICADES, WITH "ROAD CLOSED" SIGN R11-2 ATTACHED, OF SUFFICIENT LENGTH TO CLOSE ENTIRE ROADWAY.
- PAVEMENT MARKINGS AND MARKERS
- R) INSTALL PAVEMENT MARKINGS AND PAVEMENT MARKERS ON THE FINAL SURFACE AS FOLLOWS:
- | ROAD NAME | MARKING | MARKER |
|-----------|---------------|---------------------|
| NC 226 | THERMOPLASTIC | SNOWFLOWABLE RAISED |
- S) INSTALL TEMPORARY PAVEMENT MARKINGS AND TEMPORARY PAVEMENT MARKERS ON INTERIM LAYERS OF PAVEMENT AS FOLLOWS:
- | ROAD NAME | MARKING | MARKER |
|-----------|---------|------------------|
| NC 226 | PAINT | TEMPORARY RAISED |
- T) PLACE ONE APPLICATION OF PAINT FOR TEMPORARY TRAFFIC PATTERNS. PLACE A SECOND APPLICATION OF PAINT SIX (6) MONTHS AFTER THE INITIAL APPLICATION AND EVERY SIX MONTHS AS DIRECTED BY THE ENGINEER.
- U) TIE PROPOSED PAVEMENT MARKING LINES TO EXISTING PAVEMENT MARKING LINES.
- V) REMOVE/REPLACE ANY CONFLICTING/DAMAGED PAVEMENT MARKINGS AND MARKERS BY THE END OF EACH DAY'S OPERATION.

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PROJECT PHASING

PROJ. REFERENCE NO.	SHEET NO.
R-5002B	TCP-3

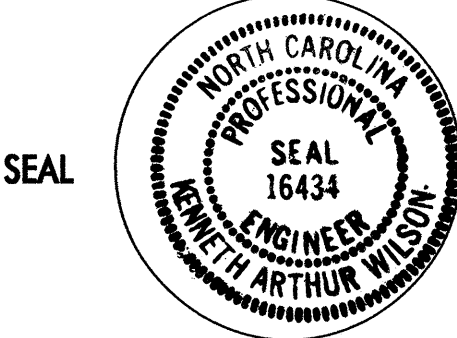

Phase I

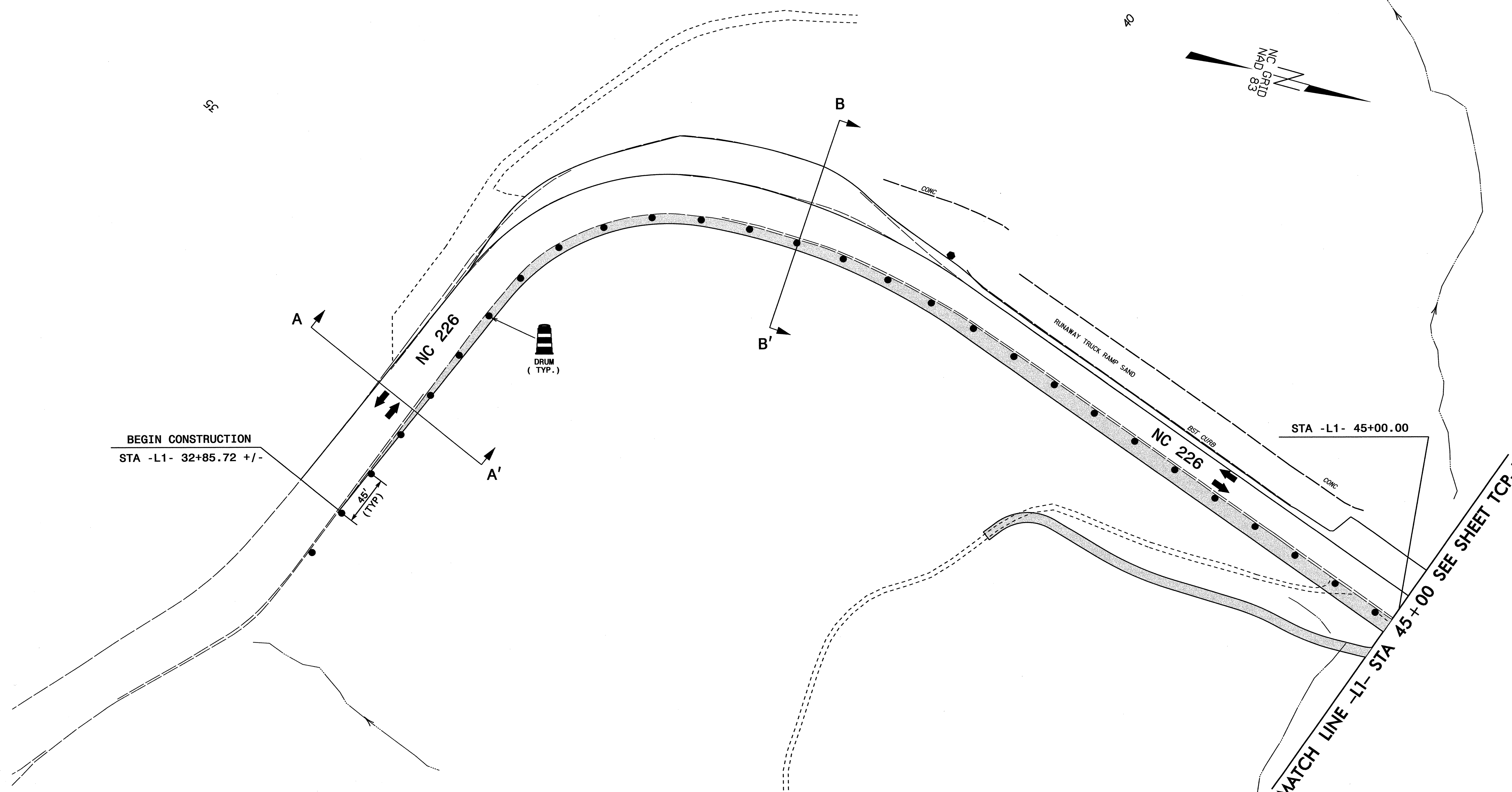
- STEP 1: - INSTALL WORKZONE SIGNS ON -L1- AS SHOWN ON SHEET TCP-8.
- STEP 2: - USING ROADWAY STANDARD DRAWING 1101.02, SHEET 1 OF 9, BEGIN INSTALLATION OF REQUIRED DRAINAGE AS MUCH AS POSSIBLE.
- STEP 3: - USING ROADWAY STANDARD DRAWING 1101.02, SHEET 1 OF 9, BEGIN WIDENING OF EXISTING NC 226 UP TO EXISTING EDGE OF PAVEMENT ELEVATION AND BUILD DRIVE AT -L1- STA 45+21.89 AT THE FOLLOWING LOCATIONS
- L1- STA. 32+85.72 TO -L1- STA. 50+80.00 (RIGHT SIDE)
- STEP 4: - USING ROADWAY STANDARD DRAWING 1101.02, SHEET 1 OF 9, PLACE INTERIM PAVEMENT MARKING LINES (PAINT) AND MARKERS (TEMPORARY RAISED) AND SHIFT NC 226 ONTO INTERIM ALIGNMENT AS SHOWN ON TCP-5
- L1- STA. 48+20.00 TO -L1- STA. 60+55.00
- STEP 5: - USING ROADWAY STANDARD DRAWING 1101.02, SHEET 1 OF 9, PLACE PORTABLE CONCRETE BARRIER FROM -L1- STA. 49+50 TO -L1- STA. 59+09, SEE TCP-5 AND TCP-5 SECTION D-D' FOR PLACEMENT. CONSTRUCT, AS MUCH AS POSSIBLE, THE LEFT AND RIGHT SIDE OF PROPOSED WIDENING UTILIZING A TEMPORARY SLOPE OF 2:1 SEE TCP-5 SECTION D-D' FROM:
- L1- STA 50+80.00 TO -L1- STA. 58+75.09 (BOTH SIDES)
- STEP 6: - BEHIND PORTABLE CONCRETE BARRIER, COMPLETE SHOULDER WORK AS POSSIBLE AND INSTALL PROPOSED GUARDRAIL FROM -L1- STA. +/-49+53.04 TO -L1- STA. 58+81.28. UPON COMPLETION OF GUARDRAIL, REMOVE PORTABLE CONCRETE BARRIER AND REPLACE WITH DRUMS, SEE TCP-5 SECTION D-D'.
- STEP 7: - USING ROADWAY STANDARD DRAWING 1101.02, SHEET 1 OF 9, PLACE INTERIM PAVEMENT MARKING LINES (PAINT) AND MARKERS (TEMPORARY RAISED) AND PLACE TRAFFIC BACK IN ORIGINAL TRAFFIC PATTERN FROM:
- L1- STA. 48+20.00 TO -L1- STA. 60+55.00
- STEP 8: - USING ROADWAY STANDARD DRAWING 1101.02, SHEET 1 OF 9, FINISH INSTALLATION OF REQUIRED DRAINAGE.

