7/2/99

PROJECT REFERENCE NO.	SHEET NO.
I-5132	1

MAP NO.2
WEST
BOUND LANES

MAP NO.1
EAST
BOUND LANES

Butt Joint across roadway to tie to existing Pavement Joint(See Sheet 4)

End Project Limits at Existing Pavement Joint East and West bound lanes

Match Line See Sheet No.2

CITY LIMIT

Note:
1. Mill Bridge all Approaches and Departures
2. Mill under Bridges maintaining existing clearances

I-40,
Jonestown Road to Harper Road
Winston-Salem, Forsyth County
East and West Bound
Pavement Rehabilitation

Forsyth County

WBS# 45087.3.STI

7/2/99

PROJECT REFERENCE NO.	SHEET NO.
I-5132	2

Match Line See Sheet No.1

Pave up and down ramps from I-40 to stop bars

MAP NO. 1
EAST
BOUND LANES

MAP NO. 2
WEST
BOUND LANES

Mill off existing Friction course create Butt Joint (See Sheet 4)

End Project Limits at Existing Pavement Joint East and West bound lanes

CITY LIMIT

CLEMMONS
POP. 16,350

Note:
1. Mill Bridge all Approaches and Departures
2. Mill under Bridges maintaining existing clearances

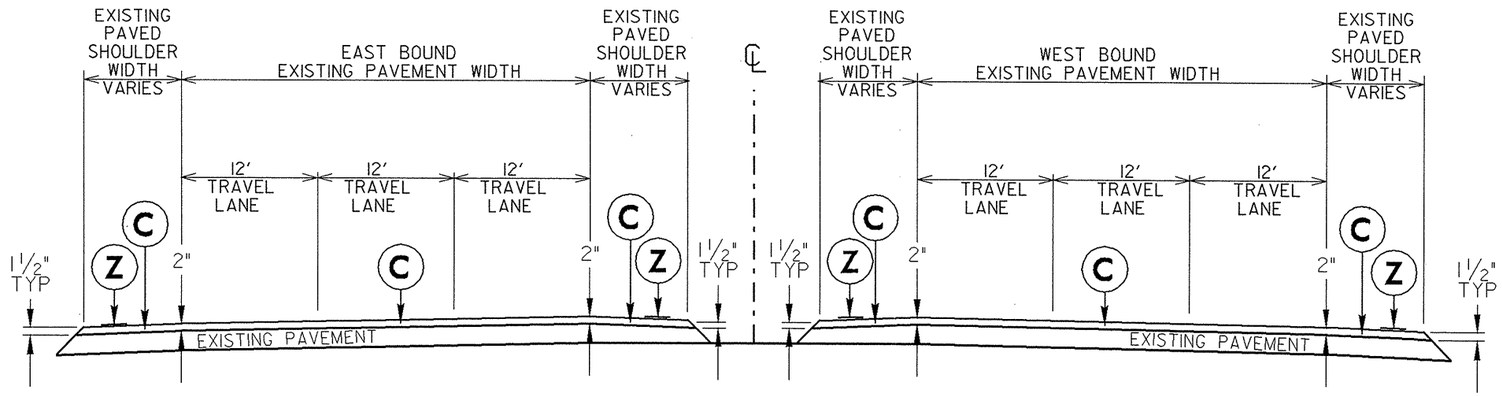
I-40,
Jonestown Road to Harper Road
Winston-Salem, Forsyth County
East and West Bound
Pavement Rehabilitation

Forsyth County

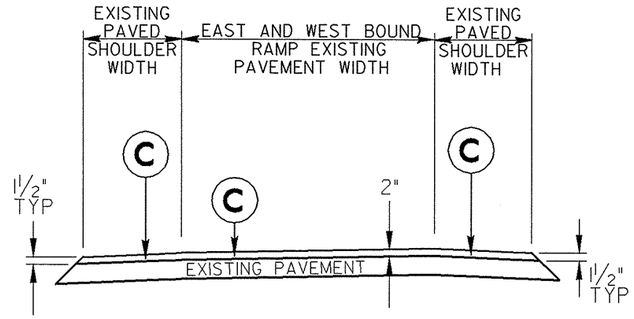
WBS# 45087.3.STI

4/3/09

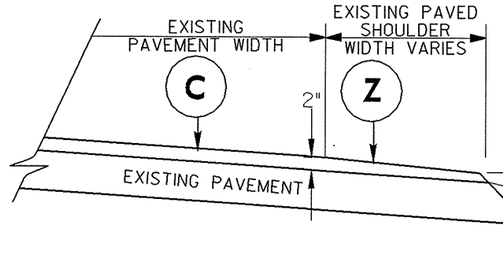
PROJECT REFERENCE NO.	SHEET NO.
I-5132	3



TYPICAL SECTION NO.1

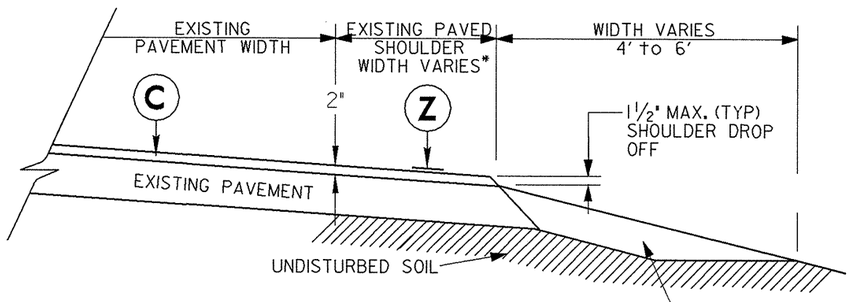


TYPICAL RAMP SECTION



NOTE:
CONTRACTOR TO TAPER SURFACE COURSE TO A THICKNESS OF 1 1/2" AT EXISTING EDGE OF PAVEMENT (TYP)

PAVED SHOULDER DETAIL



SHOULDER RECONSTRUCTION AT LOCATIONS WITH NO GUARDRAIL

*NOTE: MEDIAN & OUTSIDE PAVED SHOULDERS

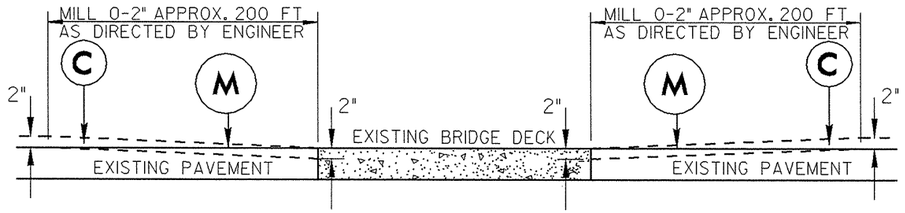
FILL MATERIAL FOR SHOULDER RECONSTRUCTION AT LOCATIONS AS DIRECTED BY THE ENGINEER. MATERIAL PROVIDED BY CONTRACTOR (SEE CONTRACT)

PAVEMENT SCHEDULE

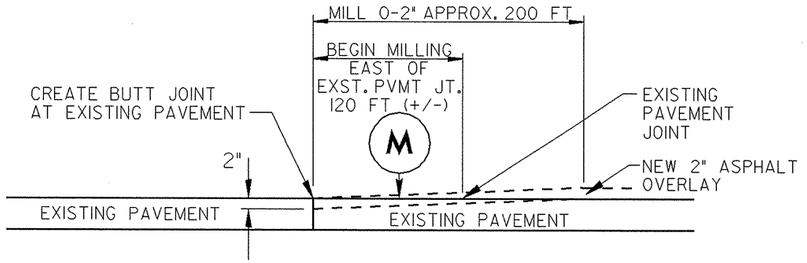
C	PROP. APPROX. 2" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 224 LBS. PER SQ. YD.
M	VARIABLE DEPTH MILLING, 0" TO 2" DEPTH.
M1	MILLING, 2" DEPTH.
Z	MILLED RUMBLE STRIPS

