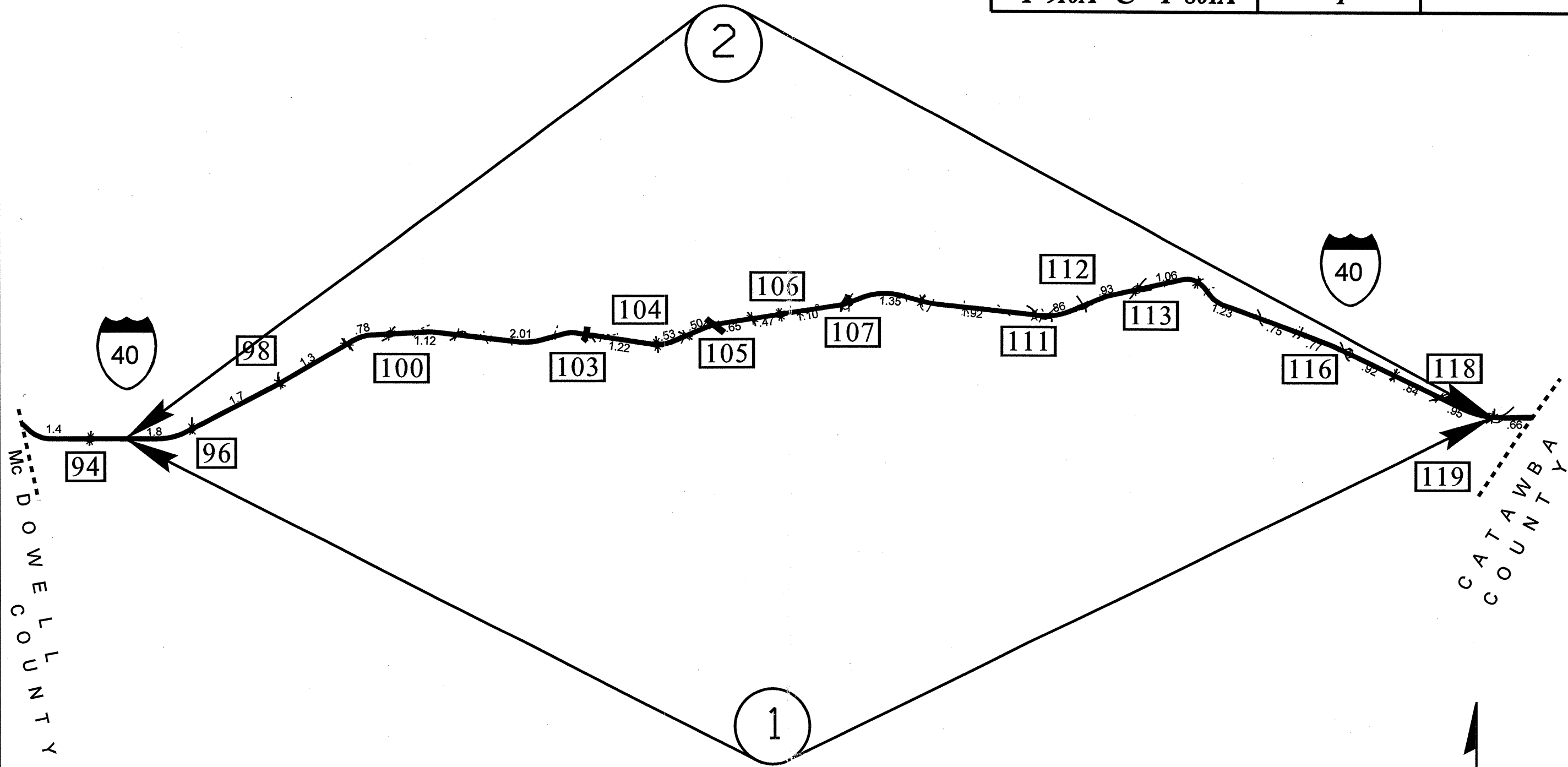


PROJECT NO.	SHEET NO.	TOTAL SHEETS
I-910A & I-801A	1	



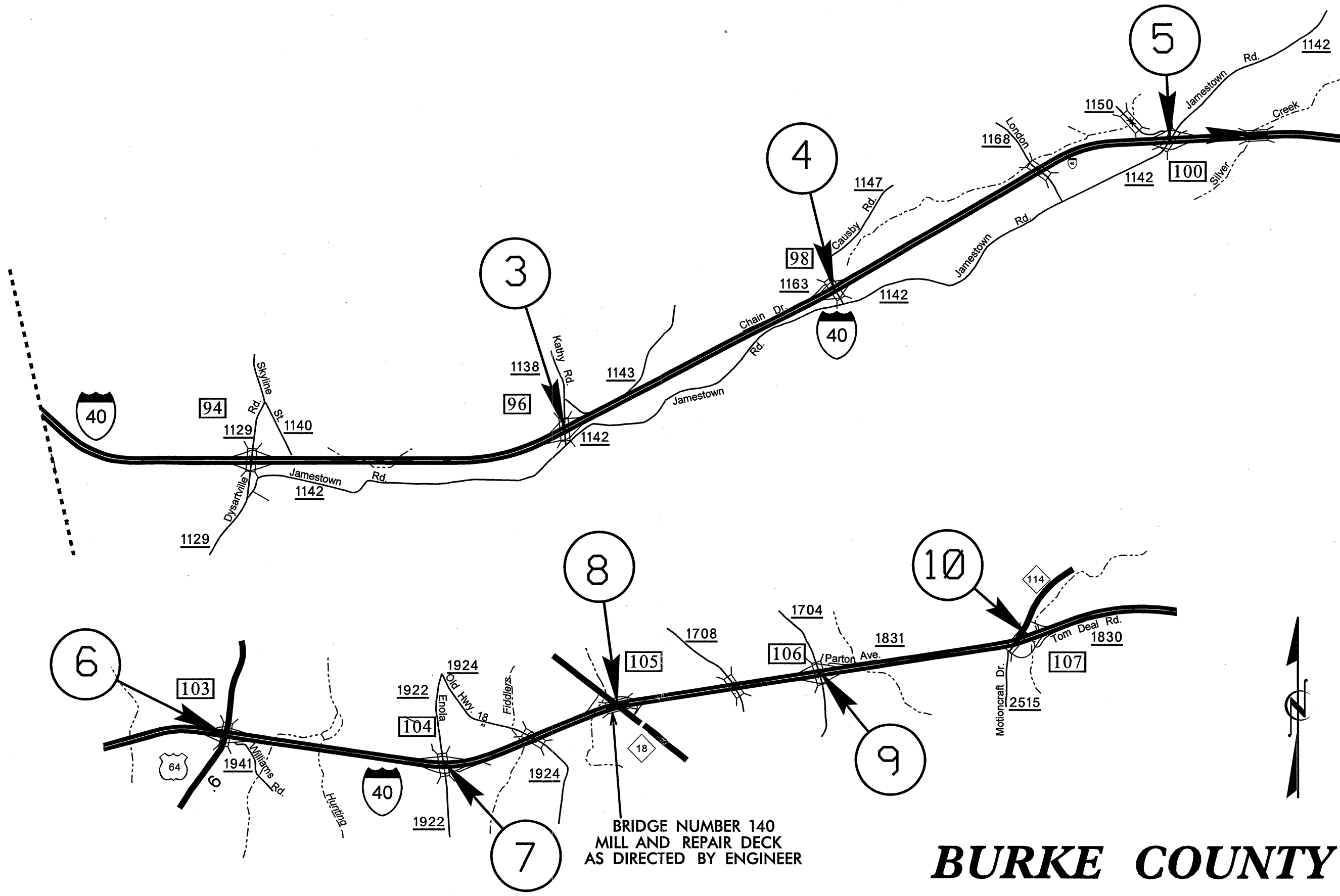
Mc