Phase II

- STEP 1: - USING ROADWAY STANDARD DRAWING 1101.02, SHEET 1 OF 9, PAVE/WEDGE EXISTING NC 226 AND FINISH PAVING WIDENING ON RIGHT SIDE UP TO BUT NOT INCLUDING FINAL LAYER OF SURFACE COURSE, COMPLETE SHOULDER WORK AS POSSIBLE AND AS POSSIBLE PLACE TEMPORARY PAVEMENT MARKINGS ACCORDING TO THE FINAL PAVEMENT MARKING PLAN FROM -L1- STA. 32+85.72 TO -L1- STA. 35+50.00. (SEE SHEETS PM-02 AND PM-03)
- STEP 2: - USING ROADWAY STANDARD DRAWING 1101.02, SHEET 1 OF 9, PAVE/WEDGE EXISTING NC 226, BRAKE COOLING AREA AND FINISH PAVING WIDENING ON RIGHT SIDE UP TO BUT NOT INCLUDING FINAL LAYER OF SURFACE COURSE, COMPLETE SHOULDER WORK AS POSSIBLE AND AS POSSIBLE PLACE TEMPORARY PAVEMENT MARKINGS ACCORDING TO THE FINAL PAVEMENT MARKING PLAN FROM -L1- STA 35+50.00 TO -L1- STA. 40+00.00. (SEE SHEETS PM-02 AND PM-03)
- STEP 3: - USING ROADWAY STANDARD DRAWING 1101.02, SHEET 1 OF 9, PAVE/WEDGE EXISTING NC 226 AND FINISH PAVING WIDENING ON RIGHT SIDE UP TO BUT NOT INCLUDING FINAL LAYER OF SURFACE COURSE, COMPLETE SHOULDER WORK AS POSSIBLE AND AS POSSIBLE PLACE TEMPORARY PAVEMENT MARKINGS ACCORDING TO THE FINAL PAVEMENT MARKING PLAN FROM -L1- STA 40+00.00 TO -L1- STA 44+09. (SEE SHEETS PM-02 AND PM-03)
- STEP 4: - USING ROADWAY STANDARD DRAWING 1101.02, SHEET 1 OF 9, PAVE/WEDGE EXISTING NC 226, FINISH PAVING WIDENING ON RIGHT SIDE AND PAVE RUNAWAY TRUCK RAMP AREA UP TO BUT NOT INCLUDING FINAL LAYER OF SURFACE COURSE, COMPLETE SHOULDER WORK AS POSSIBLE. MOVE EXISTING BARRIER AS NEEDED AND EXTEND PROPOSED GUARDRAIL AS NEEDED TO COVER BARRIER REMOVAL. PLACE TEMPORARY PAVEMENT MARKINGS ACCORDING TO THE FINAL PAVEMENT MARKING PLAN FROM -L1- STA 44+09 TO -L1- STA 50+50.00. (SEE SHEETS PM-02 AND PM-03)
- STEP 5: - USING ROADWAY STANDARD DRAWING 1101.02, SHEET 1 OF 9, PAVE/WEDGE EXISTING NC 226 AND FINISH PAVING WIDENING ON RIGHT SIDE UP TO BUT NOT INCLUDING FINAL LAYER OF SURFACE COURSE, COMPLETE SHOULDER WORK AS POSSIBLE AND AS POSSIBLE PLACE TEMPORARY PAVEMENT MARKINGS ACCORDING TO THE FINAL PAVEMENT MARKING PLAN FROM -L1- STA 50+50.00 TO -L1- STA 58+75.09 (SEE SHEETS PM-02 AND PM-03)
- STEP 6: - USING ROADWAY STANDARD DRAWING 1101.02, SHEET 1 OF 9, PLACE FINAL LAYER OF SURFACE COURSE, FINAL PAVEMENT MARKINGS (THERMOPLASTIC) AND MARKERS (SNOWPLOWABLE RAISED). REMOVE ANY REMAINING TRAFFIC CONTROL DEVICES.