4/3/09

PROJECT REFERENCE NO.	SHEET NO.
I-5132	4

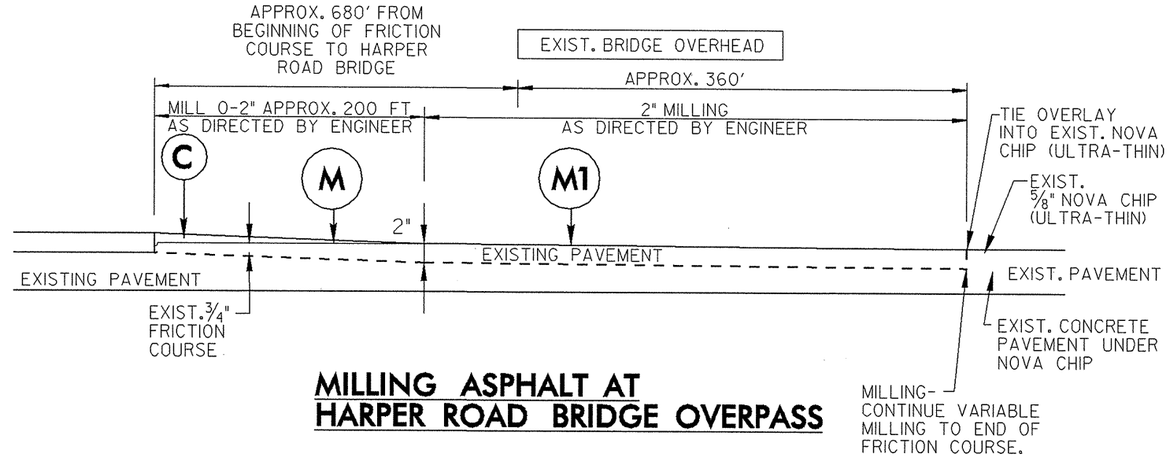


MILLING AT BRIDGE APPROACH

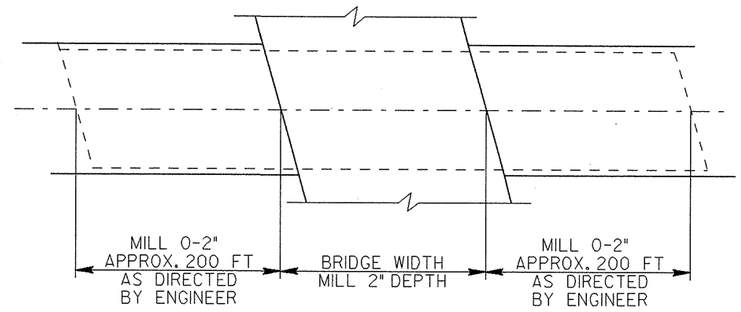


MILLING AT BEGIN AND END OF PROJECT EAST AND WEST BOUND

PAVEMENT SCHEDULE	
C	PROP. APPROX. 2" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 224 LBS. PER SQ. YD.
M	VARIABLE DEPTH MILLING, 0" TO 2" DEPTH.
M1	MILLING, 2" DEPTH.
Z	MILLED RUMBLE STRIPS



MILLING ASPHALT AT HARPER ROAD BRIDGE OVERPASS

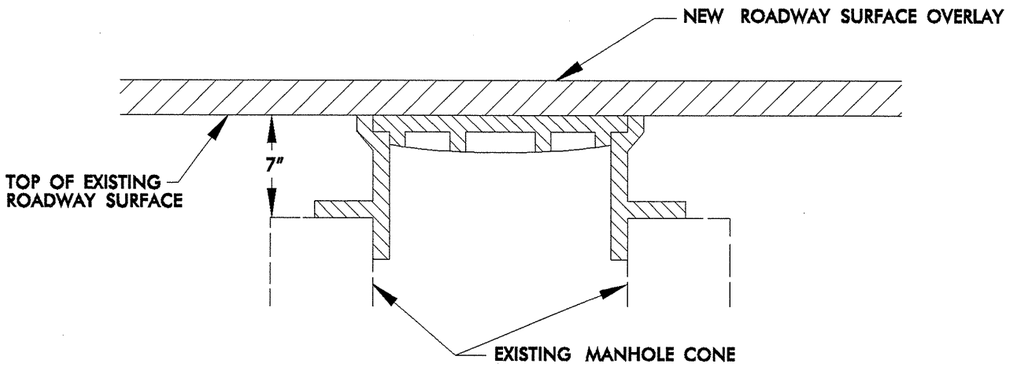


MILL ASPHALT
(FULL WIDTH OF PAVEMENT)
TO MAINTAIN EXISTING BRIDGE
CLEARANCE

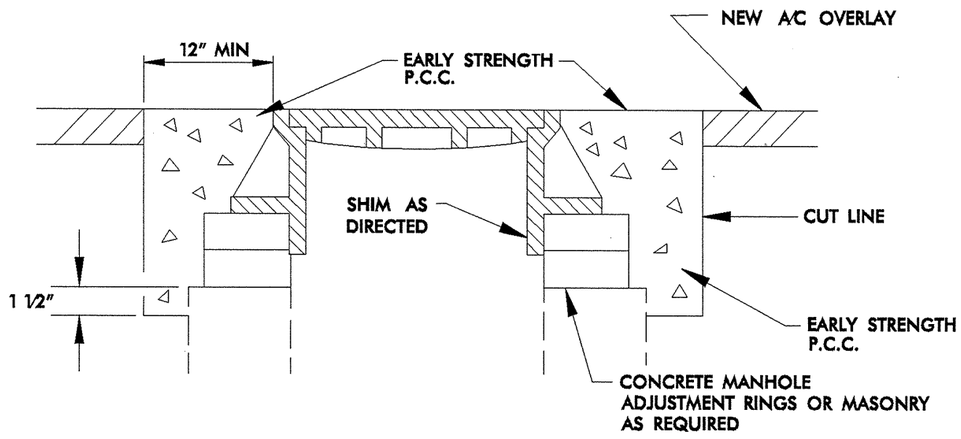
MILLING ASPHALT PAVEMENT AT BRIDGE OVERPASS TO MAINTAIN VERTICAL BRIDGE CLEARANCE

7/2/99

PROJECT REFERENCE NO.	SHEET NO.
I-5132	5



STEP 1



STEPS 2,3, & 4

STEP 1 COVER EXISTING MANHOLE WITH APPROVED MATERIAL AND CONSTRUCT OVERLAY ACROSS TOP OF MANHOLE

STEP 2 SAW CUT EXCAVATION AROUND MANHOLE 12" MIN. FROM MANHOLE FRAME.

STEP 3 RAISE MANHOLE FRAME RINGS TO FINISH PAVEMENT PROFILE AND CROSS SLOPE.

STEP 4 BACKFILL WITH EARLY STRENGTH P.C.C. TO DEPTHS AS DIRECTED.

MANHOLE ADJUSTMENT DETAIL

CONSTRUCTION NOTES:

1. ALL QUANTITIES ARE "ESTIMATED" AS INDICATED IN THE "SUMMARY OF QUANTITIES".

2. CONSTRUCTION SHALL PROGRESS IN PHASES, IN THE ORDER INDICATED BELOW:

   PHASE 1 – PATCHING (WHEN REQUIRED)
   PHASE 2 – LEVELING (AS DIRECTED BY ENGINEER)
   PHASE 3 – SURFACE OVERLAY
   PHASE 4 – SHOULDER DROP–OFF REPAIR (AS NEEDED AND DIRECTED BY ENGINEER)
   PHASE 5 – UTILITY ADJUSTMENTS (MANHOLE RING/COVER, VALVE/METER BOX RING/COVER, CATCH BASIN GRATE/COVER, DROP INLET GRATE/COVER, ETC.) WHEN REQUIRED.

