

STATE OF NORTH CAROLINA
DIVISION OF HIGHWAYS

STATE PROJECT REFERENCE NO. SHEET NO.

U-3462

TCP-1

**PLAN FOR PROPOSED
TRAFFIC CONTROL, MARKING & DELINEATION
BRUNSWICK COUNTY**

LEGEND

ROADWAY STANDARD DRAWINGS

THE FOLLOWING ROADWAY STANDARDS AS APPEAR IN "ROADWAY STANDARD DRAWINGS" - ROADWAY DESIGN UNIT-N.C. DEPARTMENT OF TRANSPORTATION-RALEIGH, N.C., DATED JULY 2006 ARE APPLICABLE TO THIS PROJECT AND BY REFERENCE HEREBY ARE CONSIDERED A PART OF THESE PLANS:

STD. NO.	TITLE
1101.02	TEMPORARY LANE CLOSURES
1101.03	TEMPORARY ROAD CLOSURES
1101.04	TEMPORARY SHOULDER CLOSURES
1101.05	WORK ZONE VEHICLE ACCESSES
1101.11	TRAFFIC CONTROL DESIGN TABLES
1110.01	STATIONARY WORK ZONE SIGNS
1110.02	PORTABLE SIGNS
1115.01	FLASHING ARROW PANELS
1130.01	DRUMS
1135.01	CONES
1145.01	BARRICADES
1150.01	FLAGGERS
1160.01	TEMPORARY CRASH CUSHION
1165.01	TRUCK MOUNTED IMPACT ATTENUATOR
1180.01	SKINNY-DRUM
1205.01	PAVEMENT MARKINGS - LINE TYPES & OFFSETS
1205.02	PAVEMENT MARKINGS - 2 LANE & MULTILANE ROADWAYS
1205.04	PAVEMENT MARKINGS - INTERSECTIONS
1205.05	PAVEMENT MARKINGS - TURN LANES
1205.06	PAVEMENT MARKINGS - THRU LANE DROPS
1205.07	PAVEMENT MARKINGS - PEDESTRIAN CROSSWALKS
1205.08	PAVEMENT MARKINGS - SYMBOLS & WORD MESSAGES
1205.09	PAVEMENT MARKINGS - PAINTED ISLANDS
1250.01	PAVEMENT MARKER SPACING
1251.01	RAISED PAVEMENT MARKERS (PERMANENT & TEMPORARY)
1253.01	SNOWPLOWABLE RAISED PAVEMENT MARKERS
1261.01	GUARDRAIL & BARRIER DELINEATOR SPACING
1261.02	GUARDRAIL & BARRIER DELINEATOR TYPES
1262.01	GUARDRAIL END DELINEATION

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GENERAL

- DIRECTION OF TRAFFIC FLOW
- NORTH ARROW
- PROPOSED PVMT. EXIST. PVMT.
- PROPOSED CONSTRUCTION
- REMOVAL OF EXISTING PAVEMENT

TRAFFIC CONTROL DEVICES

- TYPE III BARRICADE
- CONE
- DRUM
- FLASHING ARROW PANEL (TYPE C)
- STATIONARY SIGN
- PORTABLE SIGN
- STATIONARY OR PORTABLE SIGN
- WARNING FLAGS
- CRASH CUSHION
- CHANGEABLE MESSAGE SIGN
- TRUCK MOUNTED IMPACT ATTENUATOR (TMIA)
- POLICE
- FLAGGER

PAVEMENT MARKINGS

- CRYSTAL/CRYSTAL PAVEMENT MARKER
- YELLOW/YELLOW PAVEMENT MARKER
- CRYSTAL/RED PAVEMENT MARKER
- PAVEMENT MARKING SYMBOLS

U-3462

TIP PROJECT:

PLAN REVIEWED BY: WORK ZONE TRAFFIC CONTROL UNIT J. S. BOURNE, P.E. TRAFFIC CONTROL ENGINEER G. L. GETTIER, P.E. TRAFFIC CONTROL PROJECT ENGINEER J. W. GILSTRAP TRAFFIC CONTROL PROJ. DESIGN ENGINEER TRAFFIC CONTROL DESIGN ENGINEER TRAFFIC CONTROL DESIGN TECHNICIAN		APPROVED: <i>Michael T. Rzepka</i> DATE: 8-27-08 SEAL 	PLAN PREPARED FOR N.C.D.O.T. BY: M. T. RZEPKA, P.E. PROJECT ENGINEER G. E. PARKER DESIGN ENGINEER B. L. MARIOTTE DESIGN TECHNICIAN	 KO & ASSOCIATES, P.C. Consulting Engineers 614 S. WILSON WAY, SUITE 100, RALEIGH, N.C. 27607 (919) 885-0000
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PROJECT NOTES

GENERAL NOTES

CHANGES MAY BE REQUIRED WHEN PHYSICAL DIMENSIONS IN THE DETAIL DRAWINGS, STANDARD DETAILS AND ROADWAY DETAILS ARE NOT ATTAINABLE TO MEET FIELD CONDITIONS, OR RESULT IN DUPLICATE, OR UNDESIRE OVERLAPPING OF DEVICES. MODIFICATION MAY INCLUDE: MOVING, SUPPLEMENTING, COVERING OR REMOVAL OF DEVICES, AS DIRECTED BY THE ENGINEER.

THE FOLLOWING GENERAL NOTES APPLY AT ALL TIMES FOR THE DURATION OF THE CONSTRUCTION PROJECT, EXCEPT WHEN OTHERWISE NOTED IN THE PLAN, OR DIRECTED BY THE ENGINEER.

TIME RESTRICTIONS

A) DO NOT CLOSE OR NARROW TRAVEL LANES AS FOLLOWS:

<u>ROAD NAME</u>	<u>DAY AND TIME RESTRICTIONS</u>
SR 1357, SMITH AVE.	7:00am - 7:00pm MONDAY THRU SUNDAY
US 17 BUS.	7:00am - 7:00pm MONDAY THRU SUNDAY
NC 130, HOLDEN BEACH RD.	7:00am - 7:00pm MONDAY THRU SUNDAY

B) DO NOT CLOSE OR NARROW TRAVEL LANES DURING HOLIDAYS AND SPECIAL EVENTS AS FOLLOWS:

<u>ROAD NAME</u>
SR 1357, SMITH AVE.
US 17 BUS.
NC 130, HOLDEN BEACH RD.

HOLIDAY

1. FOR ANY UNEXPECTED OCCURRENCE THAT CREATES UNUSUALLY HIGH TRAFFIC VOLUMES, AS DIRECTED BY THE ENGINEER.
2. FOR NEW YEAR'S, BETWEEN THE HOURS OF 7:00 P.M. DECEMBER 31st TO 7:00 A.M. JANUARY 2ND. IF NEW YEAR'S DAY IS ON A FRIDAY, SATURDAY, SUNDAY, OR MONDAY THEN UNTIL 7:00 A.M. THE FOLLOWING TUESDAY.
3. FOR EASTER, BETWEEN THE HOURS OF 7:00 P.M. FRIDAY AND 7:00 A.M. MONDAY.
4. FOR MEMORIAL DAY, BETWEEN THE HOURS OF 7:00 P.M. FRIDAY TO 7:00 A.M. TUESDAY.
5. FOR INDEPENDENCE DAY, BETWEEN THE HOURS OF 7:00 P.M. THE DAY BEFORE INDEPENDENCE DAY AND 7:00 A.M. THE DAY AFTER INDEPENDENCE DAY.

IF INDEPENDENCE DAY IS ON A FRIDAY, SATURDAY, SUNDAY OR MONDAY THEN BETWEEN THE HOURS OF 7:00 P.M. THE THURSDAY BEFORE INDEPENDENCE DAY AND 7:00 A.M. THE TUESDAY AFTER INDEPENDENCE DAY.
6. FOR LABOR DAY, BETWEEN THE HOURS OF 7:00 P.M. FRIDAY AND 7:00 A.M. TUESDAY.
7. FOR THANKSGIVING DAY, BETWEEN THE HOURS OF 7:00 P.M. TUESDAY TO 7:00 A.M. MONDAY.
8. FOR CHRISTMAS, BETWEEN THE HOURS OF 7:00 P.M. THE FRIDAY BEFORE THE WEEK OF CHRISTMAS DAY AND 7:00 A.M. THE FOLLOWING TUESDAY AFTER THE WEEK OF CHRISTMAS.
9. FOR THE CHRISTMAS PARADE OCCURRING AT DOWNTOWN SHALLOTTE BETWEEN 4 HOURS BEFORE THE START AND 4 HOURS AFTER THE END OF THE THE CHRISTMAS PARADE .

C) DO NOT STOP TRAFFIC AS FOLLOWS:

<u>ROAD NAME</u>	<u>DAY AND TIME RESTRICTIONS</u>	<u>DURATION AND OPERATION</u>
SR 1357, SMITH AVE.	7:00am - 7:00pm MONDAY THRU SUNDAY	15 MIN. / TRAFFIC OPERATIONS
US 17 BUS.	7:00am - 7:00pm MONDAY THRU SUNDAY	15 MIN. / TRAFFIC OPERATIONS
NC 130, HOLDEN BEACH RD.	7:00am - 7:00pm MONDAY THRU SUNDAY	15 MIN. / TRAFFIC OPERATIONS

D) DO NOT CONDUCT MULTI-VEHICLE HAULING AS FOLLOWS:

<u>ROAD NAME</u>	<u>DAY AND TIME RESTRICTIONS</u>
US 17 BUS.	7:00am - 7:00pm MONDAY THRU SUNDAY

E) DO NOT CONDUCT ANY HAULING OPERATIONS AGAINST THE FLOW OF TRAFFIC OF AN OPEN TRAVELWAY UNLESS THE HAULING OPERATION IS PROTECTED BY BARRIER OR GUARDRAIL OR AS DIRECTED BY THE ENGINEER.

LANE AND SHOULDER CLOSURE REQUIREMENTS

F) REMOVE LANE CLOSURE DEVICES FROM THE LANE WHEN WORK IS NOT BEING PERFORMED BEHIND THE LANE CLOSURE OR WHEN A LANE CLOSURE IS NO LONGER NEEDED, OR AS DIRECTED BY THE ENGINEER.

G) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN 15 FT OF AN AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN SHOULDER USING ROADWAY STANDARD DRAWING NO. 1101.04 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL OR A LANE CLOSURE IS INSTALLED.



H) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING ON THE SHOULDER ADJACENT TO AN UNDIVIDED FACILITY AND WITHIN 5 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN TRAVEL LANE USING ROADWAY STANDARD DRAWING NO. 1101.02 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.

WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING ON THE SHOULDER ADJACENT TO A DIVIDED FACILITY AND WITHIN 10 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN TRAVEL LANE USING ROADWAY STANDARD DRAWING NO. 1101.02 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.

I) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN A LANE OF TRAVEL OF AN UNDIVIDED OR DIVIDED FACILITY, CLOSE THE LANE ACCORDING TO THE TRAFFIC CONTROL PLANS, ROADWAY STANDARD DRAWINGS OR AS DIRECTED BY THE ENGINEER. CONDUCT THE WORK SO THAT ALL PERSONNEL AND/OR EQUIPMENT REMAIN WITHIN THE CLOSED TRAVEL LANE.

J) DO NOT WORK SIMULTANEOUSLY WITHIN 15 FT ON BOTH SIDES OF AN OPEN TRAVELWAY, RAMP OR LOOP WITHIN THE SAME LOCATION UNLESS PROTECTED WITH GUARDRAIL OR BARRIER.

K) PROVIDE TRAFFIC CONTROL FOR APPROPRIATE LANE CLOSURES FOR SURVEYING DONE BY THE DEPARTMENT.

APPROVED: <i>Michael T. Keph</i> DATE: 8-26-08		PROJECT NOTES	
SEAL 	SCALE: NONE		REVISIONS
	DATE: 8-08		
	DWG. BY: BLM		
	DESIGN BY: BLM		
REVIEWED BY: MATR			

PROJECT NOTES

GENERAL NOTES (CONT'D)

PAVEMENT EDGE DROP OFF REQUIREMENTS

L) BACKFILL AT A 6:1 SLOPE UP TO THE EDGE AND ELEVATION OF EXISTING PAVEMENT IN AREAS ADJACENT TO AN OPENED TRAVEL LANE THAT HAS AN EDGE OF PAVEMENT DROP-OFF AS FOLLOWS:

BACKFILL DROP-OFFS THAT EXCEED 2 INCHES ON ROADWAYS WITH POSTED SPEED LIMITS OF 45 MPH OR GREATER.

BACKFILL DROP-OFFS THAT EXCEED 3 INCHES ON ROADWAYS WITH POSTED SPEED LIMITS LESS THAN 45 MPH.

BACKFILL WITH SUITABLE COMPACTED MATERIAL, AS APPROVED BY THE ENGINEER, AT NO EXPENSE TO THE DEPARTMENT.

M) DO NOT EXCEED A DIFFERENCE OF 2 INCHES IN ELEVATION BETWEEN OPEN LANES OF TRAFFIC FOR NOMINAL LIFTS OF 1.5 INCHES. INSTALL ADVANCE WARNING "UNEVEN LANES" SIGNS (W8-11) 200' IN ADVANCE AND A MINIMUM OF EVERY HALF MILE THROUGHOUT THE UNEVEN AREA.

TRAFFIC PATTERN ALTERATIONS

N) NOTIFY THE ENGINEER TWENTY ONE (21) CALENDAR DAYS PRIOR TO ANY TRAFFIC PATTERN ALTERATION.

SIGNING

O) INSTALL ADVANCE WORK ZONE WARNING SIGNS WHEN WORK IS WITHIN 40 FT FROM THE EDGE OF TRAVEL LANE AND NO MORE THAN THREE (3) DAYS PRIOR TO THE BEGINNING OF CONSTRUCTION.

P) PROVIDE PERMANENT SIGNING.

Q) STATE FORCES WILL BE RESPONSIBLE FOR DETOUR SIGNING OFF THE PROJECT LIMITS.

R) STATE FORCES WILL COVER OR REMOVE ALL DETOUR SIGNS OFF THE PROJECT LIMITS WHEN A DETOUR IS NOT IN OPERATION.

S) ENSURE ALL NECESSARY SIGNING IS IN PLACE PRIOR TO ALTERING ANY TRAFFIC PATTERN.

TRAFFIC BARRIER

T) INSTALL TEMPORARY BARRIER ACCORDING TO THE TRAFFIC CONTROL PLANS A MAXIMUM OF TWO (2) WEEKS PRIOR TO BEGINNING WORK IN ANY LOCATION. ONCE TEMPORARY BARRIER IS INSTALLED AT ANY LOCATION, PROCEED IN A CONTINUOUS MANNER TO COMPLETE THE PROPOSED WORK IN THAT LOCATION UNLESS OTHERWISE STATED IN THE TRAFFIC CONTROL PLANS OR AS DIRECTED BY THE ENGINEER.

DO NOT PLACE BARRIER DIRECTLY ON ANY SURFACE OTHER THAN ASPHALT OR CONCRETE.

ONCE TEMPORARY BARRIER IS INSTALLED AT ANY LOCATION AND NO WORK IS PERFORMED BEHIND THE TEMPORARY BARRIER FOR A PERIOD LONGER THAN TWO (2) MONTHS, REMOVE/RESET TEMPORARY BARRIER AT NO COST TO THE DEPARTMENT UNLESS OTHERWISE STATED IN THE TRAFFIC CONTROL PLANS, TEMPORARY BARRIER IS PROTECTING A HAZARD, OR AS DIRECTED BY THE ENGINEER.

INSTALL TEMPORARY BARRIER WITH THE TRAFFIC FLOW, BEGINNING WITH THE UPSTREAM SIDE OF TRAFFIC. REMOVE TEMPORARY BARRIER AGAINST THE TRAFFIC FLOW, BEGINNING WITH THE DOWNSTREAM SIDE OF TRAFFIC.

INSTALL AND SPACE DRUMS NO GREATER THAN TWICE THE POSTED SPEED LIMIT (MPH) TO CLOSE OR KEEP THE SECTION OF THE ROADWAY CLOSED UNTIL THE TEMPORARY BARRIER CAN BE PLACED OR AFTER THE TEMPORARY BARRIER IS REMOVED.

TRAFFIC CONTROL DEVICES

U) SPACE CHANNELIZING DEVICES IN WORK AREAS NO GREATER THAN TWICE THE POSTED SPEED LIMIT (MPH), EXCEPT 10 FT ON-CENTER IN RADII, AND 3 FT OFF THE EDGE OF AN OPEN TRAVELWAY, WHEN LANE CLOSURES ARE NOT IN EFFECT. WHEN SKINNY DRUMS ARE ALLOWED, REFER TO SECTION 1180 OF STANDARD SPECIFICATIONS FOR ROADS AND STRUCTURES OR AS SHOWN IN THE PLANS.

V) PLACE TYPE III BARRICADES, WITH "ROAD CLOSED" SIGN R11-2 ATTACHED, OF SUFFICIENT LENGTH TO CLOSE ENTIRE ROADWAY.

W) PLACE ADDITIONAL SETS OF THREE CHANNELIZING DEVICES DRUMS PERPENDICULAR TO THE EDGE OF TRAVELWAY ON 500 FT CENTERS WHEN UNOPENED LANES ARE CLOSED TO TRAFFIC.

PAVEMENT MARKINGS AND MARKERS

X) INSTALL PAVEMENT MARKINGS AND PAVEMENT MARKERS ON THE FINAL SURFACE AS FOLLOWS:

ROAD NAME	MARKING	MARKER
ALL	THERMOPLASTIC	SNOWPLOWABLE/ PERMANENT RAISED

Y) INSTALL TEMPORARY PAVEMENT MARKINGS AND TEMPORARY PAVEMENT MARKERS ON INTERIM LAYERS OF PAVEMENT AS FOLLOWS:

ROAD NAME	MARKING	MARKER
ALL	PAINT	TEMPORARY RAISED

Z) PLACE ONE APPLICATION OF PAINT FOR TEMPORARY TRAFFIC PATTERNS. PLACE A SECOND APPLICATION OF PAINT SIX (6) MONTHS AFTER THE INITIAL APPLICATION AND EVERY SIX MONTHS AS DIRECTED BY THE ENGINEER.

AA) TIE PROPOSED PAVEMENT MARKING LINES TO EXISTING PAVEMENT MARKING LINES.

BB) REMOVE/REPLACE ANY CONFLICTING/DAMAGED PAVEMENT MARKINGS AND MARKERS BY THE END OF EACH DAY'S OPERATION.