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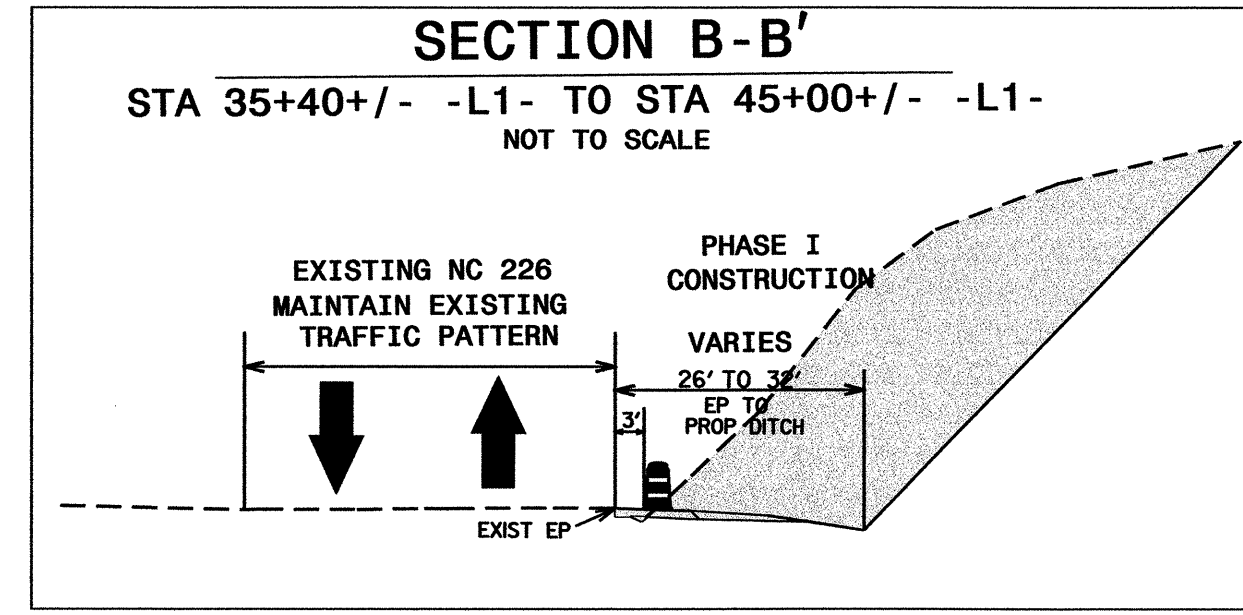
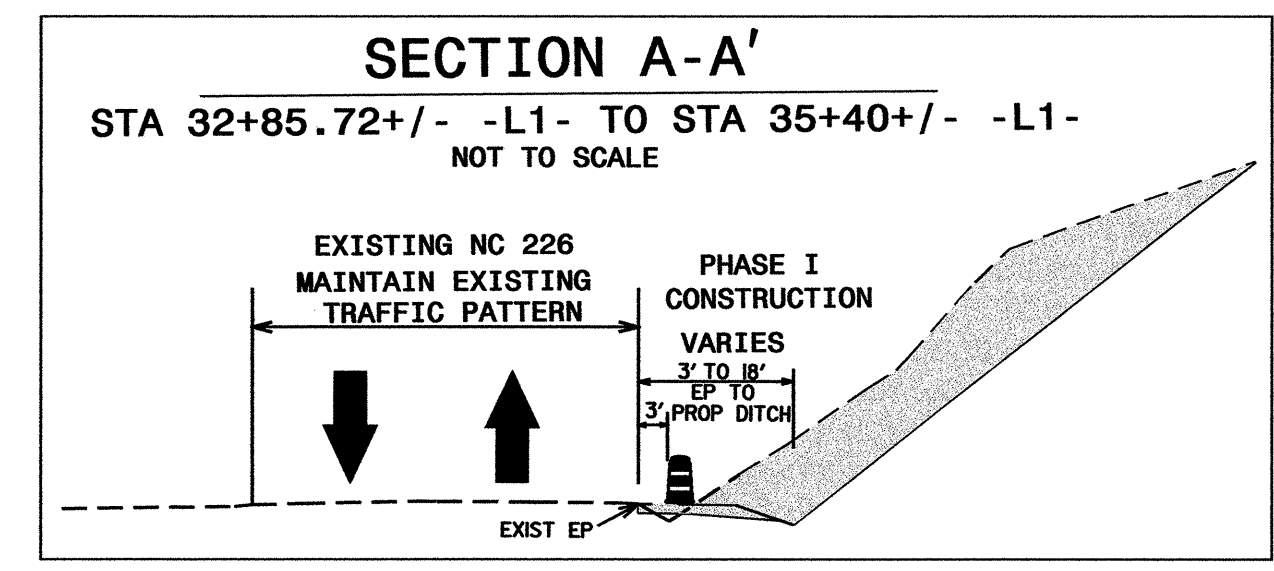
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BEGIN CONSTRUCTION
STA -L1- 32+85.72 +/-