DOWNE
COUNTY

CATAWBA
COUNTY



BURKE COUNTY

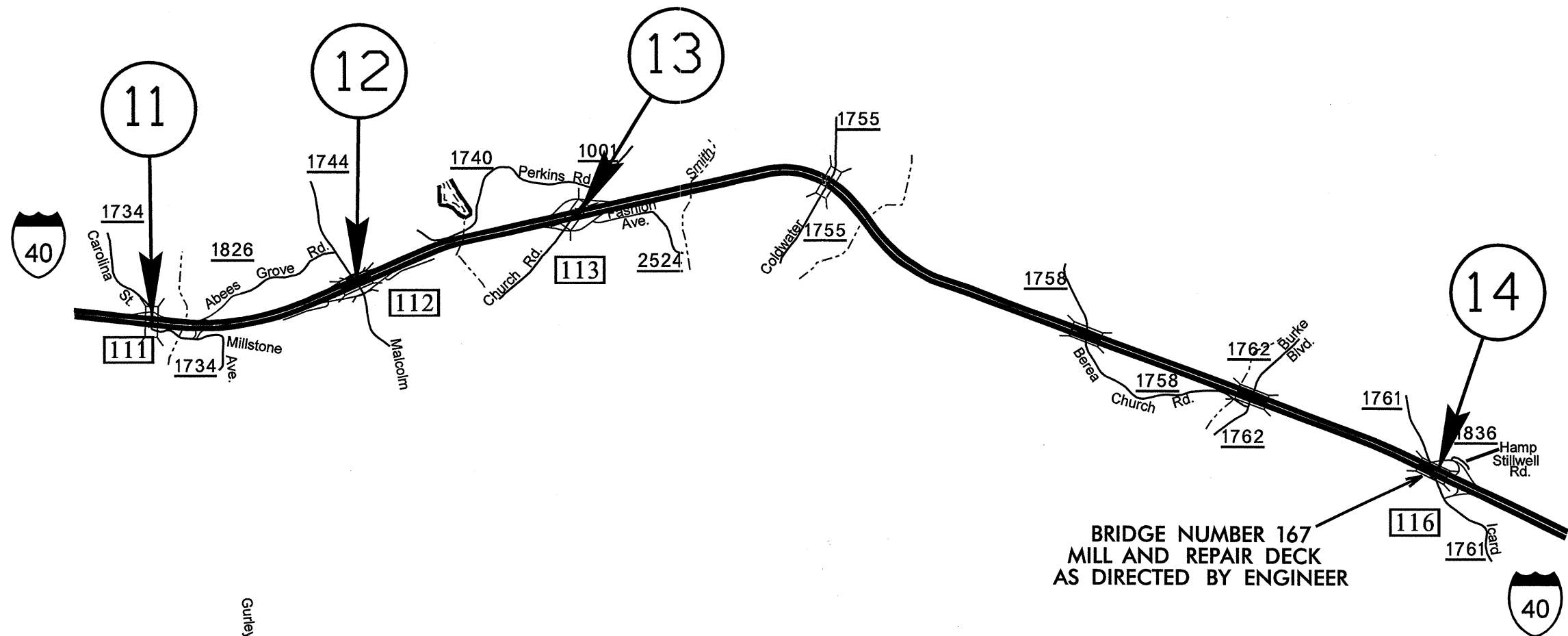
PROJECT NO.	SHEET NO.	TOTAL SHEETS
I-910A & I-801A	2	