3. BRIDGES THAT HAVE FLOOR DRAINS, SHALL HAVE ALL FLOOR DRAINS LEFT OPEN. EXTRA CARE SHALL BE EXERCISED IN MILLING (IF REQUIRED) AND IN PLACING THE WEARING SURFACE AROUND FLOOR DRAINS SO AS NOT TO HINDER EFFECTIVE DRAINAGE.

4. TEMPORARY ASPHALT WEDGING SHALL BE PLACED ON THE SAME DAY THAT BRIDGE AND/OR RAILROAD APPROACHES ARE MILLED (AND IF APPROACHES ARE MILLED PRIOR TO BRIDGE DECK).

5. SOME MAPS MAY REQUIRE EXTRA ASPHALT SURFACE (LEVELING) TO BE PLACED TO ELIMINATE UNEVEN PAVEMENT, WASH BOARDING OR TO RE–ESTABLISH THE CROWN. THE QUANTITY AND LOCATION OF THIS ITEM SHALL BE AS DIRECTED BY THE ENGINEER.

6. PAPER JOINTS ARE TO BE PLACED BETWEEN DAY'S OF PAVING OPERATIONS AS SPECIFIED IN THE STANDARD SPECIFICATIONS SECTION 610–11.

7. ALL MILLED AREAS WILL BE PAVED DURING THE SAME DAYS OPERATION UNLESS APPROVED BY THE ENGINEER.

8. REPLACE ANY PORTION OF STOP BARS AND OTHER PAVEMENT MARKINGS AT ANY INTERSECTION INCLUDING Y–LINES NOT ACTUALLY BEING PAVED OVER, THAT ARE OBLITERATED BY THE PAVING OPERATION EITHER BY HAULING WHEEL TRACKS OR TACK TRUCK BY THE END OF EACH RESURFACING OPERATION.

PROJECT REFERENCE NO.	SHEET NO.
I-5132	6

Forsyth County I-40 Resurfacing Bridge Listing

Map No.	Route No.	Route Name	Bridge No.	Feature Intersected	Floor Construction	Clear Roadway Width (Ft)	Horizontal Clearance Under (Ft)	Vertical Clearance Under	Length (Ft)	Posting	Recommended Treatment, From Bridge Maintenance
1,2	I-40 WBL/EBL	I-40	193	JONESTOWN ROAD	N/A	N/A	N/A	17Ft 2 In WBL 16Ft 06 In EBL	N/A	N/A	Mill 2" under and 200' Each Side of bridge
1,2	I-40 WBL/EBL	I-40	183	McGREGOR ROAD	N/A	N/A	N/A	18Ft 07 In WBL 16Ft 10 In EBL	N/A	N/A	Mill 2" under and 200' Each Side of bridge
1	I-40 WBL	I-40	162	MUDDY CREEK	5.25 RC 3.5 PPC	76	N/A	N/A	N/A	N/A	Do NOT Pave Mill Approaches 200' Each Side
2	I-40 EBL	I-40	161	MUDDY CREEK	5.25 RC 3.5 PPC	76	N/A	N/A	N/A	N/A	Do NOT Pave Mill Approaches 200' Each Side
1,2	I-40 WBL/EBL	I-40	135	KINNAMON ROAD	N/A	N/A	N/A	16Ft 02 In WBL 18Ft 00 In EBL	N/A	N/A	Mill 2" under and 200' Each Side of bridge
1,2	I-40 WBL/EBL	I-40	104	LEWISVILLE-CLEMMONS ROAD	N/A	N/A	N/A	16Ft 06 In WBL 16Ft 06 In EBL	N/A	N/A	Mill 2" under and 200' Each Side of bridge
1,2	I-40 WBL/EBL	I-40	50	HARPER ROAD	N/A	N/A	N/A	17Ft 00 In WBL 17Ft 08 In EBL	N/A	N/A	Mill 2" under and 200' Each Side of bridge

PROJECT NO.	SHEET NO.	TOTAL NO.
I-5132	7	8
45087.3.ST1		

SUMMARY OF QUANTITIES

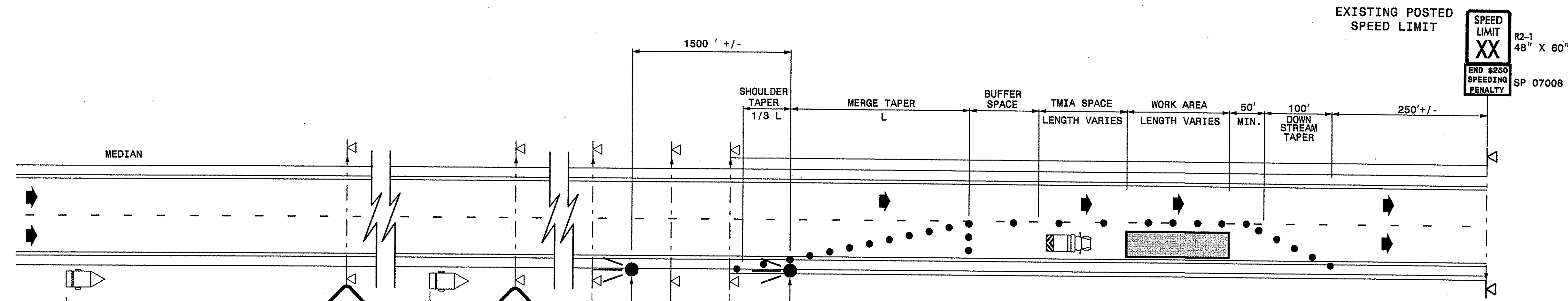
PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LENGTH MI	FINAL SURFACE TESTING	WIDTH FT	SHOULDER RECONSTRUCT. SMI	2" MILLING SY	VARIABLE DEPTH MILLING 0" - 2" SY	SURFACE COURSE, S9.5C TONS	PG 70-22 PLANT MIX TONS	PATCHING EXISTING PAVEMENT TONS	MILLED RUMBLE STRIPS LF	ADJUSTMENT OF MANHOLES EA	PORTABLE LIGHTING LS
I-5132	Forsyth	1	I-40 EASTBOUND	FROM PAVEMENT JOINT WEST OF HARPER ROAD TO PAVEMENT JOINT EAST OF JONESTOWN ROAD	1	4.6	NO	58	2.55	2,400	13,900	21,672	1,300	940	48,576	1	0.50
"	"	2	I-40 WESTBOUND	FROM PAVEMENT JOINT WEST OF JONESTOWN ROAD TO PAVEMENT JOINT WEST OF HARPER ROAD	1	4.6	NO	58	2.55	2,400	13,900	21,652	1,299	939	48,576		0.50
TOTAL FOR PROJ NO. I-5132						**9.2**			**5.1**	**4,800**	**27,800**	**43,324**	**2,599**	**1,879**	**97,152**	**1**	**1.00**
GRAND TOTAL						**9.2**			**5.1**	**4,800**	**27,800**	**43,324**	**2,599**	**1,879**	**97,152**	**1**	**1.00**

