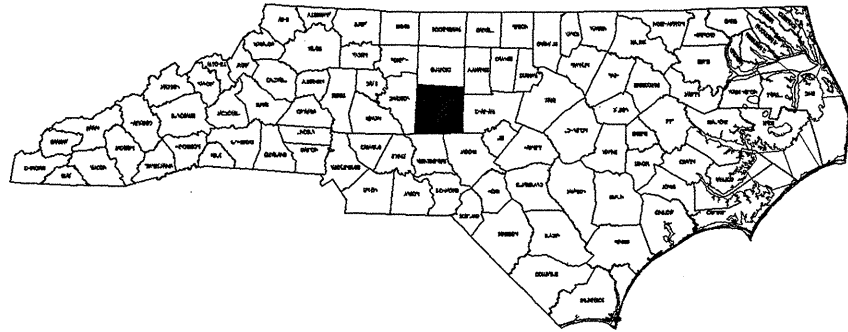


TIP PROJECT: B-4700F

CONTRACT:

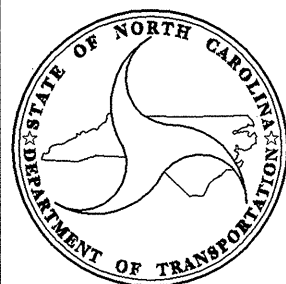
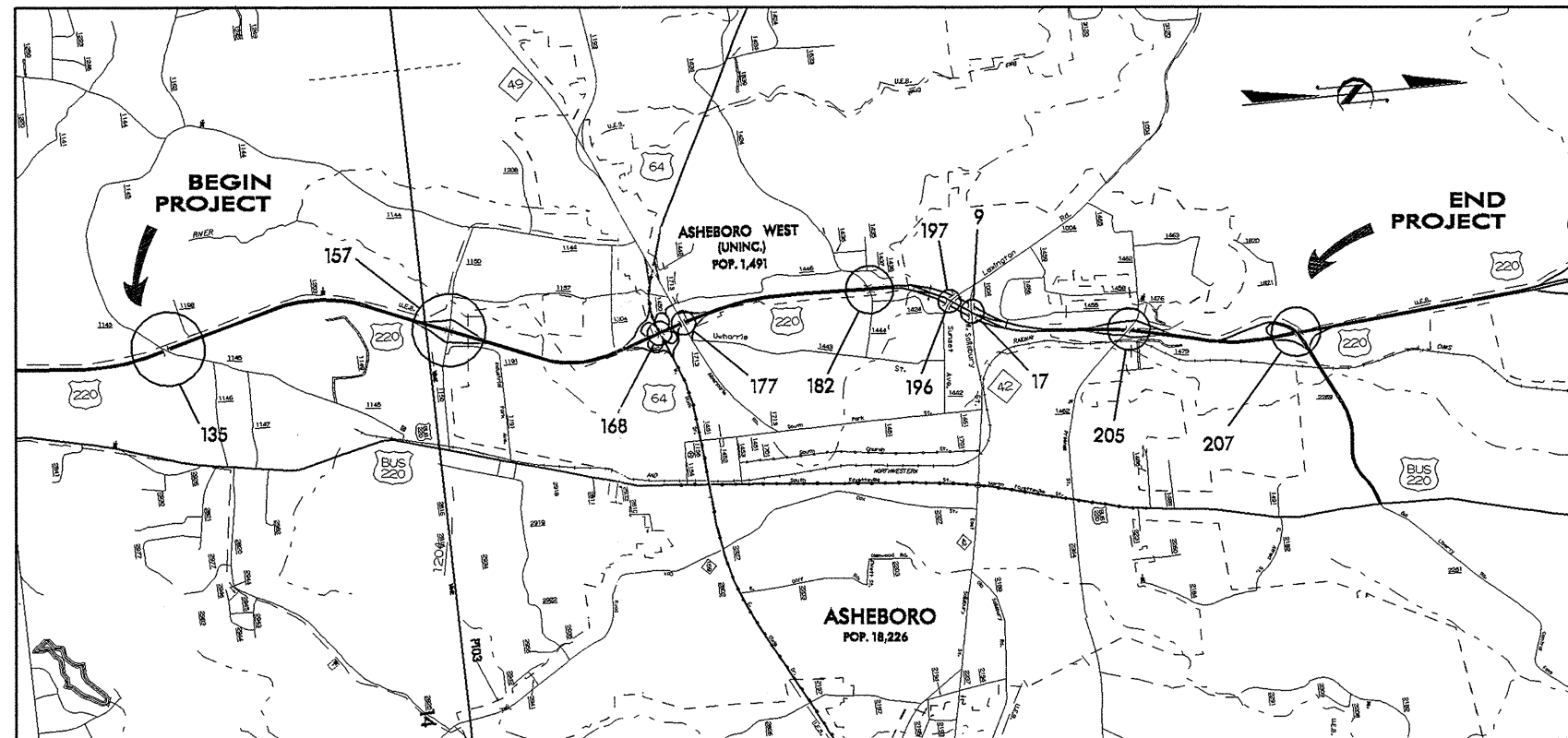