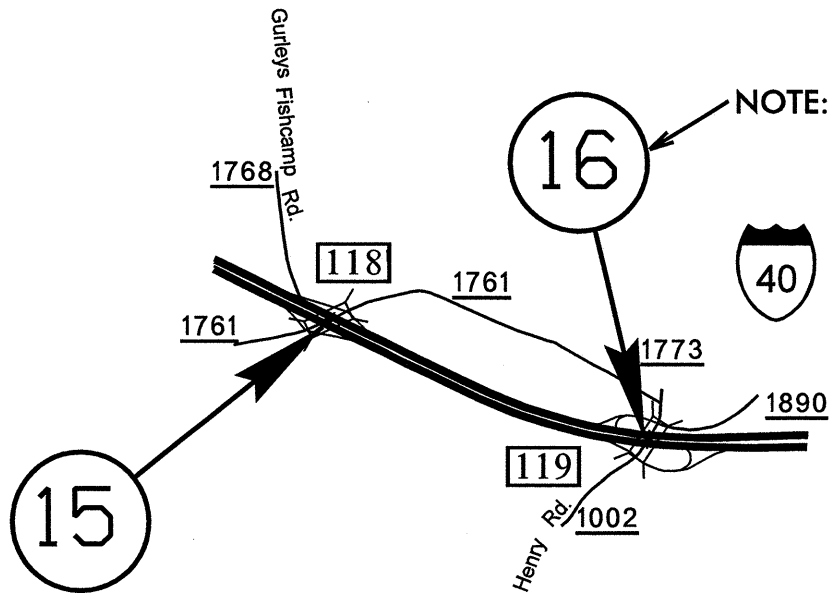
BRIDGE NUMBER 140
MILL AND REPAIR DECK
AS DIRECTED BY ENGINEER

BURKE COUNTY

PROJECT NO.	SHEET NO.	TOTAL SHEETS
I-910A & I-801A	3	



BRIDGE NUMBER 167
MILL AND REPAIR DECK
AS DIRECTED BY ENGINEER

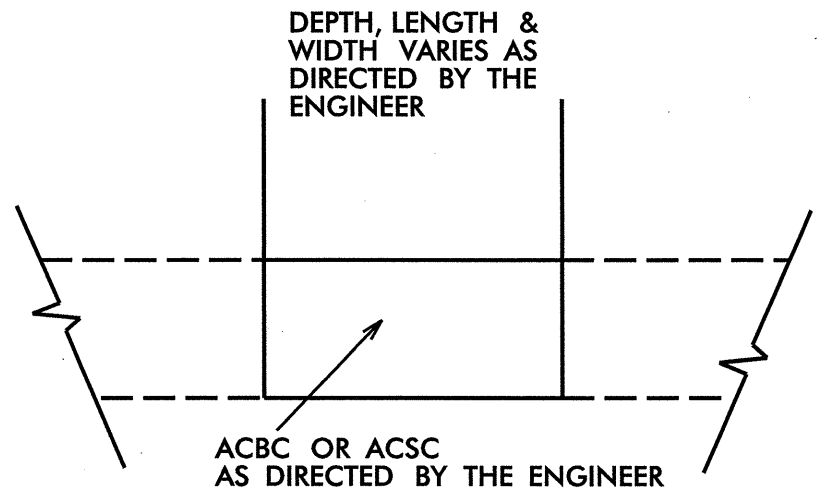
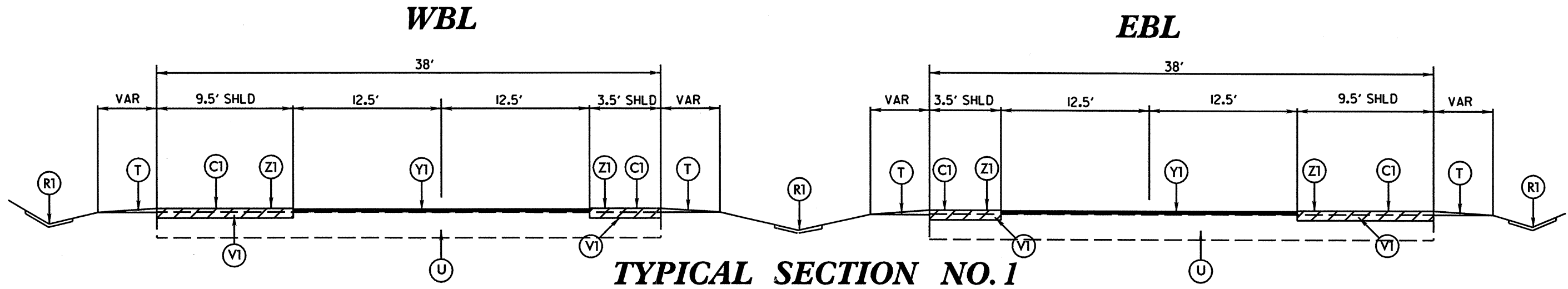


NOTE: DO THE RAMPS ON WEST SIDE OFF BRIDGE,
DO EBL OFF RAMP, WBL OFF RAMP
AND WBL ON RAMP ONLY



BURKE COUNTY

PROJECT NO.	SHEET NO.	TOTAL SHEETS
I-910A & I-801A	4	

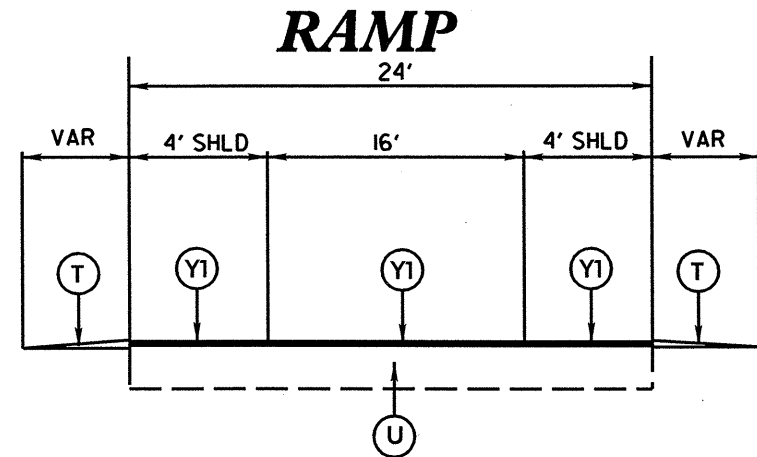


PATCHING DETAIL
PATCH EXISTING PAVEMENT

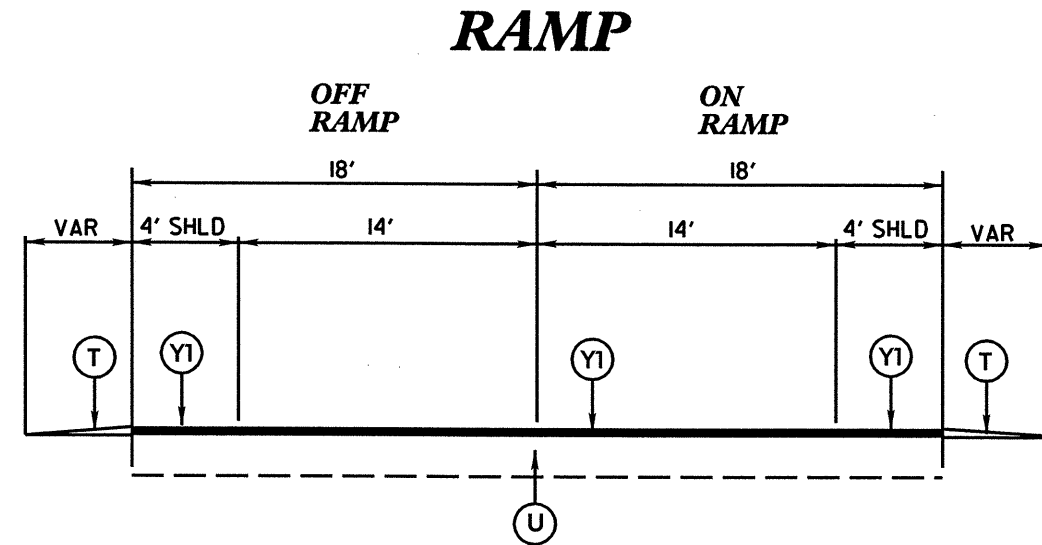
PAVEMENT SCHEDULE	
C1	PROP. APPROX. 1-1/2" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD. PLACE AS SHOWN ON TYPICAL TO FILL IN MILLED AREA ON I-40 SHOULDERS.
R1	4" CONCRETE PAVED DITCH, REPAIR EXISTING CONCRETE DITCH AS DIRECTED BY THE ENGINEER
T	SHOULDER RECONSTRUCTION USING EARTH MATERIAL, LOCATIONS AS DIRECTED BY ENGINEER
U	EXISTING PAVEMENT
V1	MILL 1", OVER ENTIRE SHOULDER WIDTH, MILL WIDTH AS SHOWN ON TYPICAL 1 UP RAMP SHOULDERS TO REMOVE EXISTING MILLED RUMBLE STRIPS
V2	MILL 5/8" ON BRIDGE NUMBERS 140 AND 167
Y1	PROP APPROX 5/8" ULTRATHIN HOT MIX ASPHALT, TYPE B, AT AN AVERAGE RATE OF 70 LBS PER SQ YARD
Z1	MILLED RUMBLE STRIPS IN ACCORDANCE WITH STD. DWG. 665.01