CC) TRACE THE EXISTING AND PROPOSED MONOLITHIC ISLAND LOCATIONS WITH PROPER COLOR PAVEMENT MARKINGS PRIOR TO REMOVAL AND INSTALLATION. PLACE DRUMS OR TUBULAR MARKERS TO DELINEATE ANY EXISTING AND PROPOSED MONOLITHIC ISLANDS AFTER REMOVAL AND BEFORE INSTALLATION.

TEMPORARY / FINAL SIGNALS

DD) SHIFT AND REVISE ALL SIGNAL HEADS AS SHOWN ON THE SIGNAL PLANS.

MISCELLANEOUS

EE) POLICE MAY BE USED TO MAINTAIN TRAFFIC THROUGH THE WORK AREA AND/OR INTERSECTIONS, AS DIRECTED BY THE ENGINEER.

LOCAL NOTES

1) WHEN CONSTRUCTING DRAINAGE STRUCTURES ADJACENT TO TRAFFIC, INSTALL TEMPORARY STEEL PLATES, AS DIRECTED BY THE ENGINEER. MAY WORK EACH LOCATION INDEPENDENTLY OR CONCURRENTLY, UNLESS OTHERWISE DIRECTED BY THE ENGINEER. WORK IN A CONTINUOUS MANNER TO PERFORM THE WORK IN THE FOLLOWING SEQUENCE, STEPS 'A' THRU 'E'.

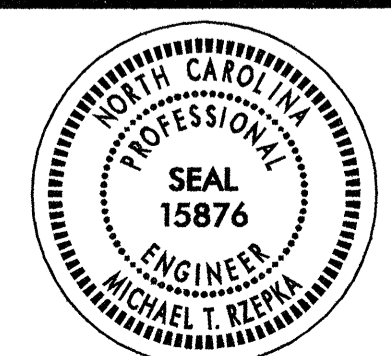



A: CLOSE THE APPROPRIATE TRAVEL LANE TO TRAFFIC USING ROADWAY STANDARD DRAWING NO. 1101.02, SHEET 1 OF 9.

B: CONSTRUCT PROPOSED STRUCTURE OR INSTALL PRE-CAST DRAINAGE STRUCTURE AS SHOWN IN CONSTRUCTION PLANS AND COVER WITH STEEL PLATES TO PROTECT STRUCTURE DURING CURING.

C: OPEN TRAVEL LANE TO EXISTING TRAFFIC PATTERN BY THE END OF EACH WORK PERIOD.

D: WHEN PROPERLY CURED, CLOSE THE APPROPRIATE TRAVEL LANE USING ROADWAY STANDARD DRAWING NO. 1101.02, SHEET 1 OF 9. BACKFILL & PAVE, IF REQUIRED, UP TO THE EDGE AND ELEVATION OF EXISTING PAVEMENT (SEE CONSTRUCTION PLANS).

E: OPEN TRAVEL LANE TO EXISTING TRAFFIC PATTERN BY THE END OF EACH WORK PERIOD.

APPROVED: <i>Michael T. Repp</i> DATE: 8-26-08 <div style="text-align: center;">  SEAL </div>	<h3 style="margin: 0;">PROJECT NOTES (CONT'D)</h3> <table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <td style="width: 30%;">SCALE: NONE</td> <td rowspan="5" style="width: 30%; text-align: center;">  </td> <td style="width: 40%; text-align: center;">REVISIONS</td> </tr> <tr> <td>DATE: 8-08</td> <td style="border: none;"></td> </tr> <tr> <td>DWG. BY: BLM</td> <td style="border: none;"></td> </tr> <tr> <td>DESIGN BY: BLM</td> <td style="border: none;"></td> </tr> <tr> <td>REVIEWED BY: MTR</td> <td style="border: none;"></td> </tr> </table>	SCALE: NONE		REVISIONS	DATE: 8-08		DWG. BY: BLM		DESIGN BY: BLM		REVIEWED BY: MTR	
SCALE: NONE		REVISIONS										
DATE: 8-08												
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PHASE I PHASING

(SEE OVERVIEW, SHEET TCP-5)

STEP 1: - INSTALL WORK ZONE SIGNS ALONG EXISTING SR 1357 (SMITH AVE.), US 17 BUS., NC 130 (HOLDEN BEACH RD.) AND ALL -Y- LINES AS SHOWN ON SHEET TCP-20.

- CONTRACTOR SHALL INSTALL CHANGEABLE MESSAGE SIGNS (CMS) ON THE PROJECT AS DIRECTED BY THE ENGINEER.

CONTRACTOR MAY WORK IN PHASE I, STEP 2 AND STEP 3 CONCURRENTLY OR SEPARATE AS DIRECTED BY THE ENGINEER.

STEP 2: - AWAY FROM TRAFFIC, BEGIN CONSTRUCTION UP TO BUT NOT INCLUDING FINAL LAYER OF SURFACE COURSE (INCLUDING CURB & GUTTER AND ALL DRAINAGE IN THE FOLLOWING LOCATIONS: (SEE ROADWAY PLANS AND PHASE I, DETAIL 2, SHEETS TCP-8 THRU TCP-10)

NOTE: CONTRACTOR SHALL INSTALL AND ACTIVATE PROPOSED SIGNAL AT THE INTERSECTION OF PROPOSED (-L-) SMITH AVE. EXTENSION AND (-CONN-) EDGEWATER DR. TO CONTROL HAULING OPERATIONS DURING CONSTRUCTION. (SEE SHEET TCP-10 AND SIGNAL PLANS)

FROM -L- STA. 33+00± TO -L- STA. 35+24± (RIGHT SIDE) (INCLUDING STAGE I OF PROPOSED CULVERT AT -L- STA. 33+75± UTILIZING TEMPORARY SHORING) (REFER TO TYPICAL SECTION C-C' ON SHEET TCP-12)
 FROM -L- STA. 36+09± TO -L- STA. 41+50± (FULL WIDTH) (INCLUDING DRIVE 2)
 FROM -L- STA. 41+50± TO -LREV.- STA. 47+56± (RIGHT SIDE)
 FROM -LREV.- STA. 47+56± TO -L- STA. 66+45± (FULL WIDTH)
 FROM -L- STA. 67+07± TO -L- STA. 74+50± (FULL WIDTH)
 FROM -CONN- STA. 24+75± TO -CONN- STA. 27+00± (FULL WIDTH)

NOTE: ACCESS TO PARCEL 15 FROM (-L-) SR 1357 (SMITH AVE.) SHALL BE MAINTAINED AT ALL TIMES DURING LIFE OF PROJECT AS DIRECTED BY ENGINEER.

NOTE: CONTRACTOR SHALL LEAVE A TEMPORARY OPENING IN THE PROPOSED MEDIAN AT -L- STA. 40+50± FOR ACCESS TO SHOPPING CENTER DURING PHASE II.

- USING ROADWAY STANDARD DRAWING NO 1101.02, SHEETS 1 & 2 OF 9:

- BEGIN CONSTRUCTION OF (-L-) SMITH AVE. EXTENSION FROM -L- STA. 74+50± TO -L- STA. 77+75± UP TO EXISTING NC 130 (HOLDING BEACH RD.) EDGE OF PAVEMENT ELEVATION. (SEE ROADWAY PLANS AND PHASE I, DETAIL 2, SHEET TCP-10)

- BEGIN CONSTRUCTION OF (-CONN-) EDGEWATER DR. FROM -CONN- STA. 20+50± TO -CONN- STA. 24+75± AND -DRIVE- FROM -DRIVE- STA. 10+00± TO -DRIVE- STA. 11+21± UP TO EXISTING NC 130 (HOLDING BEACH RD.) EDGE OF PAVEMENT ELEVATION. (SEE ROADWAY PLANS AND PHASE I, DETAIL 2, SHEET TCP-10)

- BEGIN CONSTRUCTION OF (-CONN-) EDGEWATER DR. FROM -CONN- STA. 27+00± TO -CONN- STA. 32+25± AND (-L-) SMITH AVE. EXTENSION FROM -L- STA. 66+45± TO -L- STA. 67+07± (FULL WIDTH) UP TO BUT NOT INCLUDING FINAL LAYER OF SURFACE COURSE. AT THE END OF EACH DAY'S OPERATION, PLACE INCIDENTAL STONE OR ASPHALT TO MAINTAIN ACCESS. USE DRUMS TO DELINEATE THE TRAVEL WAY DURING CONSTRUCTION ON INCIDENTAL STONE AND TEMPORARY PAVEMENT MARKINGS (PAINT) ON ASPHALT. (SEE ROADWAY PLANS AND PHASE I, DETAIL 2, SHEET TCP-10)

- CONSTRUCT 6' TEMPORARY WIDENING ON LEFT SIDE OF EXISTING (-L-) SR 1357 (SMITH AVE.) FROM -L- STA. 29+50± TO -L- STA. 33+00±. (SEE PHASE I, DETAIL 1, SHEET TCP-6)

- CONSTRUCT 4' TEMPORARY ASPHALT WIDENING AND PLACE WATER FILLED ALONG (-Y-) US 17 BUSINESS FROM DRIVE AT -Y-, STA. 15+33± TO -Y- STA. 16+50± AS SHOWN ON SHEET TCP-11.

COMPLETE THE WORK REQUIRED OF PHASE I, STEP 3 FROM 7:00PM THRU 7:00AM, ANY NIGHT (EXCEPT FRIDAY AND SATURADAY) THE CONTRACTOR ELECTS TO DO THE WORK. (SEE SPECIAL PROVISIONS AND LIQUIDATED DAMAGES)

STEP 3: - USING ROADWAY STANDARD DRAWING NO. 1101.03, SHEET 1 OF 9, CLOSE SR 1357, SMITH AVE. BETWEEN -L- STA. 23+00± AND -L- STA. 24+50±. INSTALL 48" CROSS PIPE AT -L- STA. 24+32±, RESTORE EXISTING PAVEMENT, REMOVE ALL TRAFFIC CONTROL DEVICES CLOSING SR 1357, SMITH AVE., AND RESTORE TRAFFIC TO A TWO-LANE, TWO-WAY TRAFFIC PATTERN.

STEP 4: - USING ROADWAY STANDARD DRAWING NO 1101.02, SHEETS 1, 2, 3, & 4 OF 9:

- PAVE/WEDGE EXISTING SR 1357 (SMITH AVE.) PAVEMENT AND CONSTRUCT PROPOSED WIDENING RIGHT OF EXISTING SR 1357 (SMITH AVE.) UP TO BUT NOT INCLUDING FINAL LAYER OF SURFACE COURSE (INCLUDING PROPOSED CURB & GUTTER AND PROPOSED DRAINAGE) FROM -L- STA. 16+00± TO -L- STA. 29+50±. (SEE ROADWAY PLANS AND PHASE I, DETAIL 2, SHEET TCP-7) (SEE LOCAL NOTE 1)

- PLACE TEMPORARY PAVEMENT MARKING LINES FROM -L- STA. 29+50± TO -L- STA. 33+00± AND WATER FILLED BARRIER ALONG RIGHT EDGE LINE FROM -L- STA. 30+00± TO -L- STA. 32+00± AS SHOWN ON PHASE I, DETAIL 2, SHEETS TCP-7 AND TCP-8. (REFER TO TYPICAL SECTION B-B' ON SHEET TCP-12)

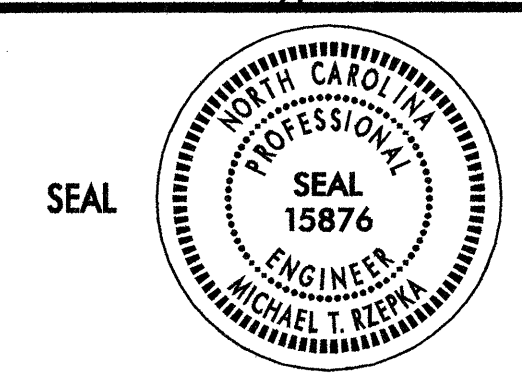
- BEHIND WATER FILLED BARRIER, CONSTRUCT PROPOSED WIDENING RIGHT OF EXISTING SR 1357 (SMITH AVE.) UP TO BUT NOT INCLUDING FINAL LAYER OF SURFACE COURSE (INCLUDING PROPOSED CURB & GUTTER AND PROPOSED DRAINAGE) FROM -L- STA. 29+50± TO -L- STA. 33+00±. (SEE ROADWAY PLANS AND PHASE I, DETAIL 2, SHEETS TCP-7 AND TCP-8)

- CONSTRUCT PROPOSED (-Y-) US 17 BUSINESS, ON BOTH SIDES (INCLUDING PROPOSED CULVERT EXTENSION UTILIZING WATER FILLED BARRIER & TEMPORARY SHORING, PROPOSED CURB & GUTTER AND DRAINAGE) FROM -Y- STA. 12+00± TO -Y- STA. 30+25± UP TO EDGE AND ELEVATION OF EXISTING PAVEMENT. (SEE ROADWAY PLANS AND PHASE I, DETAIL 2, SHEETS TCP-8 AND TCP-11) (SEE LOCAL NOTE 1)

- CONSTRUCT PROPOSED DRIVEWAY TO SHOPPING CENTER RIGHT OF -Y- STA. 23+95±. (SEE ROADWAY PLANS AND PHASE I, DETAIL 2, SHEET TCP-8)

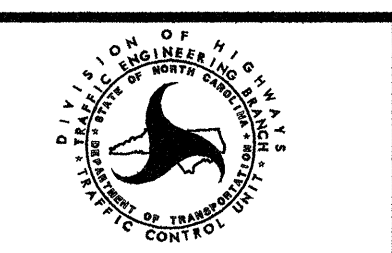
- INSTALL PROPOSED SIGNAL AT THE INTERSECTION OF PROPOSED (-L-) SR 1357 (SMITH AVE.) AND (-Y-) US 17 BUSINESS FOR TEMPORARY TRAFFIC PATTERN AS SHOWN ON SHEET TCP-16 AND SIGNAL PLANS.