STATE OF NORTH CAROLINA
DIVISION OF HIGHWAYS

RANDOLPH COUNTY

| | | | |
|-------------------|-----------------------------|-------------|--------------|
| STATE | STATE PROJECT REFERENCE NO. | SHEET NO. | TOTAL SHEETS |
| N.C. | B-4700F | 1 | |
| STATE PROJECT NO. | F.A. PROJ. NO. | DESCRIPTION | |
| 36727.3.7 | BRNHS-000S(370) | | |
| 8SP.10764.04 | | | |
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| | | | |

LOCATION: US 220 BRIDGE MAINTENANCE
TYPE OF WORK: CLEANING & PAINTINGS OF BRIDGES #207, #205, #9, #17, #197, #196, #182, #177, #168, #157, #135



DESIGN DATA

ADT 2005 = 38000

PROJECT LENGTH

LENGTH ROADWAY TIP PROJECT = 6.000 MILES

TOTAL LENGTH TIP PROJECT = 6.000 MILES

Prepared in the Office of:
BRIDGE MANAGEMENT UNIT
NORTH CAROLINA DEPARTMENT OF TRANSPORTATION

2006 STANDARD SPECIFICATIONS

LETTING DATE:
December 16, 2008

DAN HOLDERMAN, PE
STATE BRIDGE
MANAGEMENT ENGINEER

NCDOT CONTACT:

MIKE SUMMERS
BRIDGE MANAGEMENT
PROJECT MANAGER

RICK NELSON, PE
DESIGN ENGINEER

STATE OF NORTH CAROLINA
DIVISION OF HIGHWAYS

**PLAN FOR PROPOSED
TRAFFIC CONTROL, MARKING & DELINEATION
RANDOLPH COUNTY**

| | |
|---|--------------------|
| STATE PROJECT REFERENCE NO. B-4700F | SHEET NO. TCP-1 |
|---|--------------------|

B-4700F

ROADWAY STANDARD DRAWINGS

THE FOLLOWING ROADWAY STANDARDS AS APPEAR IN "ROADWAY STANDARD DRAWINGS"-
ROADWAY DESIGN UNIT-N.C. DEPARTMENT OF TRANSPORTATION-RALEIGH, N.C.,
DATED JANUARY 2006 ARE APPLICABLE TO THIS PROJECT AND BY REFERENCE HEREBY ARE
CONSIDERED A PART OF THESE PLANS:

| STD. NO. | TITLE |
|----------|---------------------------------|
| 1101.02 | TEMPORARY LANE CLOSURES |
| 1101.03 | TEMPORARY ROAD CLOSURES |
| 1101.04 | TEMPORARY SHOULDER CLOSURES |
| 1101.11 | TRAFFIC CONTROL DESIGN TABLES |
| 1110.02 | PORTABLE WORK ZONE SIGNS |
| 1115.01 | FLASHING ARROW PANELS |
| 1130.01 | DRUM |
| 1145.01 | BARRICADES |
| 1150.01 | FLAGGING DEVICES |
| 1165.01 | TRUCK MOUNTED IMPACT ATTENUATOR |
| 1180.01 | SKINNY-DRUM |