BURKE COUNTY

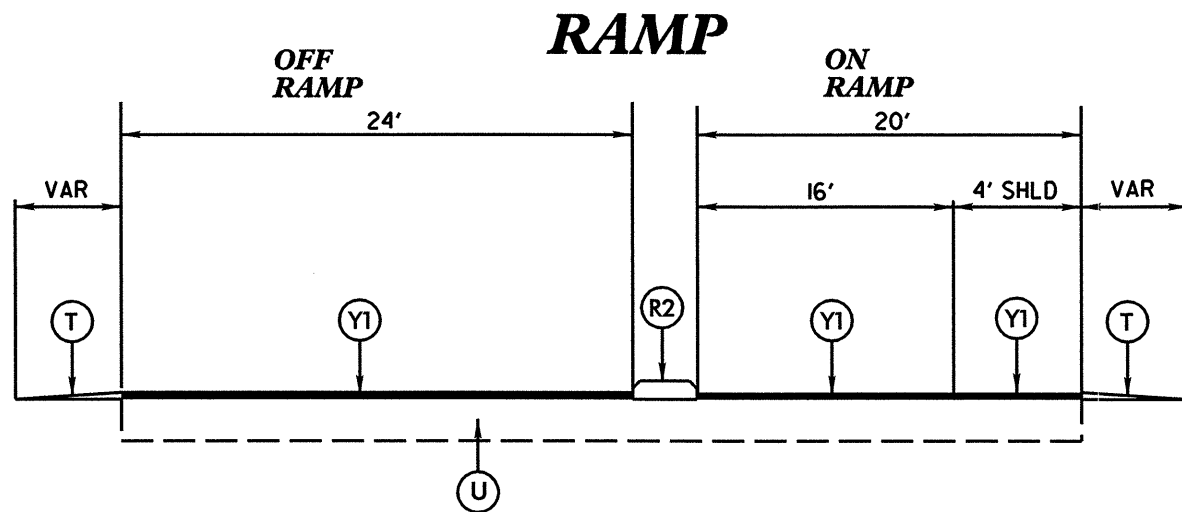
PROJECT NO.	SHEET NO.	TOTAL SHEETS
I-910A & I-801A	5	



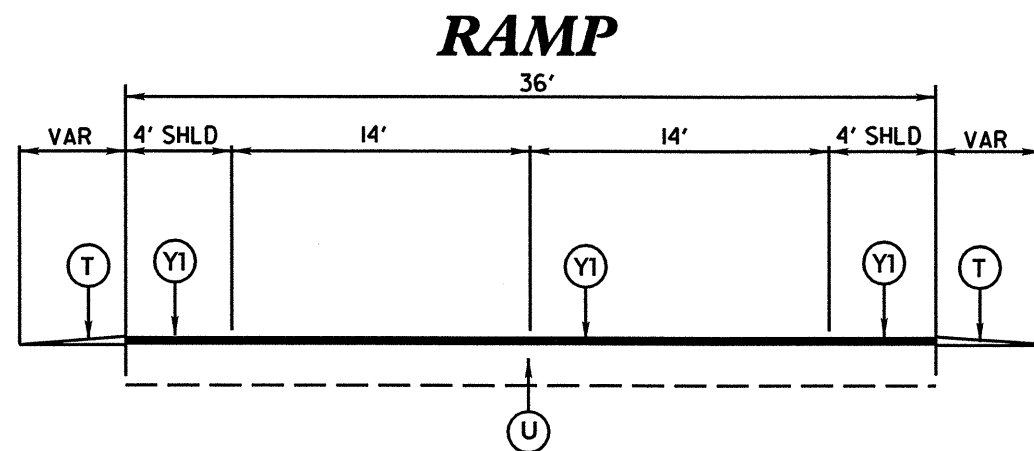
TYPICAL SECTION NO. 2



TYPICAL SECTION NO. 5



TYPICAL SECTION NO. 3

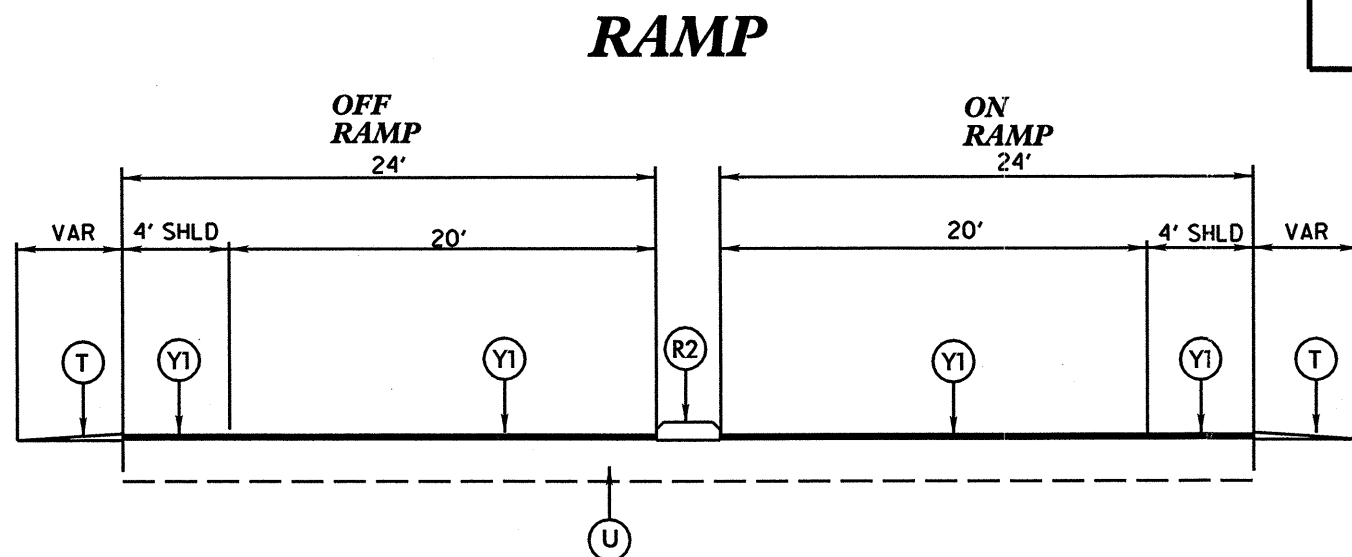


TYPICAL SECTION NO. 4

PAVEMENT SCHEDULE	
R2	EXISTING CONCRETE ISLAND
T	SHOULDER RECONSTRUCTION USING EARTH MATERIAL, LOCATIONS AS DIRECTED BY ENGINEER
U	EXISTING PAVEMENT
Y1	PROP APPROX 5/8" ULTRATHIN HOT MIX ASPHALT, TYPE B, AT AN AVERAGE RATE OF 70 LBS PER SQ YARD