APPROVED: *Michael T. Rieppel* DATE: 8-26-08

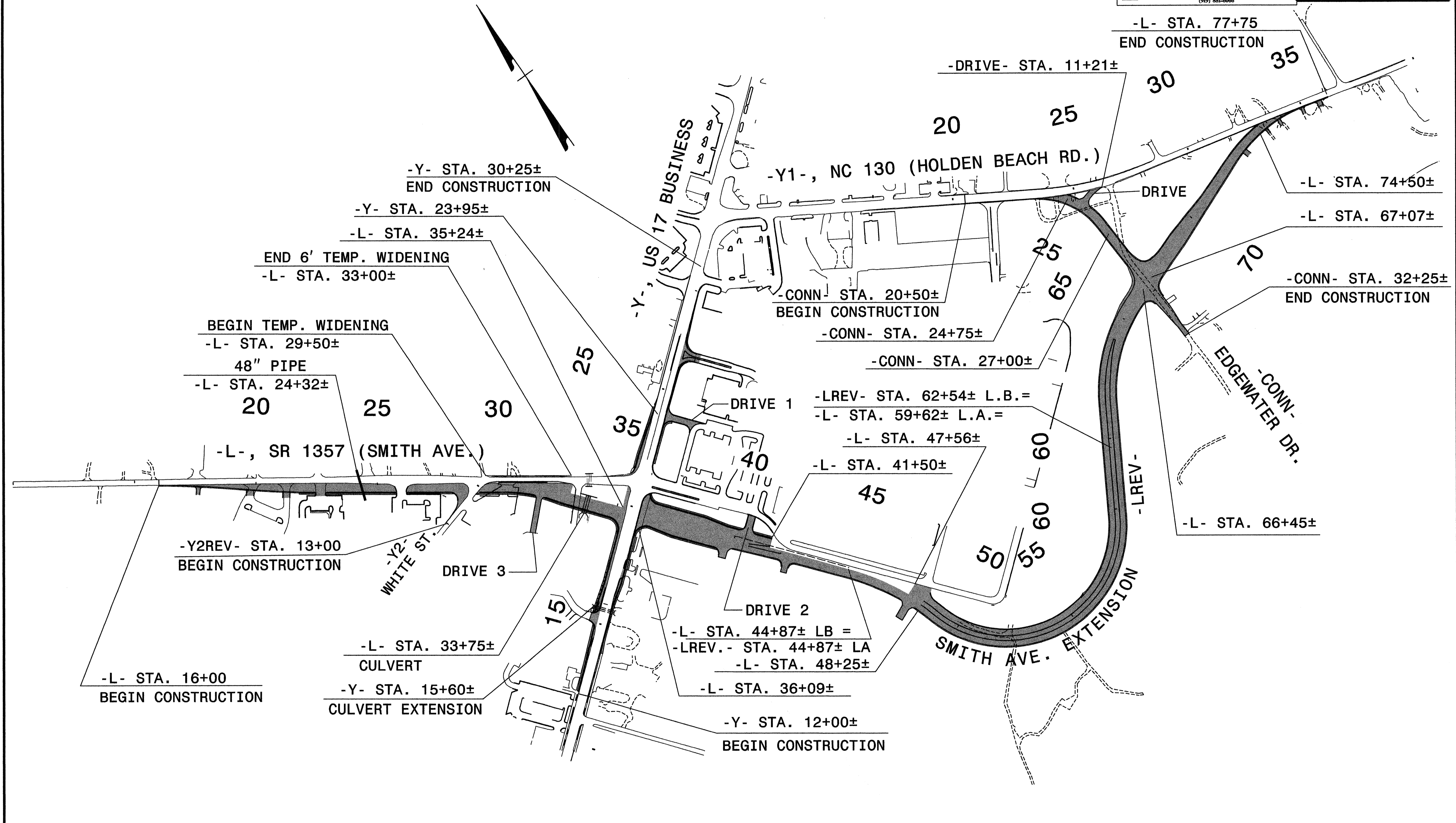


PHASE I PHASING

SCALE: NONE
 DATE: 8-08
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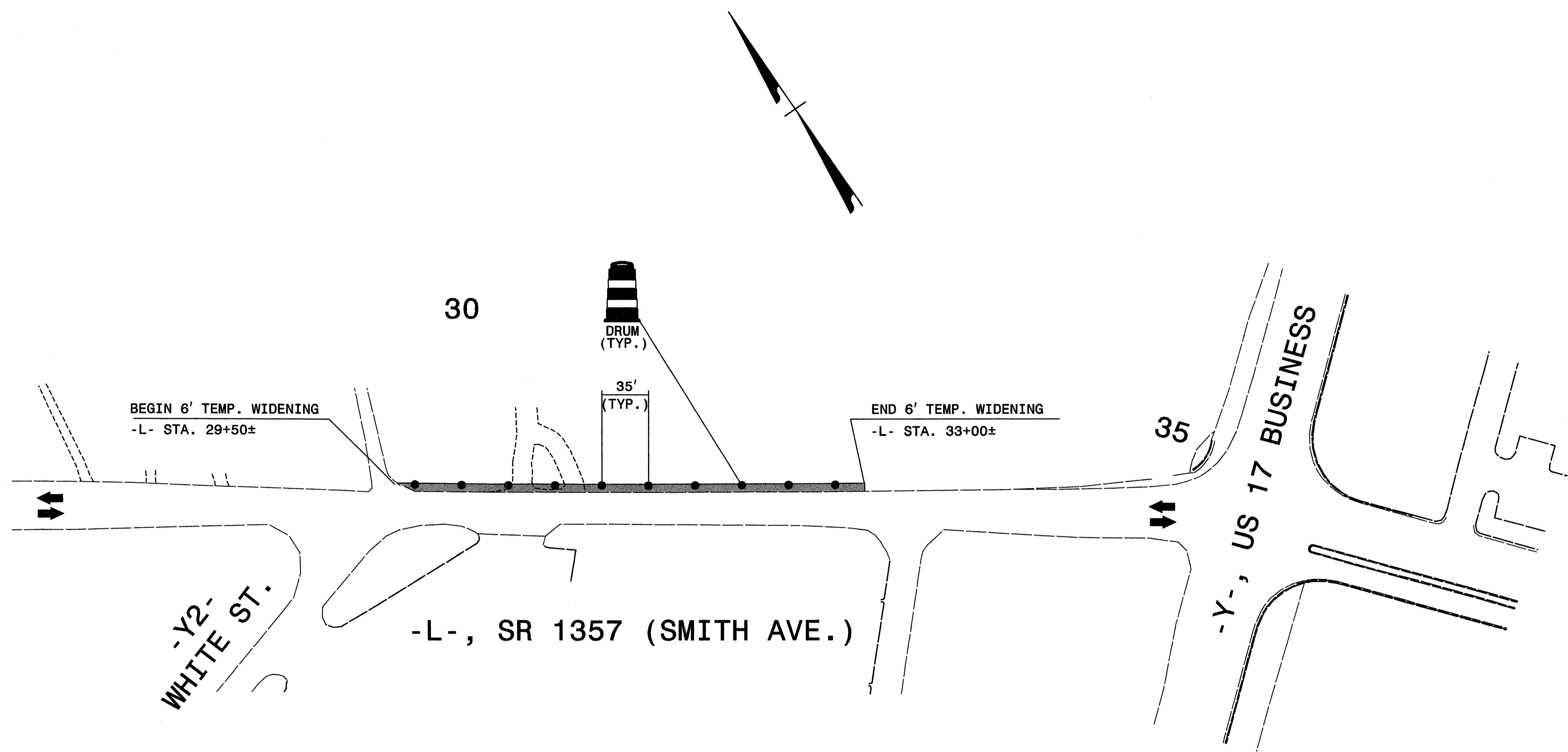
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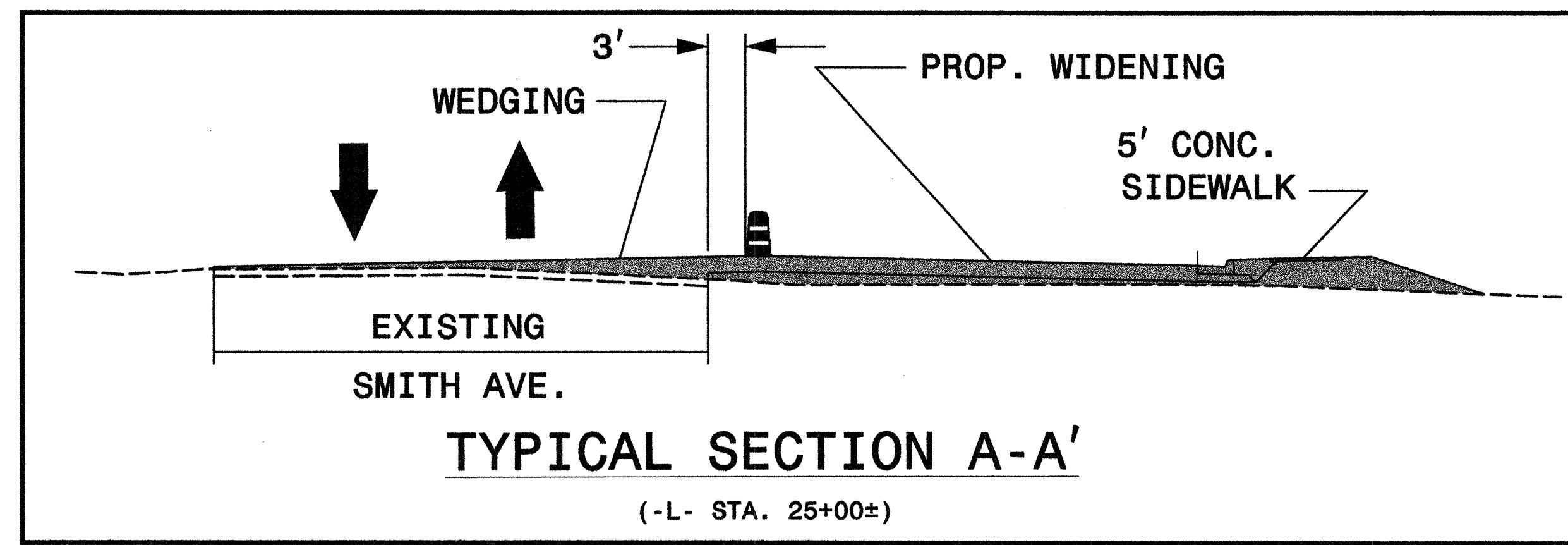
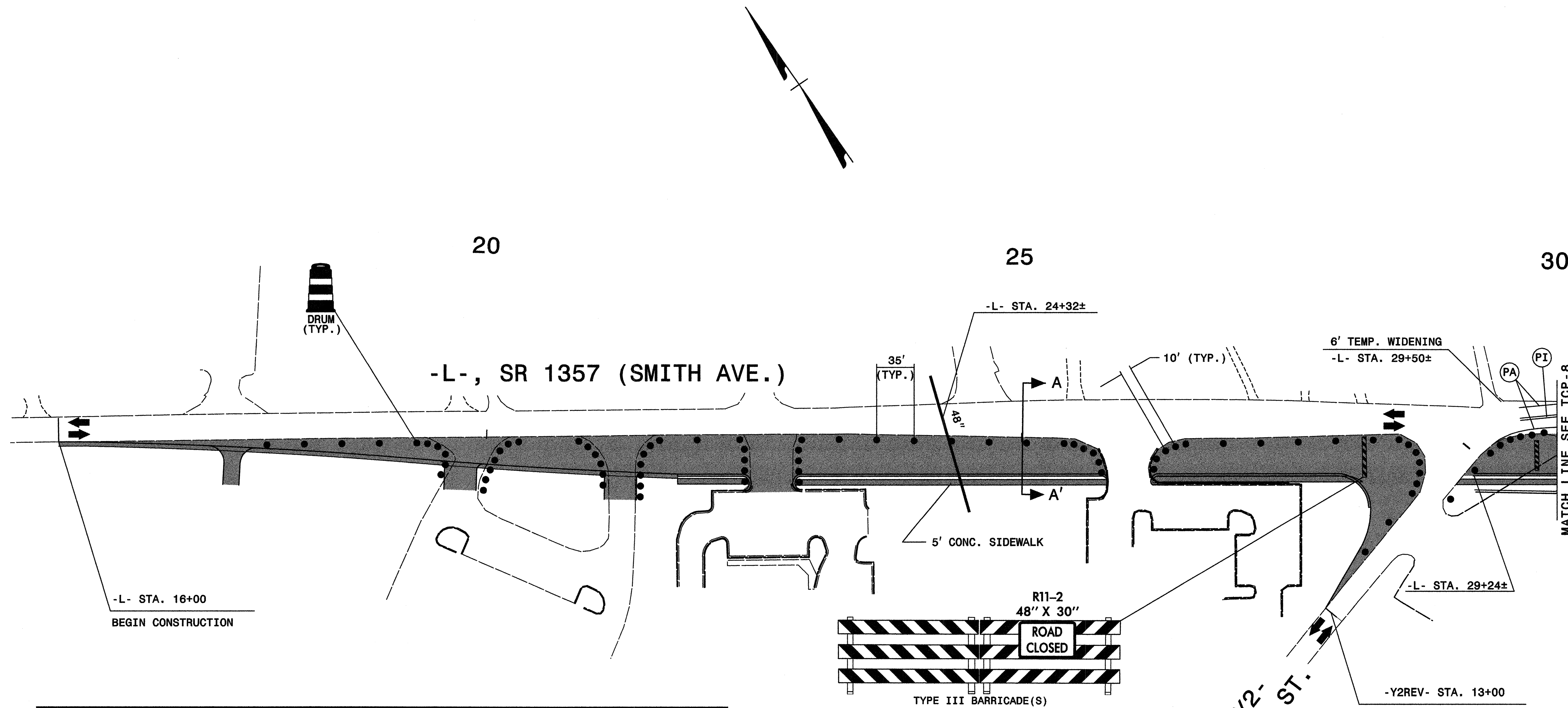
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APPROVED: <i>M. S. A.</i> DATE: 8-26-08				PHASE I OVERVIEW							
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DESIGN BY:	B.L.M.										
DESIGN BY:	G.E.P.										
REVIEWED BY:	M.S.A.										

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APPROVED: <i>Michael T. Kiepa</i> DATE: 8-26-08		PHASE I, DETAIL 1	
	SCALE: NONE		REVISIONS
	DATE: 8-08		
	DWG. BY: BLM		
	DESIGN BY: BLM		
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TEMPORARY PAVEMENT MARKING SCHEDULE	
PA	- WHITE EDGELINE
PI	- YELLOW DOUBLE CENTER LINE

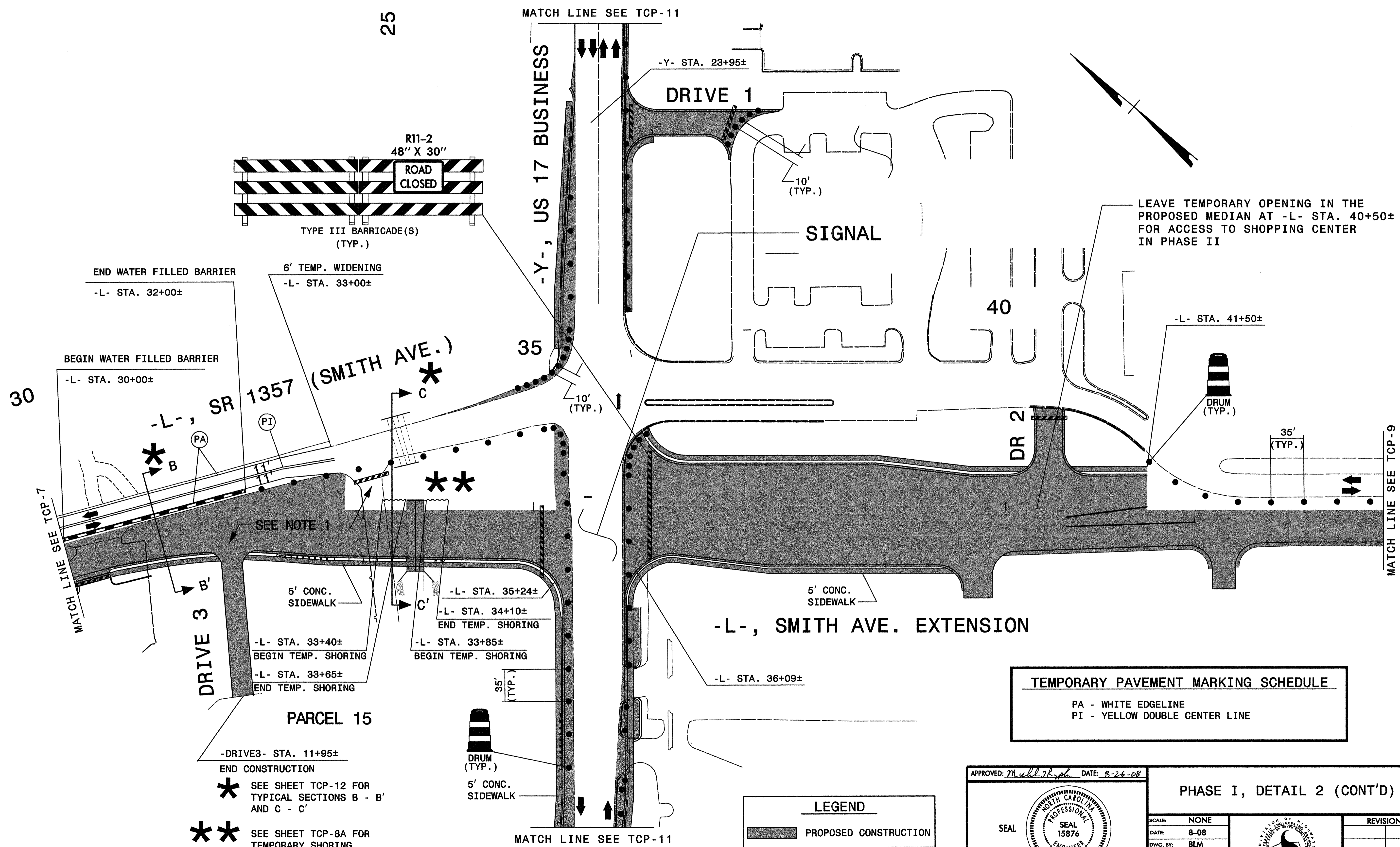
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APPROVED: *Michael T. Kephart* DATE: 9-24-08

SEAL

PHASE I, DETAIL 2		
SCALE: NONE		REVISIONS
DATE: 9-08		
DWG. BY: BLM		
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NOTE 1: ACCESS TO PARCEL 15 FROM (-L-) SR 1357 (SMITH AVE.) SHALL BE MAINTAINED AT ALL TIMES DURING LIFE OF PROJECT AS DIRECTED BY ENGINEER.



TEMPORARY PAVEMENT MARKING SCHEDULE

PA	- WHITE EDGELINE
PI	- YELLOW DOUBLE CENTER LINE

LEGEND

	PROPOSED CONSTRUCTION
--	-----------------------

APPROVED: *M. J. R. ph* DATE: 8-26-08

SEAL

PHASE I, DETAIL 2 (CONT'D)

SCALE:	NONE
DATE:	8-08
DWG. BY:	BLM
DESIGN BY:	BLM
REVIEWED BY:	MTR

NO.	REVISIONS

- * SEE SHEET TCP-12 FOR TYPICAL SECTIONS B - B' AND C - C'
- ** SEE SHEET TCP-8A FOR TEMPORARY SHORING INFORMATION.

Temporary Shoring No. 1

FOR TEMPORARY SHORING, SEE TEMPORARY SHORING SPECIAL PROVISION.

DO NOT USE STANDARD TEMPORARY SHORING FROM STATION 33 + 40 -L-, 9 FEET LEFT OF THE CENTER LINE, TO STATION 34 + 10 -L-, 9 FEET LEFT OF THE CENTER LINE. A TEMPORARY MSE WALL OR CONTRACTOR DESIGNED SHORING IS REQUIRED. SEE TEMPORARY SHORING SPECIAL PROVISION

WHEN USING CONTRACTOR DESIGNED SHORING FROM STATION 33 + 40 -L-, 9 FEET LEFT OF THE CENTER LINE, TO STATION 34 + 10 -L-, 9 FEET LEFT OF THE CENTER LINE, USE THE FOLLOWING SOIL PARAMETERS:

- UNIT WEIGHT OF SOIL ABOVE WATER TABLE, $\gamma = 120$ PCF
- UNIT WEIGHT OF SOIL BELOW WATER TABLE, $\gamma = 60$ PCF
- FRICTION ANGLE, $\phi = 30$ DEGREES
- COHESION, $c = 0$ PSF

FOR CONTRACTOR DESIGNED SHORING, SURVEY THE SHORING LOCATION TO DETERMINE EXISTING ELEVATIONS AND ACTUAL DESIGN HEIGHTS BEFORE BEGINNING DESIGN.

FOR PORTABLE CONCRETE BARRIERS AT THE TEMPORARY SHORING, USE AN NCDOT PORTABLE CONCRETE BARRIER (UNANCHORED OR ANCHORED) OR AN OREGON TALL F-SHAPE CONCRETE BARRIER IN ACCORDANCE WITH THE TRAFFIC CONTROL PLANS AND THE TEMPORARY SHORING SPECIAL PROVISION.

LIMITED SUBSURFACE INFORMATION IS AVAILABLE IN THE VICINITY OF THE TEMPORARY SHORING FROM STATION 33 + 40 -L-, 9 FEET LEFT OF THE CENTER LINE, TO STATION 34 + 10 -L-, 9 FEET LEFT OF THE CENTER LINE. THE INFORMATION PROVIDED FOR DESIGN WAS ASSUMED AND MAY NOT BE APPLICABLE TO THE ACTUAL SITE CONDITIONS ENCOUNTERED DURING CONSTRUCTION.

Temporary Shoring No. 2

FOR TEMPORARY SHORING, SEE TEMPORARY SHORING SPECIAL PROVISION.

IT MAY BE POSSIBLE TO USE A 1.5:1 (H:V) SLOPE OR FLATTER IN LIEU OF TEMPORARY SHORING FROM STATION 15 + 67 -Y-, 31 FEET LEFT OF THE CENTER LINE TO STATION 15 + 84 -Y-, 31 FEET LEFT OF THE CENTER LINE AS SHOWN ON THE PLANS.

DO NOT USE A TEMPORARY MSE WALL FROM STATION 15 + 42 -Y-, 47 FEET LEFT OF THE CENTER LINE TO STATION 15 + 84 -Y-, 31 FEET LEFT OF THE CENTER LINE. STANDARD TEMPORARY SHORING OR CONTRACTOR DESIGNED SHORING IS REQUIRED. SEE TEMPORARY SHORING SPECIAL PROVISION.