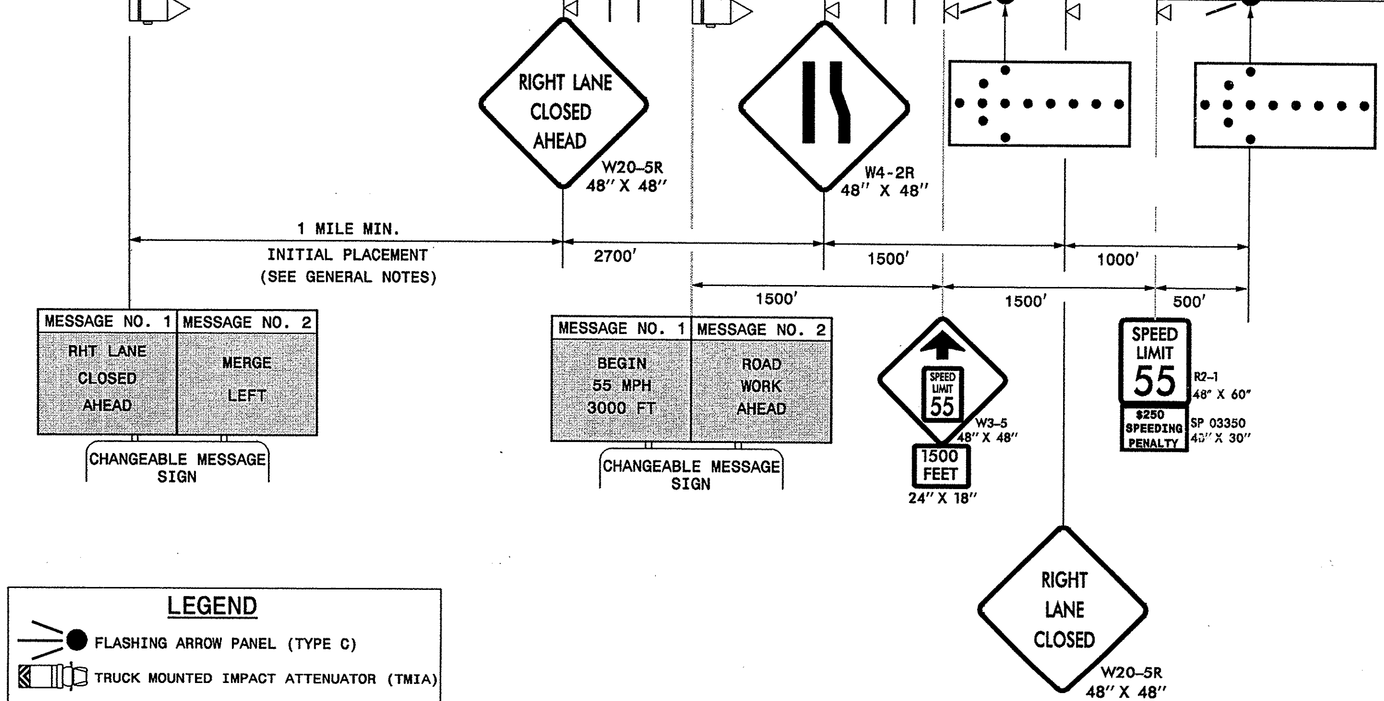
PROJECT NO.	SHEET NO.	TOTAL NO.
I-5132	8	8
45087.3.ST1		

THERMOPLASTIC AND PAINT QUANTITIES

PROJECT	COUNTY	MAP	ROUTE	DESCRIPTION	4605000000-N	4589000000-N	4686000000-E	4815000000-E	4847100000-E		4847120000-E	4710000000-E	4721000000-E		4725000000-E					4855000000-E	4905000000-N
					LAW ENFORCEMENT	TRAFFIC CONTROL	8" X 120 M WHITE THERMO	6" WHITE PAINT	6" WHITE POLYUREA (H.R.E.)	6" YELLOW POLYUREA (H.R.E.)	12" WHITE POLYUREA (H.R.E.)	24" X 120 M WHITE THERMO	THERMO MSG ONLY 120 M	THERMO MSG AHEAD 120 M	THERMO RAMP ARROW 90 M	THERMO STR ARROW 90 M	THERMO STR & LT ARROW 90 M	THERMO RT ARROW 90 M	THERMO LT ARROW 90 M	6" LINE REMOVAL	SNOW PLOWABLE MARKERS
NO		NO			HR	LS	LF	LF	LF	LF	LF	LF	EA	EA	EA	EA	EA	EA	EA	LF	EA
I-5132	Forsyth	1	I-40 EASTBOUND	FROM PAVEMENT JOINT WEST OF HARPER ROAD TO PAVEMENT JOINT EAST OF JONESTOWN ROAD	90	0.50	350	52,800	36,540	23,760.00	10,638		4		1	6	3	2		48,912	596
"	"	2	I-40 WESTBOUND	FROM PAVEMENT JOINT WEST OF JONESTOWN ROAD TO PAVEMENT JOINT WEST OF HARPER ROAD	90	0.50	350	52,800	37,352	24,288.00	2,470	78	16	15	2	9	5	4	4	48,532	604
TOTAL FOR PROJ NO. I-5132					**180**	**1**	**700**	**105,600**	**73,892**	**48,048**	**13,108**	**78**	**20**	**15**	**3**	**15**	**8**	**6**	**4**	**97,444**	**1,200**
									121,940				**35**		**36**						
GRAND TOTAL					**180**	**1**	**700**	**105,600**	**73,892**	**48,048**	**13,108**	**78**	**20**	**15**	**3**	**15**	**8**	**6**	**4**	**97,444**	**1,200**
									121,940				**35**		**36**						

PROJ. REFERENCE NO.	SHEET NO.
I-5132	TCP-1





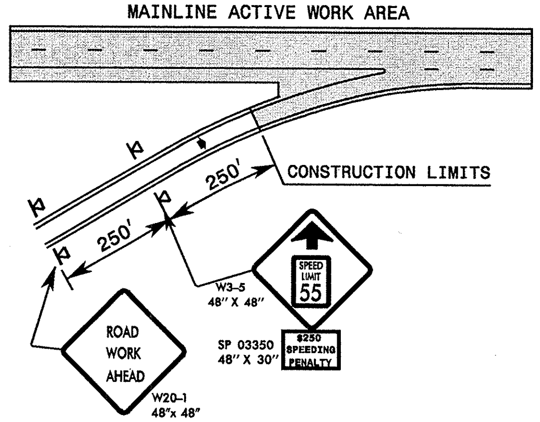
LEGEND

- FLASHING ARROW PANEL (TYPE C)
- TRUCK MOUNTED IMPACT ATTENUATOR (TMIA)
- DRUM
- PORTABLE SIGN
- DIRECTION OF TRAFFIC FLOW
- CHANGEBLE MESSAGE SIGN