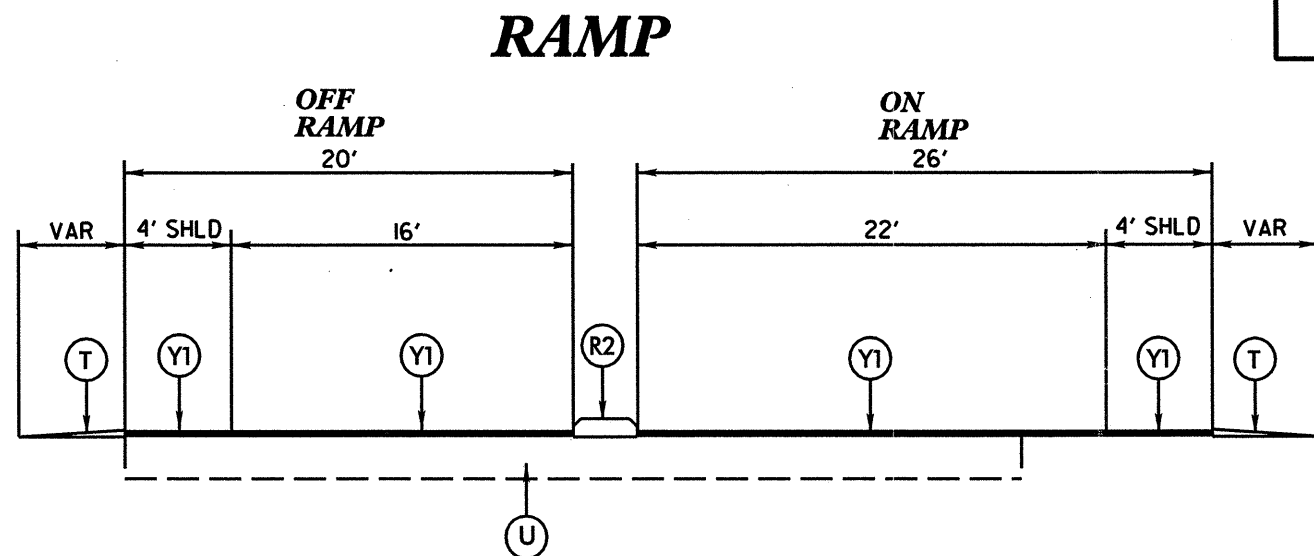
BURKE COUNTY

<i>PROJECT NO.</i>	<i>SHEET NO.</i>	<i>TOTAL SHEETS</i>
I-910A & I-801A	6	



TYPICAL SECTION NO. 6

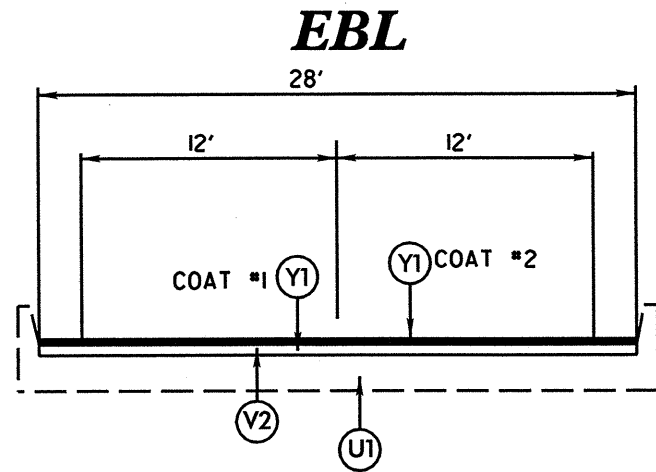
PAVEMENT SCHEDULE	
R2	EXISTING CONCRETE ISLAND
T	SHOULDER RECONSTRUCTION USING EARTH MATERIAL, LOCATIONS AS DIRECTED BY ENGINEER
U	EXISTING PAVEMENT
Y1	PROP APPROX 5/8" ULTRATHIN HOT MIX ASPHALT, TYPE B, AT AN AVERAGE RATE OF 70 LBS PER SQ YARD



TYPICAL SECTION NO. 7

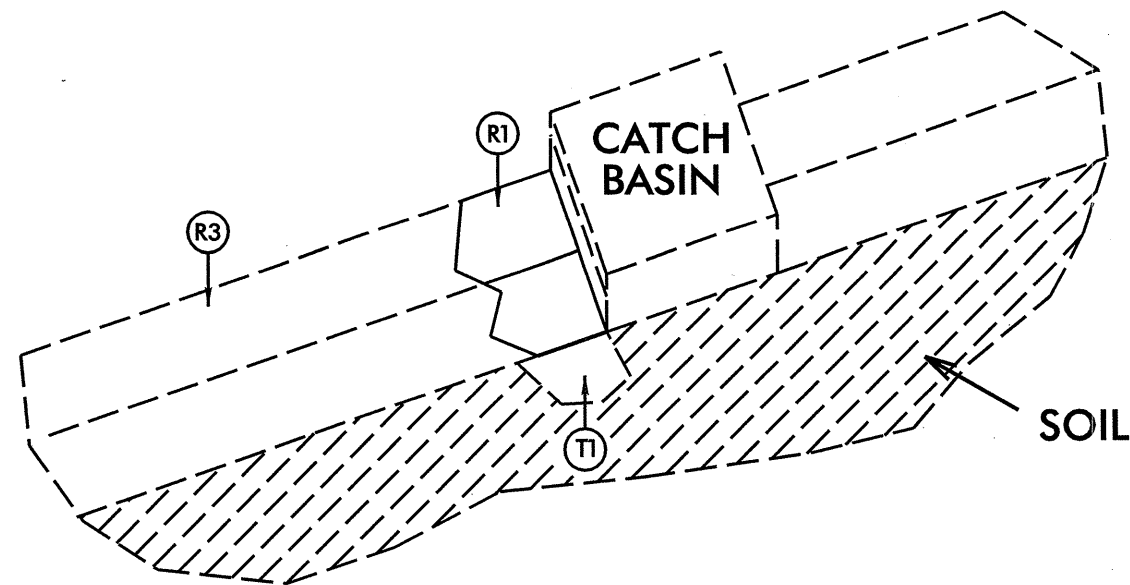
BURKE COUNTY

PROJECT NO.	SHEET NO.	TOTAL SHEETS
I-910A & I-801A	7	



BRIDGE DETAIL

BRIDGE NUMBERS 140 AND 167
MILL OFF EXISTING ULTRATHIN PAVEMENT
PATCH BRIDGE DECK AS NEEDED
THEN RESURFACE BRIDGE DECK WITH ULTRATHIN
HOT MIX TYPE B AND ADD A SECOND COAT
OF ULTRATHIN TYPE B WHEN RESURFACE EBL I-40
ALL OTHER BRIDGES JUST RESURFACE
SEE MAPS FOR LOCATION ON 140 AND 167