STA -L1- 45+00.00

MATCH LINE -L1- STA 45+00 SEE SHEET TCP-5



APPROVED: *James G. Wilson* DATE: 6-5-09

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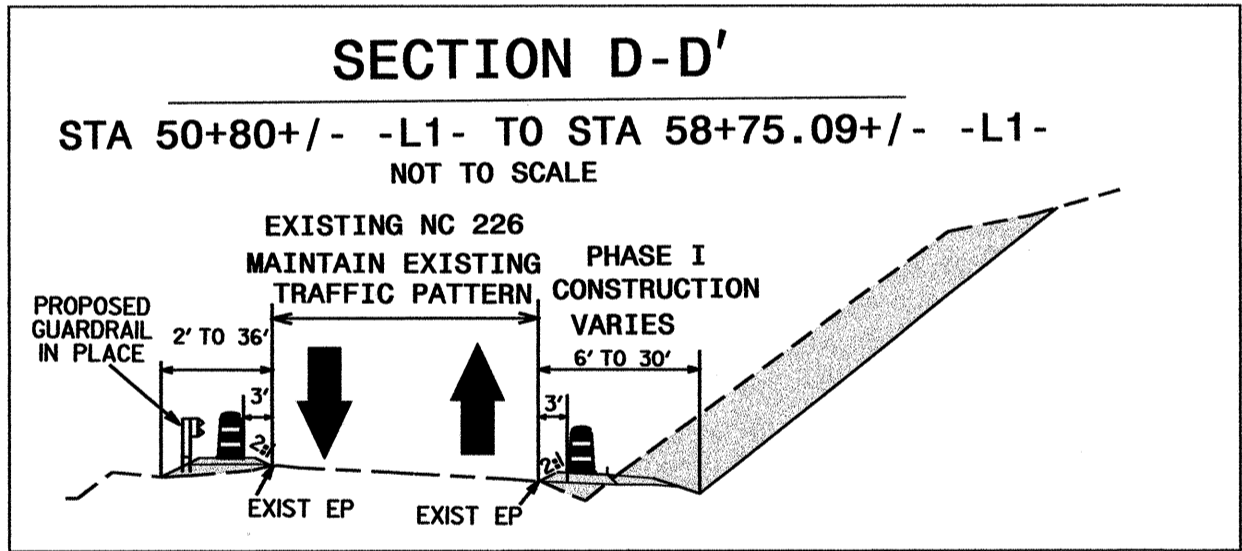
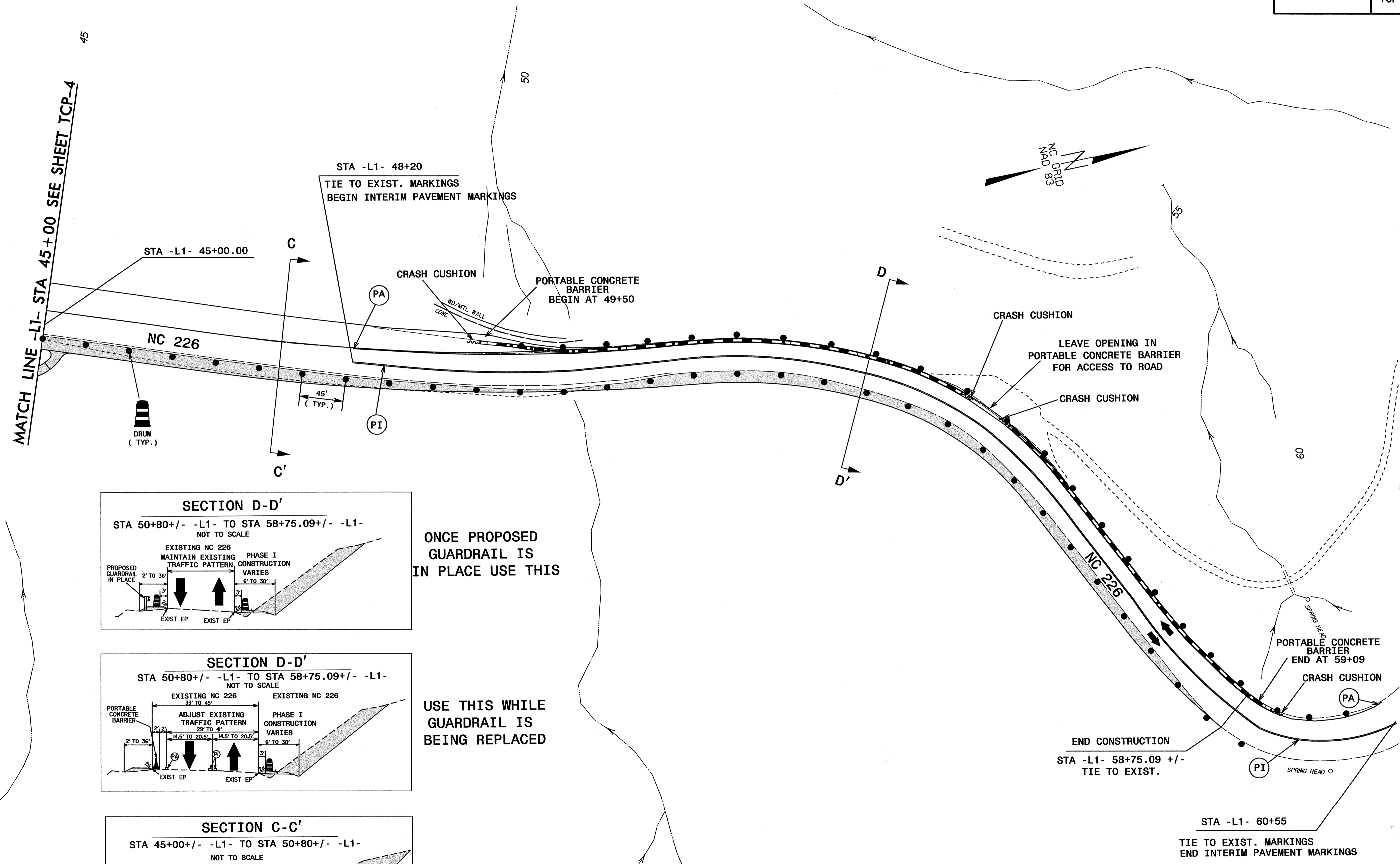