GENERAL NOTES

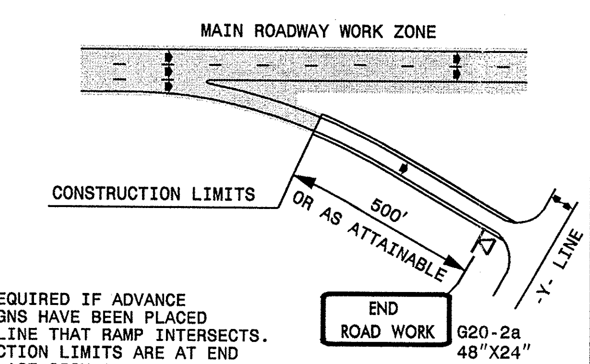
1. THIS DRAWING IS INTENDED TO SHOW THE PORTABLE SIGN LOCATIONS REQUIRED FOR A "WORK ZONE VARIABLE SPEED LIMIT" REDUCTION ON A FREEWAY WHICH IS TO BE REDUCED TO 55 MPH. REFER TO THE ROADWAY STANDARD DRAWING 1101.02 SHEET 3 OF 9 FOR ADDITIONAL LANE CLOSURE REQUIREMENTS AND GENERAL NOTES.
2. EACH DIRECTION OF THE PROJECT IS TO BE EVALUATED FOR THE "WORK ZONE VARIABLE SPEED LIMIT" REDUCTION. THIS DRAWING INTENTIONALLY HAS 1 DIRECTION SIGNED AS A REMINDER TO CAREFULLY CONSIDER WHETHER BOTH DIRECTIONS OF THE PROJECT NEED TO HAVE THE SPEED LIMIT REDUCED.
3. IN ADDITION, FOR ACTIVE WORK AREAS THAT EXCEED 1 MILE IN LENGTH, AN EVALUATION IS TO BE MADE TO DETERMINE IF ADDITIONAL SIGNS ARE NEEDED TO SUPPLEMENT THE INITIAL ONES. PORTABLE MOUNTED W3-5 SIGNS WITH SPEED PENALTY SIGNS ARE TO BE PLACED ALONG ENTRANCE RAMPS LOCATED WITHIN THE ACTIVE WORK AREA.
4. THE $250 SPEEDING PENALTY APPLIES FOR ALL PROJECTS THAT QUALIFY FOR A "WORK ZONE VARIABLE SPEED LIMIT" REDUCTION. PORTABLE SIGNS ARE TO BE USED TO DISPLAY THE $250 SPEEDING PENALTY.
5. THE "WORK ZONE VARIABLE SPEED LIMIT" REDUCTION IS ONLY IN EFFECT WHEN A LANE CLOSURE IS IN PLACE OR WHEN A 2" DROP-OFF BETWEEN OPEN LANES OF TRAFFIC EXISTS (SEE SHEET TCP-3). AT THE COMPLETION OF THE PROJECT, THE RESIDENT ENGINEER SHALL NOTIFY THE REGIONAL TRAFFIC ENGINEER TO RESCIND THE ORDINANCE.
6. WHEN "WORK ZONE VARIABLE SPEED LIMIT" REDUCTIONS ARE IN EFFECT, THE CONTRACTOR IS TO COVER ANY EXISTING SPEED LIMIT SIGNS LOCATED WITHIN THE ACTIVE WORK AREA THAT CONFLICT WITH THE "WORK ZONE VARIABLE SPEED LIMIT" REDUCTION.
7. DO NOT INSTALL ADVANCE WARNING SIGNS MORE THAN 3 DAYS PRIOR TO BEGINNING OF WORK.
8. ALL SIGN SPACING DIMENSIONS ARE APPROXIMATE, FIELD ADJUST AS NECESSARY OR AS DIRECTED BY THE ENGINEER.
9. SINGLE MOUNTED ADVANCE WARNING SIGN CONFIGURATION MAY BE USED ON MULTI-LANE FACILITIES WHERE CONDITIONS LIMIT THE USE OF DUAL MOUNTED SIGNS AS DETERMINED BY THE ENGINEER.
10. THE "WORK ZONE VARIABLE SPEED LIMIT" REDUCTION MUST BE ORDINANCED AND SIGNED BY THE STATE TRAFFIC ENGINEER BEFORE ANY SPEED LIMIT SIGNS ARE USED FOR REDUCING THE SPEED LIMIT.
11. SYMBOLS SHOWN ARE FOR RIGHT LANE CLOSURE, USE APPROPRIATE SIGNS, MESSAGES AND ARROWS FOR LEFT LANE CLOSURE. PORTABLE CMSs TO REMAIN POSITIONED AS SHOWN.

ENTRANCE RAMPS



DETAIL B

EXIT RAMPS



NOTE:
SIGN NOT REQUIRED IF ADVANCE WARNING SIGNS HAVE BEEN PLACED ALONG -Y- LINE THAT RAMP INTERSECTS. IF CONSTRUCTION LIMITS ARE AT END OF RAMP, PLACE SIGN AT END OF RAMP.

APPROVED:________ DATE:________

SEAL

NORTH CAROLINA PROFESSIONAL ENGINEER SEAL 8973 G. L. GETTIER

29APR09

"WORK ZONE VARIABLE SPEED LIMIT" REDUCTION WITH PORTABLE SIGNS

SCALE:	NONE
DATE:	04-09
DWG. BY:	KPB
DESIGN BY:	KPB
REVIEWED BY:	JWG

DIVISION OF HIGHWAYS TRAFFIC ENGINEERING BRANCH STATE OF NORTH CAROLINA DEPARTMENT OF TRANSPORTATION TRAFFIC CONTROL UNIT

REVISIONS

CADD FILE

29-APR-2009 13:06
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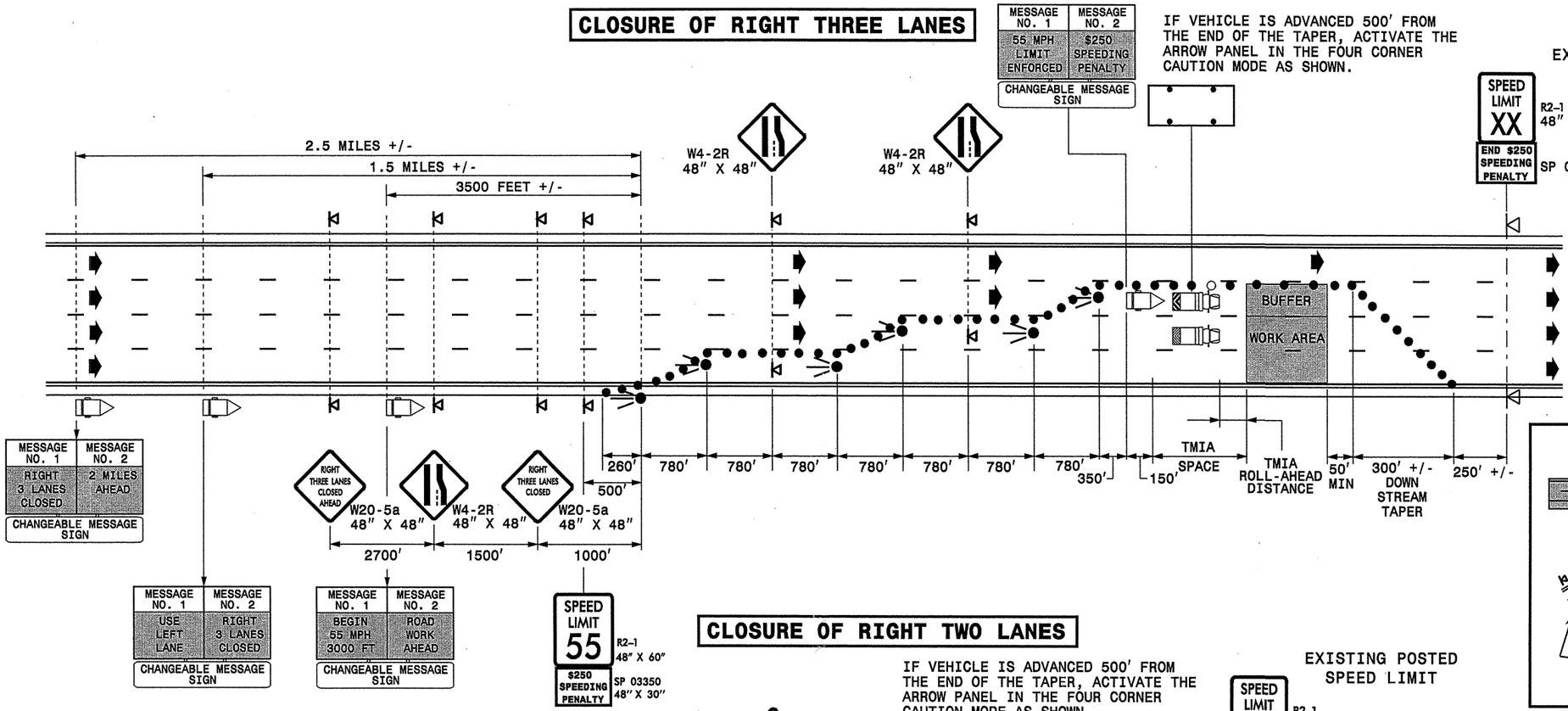
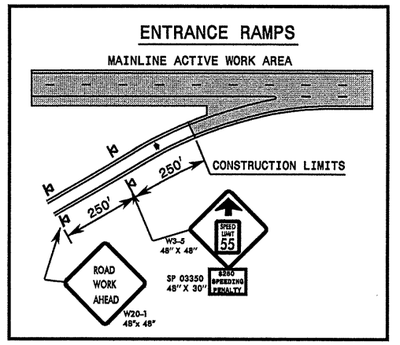
sngreen AT WZTC244733