4" CONCRETE PAVED DITCH DETAIL

USE INCIDENTAL STONE TO FILL IN
WHERE SOIL HAS WASHED AWAY
AND REPAIR CONCRETE DITCH
AS DIRECTED BY ENGINEER

PAVEMENT SCHEDULE	
R1	4" CONCRETE PAVED DITCH, REPAIR EXISTING CONCRETE DITCH AS DIRECTED BY THE ENGINEER
R3	EXISTING CONCRETE DITCH
T1	INCIDENTAL STONE BASE, USE WHERE SOIL HAS BEEN WASHED AWAY, LOCATIONS AS DIRECTED BY ENGINEER
U1	EXISTING PAVEMENT BRIDGE
V2	MILL 5/8" ON BRIDGE NUMBERS 140 AND 167
Y1	PROP APPROX 5/8" ULTRATHIN HOT MIX ASPHALT, TYPE B, AT AN AVERAGE RATE OF 70 LBS PER SQ YARD. TWO COATS ON BRIDGE NUMBER 140 AND 167

BURKE COUNTY

SUMMARY OF QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LENGTH MI	WIDTH FT	SEALING EXIST. PAVEMENT CRACKS LBS	INCIDENTAL STONE BASE TONS	SHOULDER RECONSTRUCTION SMI	5/8" MILLING SY	1" MILLING SY	SURFACE COURSE, S9.5C TONS	PG 70-22 PLANT MIX TONS	PATCHING EXISTING PAVEMENT TONS	ULTRATHIN HOT MIX ASPHALT, TYPE B TON	APPLICATION OF ULTRATHIN HOT MIX ASPHALT SY	MILLED RUMBLE STRIPS LF	PG 70-28 PLANT MIX TON	4" CONCRETE PAVED DITCH SY	PORTABLE LIGHTING LS	SEED & MULCHING AC	CONCRETE DECK REPAIR USING ELASTOMERIC CONCRETE SY
TBD	Burke	1	I-40 EBL	FROM MP 95.17 TO MP 119	1	23.83	38	50,000	100	2.00	1,101	181,744	15,413	925	3,340	12,233	349,507	251,645	709	475		1.21	330
		2	I-40 WBL	FROM MP 119 TO MP 95.17	1	23.83	38	50,000	100	2.00		181,744	15,413	925	3,340	12,233	349,507	251,645	709	475		1.21	
		3	I-40 RAMPS EXIT 96	I-40 TO SR 1138 OR SR 1138 TO I-40	2 & 4	1.03	24, 36	700	1	0.10					30	525	15,010	200	30		*	0.06	
		4	I-40 RAMPS EXIT 98	I-40 TO SR 1147 OR SR 1147 TO I-40	2 & 4	0.89	24, 36	700	1	0.10					30	446	12,740	200	26		*	0.06	
		5	I-40 RAMPS EXIT 100	I-40 TO SR 1142 OR SR 1142 TO I-40	2 & 4	0.98	24, 36	700	1	0.10					30	531	15,178	200	31		*	0.06	
		6	I-40 RAMPS EXIT 103	I-40 TO US 64 OR US 64 TO I-40	2 & 4	1.1	24, 36	700	1	0.10					30	558	15,953	200	32		*	0.06	
		7	I-40 RAMPS EXIT 104	I-40 TO SR 1922 OR SR 1922 TO I-40	2 & 4	1.12	24 - 36	700	1	0.10					30	568	16,243	200	33		*	0.06	
		8	I-40 RAMPS EXIT 105	I-40 TO NC 18 OR NC 18 TO I-40	2 & 3	0.98	20, 24	700	1	0.10					30	479	13,706	200	28		*	0.06	
		9	I-40 RAMPS EXIT 106	I-40 TO SR 1704 OR SR 1704 TO I-40	2 & 4	0.98	24, 36	700	1	0.10					30	497	14,212	200	29		*	0.06	
		10	I-40 RAMPS EXIT 107	I-40 TO NC 114 OR NC 114 TO I-40	2 & 5	0.67	24, 36	700	1	0.10					30	304	8,702	200	18		*	0.06	
		11	I-40 RAMPS EXIT 111	I-40 TO SR'S OR SR'S TO I-40	2	0.72	24	700	1	0.10					30	355	10,138	200	21		*	0.06	
		12	I-40 RAMPS EXIT 112	I-40 TO SR 1744 OR SR 1744 TO I-40	2 & 4	1.17	24, 36	700	1	0.10					30	637	18,203	200	37		*	0.06	
		13	I-40 RAMPS EXIT 113	I-40 TO SR 1001 OR SR 1001 TO I-40	2 & 4	0.99	24, 36	700	1	0.10					30	527	15,054	200	31		*	0.06	
		14	I-40 RAMPS EXIT 116	I-40 TO SR 1761 OR SR 1761 TO I-40	2 & 6	1.16	24	700	1	0.10					30	572	16,333	200	33		*	0.06	
		15	I-40 RAMPS EXIT 118	I-40 TO SR 1761 OR SR 1761 TO I-40	2 & 4	1.18	24, 36	700	1	0.10					30	615	17,611	200	36		*	0.06	
		16	I-40 RAMPS EXIT 119, RAMPS ON WESTSIDE OF BRIDGE ONLY	I-40 TO SR 1002 OR SR 1002 TO I-40	2 & 7	0.79	20, 24, 26	700	1	0.10					30	386	11,007	150	22		*	0.06	
TOTAL FOR PROJ NO. I-0910A						61.42		109,800	214	5.40	1,101	363,488	30,826	1,850	7,100	31,466	899,104	506,040	1,825	950	1	3.26	330
GRAND TOTAL						61.42		109,800	214	5.40	1,101	363,488	30,826	1,850	7,100	31,466	899,104	506,040	1,825	950	1	3.26	330