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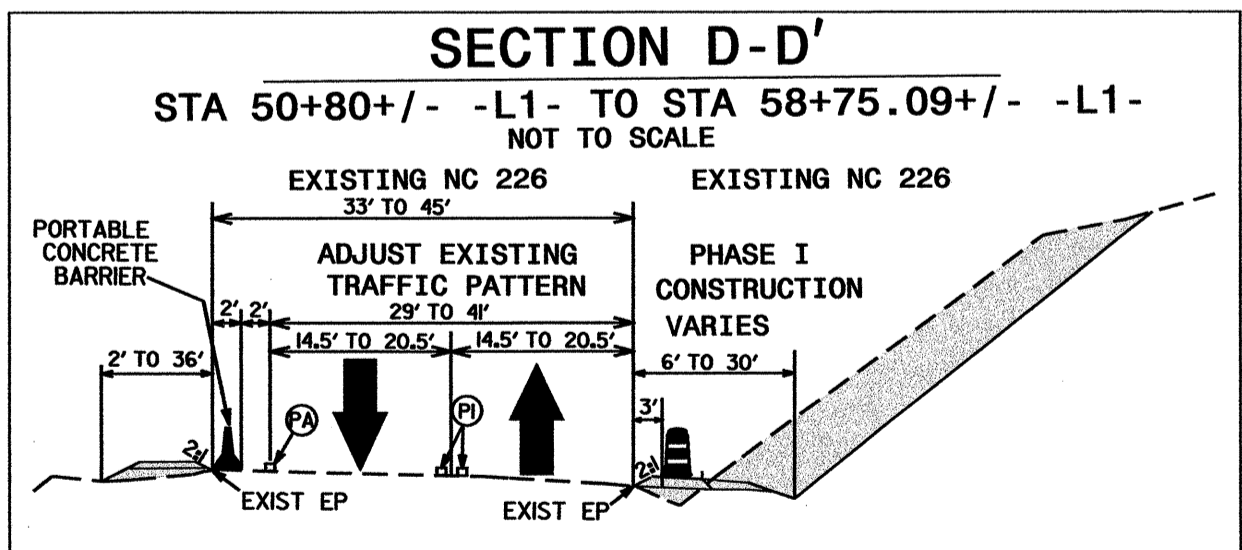
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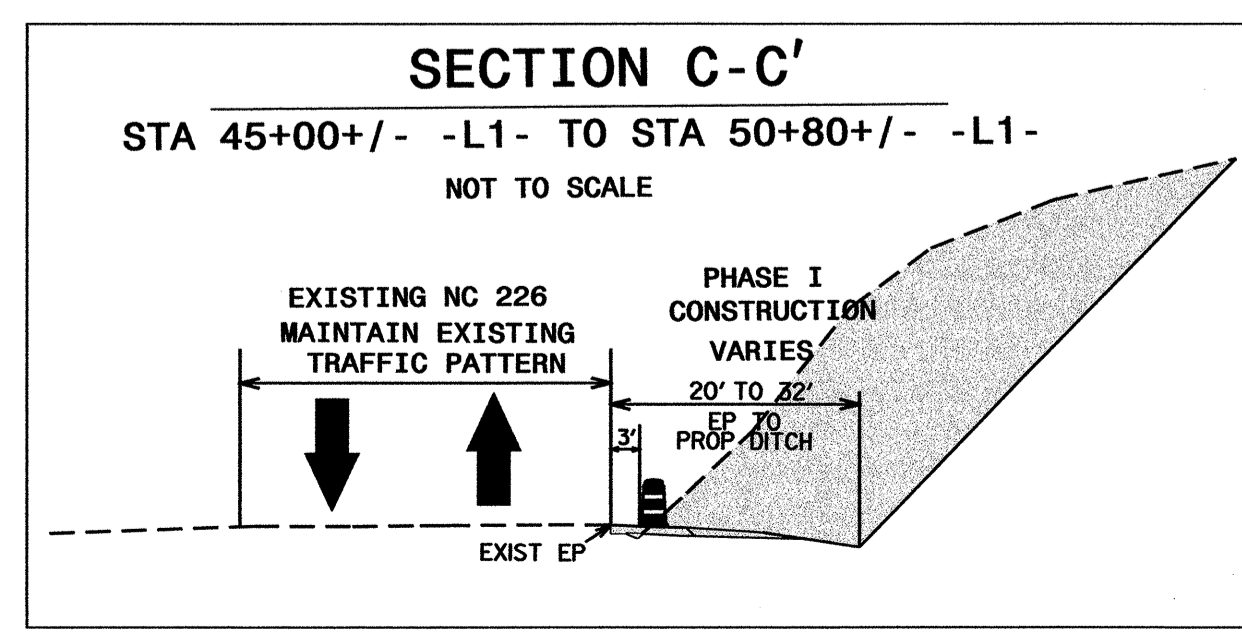
MATCH LINE -L1- STA 45+00 SEE SHEET TCP-4