PROJ. REFERENCE NO.	SHEET NO.
I-5132	TCP-2

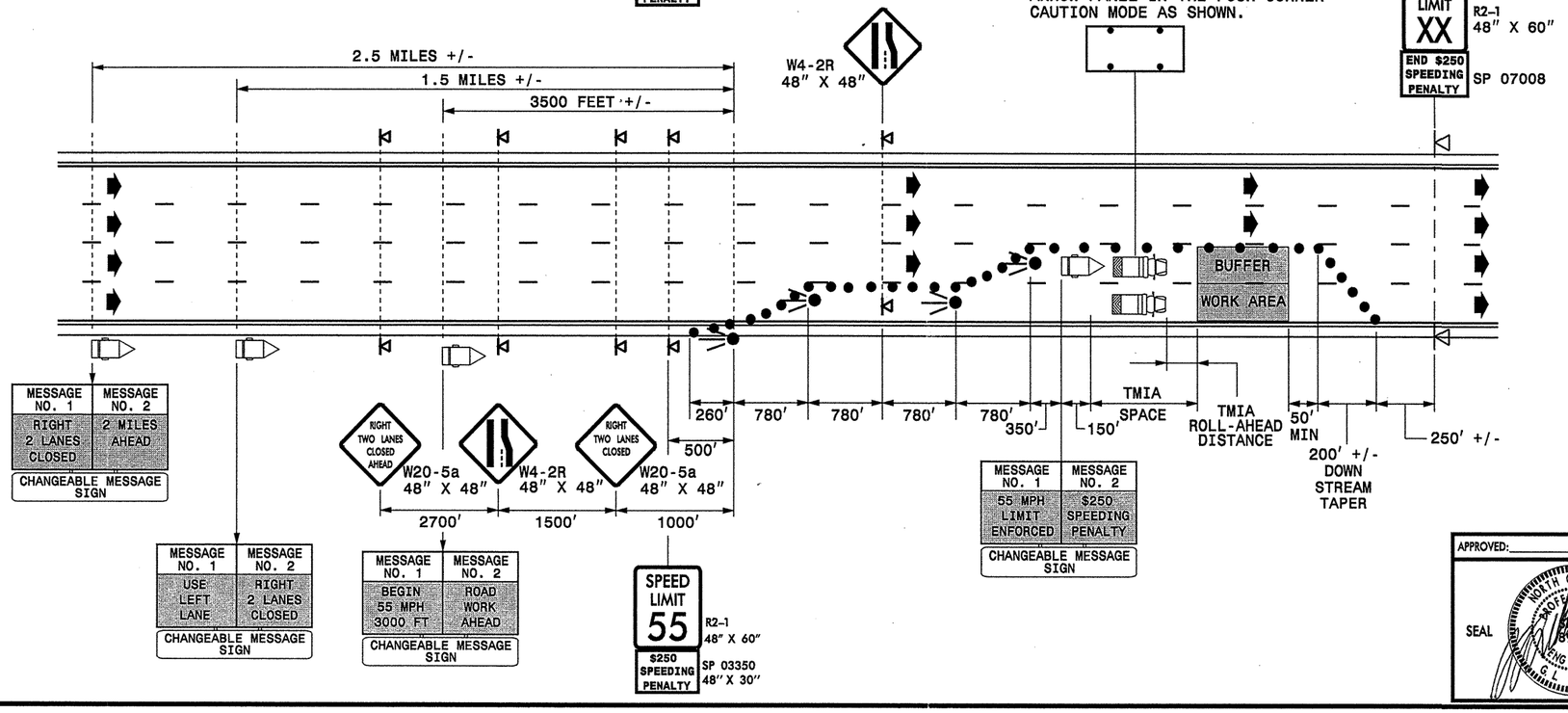
CLOSURE OF RIGHT THREE LANES



ENTRANCE RAMPS



CLOSURE OF RIGHT TWO LANES



GENERAL NOTES

1-Refer to Notes on Rdwy Std 1101.02 Sheet 3 of 9.

2-Coordinate use of overhead DMSs with the Division.

3-Use following configuration for FAP (Flashing Arrow Panel)

4-Symbols shown are for Right Lane Closure, use appropriate signs, messages and arrows for Left Lane Closure. Portable CMSs to remain positioned as shown.

LEGEND

- FLASHING ARROW PANEL (TYPE C)
- TRUCK MOUNTED IMPACT ATTENUATOR (TMIA)
- DRUM
- PORTABLE SIGN
- CHANGEABLE MESSAGE SIGN (CMS)
- DIRECTION OF TRAFFIC FLOW

APPROVED:_____ DATE:_____

SEAL — NORTH CAROLINA PROFESSIONAL SEAL 18973 ENGINEER G. L. GETTES — 29 MAR 09

INTERSTATE DUAL/TRIPLE LANE CLOSURE

SCALE:	NONE
DATE:	04-09
DWG. BY:	KPB
DESIGN BY:	KPB
REVIEWED BY:	JWG

REVISIONS

CADD FILE

29-APR-2009 09:40
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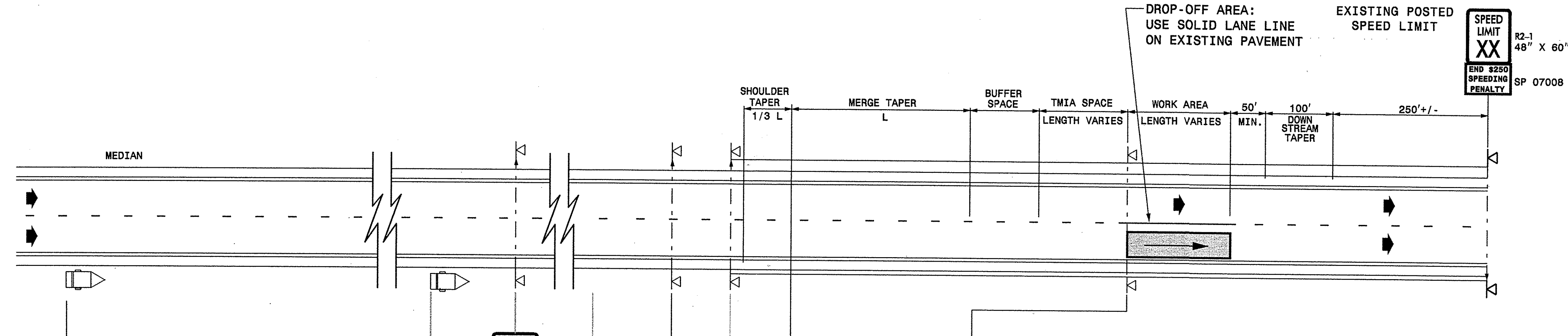
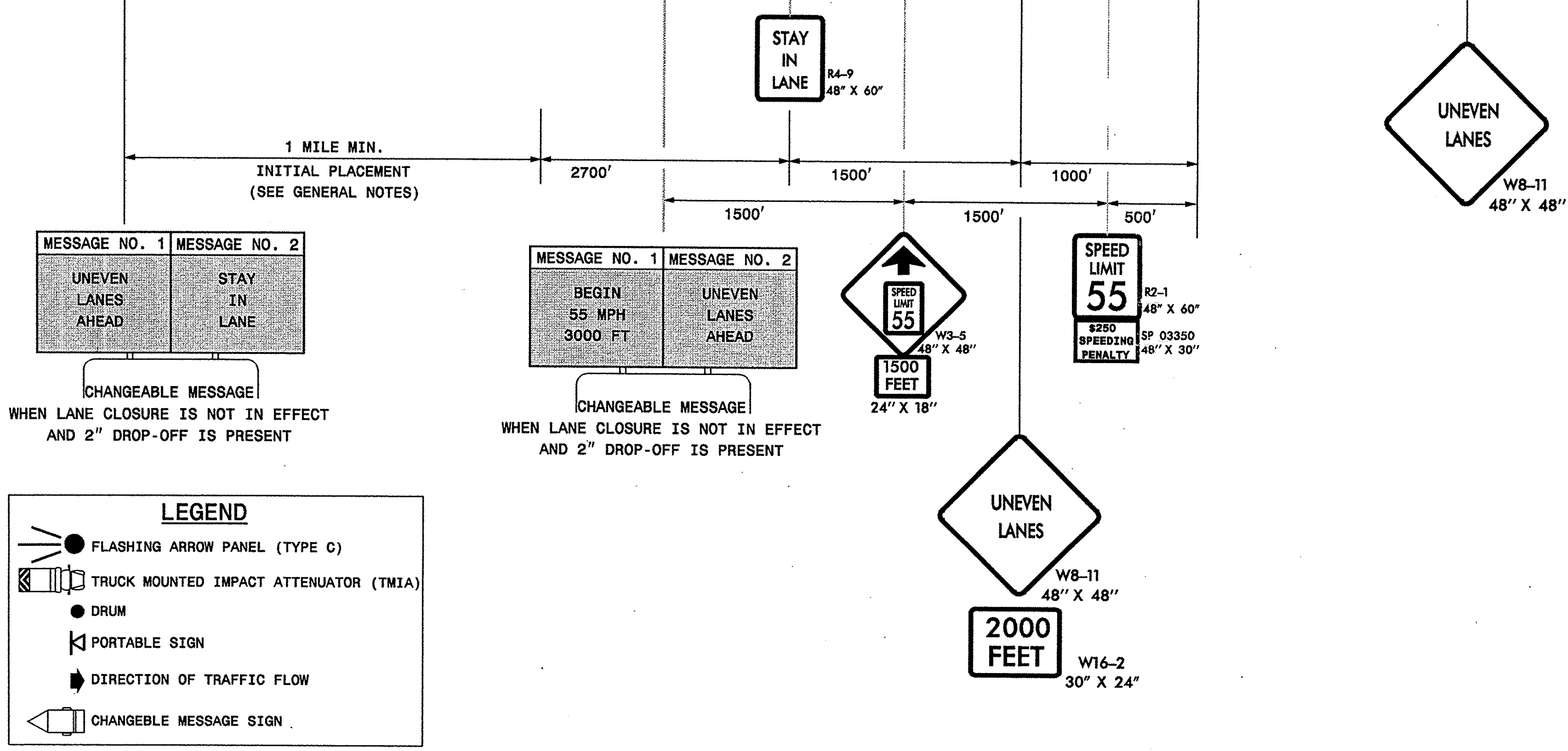
kbroadwell AT WZTC23745