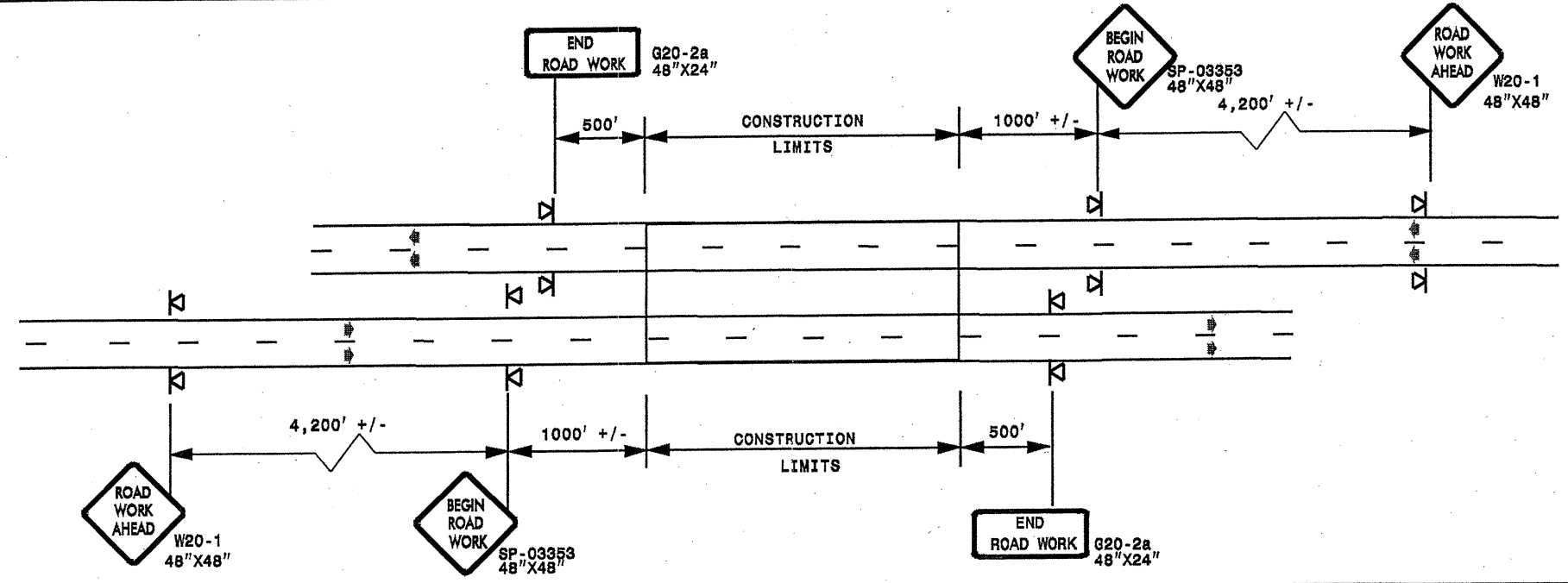
All 4 ramps are to be resurfaced on Maps 2 - 15. Only 3 ramps on Map 16 will be resurfaced.

THERMOPLASTIC AND PAINT QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	4405000000-E	4415000000-E	4420000000-N	4430000000-E	4480000000-N	4847100000-E			4725000000-E					4815000000-E		4825000000-E	4835000000-E	4845000000-N								
					PORTABLE WORK ZONE SIGN	FLASHING ARROW PANELS, TYPE C	CHANGEABLE MESSAGE SIGN	DRUMS	TRUCK MTD. IMPACT ATTENUATOR (60 MPH)	6" WHITE POLYUREA	6" YELLOW POLYUREA	12" WHITE POLYUREA	24" X 120 M WHITE THERMO	THERMO STR ARROW 90 M	THERMO LT ARROW 90 M	THERMO RT ARROW 90 M	THERMO STR & RT ARROW 90 M	THERMO STR & LT ARROW 90 M	THERMO LT STR RT ARROW 90 M	6" YELLOW PAINT	6" WHITE PAINT	12" WHITE PAINT	24" WHITE PAINT	PAINT STR ARROW	PAINT LT ARROW	PAINT RT ARROW	PAINT STR & RT ARROW	PAINT STR & LT ARROW	PAINT LT STR RT ARROW	
					SF	EA	EA	EA	EA	LF	LF	LF	LF	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA
TBD	Burke	1	I-40 EBL	FROM MP 95.17 TO MP 119	21,856	4	4	500	2	159,148	125,822	11,951																		
		2	I-40 WBL	FROM MP 119 TO MP 95.17						159,268	125,822	11,951																		
		3	I-40 RAMPS EXIT 96	I-40 TO SR 1138 OR SR 1138 TO I-40						5,438	5,438				3															
		4	I-40 RAMPS EXIT 98	I-40 TO SR 1147 OR SR 1147 TO I-40						4,699	4,699				3															
		5	I-40 RAMPS EXIT 100	I-40 TO SR 1142 OR SR 1142 TO I-40						5,174	5,174		200		6	7	2	5												
		6	I-40 RAMPS EXIT 103	I-40 TO US 64 OR US 64 TO I-40						5,808	5,808		30		6		9		9											
		7	I-40 RAMPS EXIT 104	I-40 TO SR 1922 OR SR 1922 TO I-40						5,914	5,914		160		3	6	4	6	4											
		8	I-40 RAMPS EXIT 105	I-40 TO NC 18 OR NC 18 TO I-40						5,174	5,174		40		6	3	3													
		9	I-40 RAMPS EXIT 106	I-40 TO SR 1704 OR SR 1704 TO I-40						5,174	5,174				7	1														
		10	I-40 RAMPS EXIT 107	I-40 TO NC 114 OR NC 114 TO I-40						3,538	3,538				6															
		11	I-40 RAMPS EXIT 111	I-40 TO SR'S OR SR'S TO I-40						3,802	3,802				6															
		12	I-40 RAMPS EXIT 112	I-40 TO SR 1744 OR SR 1744 TO I-40						6,178	6,178				4															
		13	I-40 RAMPS EXIT 113	I-40 TO SR 1001 OR SR 1001 TO I-40						5,227	5,227		80		4	3		3												
		14	I-40 RAMPS EXIT 116	I-40 TO SR 1761 OR SR 1761 TO I-40						6,125	6,125		20		4				3								3			
		15	I-40 RAMPS EXIT 118	I-40 TO SR 1761 OR SR 1761 TO I-40						6,230	6,230				4															
		16	I-40 RAMPS EXIT 119, RAMPS ON WESTSIDE OF BRIDGE ONLY	I-40 TO SR 1002 OR SR 1002 TO I-40						4,171	4,171		30		3					3							3			
TOTAL FOR PROJ NO. I-0910A					21,856	4	4	500	2	391,068	324,296	23,902	610		65	20	18	14	13	6	324,297	391,068	23,902	610	65	20	18	14	13	6
GRAND TOTAL					21,856	4	4	500	2	391,068	324,296	23,902	610		65	20	18	14	13	6	324,297	391,068	23,902	610	65	20	18	14	13	6