ONCE PROPOSED GUARDRAIL IS IN PLACE USE THIS



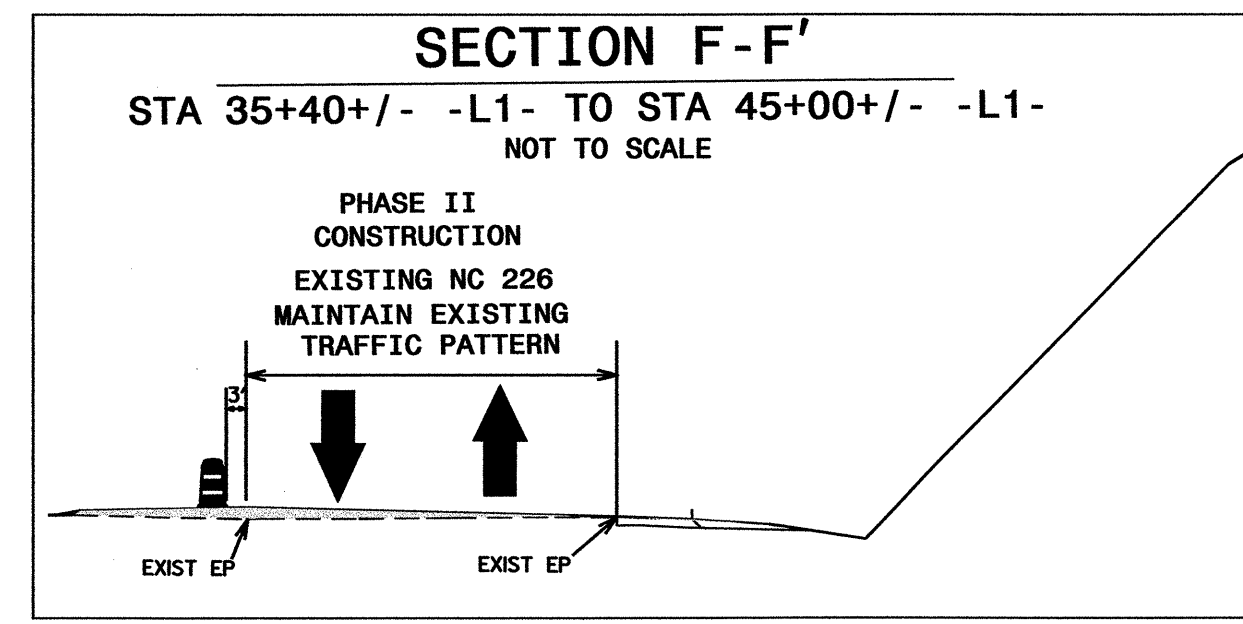
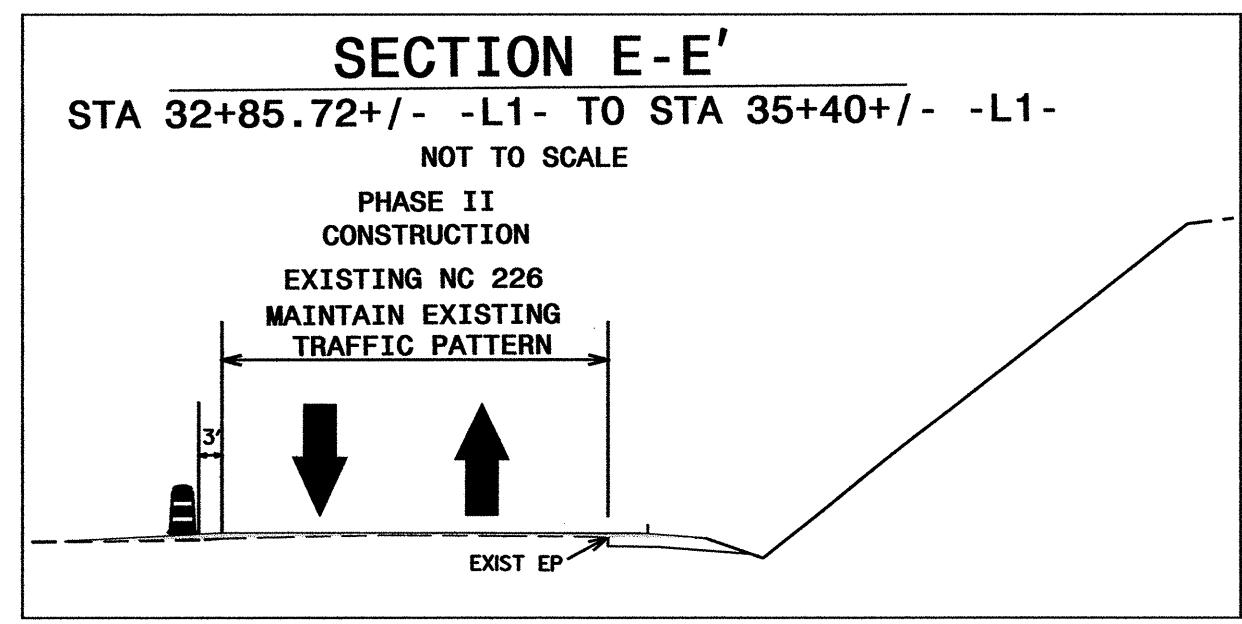
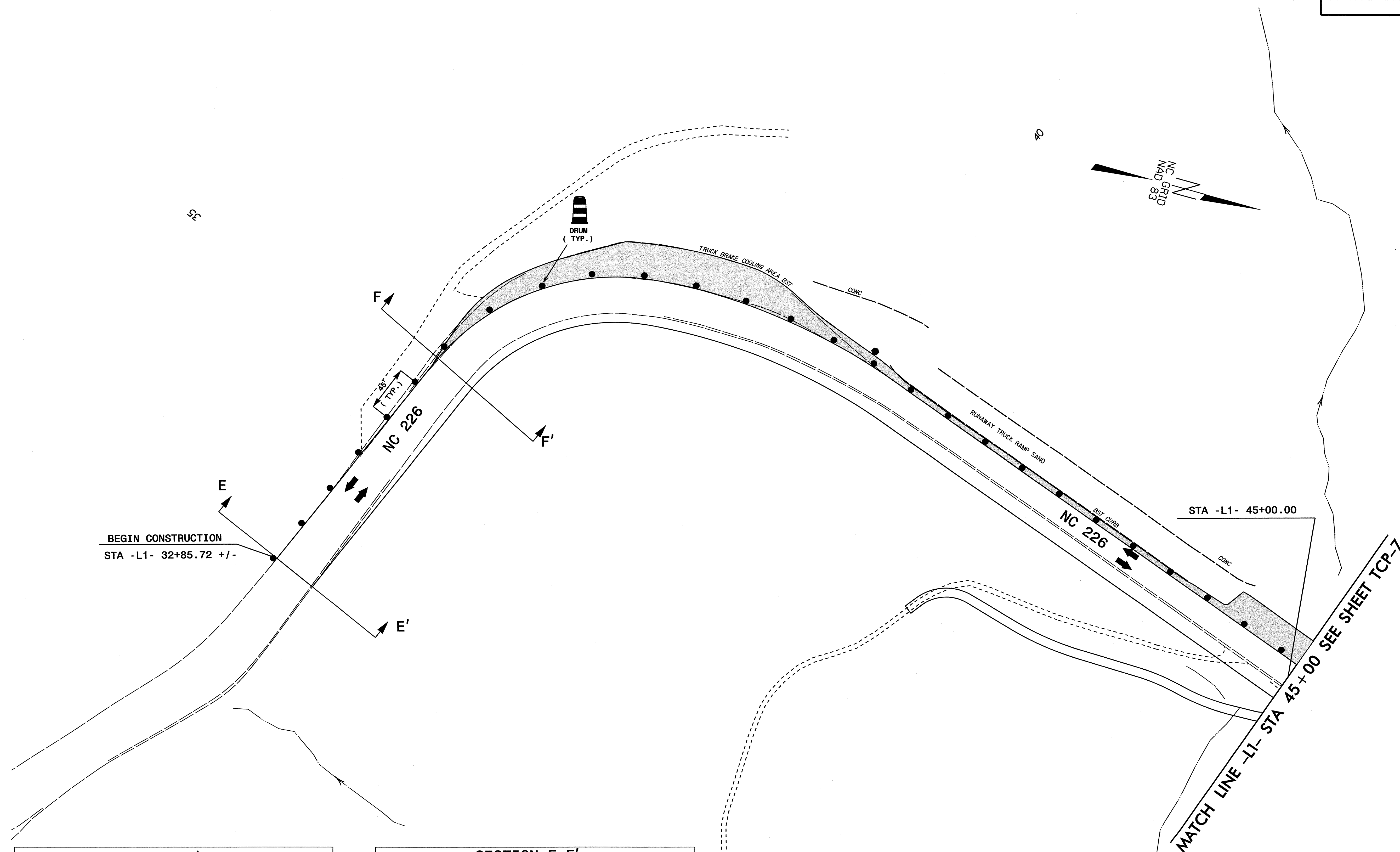
USE THIS WHILE GUARDRAIL IS BEING REPLACED