PROJ. REFERENCE NO.	SHEET NO.
I-5132	TCP-3





GENERAL NOTES

1. THIS DRAWING IS INTENDED TO SHOW THE PORTABLE SIGN LOCATIONS REQUIRED FOR A "WORK ZONE VARIABLE SPEED LIMIT" REDUCTION ON A FREEWAY WHICH IS TO BE REDUCED TO 55 MPH.

2. EACH DIRECTION OF THE PROJECT IS TO BE EVALUATED FOR THE "WORK ZONE VARIABLE SPEED LIMIT" REDUCTION. THIS DRAWING INTENTIONALLY HAS 1 DIRECTION SIGNED AS A REMINDER TO CAREFULLY CONSIDER WHETHER BOTH DIRECTIONS OF THE PROJECT NEED TO HAVE THE SPEED LIMIT REDUCED.

3. IN ADDITION, FOR ACTIVE WORK AREAS THAT EXCEED 1 MILE IN LENGTH, AN EVALUATION IS TO BE MADE TO DETERMINE IF ADDITIONAL SIGNS ARE NEEDED TO SUPPLEMENT THE INITIAL ONES. PORTABLE MOUNTED W3-5 SIGNS WITH SPEED PENALTY SIGNS ARE TO BE PLACED ALONG ENTRANCE RAMPS LOCATED WITHIN THE ACTIVE WORK AREA.

4. THE $250 SPEEDING PENALTY APPLIES FOR ALL PROJECTS THAT QUALIFY FOR A "WORK ZONE VARIABLE SPEED LIMIT" REDUCTION. PORTABLE SIGNS ARE TO BE USED TO DISPLAY THE $250 SPEEDING PENALTY.

5. THE "WORK ZONE VARIABLE SPEED LIMIT" REDUCTION IS ONLY IN EFFECT WHEN A 2" DROP-OFF BETWEEN OPEN LANES OF TRAFFIC EXISTS. AT THE COMPLETION OF THE PROJECT, THE RESIDENT ENGINEER SHALL NOTIFY THE REGIONAL TRAFFIC ENGINEER TO RESCIND THE ORDINANCE.

6. WHEN "WORK ZONE VARIABLE SPEED LIMIT" REDUCTIONS ARE IN EFFECT, THE CONTRACTOR IS TO COVER ANY EXISTING SPEED LIMIT SIGNS LOCATED WITHIN THE ACTIVE WORK AREA THAT CONFLICT WITH THE "WORK ZONE VARIABLE SPEED LIMIT" REDUCTION.

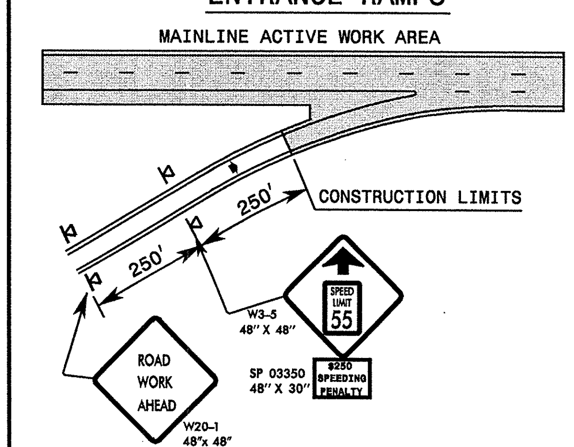
7. DO NOT INSTALL ADVANCE WARNING SIGNS MORE THAN 3 DAYS PRIOR TO BEGINNING OF WORK.

8. ALL SIGN SPACING DIMENSIONS ARE APPROXIMATE, FIELD ADJUST AS NECESSARY OR AS DIRECTED BY THE ENGINEER.

9. SINGLE MOUNTED ADVANCE WARNING SIGN CONFIGURATION MAY BE USED ON MULTI-LANE FACILITIES WHERE CONDITIONS LIMIT THE USE OF DUAL MOUNTED SIGNS AS DETERMINED BY THE ENGINEER.

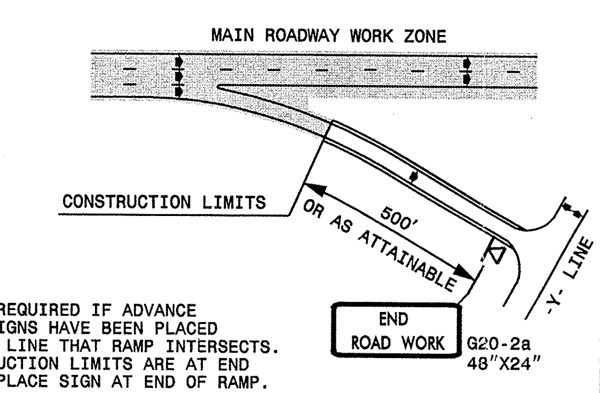
10. THE "WORK ZONE VARIABLE SPEED LIMIT" REDUCTION MUST BE ORDINANCED AND SIGNED BY THE STATE TRAFFIC ENGINEER BEFORE ANY SPEED LIMIT SIGNS ARE USED FOR REDUCING THE SPEED LIMIT.

ENTRANCE RAMPS



DETAIL B

EXIT RAMPS



NOTE:
SIGN NOT REQUIRED IF ADVANCE WARNING SIGNS HAVE BEEN PLACED ALONG -Y- LINE THAT RAMP INTERSECTS. IF CONSTRUCTION LIMITS ARE AT END OF RAMP, PLACE SIGN AT END OF RAMP.