ADVANCE WORK ZONE WARNING SIGNING FOR FREEWAYS (4 LANES OR GREATER)

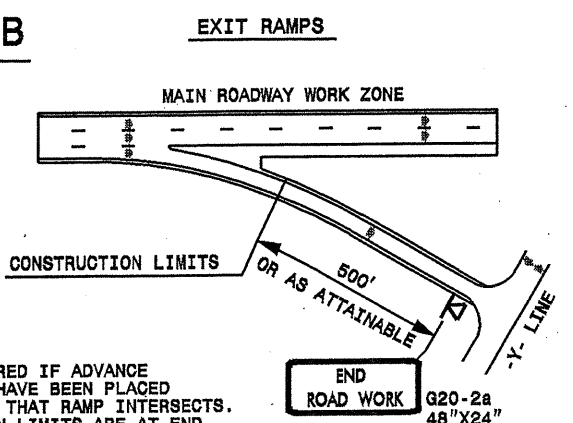
DETAIL A



STATE OF NORTH CAROLINA
DEPT. OF TRANSPORTATION
DIVISION OF HIGHWAYS
RALEIGH, N.C.

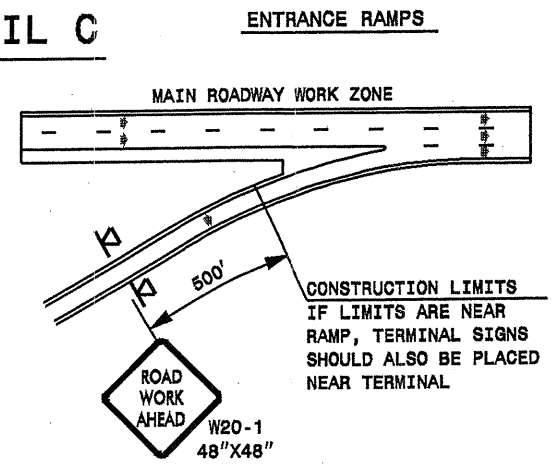
ROADWAYS INTERSECTING ALONG FREEWAY WORK ZONE (Y-LINES)

DETAIL B



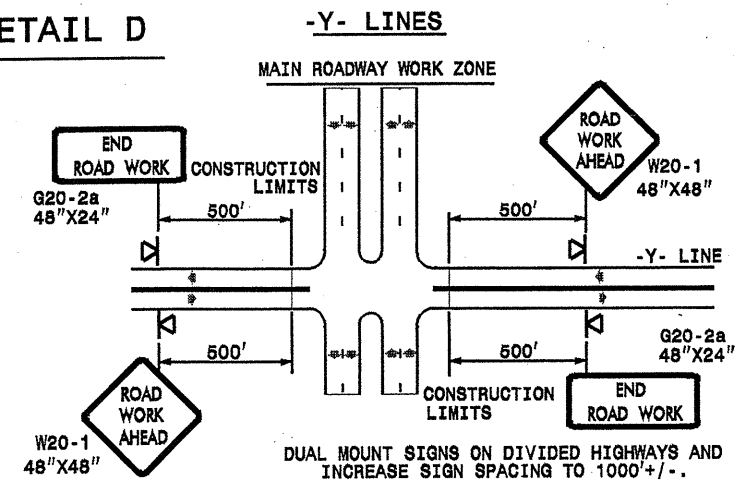
NOTE:
SIGN NOT REQUIRED IF ADVANCE WARNING SIGNS HAVE BEEN PLACED ALONG -Y- LINE THAT RAMP INTERSECTS. IF CONSTRUCTION LIMITS ARE AT END OF RAMP, PLACE SIGN AT END OF RAMP.

DETAIL C



CONSTRUCTION LIMITS IF LIMITS ARE NEAR RAMP, TERMINAL SIGNS SHOULD ALSO BE PLACED NEAR TERMINAL.

DETAIL D



DUAL MOUNT SIGNS ON DIVIDED HIGHWAYS AND INCREASE SIGN SPACING TO 1000'+/-.

DETAIL DRAWING
FOR FREEWAYS
WORK ZONE WARNING SIGNS
(SHORT-DURATION LANE CLOSURES)

GENERAL NOTES

- USE FLUORESCENT ORANGE SHEETING (TYPE VII OR HIGHER) ON ALL ADVANCE WORK ZONE SIGNS.
- DO NOT INSTALL ADVANCE WARNING SIGNS MORE THAN 3 DAYS PRIOR TO BEGINNING OF WORK.
- ALL SIGN SPACING DIMENSIONS ARE APPROXIMATE, FIELD ADJUST AS NECESSARY OR AS DIRECTED.
- USE PORTABLE WORK ZONE SIGNS ONLY WITH PORTABLE WORK ZONE SIGN STANDS SPECIFICALLY DESIGNED FOR ONE ANOTHER. PORTABLE WORK ZONE SIGNS MAY BE ROLL UP OR APPROVED COMPOSITE.
- PROVIDE PORTABLE WORK ZONE SIGN STANDS, PORTABLE SIGNS AND SIGN SHEETING WHICH ARE LISTED ON THE NORTH CAROLINA DEPARTMENT OF TRANSPORTATION'S APPROVED PRODUCT LIST OR ACCEPTED AS TRAFFIC QUALIFIED BY THE TRAFFIC CONTROL UNIT.
- ** TWO-WAY UNDIVIDED ADVANCE WARNING SIGN CONFIGURATION MAY BE USED ON MULTI-LANE FACILITIES WHERE CONDITIONS LIMIT THE USE OF DUAL MOUNTED SIGNS AS DETERMINED BY THE ENGINEER.

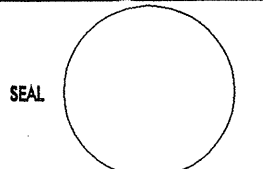
LEGEND

◀ PORTABLE SIGN

➔ DIRECTION OF TRAFFIC FLOW

SHEET 1 OF 1

25-MAR-2009 15:04 s:\signing\resurfacing\030509\resurfacing\030509.dwg 030509\resurfacing\030509.dwg 030509\resurfacing\030509.dwg 030509\resurfacing\030509.dwg 030509\resurfacing\030509.dwg 030509\resurfacing\030509.dwg 030509\resurfacing\030509.dwg 030509\resurfacing\030509.dwg 030509\resurfacing\030509.dwg 030509\resurfacing\030509.dwg

APPROVED: _____	DATE: _____	DETAIL DRAWING FOR FREEWAYS WORK ZONE WARNING SIGNS	
			
SCALE: NONE	REVISIONS	7-98	10/01
DATE: _____	10-98	03/04	
DWG. BY: _____	01/01	11/04	
DESIGN BY: _____			
REVIEWED BY: _____			