WHEN USING CONTRACTOR DESIGNED SHORING FROM STATION 15 + 42 -Y-, 47 FEET LEFT OF THE CENTER LINE TO STATION 15 + 84 -Y-, 31 FEET LEFT OF THE CENTER LINE, USE THE FOLLOWING SOIL PARAMETERS:

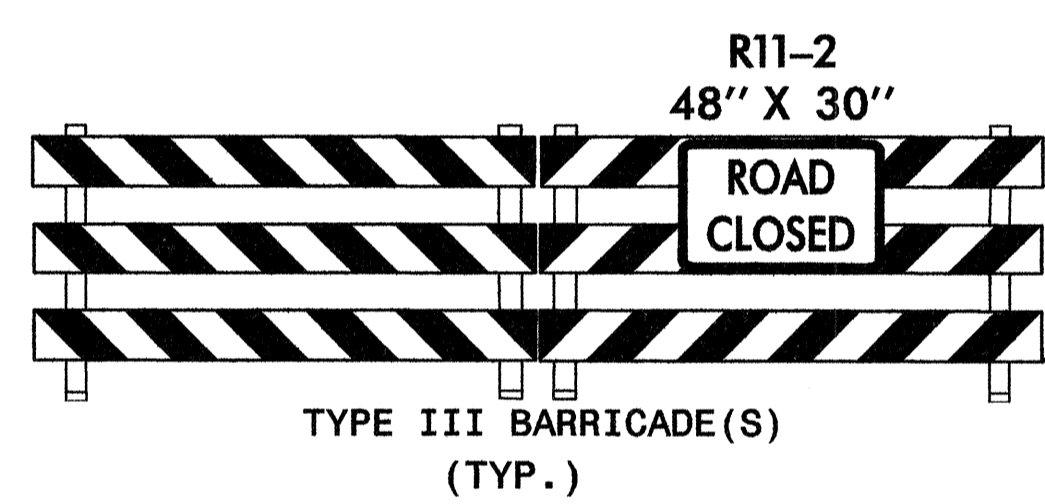
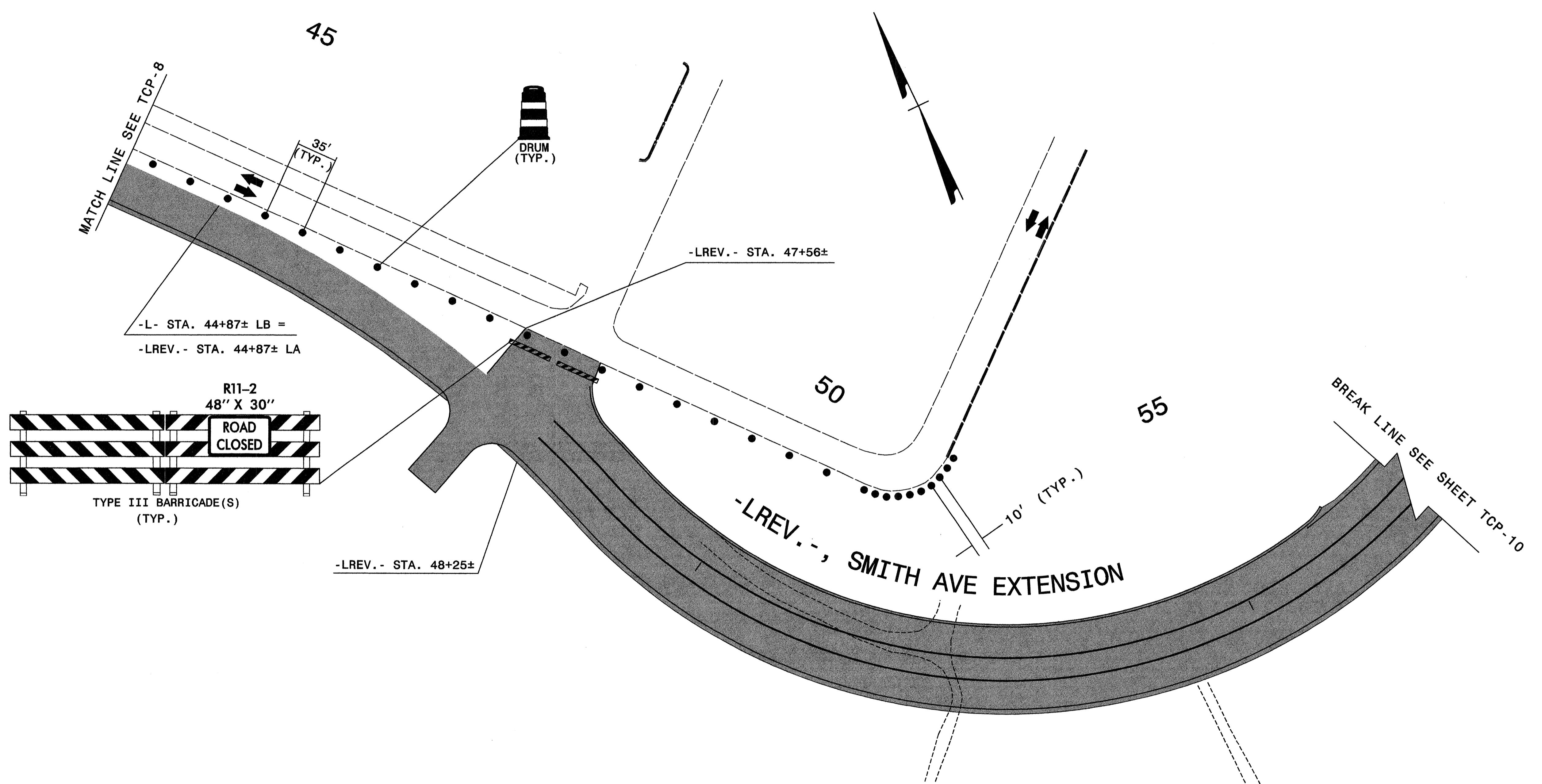
- UNIT WEIGHT OF SOIL ABOVE WATER TABLE, $\gamma = 120$ PCF
- UNIT WEIGHT OF SOIL BELOW WATER TABLE, $\gamma = 60$ PCF
- FRICTION ANGLE, $\phi = 30$ DEGREES
- COHESION, $c = 0$ PSF

FOR CONTRACTOR DESIGNED SHORING, SURVEY THE SHORING LOCATION TO DETERMINE EXISTING ELEVATIONS AND ACTUAL DESIGN HEIGHTS BEFORE BEGINNING DESIGN.

FOR PORTABLE CONCRETE BARRIERS AT THE TEMPORARY SHORING, USE AN NCDOT PORTABLE CONCRETE BARRIER (UNANCHORED OR ANCHORED) OR AN OREGON TALL F-SHAPE CONCRETE BARRIER IN ACCORDANCE WITH THE TRAFFIC CONTROL PLANS AND THE TEMPORARY SHORING SPECIAL PROVISION.

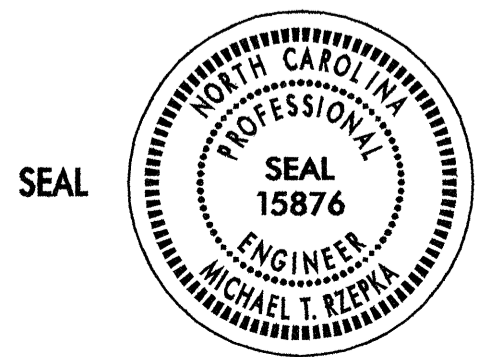
LIMITED SUBSURFACE INFORMATION IS AVAILABLE IN THE VICINITY OF THE TEMPORARY SHORING FROM STATION 15 + 42 -Y-, 47 FEET LEFT OF THE CENTER LINE TO STATION 15 + 84 -Y-, 31 FEET LEFT OF THE EXISTING CENTER LINE. THE INFORMATION PROVIDED FOR DESIGN WAS ASSUMED AND MAY NOT BE APPLICABLE TO THE ACTUAL SITE CONDITIONS ENCOUNTERED DURING CONSTRUCTION.

APPROVED: <i>Michael T. Rzepka</i> DATE: 8-26-08		PHASE I, DETAIL 2 (CONT'D)	
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	DATE: 8-08		
	DWG. BY: BLM		
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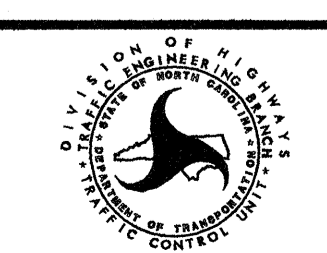
LEGEND
 PROPOSED CONSTRUCTION

APPROVED: *Michael T. Rzepka* DATE: 8-26-08

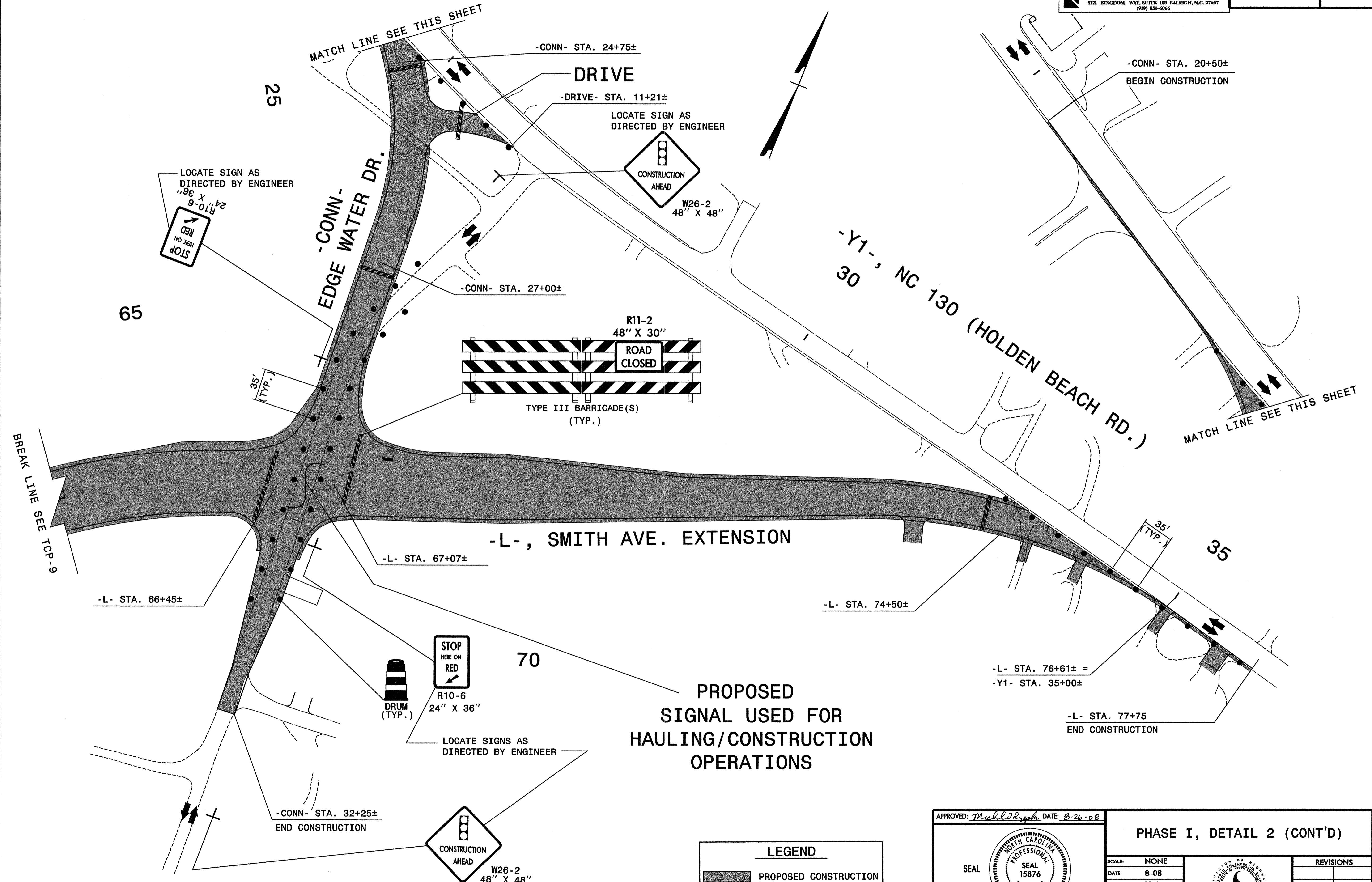


PHASE I, DETAIL 2 (CONT'D)

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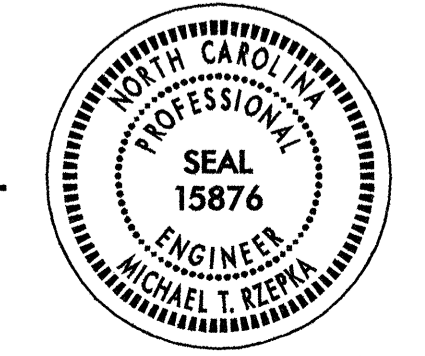


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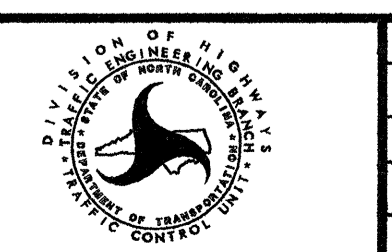
PROPOSED SIGNAL USED FOR HAULING/CONSTRUCTION OPERATIONS

APPROVED: *Michael T. Tierney* DATE: 8-26-08

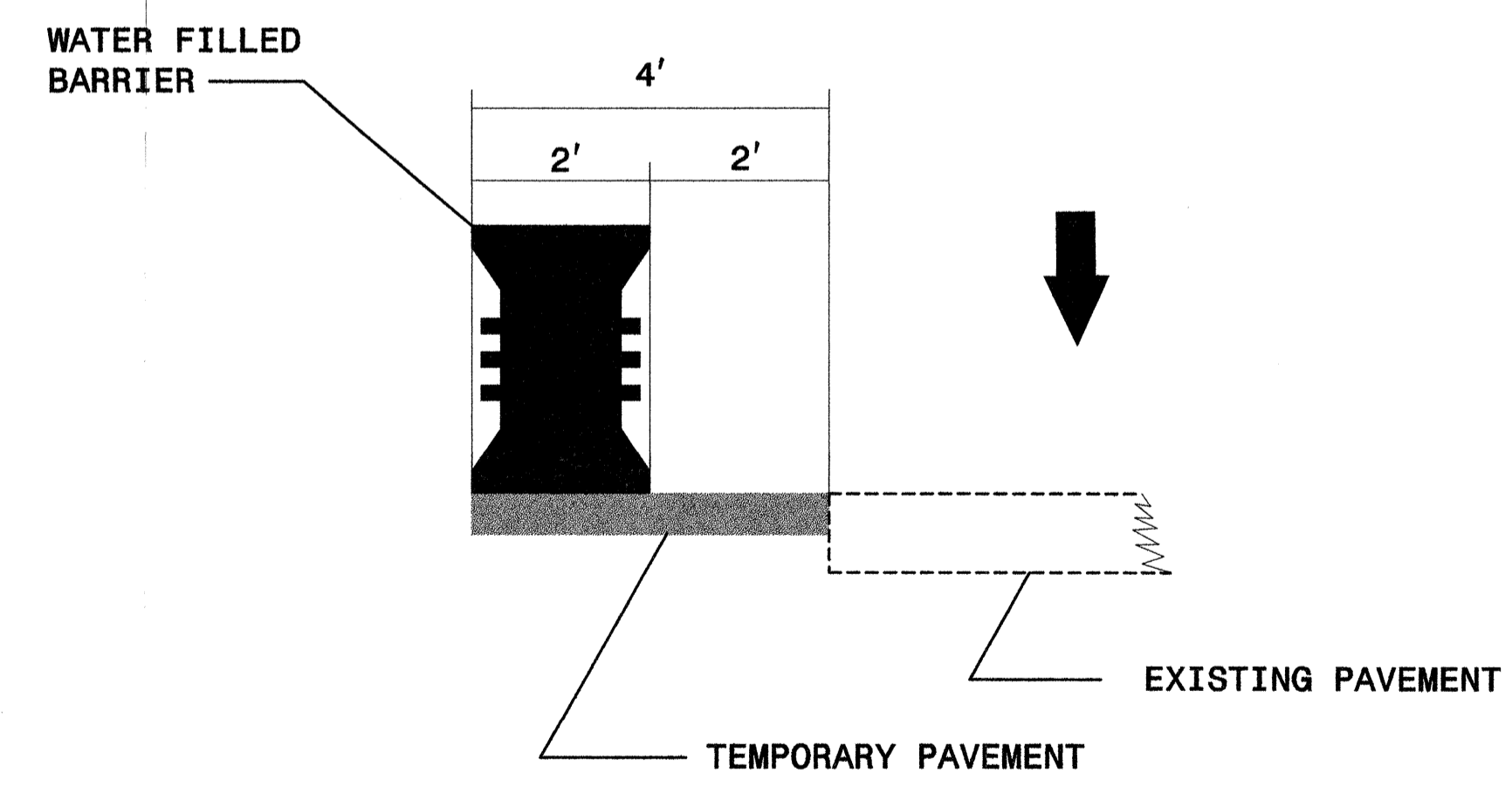
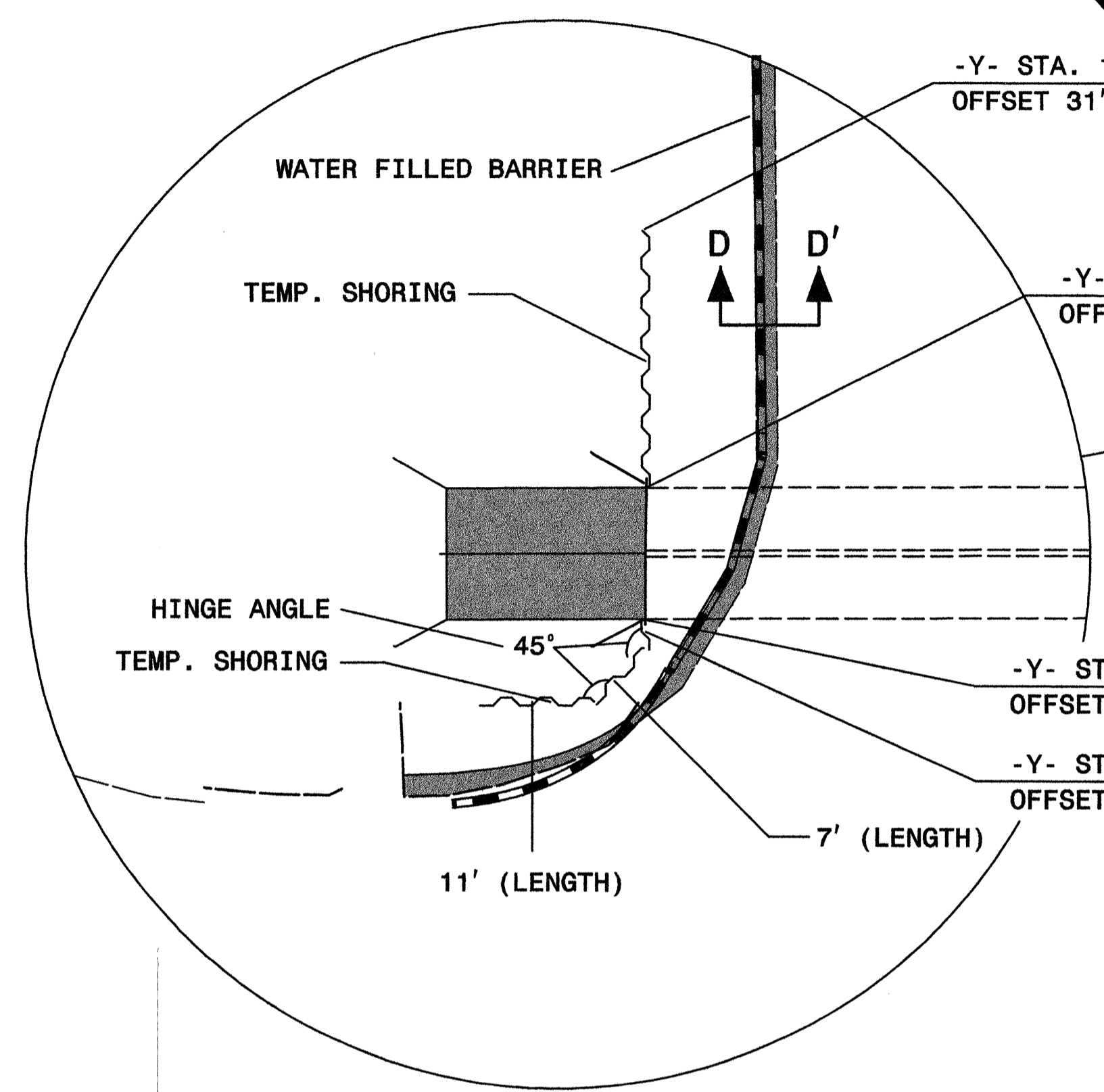