APPROVED:________ DATE:________

SEAL

NORTH CAROLINA PROFESSIONAL SEAL 8973 ENGINEER G. L. GETTIER

29 APR 09

"WORK ZONE VARIABLE SPEED LIMIT" REDUCTION WITH PORTABLE SIGNS

SCALE:	NONE
DATE:	04-09
DWG. BY:	KPB
DESIGN BY:	KPB
REVIEWED BY:	JWG

DIVISION OF HIGHWAYS · TRAFFIC ENGINEERING BRANCH · STATE OF NORTH CAROLINA · DEPARTMENT OF TRANSPORTATION · TRAFFIC CONTROL UNIT

REVISIONS

CADD FILE

29-APR-2009 13:05
\\DOT\DFSROOT01\GROUPS-WZTCCC\designgroup3\squad3b\standard drawings in work zone\I-5132_tc_tcp-3.dgn
sngreen AT WZTC244733

PROJ. REFERENCE NO.	SHEET NO.
I-5132	TCP-4

SIGN NUMBER: SP-03350 **TYPE:** D **QUANTITY:** 1
BACKG COLOR: White **COPY COLOR:** Black

SIGN WIDTH: 4'-0"
HEIGHT: 2'-6"
TOTAL AREA: 10.0 Sq.Ft.

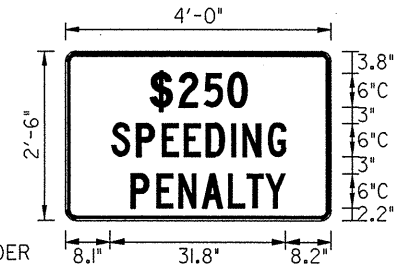
BORDER TYPE: FLUSH
RECESS: 0.4"
WIDTH: 0.5"
RADII: 1.5"

NO. Z BARS: N/A
LENGTH: N/A

SYMBOL	X	Y	WID	HT

MAT'L:

DESIGN BY: CL DOWNEY **PROJECT ID:**
CHECKED BY: **DIV:** DIV
STD #: REGULATORY **DATE:** Aug 18,2003



BORDER
R=1.5"
TH=0.5"
IN=0.4"

SP 03350

USE NOTES: 2,4
1. Legend and border shall be direct applied Type III reflective sheeting.
2. Legend and border shall be direct applied non-reflective sheeting.
3. Shields shall be Type III reflective sheeting on 0.032" (0.8mm) aluminum and demountable.
4. Background shall be Type III reflective sheeting.
5. Background shall be Type I reflective sheeting.
6. Center arrow(s) vertically on sign.
7. Bottom panel shall be yellow Type III sheeting. Legend shall be direct applied black non-reflective sheeting. Yellow panel is:

LETTER POSITIONS

Letter spacings are to start of next letter

										Series/Size Text Length
	$	2	5	0						C6
15.1	5.3	4.6	4.2	3.5	15.4					17.6
	S	P	E	E	D	I	N	G		C6
8.1	4.8	4.5	4	4	4.6	2.1	4.4	3.4	8.2	31.8
	P	E	N	A	L	T	Y			C6
11.9	4.6	4	4.3	4.7	3.4	3.3	3.7	8.2		28

Spacing Factor is 1 unless specified otherwise

SIGN NUMBER: SP07008 **TYPE:** D **QUANTITY:** 1
BACKG COLOR: White **COPY COLOR:** Black

SIGN WIDTH: 48"
HEIGHT: 30"
TOTAL AREA: 10.0 Sq.Ft.

BORDER TYPE: FLUSH
RECESS: 0.5"
WIDTH: 0.75"
RADII: 1.88"

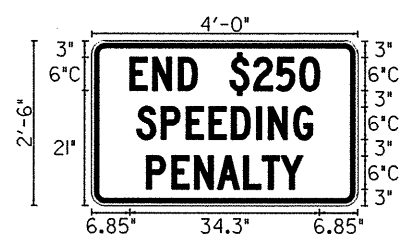
NO. Z BARS:
LENGTH:

SYMBOL	X	Y	WID	HT

MAT'L: 0.125" (3.2 mm) ALUMINUM
0.079" COMPOSITE

DESIGN BY: R. HENNEIN **PROJECT ID:** N/A
CHECKED BY: **DIV:** N/A
STD #: N/A **DATE:** Jan 12,2007

USE NOTES:
1. Legend and border shall be direct applied Type III reflective sheeting.
2. Background shall be Type III reflective sheeting.



BORDER
R=1.88"
TH=0.75"
IN=0.5"

SP 07008

LETTER POSITIONS

Letter spacings are to start of next letter

										Series/Size Text Length
	E	N	D		$	2	5	0		C 2000
6.9	4.1	4.7	3.4	6	4.3	4.1	4.3	3.5	6.9	34.3
	S	P	E	E	D	I	N	G		C 2000
8.1	4.4	4.6	4.1	4.1	4.6	2.2	4.6	3.4	8.1	31.7
	P	E	N	A	L	T	Y			C 2000
9.9	4.6	4.1	4.2	4.7	3.4	3.4	3.8	9.9		28.2

Spacing Factor is 1 unless specified otherwise

SP 03353

SIGN NUMBER: SP-03353 **TYPE:** A **QUANTITY:** 1
BACKG COLOR: Fluorescent Orange **COPY COLOR:** Black

SIGN WIDTH: 4'-0"
HEIGHT: 4'-0"
TOTAL AREA: 16.0 Sq.Ft.

BORDER TYPE: FLUSH
RECESS: 0.59"
WIDTH: 0.75"
RADII: 1.38"

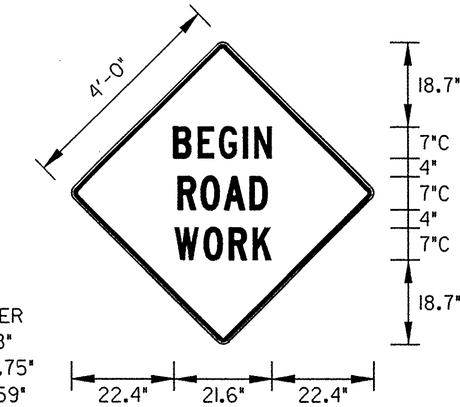
NO. Z BARS: N/A
LENGTH: N/A

SYMBOL	X	Y	WID	HT

MAT'L:

DESIGN BY: CL DOWNEY **PROJECT ID:** ALL PROJECTS
CHECKED BY: CHECKED **DIV:** DIV
STD #: W20-1 **DATE:** Aug 20,2003

USE NOTES: 2, 4
1. Legend and border shall be direct applied Type VII reflective sheeting.
2. Legend and border shall be direct applied non-reflective sheeting.
3. Shields shall be Type VII reflective sheeting on 0.032" (0.8mm) aluminum and demountable.
4. Background shall be Type VII reflective sheeting.
5. Background shall be Type I reflective sheeting.
6. Center arrow(s) vertically on sign.
7. Bottom panel shall be yellow Type III sheeting. Legend shall be direct applied black non-reflective sheeting. Yellow panel is:



BORDER
R=1.38"
TH=0.75"
IN=0.59"

LETTER POSITIONS

Letter spacings are to start of next letter

							Series/Size Text Length
	B	E	G	I	N		C7
22.4	5.3	4.6	5.4	2.5	3.8	22.4	21.6
	R	O	A	D			C7
23.4	5	5.2	5.6	3.8	23.4		19.6
	W	O	R	K			C7
22.6	6.4	5.6	5.2	4	22.6		21.2

Spacing Factor is 1 unless specified otherwise

SPECISHAK

APPROVED:____________ DATE:________

SEAL

NORTH CAROLINA PROFESSIONAL SEAL 8973 ENGINEER G. L. GETTIER

29 APR 09

SIGN DESIGNS

SCALE: NONE
DATE: 4/09
DWG. BY: KPB
DESIGN BY: KPB
REVIEWED BY: JWG

DIVISION OF HIGHWAYS • STATE OF NORTH CAROLINA • DEPARTMENT OF TRANSPORTATION • WORK ZONE TRAFFIC CONTROL UNIT

REVISIONS

CADD FILE

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kbroadwell AT WZTC237451