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DESIGN BY:	MKP			
REVIEWED BY:	KAW			

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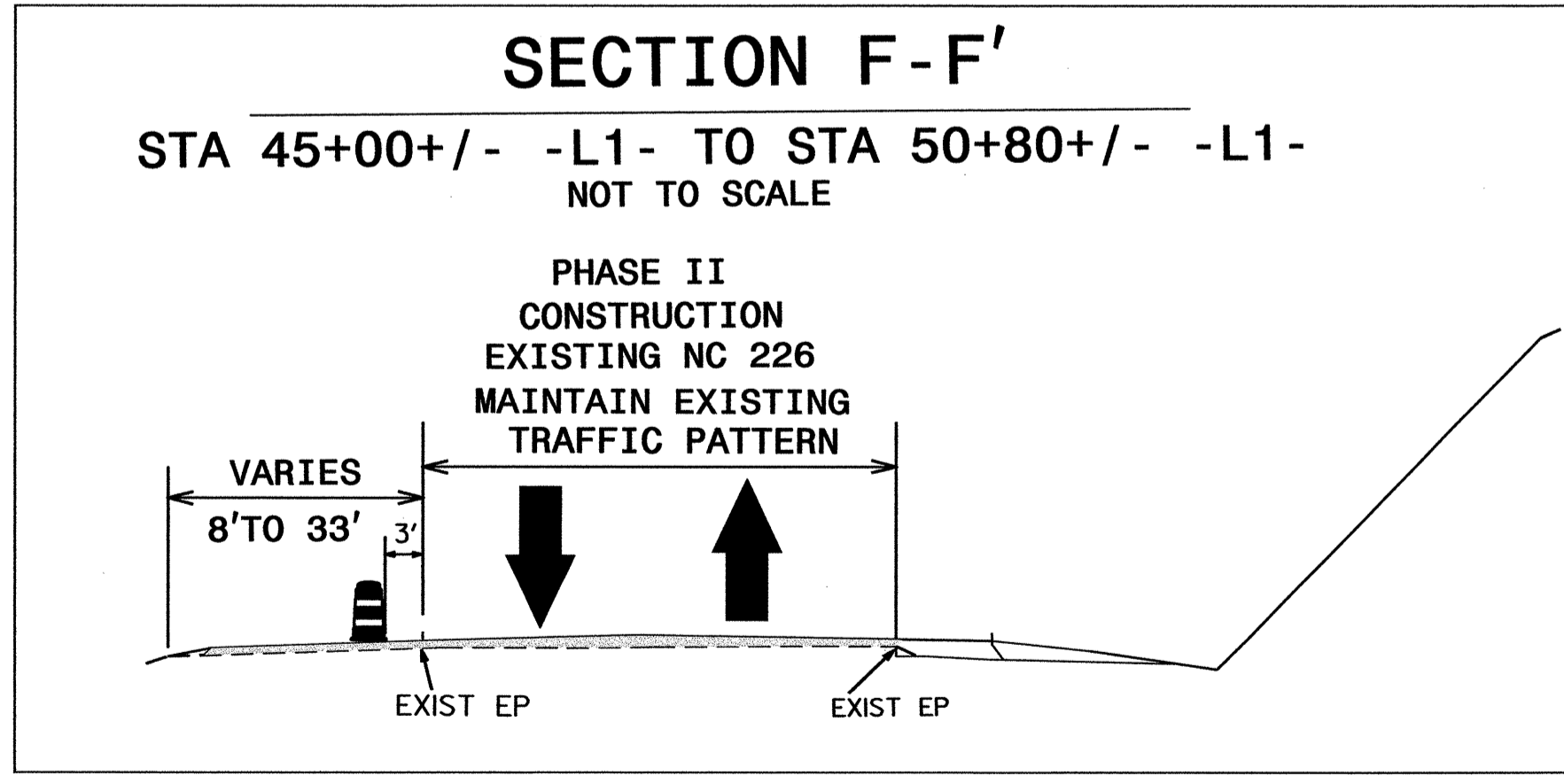
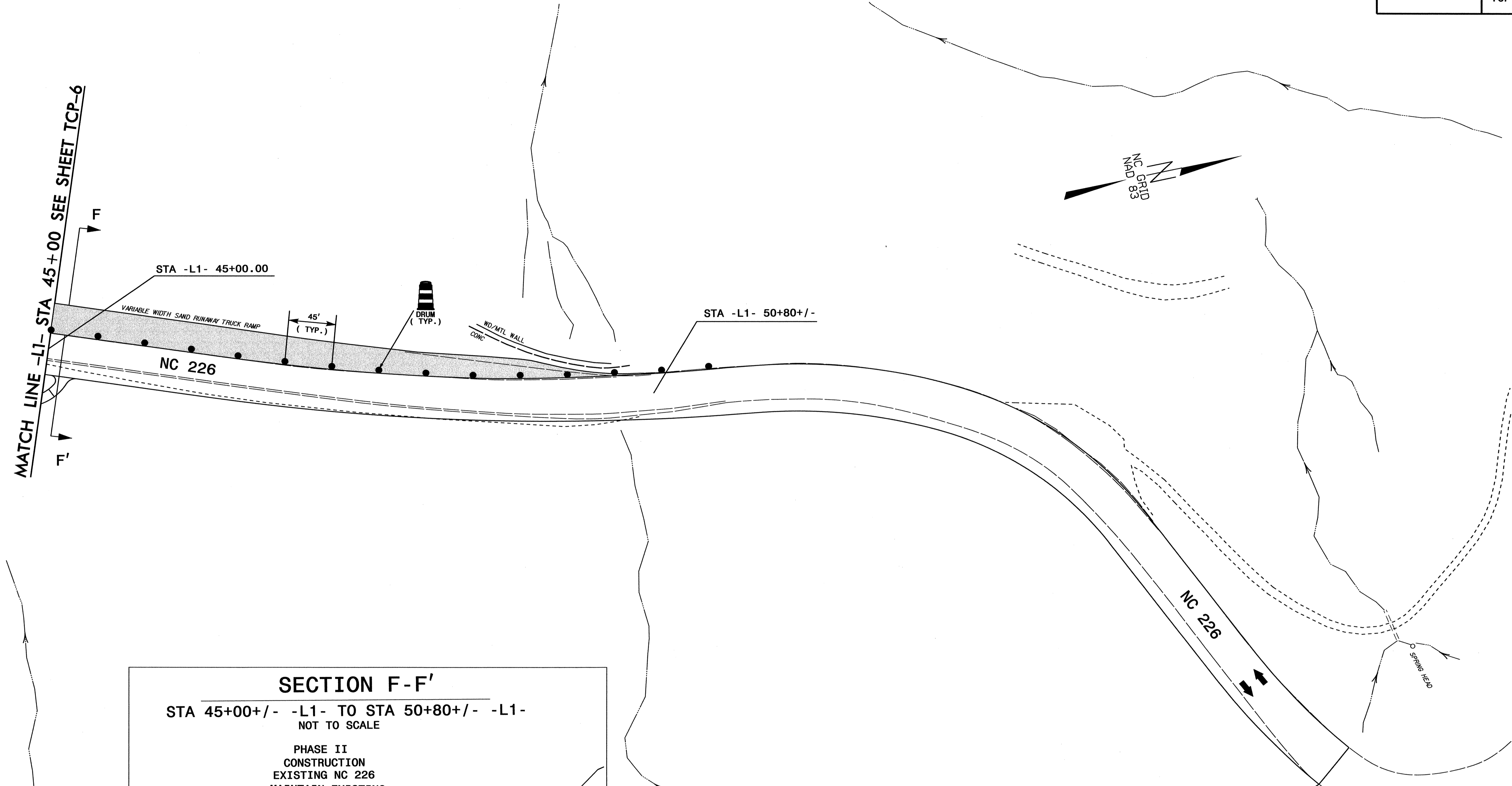
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MATCH LINE -L1- STA 45+00 SEE SHEET TCP-6

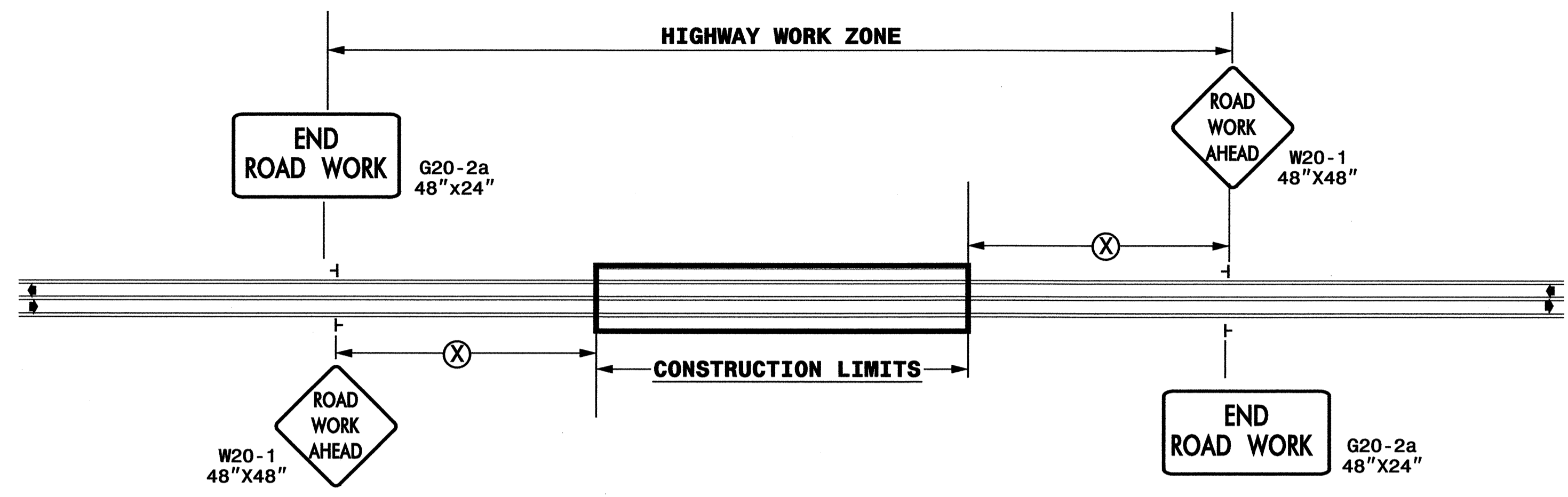


END CONSTRUCTION
 STA -L1- 58+75.09 +/-
 TIE TO EXIST.