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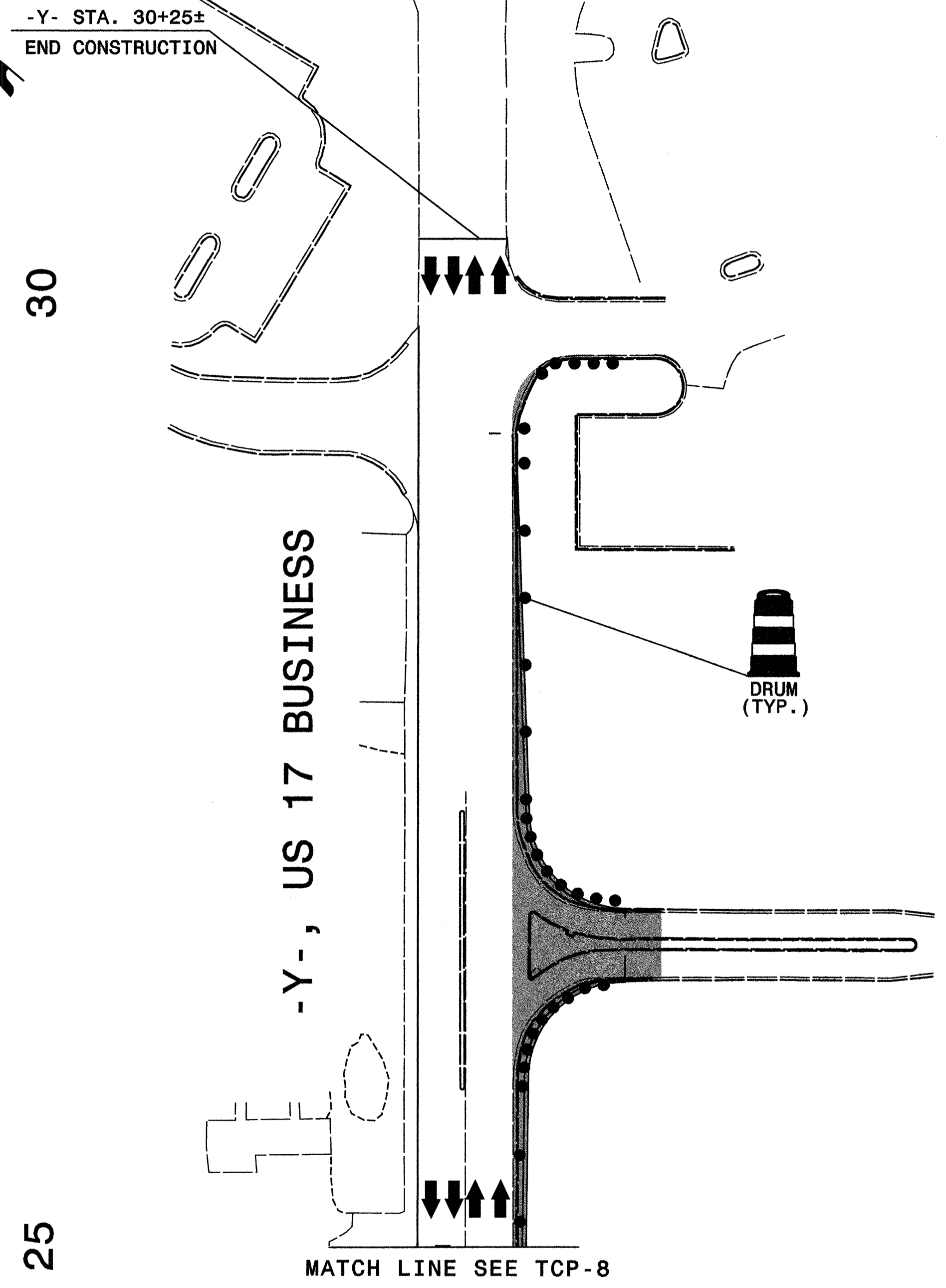
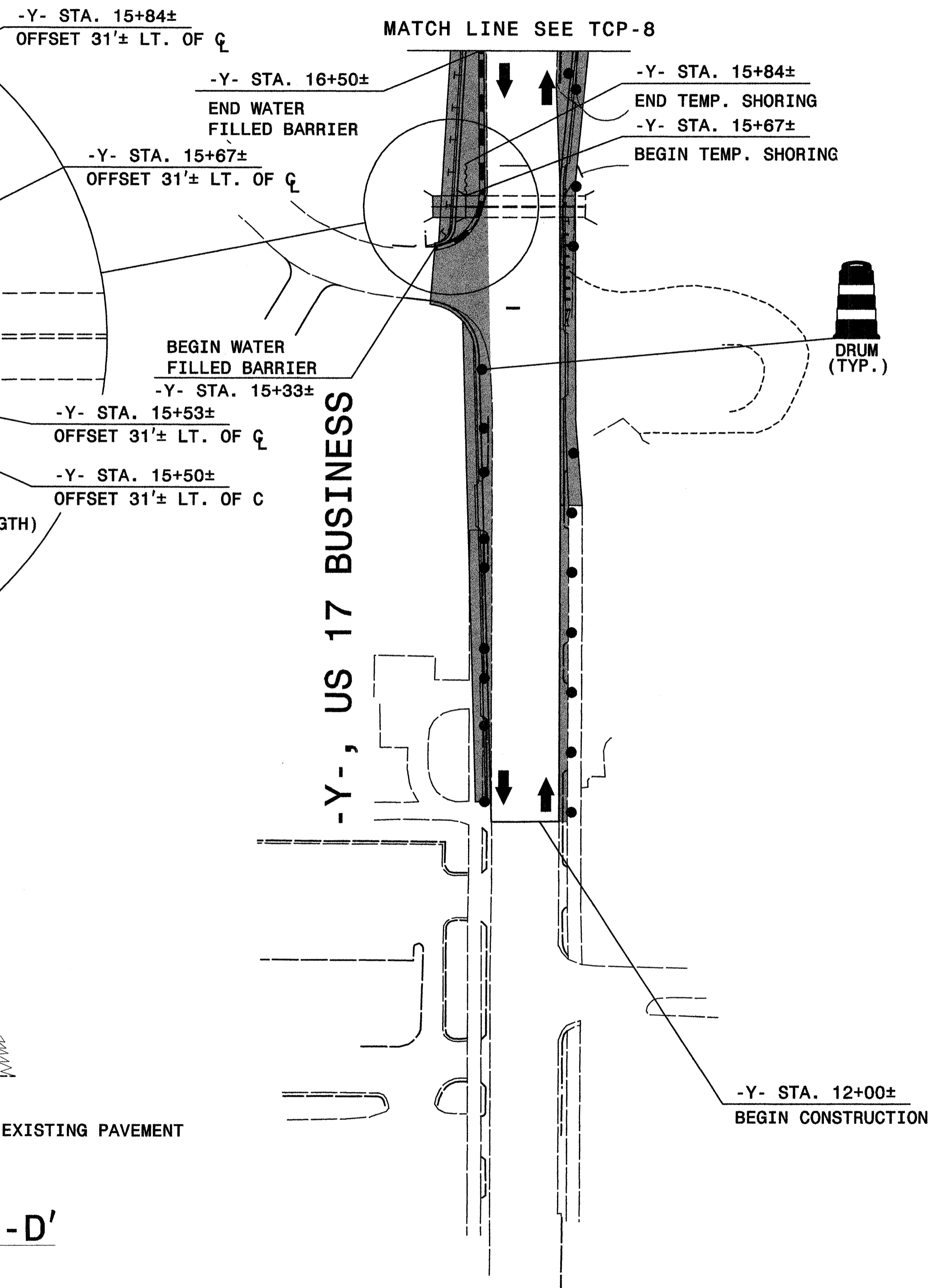
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DESIGN BY:	BLM
REVIEWED BY:	MTR



REVISIONS



TYPICAL SECTION D-D'



LEGEND

■ PROPOSED CONSTRUCTION

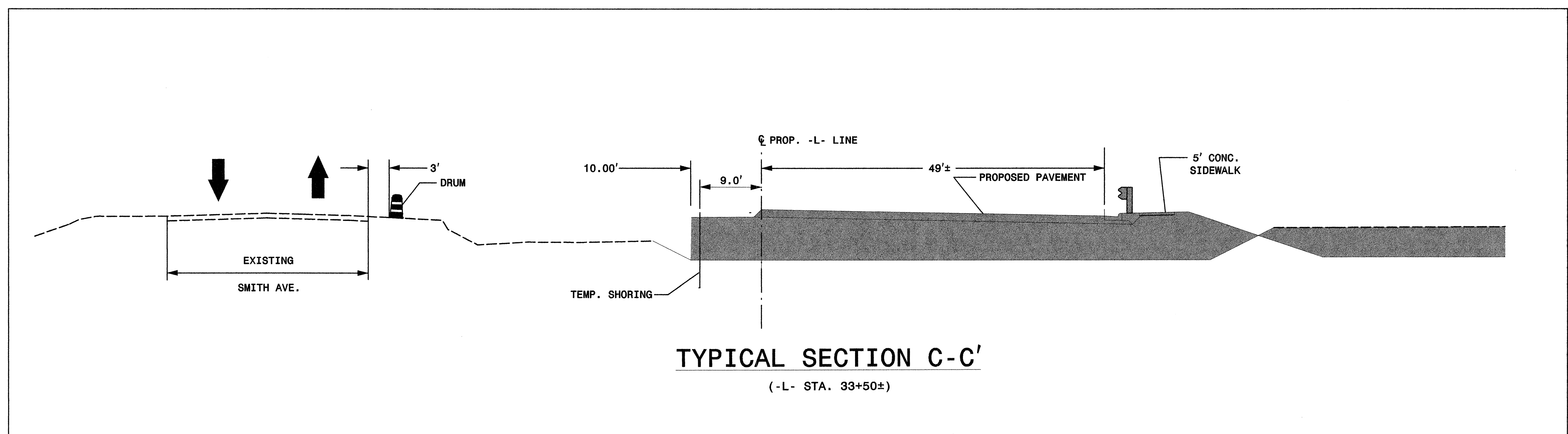
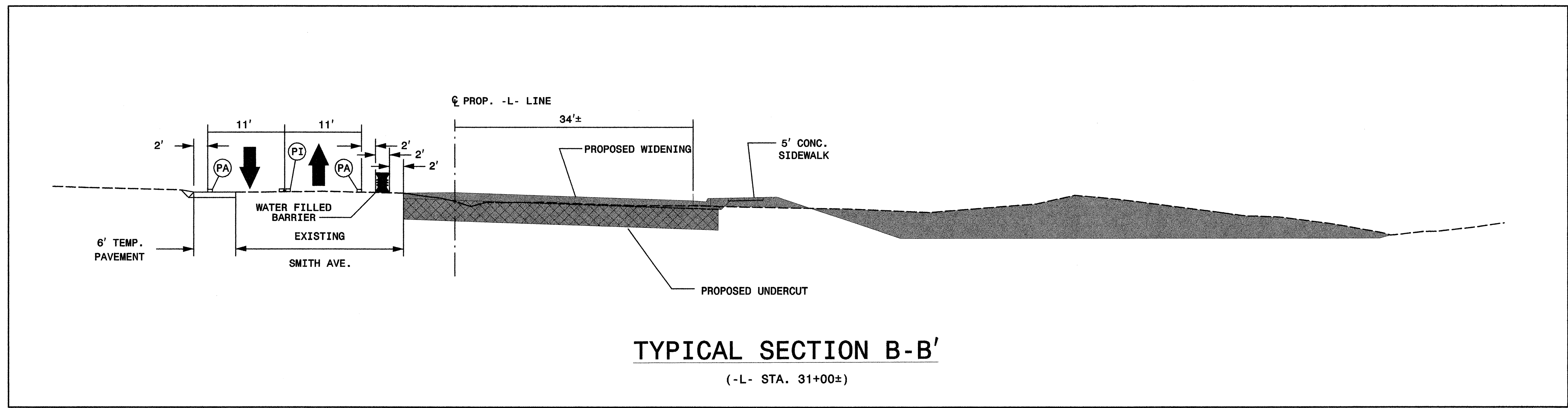
APPROVED: *Michael T. Kierka* DATE: 8-26-08

SEAL
 MICHAEL T. KIERKA
 PROFESSIONAL ENGINEER
 15876
 NORTH CAROLINA

PHASE I, DETAIL 2 (CONT'D)

SCALE: NONE		REVISIONS
DATE: 8-08		
DWG. BY: BLM		
DESIGN BY: BLM		
REVIEWED BY: MTR		

GOOD FILE



TEMPORARY PAVEMENT MARKING SCHEDULE

PA - WHITE EDGELINE
PI - YELLOW DOUBLE CENTER LINE

LEGEND

	PROPOSED CONSTRUCTION
--	-----------------------

APPROVED: *Michael Rypka* DATE: 8-26-08

SEAL

PHASE I
TYPICAL SECTIONS B - B' & C - C'

SCALE: NONE		REVISIONS
DATE: 8-08		
DWG. BY: BLM		
DESIGN BY: BLM		
REVIEWED BY: MTR		

PHASE II PHASING

(SEE OVERVIEW, SHEET TCP-14)

STEP 1: - CONTINUE ALL WORK STARTED IN PHASE I.

STEP 2: - COMPLETE CONSTRUCTION UP TO BUT NOT INCLUDING FINAL LAYER OF SURFACE COURSE IN THE FOLLOWING LOCATIONS WHICH WERE STARTED IN PHASE I, STEP 2:

FROM -L- STA. 33+00± TO -L- STA. 35+24± (RIGHT SIDE) (INCLUDING STAGE I OF PROPOSED CULVERT AT -L- STA. 33+75±)
 FROM -L- STA. 36+09± TO -L- STA. 41+50± (FULL WIDTH) (INCLUDING DRIVE 2)
 FROM -L- STA. 41+50± TO -LREV.- STA. 47+56± (RIGHT SIDE)
 FROM -LREV.- STA. 47+56± TO -LREV. STA. 48+25± (FULL WIDTH)

STEP 3: - PLACE TEMPORARY PAVEMENT MARKINGS (PAINT) AND TEMPORARY RAISED PAVEMENT MARKERS FOR INTERIM TRAFFIC PATTERN WHICH CAN BE PLACED WITHOUT INTERFERING WITH EXISTING PAVEMENT MARKINGS:

FROM -L- STA. 16+00± TO -LREV.- STA. 48+25±
 FROM -DRIVE1- STA. 10+44± TO -DRIVE1- STA. 11+41±
 FROM -DRIVE2- STA. 10+23± TO -DRIVE2- STA. 11+00±
 FROM -DRIVE3- STA. 10+51± TO -DRIVE3- STA. 11+95±
 FROM -Y2REV.- STA. 13+00± TO -Y2REV.- STA. 14+40± (SEE PHASE II, DETAIL 1, SHEETS TCP-15 THRU TCP-17)

- PLACE WATER FILLED BARRIER FROM -L- STA. 33+00± TO -L- STA. 34+50±. (SEE PHASE II, DETAIL 1, SHEET TCP-16)

STEP 4 SHALL BE COMPLETED IN ONE WORK PERIOD.

INSTALL CMS ON PROPOSED (-L-) SMITH AVE. AND (-Y-) US 17 BUSINESS, AS DIRECTED BY THE ENGINEER, TO INFORM THE TRAVELING PUBLIC OF THE TEMPORARY SHOPPING CENTER ENTRANCE.

STEP 4: - USING ROADWAY STANDARD DRAWING NO 1101.02, SHEETS 1, 2, 3 & 4 OF 9:

- PAVE EXISTING (-Y-) US 17 BUSINESS PAVEMENT AND PROPOSED WIDENING UP TO BUT NOT INCLUDING THE FINAL LAYER OF SURFACE COURSE INCLUDING TRAFFIC CONTROL DEVICES, TEMPORARY PAVEMENT MARKINGS (PAINT) AND TEMPORARY RAISED PAVEMENT MARKERS FROM -Y- STA. 12+00± TO -Y- STA. 30+25±. (SEE PHASE II, DETAIL 1, SHEETS TCP-16 AND TCP-18 AND CONSTRUCTION PLANS)

- ACTIVATE TEMPORARY SIGNAL AT THE INTERSECTION OF PROPOSED (-L-) SR 1357 (SMITH AVE.) AND (-Y-) US 17 BUSINESS AND OPEN RIGHT SIDE OF (-L-/-LREV.-) SMITH AVE./SMITH AVE. EXTENSION FROM -L- STA. 16+00± TO -LREV.- STA. 48+25±. SIMULTANEOUSLY, USING TYPE III BARRICADES, CLOSE EXISTING SMITH AVE. AND ENTRANCE TO SHOPPING CENTER AT -Y- STA. 21+00±, OPEN DRIVES 1 AND 2, AND PLACE TEMPORARY PAVEMENT MARKINGS (PAINT) AND TEMPORARY RAISED PAVEMENT MARKERS ON LEFT SIDE OF EXISTING SHOPPING CENTER ENTRANCE DRIVE (DRIVE 4) FROM -DR4- STA. 10+31± TO -DR4- STA. 13+38±. (SEE PHASE II, DETAIL 1, SHEET TCP-16 AND SIGNAL PLANS)

- PLACE REMAINDER OF TEMPORARY PAVEMENT MARKINGS (PAINT) AND TEMPORARY RAISED MARKERS ON (-L-) SMITH AVE., REMOVE ANY CONFLICTING PAVEMENT MARKING LINES AND OPEN ALL LANES TO TRAFFIC.

STEP 5: - USING ROADWAY STANDARD DRAWING NO 1101.02, SHEETS 1, 2, 3 & 4 OF 9, CONSTRUCT REMAINDER OF (-L-) SR 1357 (SMITH AVE.), INCLUDING PROPOSED CURB AND GUTTER AND STAGE II OF PROPOSED CULVERT, FROM -L- STA. 16+00± TO -L- STA. 35+24± AND FROM -L- STA. 41+50± TO -LREV.- STA. 48+25±. (SEE ROADWAY PLANS AND PHASE II, DETAIL 1, SHEETS TCP-15 THRU TCP-17)

- USING ROADWAY STANDARD DRAWING NO 1101.02, SHEETS 1, 2, 3 & 4 OF 9, COMPLETE CONSTRUCTION OF (-Y-) US 17 BUSINESS, INCLUDING CURB AND GUTTER AND SIDEWALKS IN AREA OF EXISTING SMITH AVE. AND ENTRANCE TO SHOPPING CENTER AT -Y- STA. 21+00±. (SEE ROADWAY PLANS AND PHASE II, DETAIL 1, SHEET TCP-16)

- AWAY FROM TRAFFIC, COMPLETE CONSTRUCTION UP TO BUT NOT INCLUDING FINAL LAYER OF SURFACE COURSE IN THE FOLLOWING LOCATIONS WHICH WERE STARTED IN PHASE I, STEP 2:

FROM -LREV.- STA. 48+25± TO -L- STA. 66+45± (FULL WIDTH)
 FROM -L- STA. 67+07± TO -L- STA. 74+50± (FULL WIDTH)
 FROM -CONN- STA. 24+75± TO -CONN- STA. 27+00±

- USING ROADWAY STANDARD DRAWING NO 1101.02, SHEET 1 OF 9, COMPLETE CONSTRUCTION UP TO EXISTING NC 130 EDGE OF PAVEMENT ELEVATION IN THE FOLLOWING LOCATIONS WHICH WERE STARTED IN PHASE I, STEP 2:

FROM -L- STA. 74+50± TO -L- STA. 77+75±
 FROM -CONN- STA. 20+50± TO -CONN- STA. 24+75±
 FROM -DRIVE- STA. 10+00± TO -DRIVE- STA. 11+21±

- USING ROADWAY STANDARD DRAWING NO 1101.02, SHEET 1 OF 9, COMPLETE CONSTRUCTION OF -CONN- FROM -CONN- STA. 27+00± TO STA. -CONN- STA. 32+25± AND (-L-) SMITH AVE. EXTENSION, FROM -L- STA. 66+45± TO -L- STA. 67+00± (FULL WIDTH) UP TO BUT NOT INCLUDING FINAL LAYER OF SURFACE COURSE WHICH WERE STARTED IN PHASE I, STEP 2.

- PLACE TEMPORARY PAVEMENT MARKINGS (PAINT) AND TEMPORARY RAISED PAVEMENT MARKERS FOR FINAL PATTERN ON (-L-/-LREV.) SMITH AVE./SMITH AVE. EXTENSION AND PROPOSED CONNECTOR (-CONN-) WHICH CAN BE PLACED WITHOUT INTERFERING WITH EXISTING PAVEMENT MARKINGS. (SEE SHEETS PM-2 THRU PM-5)

- INSTALL/COMPLETE INSTALLATION OF PROPOSED SIGNALS FOR FINAL TRAFFIC PATTERN AT THE INTERSECTIONS OF PROPOSED (-L-) SR 1357 (SMITH AVE.) AND (-Y-) US 17 BUSINESS, PROPOSED (-L-) SMITH AVE. EXTENSION AND (-CONN-) EDGEWATER DR., AND (-Y-) US 17 BUSINESS AND (-Y1-) NC 130 (HOLDEN BEACH RD.). (SEE SIGNAL PLANS AND PM-2 & PM-5)

STEP 6 SHALL BE COMPLETED IN ONE WORK PERIOD.

STEP 6: - USING ROADWAY STANDARD DRAWING NO 1101.02, SHEET 1 OF 9:

- PAVE/WEDGE UP TO BUT NOT INCLUDING FINAL LAYER OF SURFACE COURSE FROM -CONN- STA. 20+50± TO -CONN- STA. 24+75± AND -L- STA. 74+50± TO -L- STA. 77+75±. (SEE CONSTRUCTION PLANS)

- PLACE REMAINDER OF TEMPORARY PAVEMENT MARKINGS (PAINT) AND TEMPORARY RAISED PAVEMENT MARKERS FOR FINAL PATTERN ON (-L-/-LREV.-) SMITH AVE./SMITH AVE. EXTENSION AND ALL -Y- LINES & DRIVES. MODIFY TEMPORARY PAVEMENT MARKINGS ON (-Y-) US 17 BUSINESS AT THE -L- INTERSECTION. (SEE SHEETS PM-2 THRU PM-5)

- ACTIVATE PROPOSED SIGNALS AT THE INTERSECTION OF (-LREV.-) SMITH AVE. EXTENSION AND -CONN- AND INTERSECTION OF -L-, SMITH AVE. EXTENSION AND (-Y-) US 17 BUSINESS, REMOVE TYPE III BARRICADES AND OPEN ENTIRE PROJECT TO FINAL PATTERN. (SEE SIGNAL PLANS AND SHEETS PM-2 THRU PM-5)

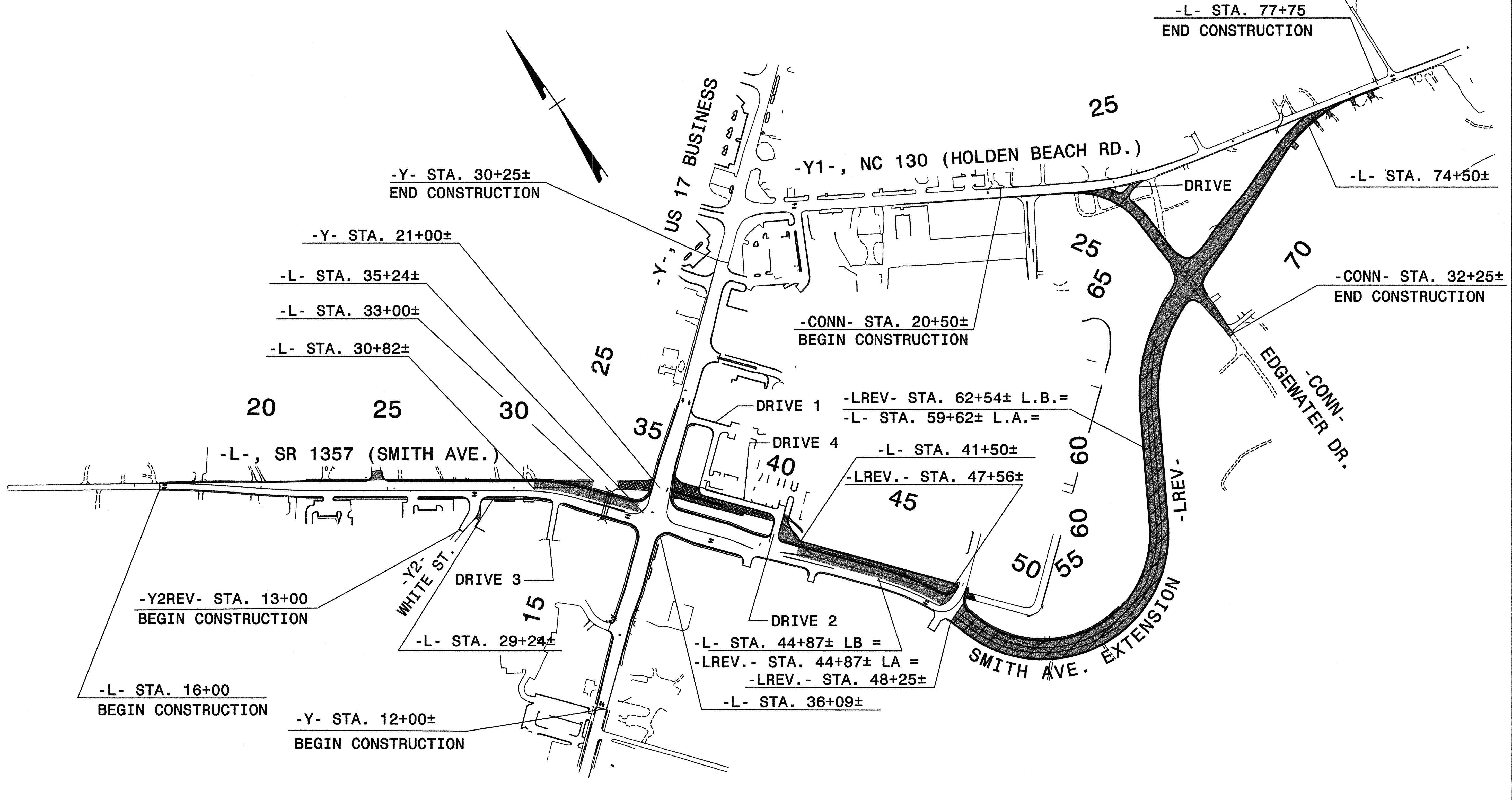
STEP 7: - USING ROADWAY STANDARD DRAWING NO 1101.02, SHEET 1 OF 9, CONSTRUCT DRIVE UP TO BUT NOT INCLUDING FINAL LAYER OF SURFACE COURSE FROM -DRIVE- STA. 10+30± TO -DRIVE- 12+00±. AT THE END OF EACH DAY'S OPERATION, PLACE INCIDENTAL STONE OR ASPHALT TO MAINTAIN ACCESS. USE DRUMS TO DELINEATE THE TRAVEL WAY DURING CONSTRUCTION ON INCIDENTAL STONE AND TEMPORARY PAVEMENT MARKINGS (PAINT) ON ASPHALT.

- USING ROADWAY STANDARD DRAWING NO. 1101.02, SHEETS 1, 2, 3 & 4 OF 9, COMPLETE ALL REMAINING CONSTRUCTION.

PHASE III PHASING

STEP 1: - USING ROADWAY STANDARD DRAWING NO 1101.02, SHEETS 1, 2, 3 & 4, PLACE THE FINAL LAYER OF SURFACE COURSE, CONCRETE ISLANDS, FINAL PAVEMENT MARKINGS (THERMOPLASTIC), SNOWPLOWABLE RAISED PAVEMENT MARKERS FOR (-L-/-LREV.-) SMITH AVE./SMITH AVE. EXTENSION, ALL -Y- LINES AND OPEN ENTIRE PROJECT TO THE FINAL TRAFFIC PATTERN. (SEE CONSTRUCTION PLANS AND SHEETS PM-2 THRU PM-5)

APPROVED: <i>Michael T. Kiepl</i> DATE: 8-26-08	PHASE II & III PHASING	
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	DWG. BY: BLM	
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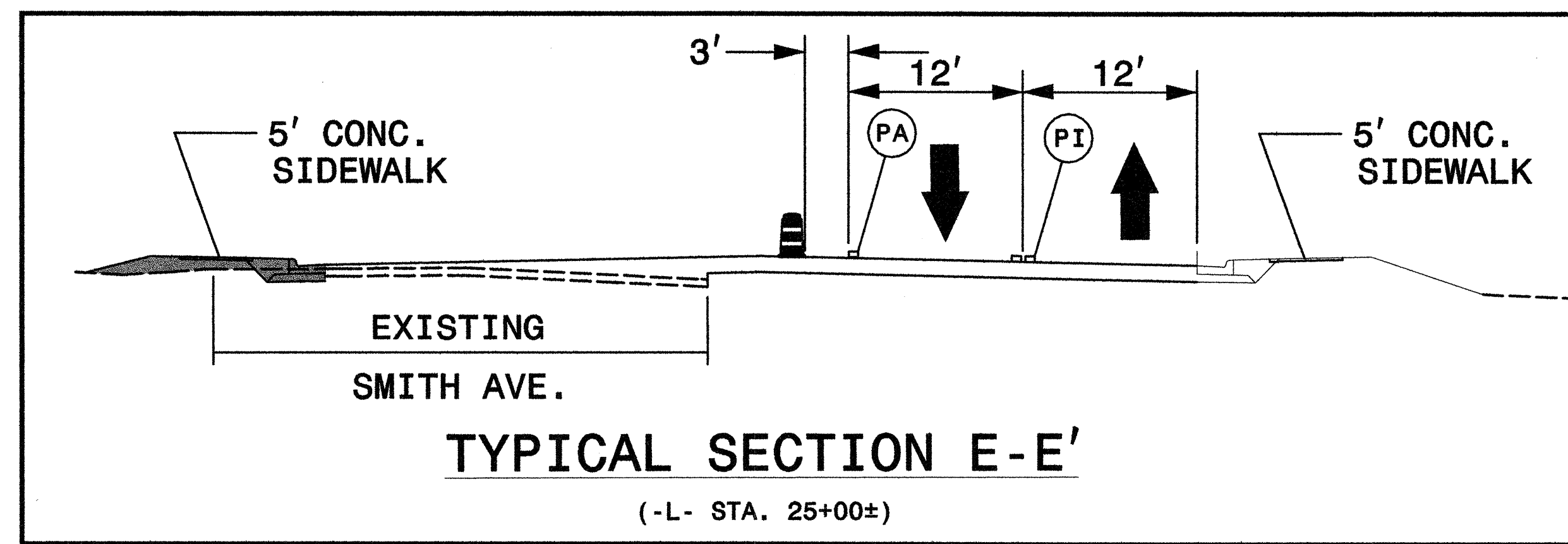
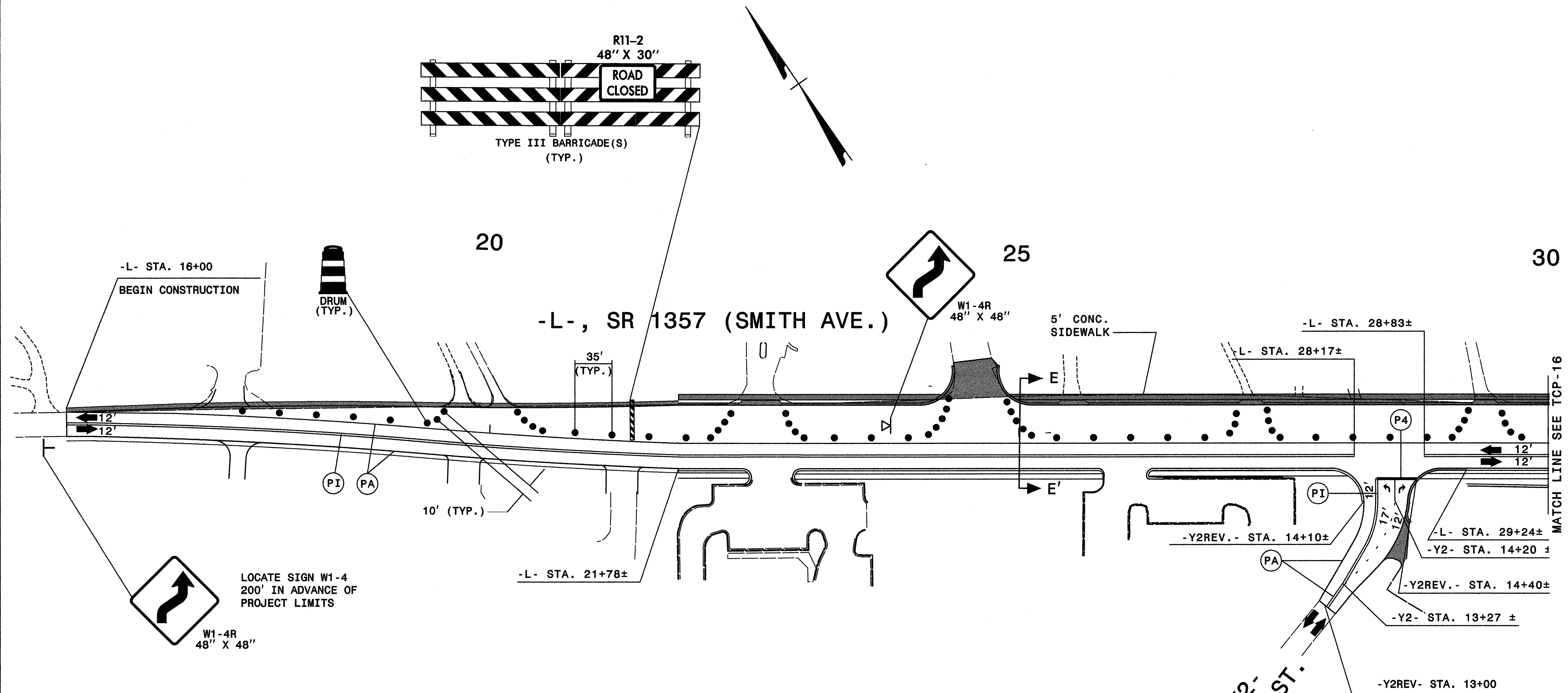


LEGEND

	PROPOSED CONSTRUCTION
	ONGOING CONSTRUCTION
	PAVEMENT REMOVAL

APPROVED: <i>Michael T. Rzepka</i> DATE: 8-24-08	PHASE II OVERVIEW	
	SCALE: NONE	
	DATE: 8-08	
	DWG. BY: B.L.M.	
	DESIGN BY: G.E.P.	
REVIEWED BY: M.S.A.	REVISIONS	

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TEMPORARY PAVEMENT MARKING SCHEDULE	
PA	- WHITE EDGELINE
PI	- YELLOW DOUBLE CENTER LINE
P4	- WHITE STOPBAR