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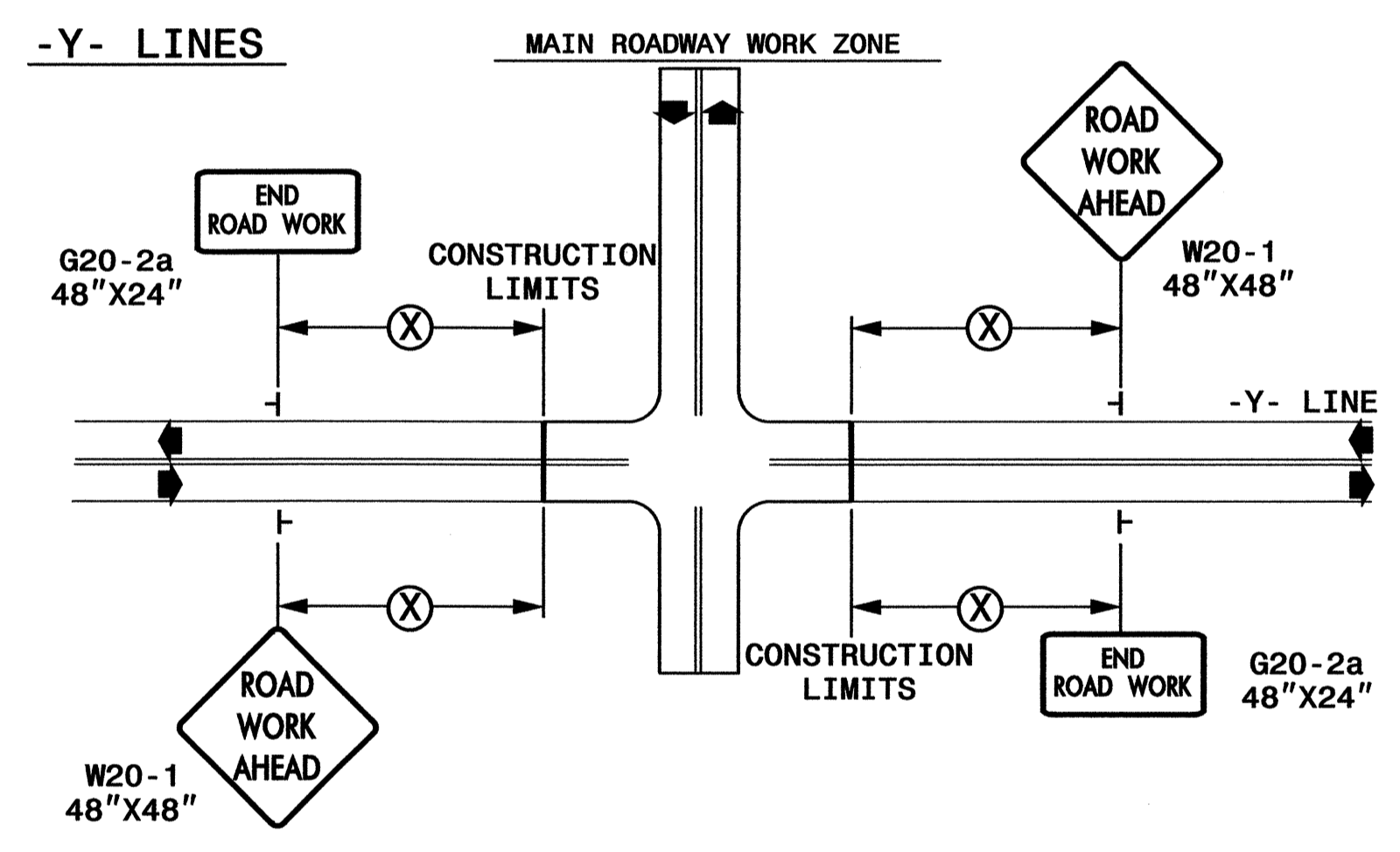
TWO-WAY UNDIVIDED ** (L-LINES)



POSTED SPEED LIMIT (M.P.H.)	RECOMMENDED MINIMUM SIGN SPACING
≤ 50	500'
≥ 55	1000'

STATE OF NORTH CAROLINA
DEPT. OF TRANSPORTATION
DIVISION OF HIGHWAYS
RALEIGH, N.C.

ROADWAYS INTERSECTING ALONG 2 WAY UNDIVIDED WORK ZONE (Y-LINES)



**DETAIL DRAWING FOR
TWO-WAY UNDIVIDED
WORK ZONE WARNING SIGNS**

GENERAL NOTES

- USE FLUORESCENT ORANGE SHEETING (TYPE VII OR HIGHER) ON ALL ADVANCED WORK ZONE SIGNS.
- DO NOT INSTALL ADVANCE WARNING SIGNS MORE THAN 3 DAYS PRIOR TO BEGINNING OF WORK.
- SIGNS SHOWN ARE REQUIRED FOR WORK ZONES THAT WILL REMAIN IN EFFECT OVERNIGHT. FOR SHORT-TERM DAILY MAINTENANCE TYPE OPERATIONS, THIS SIGNING APPLICATION IS OPTIONAL; MAY USE ONLY APPLICABLE ROADWAY STANDARD DRAWINGS INSTEAD. HOWEVER, IF THIS SIGNING APPLICATION IS USED, SIGNS MAY BE PORTABLE MOUNTED.
- ALL SIGN SPACING DIMENSIONS ARE APPROXIMATE, FIELD ADJUST AS NECESSARY OR AS DIRECTED.
- USE 3LB STEEL U-CHANNEL POST OR 4" X 4" WOOD POST FOR ALL WORK ZONE SIGNS. 3LB STEEL U-CHANNEL POSTS MUST MEET THE REQUIREMENTS OF STANDARD SPECIFICATION SECTION 1094-1(B), MAY BE GALVANIZED STEEL, OR MAY BE PAINTED GREEN BY THE POST MANUFACTURER. SQUARE STEEL TUBING POSTS HAVING EQUIVALENT STRENGTH OF THE 3 LB STEEL U-CHANNEL POST ARE ALSO ACCEPTABLE FOR USE. ERECT SIGNS PER ROADWAY STANDARD DRAWING 1110.01. PAYMENT FOR WOOD POSTS, 3LB STEEL U-CHANNEL AND SQUARE STEEL TUBING POSTS WITH SIGNS WILL BE MADE ACCORDING TO STANDARD SPECIFICATION "WORK ZONE SIGNS" SECTION 1110.
- WHEN NECESSARY, USE SPLICING IN ACCORDANCE WITH ROADWAY STANDARD DRAWING NO. 1110.01. REMOVE ENTIRE POST WHEN REMOVING SIGNS WITH SPLICED POSTS.
- DO NOT BACK BRACE SIGN SUPPORTS.
- ** TWO-WAY UNDIVIDED ADVANCE WARNING SIGN CONFIGURATION MAY BE USED ON URBAN MULTI-LANE FACILITIES WHERE CONDITIONS LIMIT THE USE OF DUAL MOUNTED SIGNS AS DETERMINED BY THE ENGINEER.

LEGEND

┌ STATIONARY SIGN

◀ DIRECTION OF TRAFFIC FLOW

SHEET 1 OF 1

APPROVED: <i>[Signature]</i> DATE: 6-5-09 	DETAIL DRAWING FOR TWO-WAY UNDIVIDED AND URBAN FREEWAYS ADVANCED WORK ZONE WARNING SIGNS	
	SCALE: NONE DATE: DWG. BY: DESIGN BY: REVIEWED BY:	

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