LEGEND	
	PROPOSED CONSTRUCTION

APPROVED: *Michael T. Klepp* DATE: 9-24-08

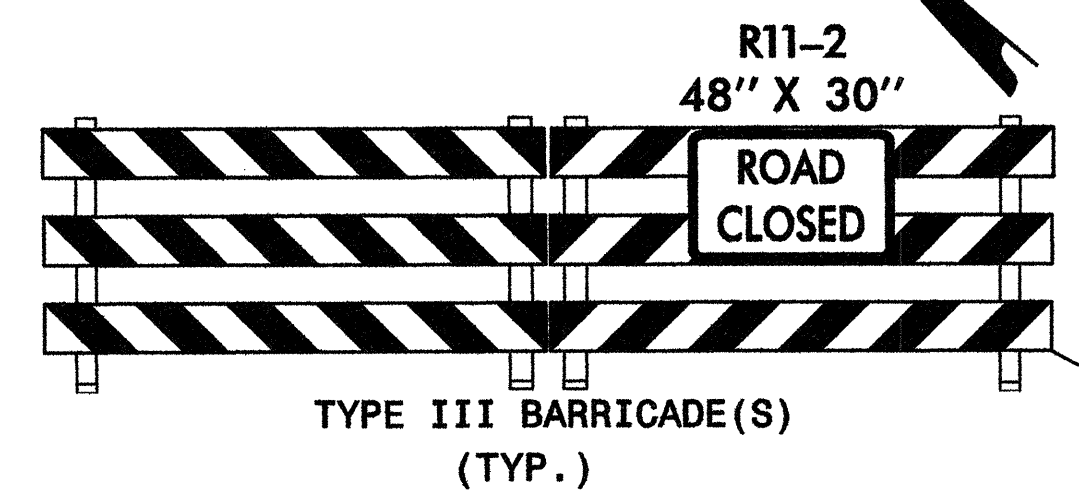
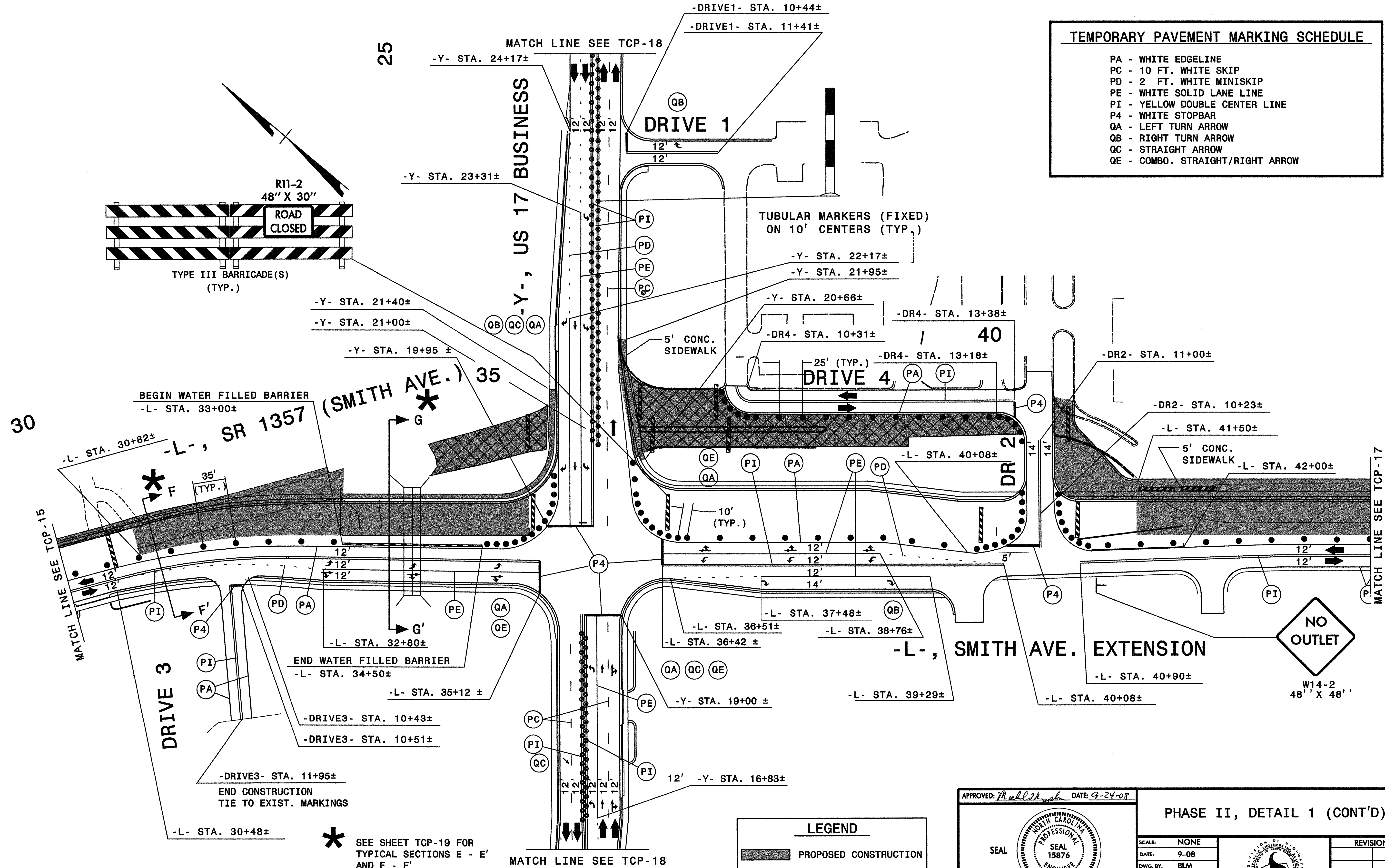
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PHASE II, DETAIL 1

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DATE:	8-08										
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REVIEWED BY: MTR

TEMPORARY PAVEMENT MARKING SCHEDULE	
PA	WHITE EDGELINE
PC	10 FT. WHITE SKIP
PD	2 FT. WHITE MINISKIP
PE	WHITE SOLID LANE LINE
PI	YELLOW DOUBLE CENTER LINE
P4	WHITE STOPBAR
QA	LEFT TURN ARROW
QB	RIGHT TURN ARROW
QC	STRAIGHT ARROW
QE	COMBO. STRAIGHT/RIGHT ARROW



BEGIN WATER FILLED BARRIER
 -L- STA. 33+00±

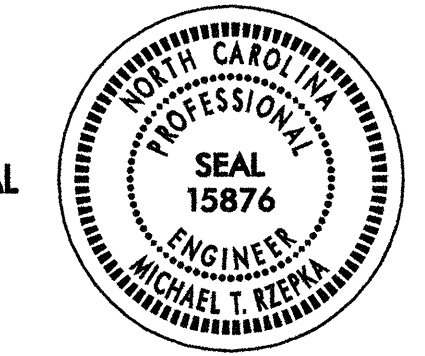
END WATER FILLED BARRIER
 -L- STA. 34+50±

END CONSTRUCTION
 TIE TO EXIST. MARKINGS

* SEE SHEET TCP-19 FOR
 TYPICAL SECTIONS E - E'
 AND F - F'

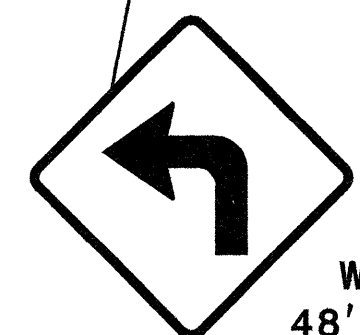
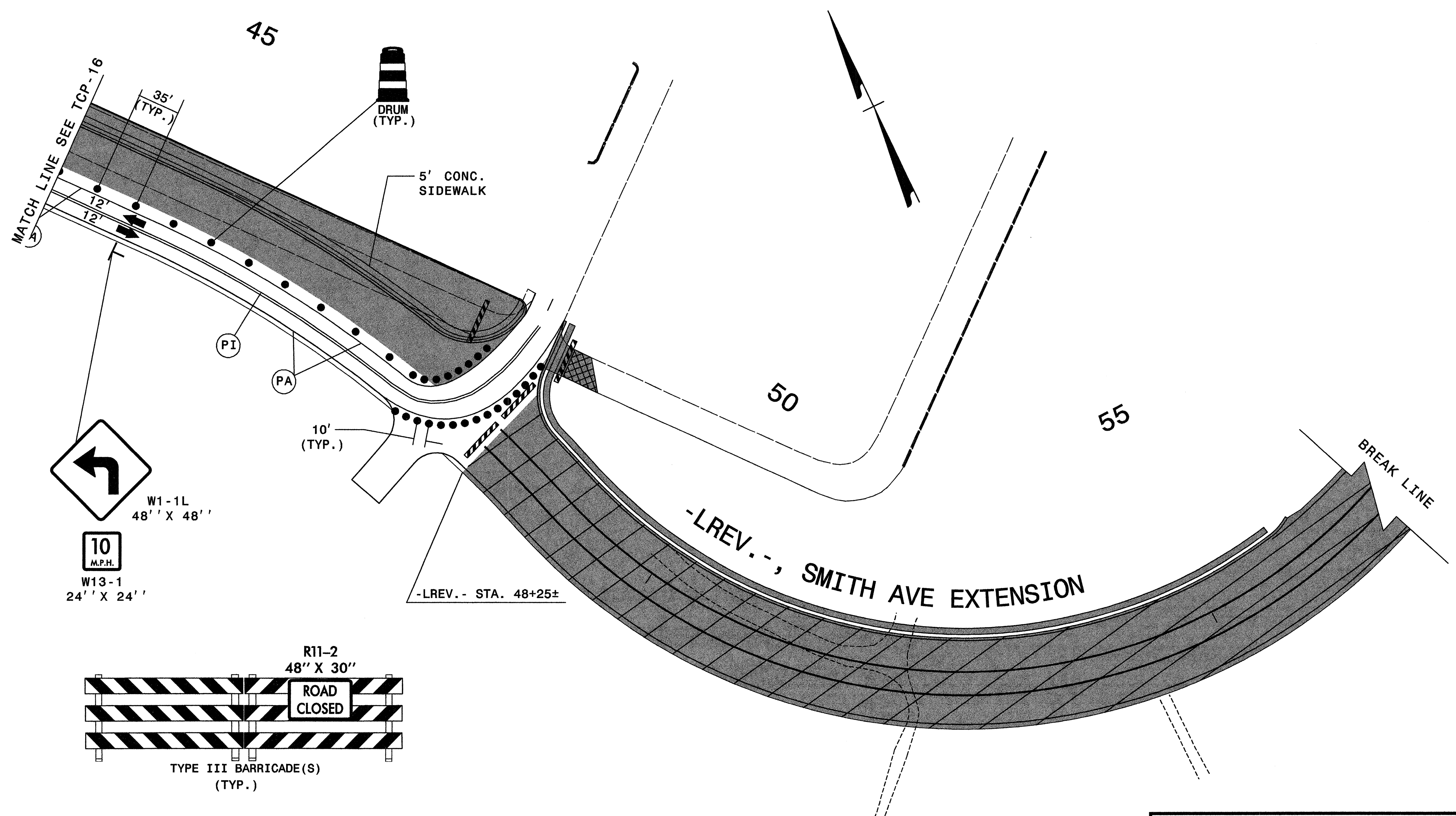
LEGEND	
	PROPOSED CONSTRUCTION
	PAVEMENT REMOVAL

APPROVED: *Michael T. Kiepl* DATE: 9-24-08



PHASE II, DETAIL 1 (CONT'D)

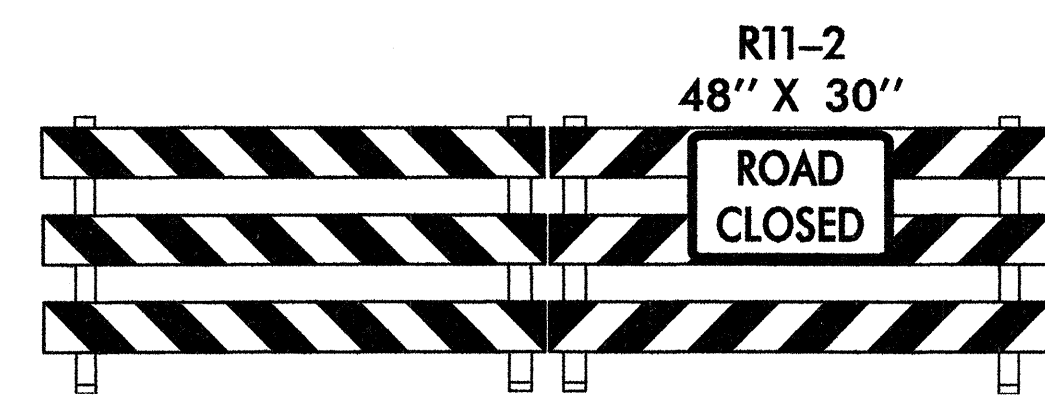
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DATE: 9-08		
DWG. BY: BLM		
DESIGN BY: BLM		
REVIEWED BY: MTR		



W1-1L
48' X 48'



W13-1
24' X 24'



R11-2
48" X 30"
ROAD CLOSED
TYPE III BARRICADE(S)
(TYP.)

-LREV. - STA. 48+25±

-LREV. - SMITH AVE EXTENSION

TEMPORARY PAVEMENT MARKING SCHEDULE
 PA - WHITE EDGELINE
 PI - YELLOW DOUBLE CENTER LINE

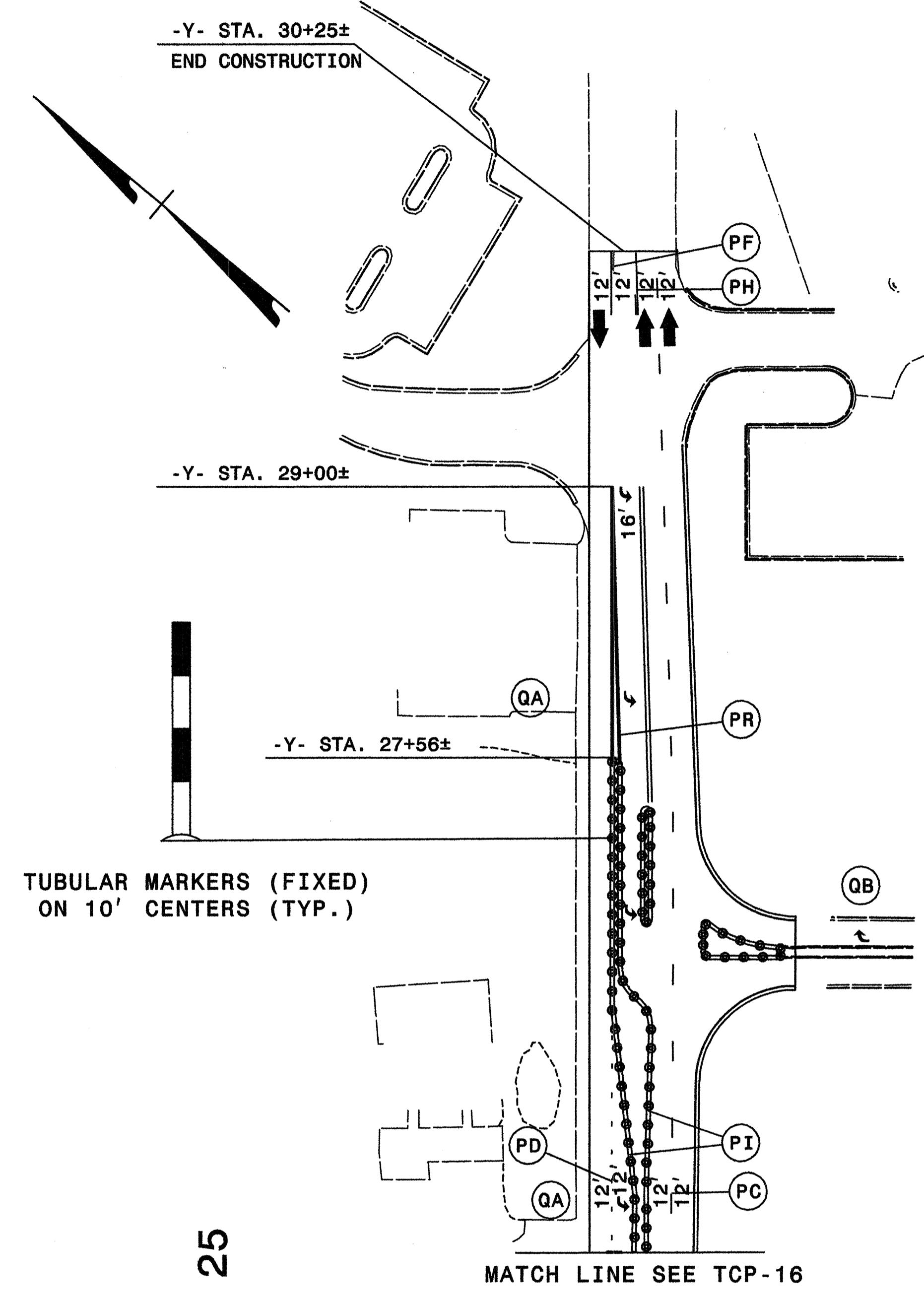
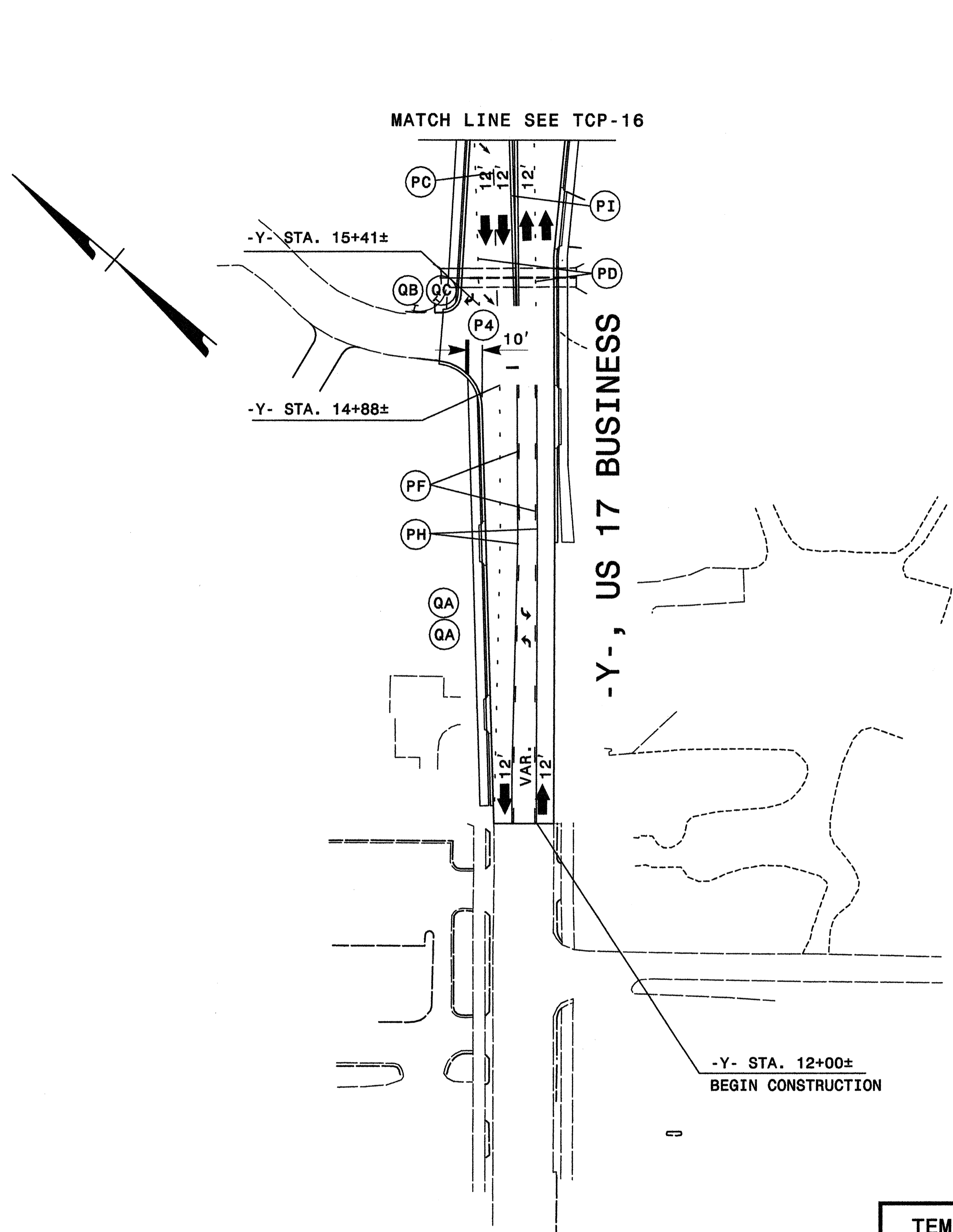
LEGEND	
	PROPOSED CONSTRUCTION
	ONGOING CONSTRUCTION
	PAVEMENT REMOVAL

APPROVED: *M. J. R. [Signature]* DATE: 8-26-08

SEAL

PHASE II, DETAIL 1 (CONT'D)

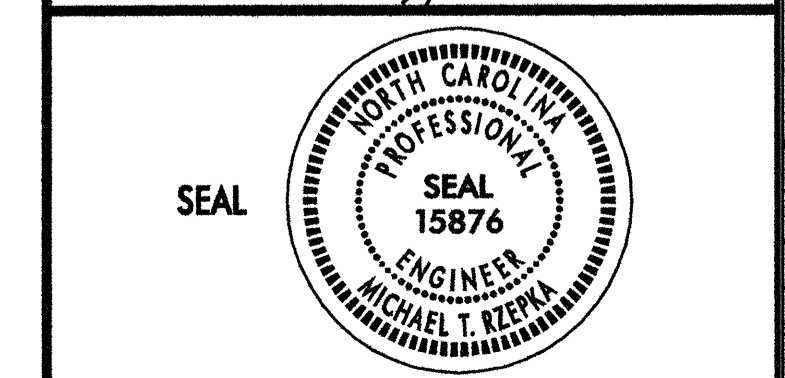
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DATE:	8-08										
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REVIEWED BY:	MTR	CADD FILE									



TUBULAR MARKERS (FIXED)
 ON 10' CENTERS (TYP.)

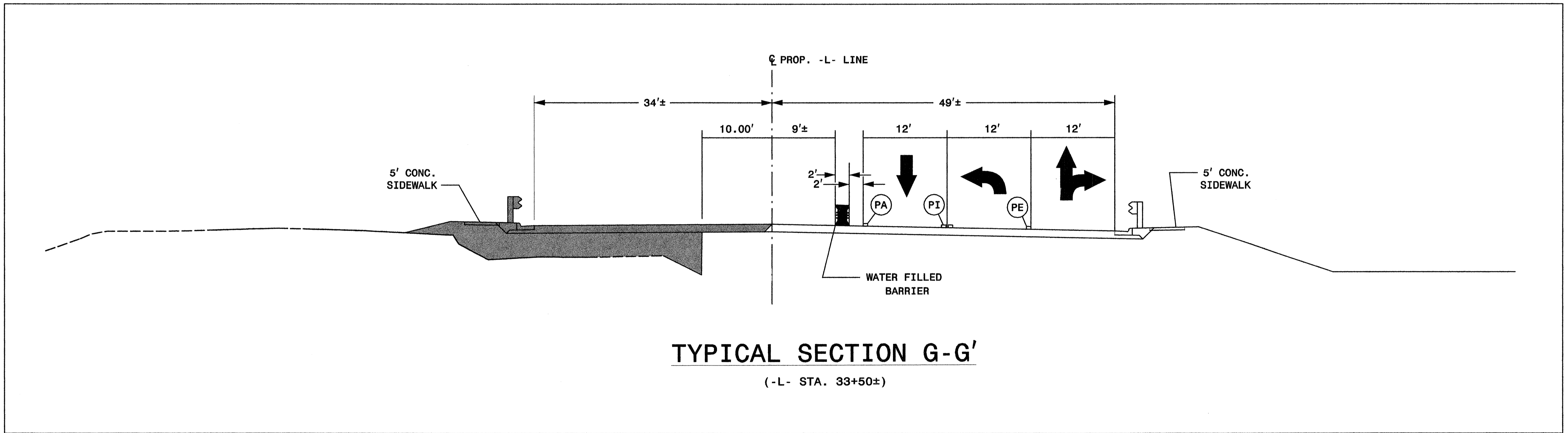
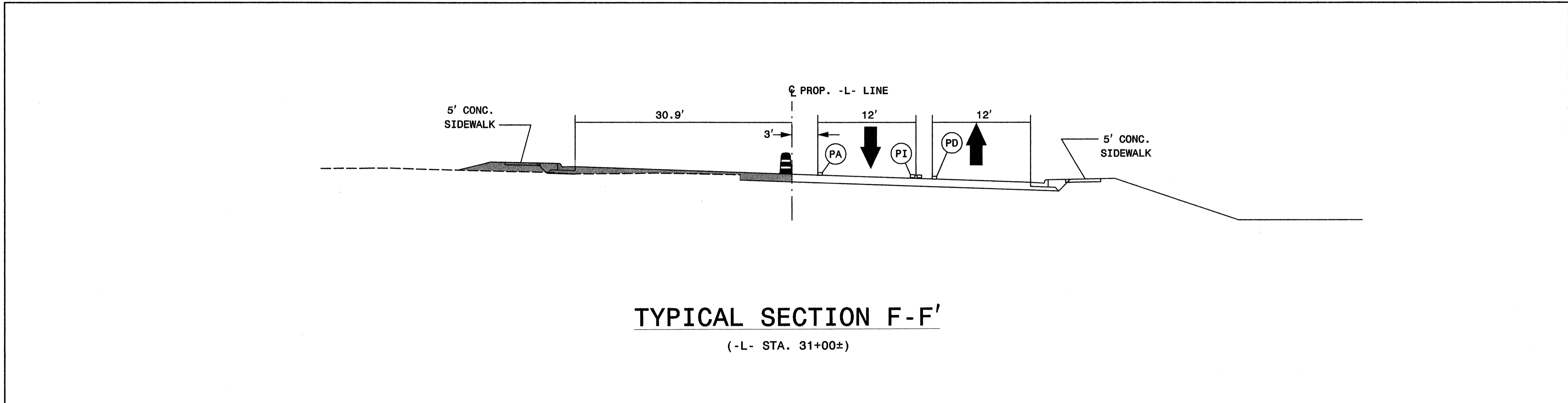
TEMPORARY PAVEMENT MARKING SCHEDULE	
PC	10 FT. WHITE SKIP
PD	2 FT. WHITE MINISKIP
PE	WHITE SOLID LANE LINE
PF	10 FT. YELLOW SKIP
PH	YELLOW SINGLE CENTER
PI	YELLOW DOUBLE CENTER LINE
PR	WHITE GORELINE
P4	WHITE STOPBAR
QA	LEFT TURN ARROW
QB	RIGHT TURN ARROW
QC	STRAIGHT ARROW

APPROVED: *Michael T. Kiepl* DATE: 9-24-08

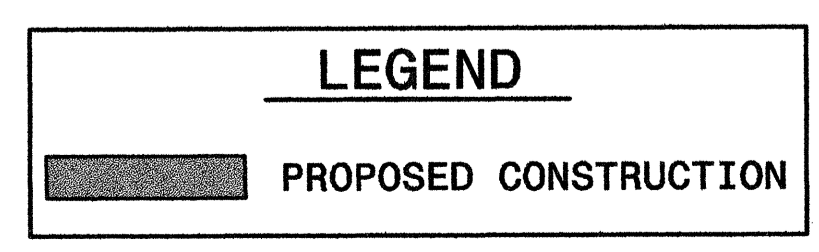


PHASE II, DETAIL 1 (CONT'D)

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TEMPORARY PAVEMENT MARKING SCHEDULE	
PA	- WHITE EDGELINE
PD	- 2 FT. WHITE MINISKIP
PE	- WHITE SOLID LANE LINE
PI	- YELLOW DOUBLE CENTER LINE

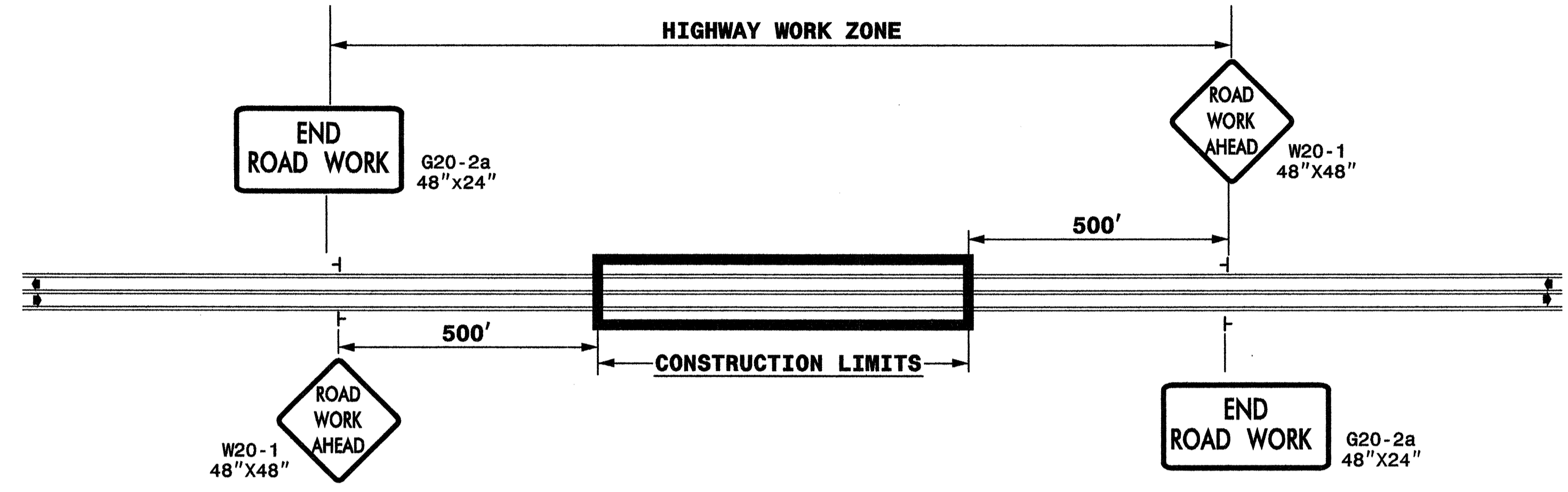


APPROVED: *Muller* DATE: 8-26-08

SEAL

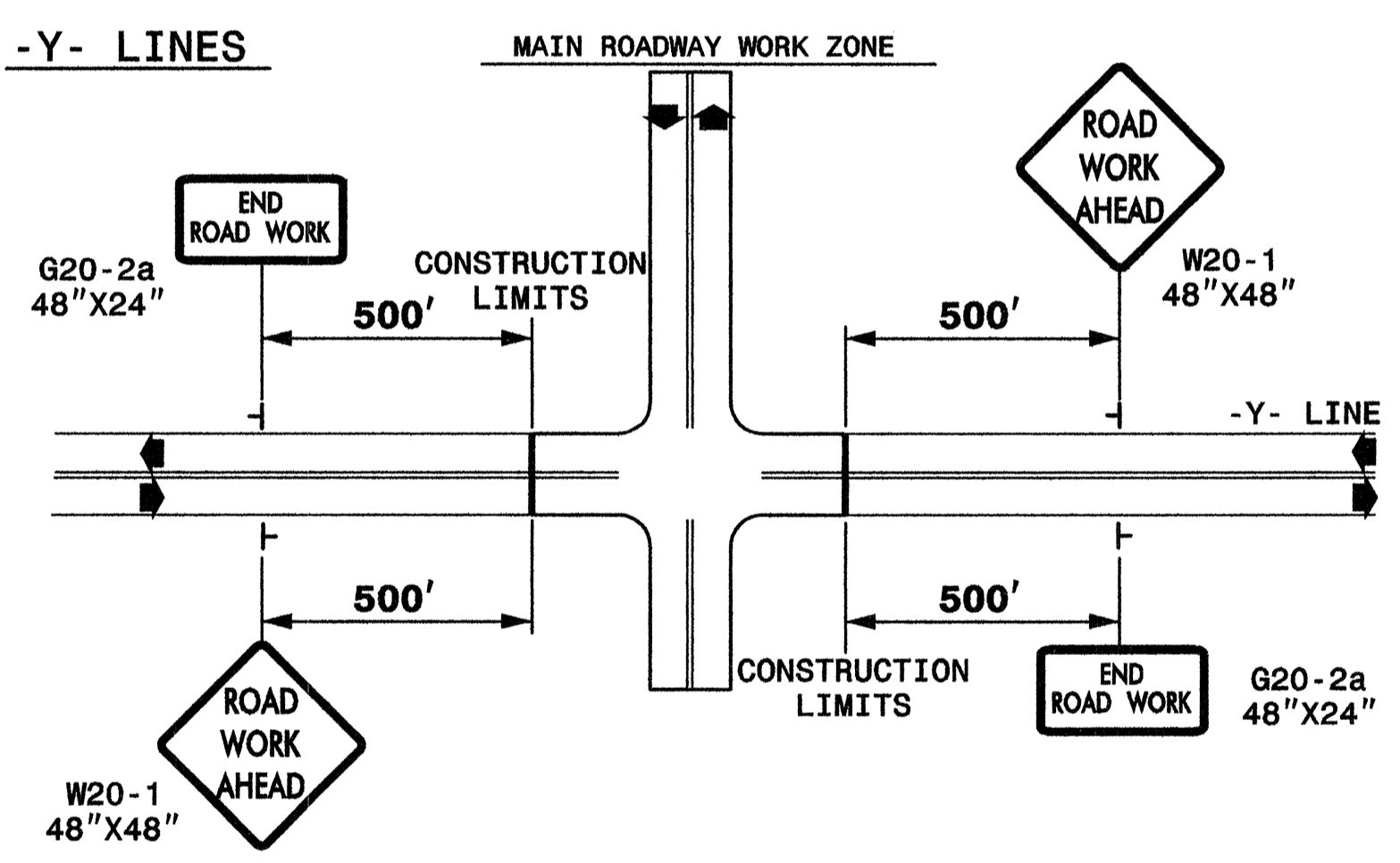
PHASE II		TYPICAL SECTIONS F - F' & G - G'							
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REVISIONS									

TWO-WAY UNDIVIDED ** (L-LINES)



STATE OF NORTH CAROLINA
 DEPT. OF TRANSPORTATION
 DIVISION OF HIGHWAYS
 RALEIGH, N.C.

ROADWAYS INTERSECTING ALONG 2 WAY UNDIVIDED WORK ZONE (Y-LINES)



DETAIL DRAWING FOR
 TWO-WAY UNDIVIDED
 WORK ZONE WARNING SIGNS

GENERAL NOTES

- USE FLUORESCENT ORANGE SHEETING (TYPE VII OR HIGHER) ON ALL ADVANCED WORK ZONE SIGNS.
- DO NOT INSTALL ADVANCE WARNING SIGNS MORE THAN 3 DAYS PRIOR TO BEGINNING OF WORK.
- SIGNS SHOWN ARE REQUIRED FOR WORK ZONES THAT WILL REMAIN IN EFFECT OVERNIGHT. FOR SHORT-TERM DAILY MAINTENANCE TYPE OPERATIONS, THIS SIGNING APPLICATION IS OPTIONAL; MAY USE ONLY APPLICABLE ROADWAY STANDARD DRAWINGS INSTEAD. HOWEVER, IF THIS SIGNING APPLICATION IS USED, SIGNS MAY BE PORTABLE MOUNTED.
- ALL SIGN SPACING DIMENSIONS ARE APPROXIMATE, FIELD ADJUST AS NECESSARY OR AS DIRECTED.
- USE 3LB STEEL U-CHANNEL POST OR 4" X 4" WOOD POST FOR ALL WORK ZONE SIGNS. 3LB STEEL U-CHANNEL POSTS MUST MEET THE REQUIREMENTS OF STANDARD SPECIFICATION SECTION 1094-1(B), MAY BE GALVANIZED STEEL, OR MAY BE PAINTED GREEN BY THE POST MANUFACTURER. SQUARE STEEL TUBING POSTS HAVING EQUIVALENT STRENGTH OF THE 3 LB STEEL U-CHANNEL POST ARE ALSO ACCEPTABLE FOR USE. ERECT SIGNS PER ROADWAY STANDARD DRAWING 1110.01. PAYMENT FOR WOOD POSTS, 3LB STEEL U-CHANNEL AND SQUARE STEEL TUBING POSTS WITH SIGNS WILL BE MADE ACCORDING TO STANDARD SPECIFICATION "WORK ZONE SIGNS" SECTION 1110.
- WHEN NECESSARY, USE SPLICING IN ACCORDANCE WITH ROADWAY STANDARD DRAWING NO. 1110.01. REMOVE ENTIRE POST WHEN REMOVING SIGNS WITH SPLICED POSTS.
- DO NOT BACK BRACE SIGN SUPPORTS.
- ** TWO-WAY UNDIVIDED ADVANCE WARNING SIGN CONFIGURATION MAY BE USED ON URBAN MULTI-LANE FACILITIES WHERE CONDITIONS LIMIT THE USE OF DUAL MOUNTED SIGNS AS DETERMINED BY THE ENGINEER.

LEGEND

┆ STATIONARY SIGN

◀ DIRECTION OF TRAFFIC FLOW

SHEET 1 OF 1

APPROVED: <i>Michael T. Rieppel</i> DATE: 8-26-08	DETAIL DRAWING FOR TWO-WAY UNDIVIDED AND URBAN FREEWAYS ADVANCED WORK ZONE WARNING SIGNS									
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DESIGN BY: G.E.P.										
REVIEWED BY: M.T.R.	CADD FILE									