LEGEND

GENERAL

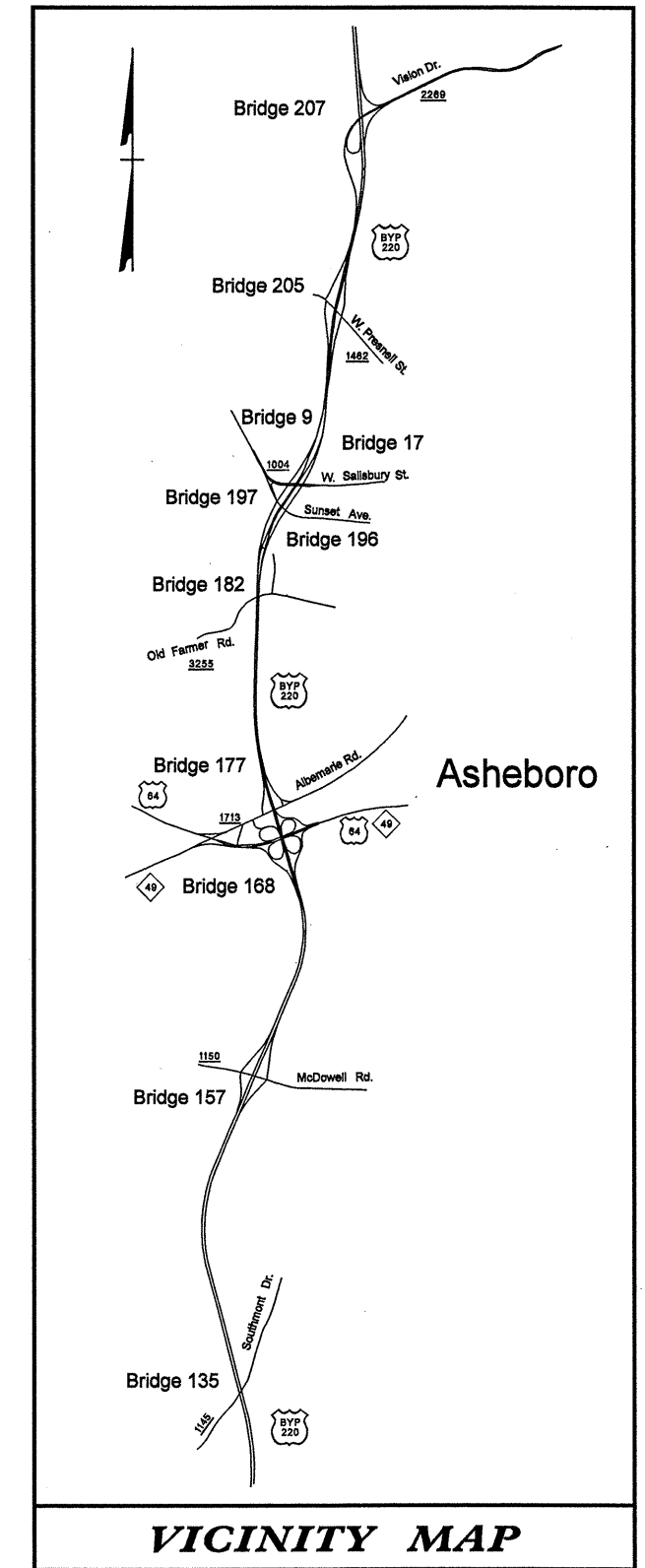
- ← DIRECTION OF TRAFFIC FLOW
- ↑ NORTH ARROW
- WORK AREA

TRAFFIC CONTROL DEVICES

- ▩ TYPE III BARRICADE
- ◎ SKINNY DRUM
- DRUM
- ⦿ FLASHING ARROW PANEL (TYPE C)
- ⚠ PORTABLE SIGN
- ◀ CHANGEABLE MESSAGE SIGN
- ⚡ FLAGGER

INDEX OF SHEETS

| SHEET NO. | TITLE |
|-----------------|--|
| TCP-1 | LIST OF APPLICABLE ROADWAY STANDARD DRAWINGS, LEGEND, INDEX OF SHEETS AND VICINITY MAP |
| TCP-2 | PROJECT NOTES |
| TCP-3 | BRIDGE #135 & #157 PHASING, ROAD CLOSURE, SIGN DETAIL AND DETOUR MAP |
| TCP-4 | BRIDGE #168 & #177 SOUTH BOUND PHASING, SIGN DETAIL AND DETOUR MAP |
| TCP-5 | BRIDGE #168 & #177 SOUTH BOUND ROAD CLOSURE AND SIGN DETAIL |
| TCP-6 | BRIDGE #168 & #177 NORTH BOUND PHASING, SIGN DETAIL AND DETOUR MAP |
| TCP-7 | BRIDGE #168 & #177 NORTH BOUND ROAD CLOSURE AND SIGN DETAIL |
| TCP-8 | BRIDGE #182 PHASING AND DETAILS |
| TCP-9 | BRIDGE #182 FLAGGING OPERATION DETAIL |
| TCP-10 & TCP-11 | BRIDGE #9, #17, #196 & #197 PHASING, ROAD CLOSURE AND SIGN DETAIL |
| TCP-12 & TCP-13 | BRIDGE #205 SOUTH BOUND PHASING, ROAD CLOSURE AND SIGN DETAIL |
| TCP-14 & TCP-15 | BRIDGE #205 NORTH BOUND PHASING, ROAD CLOSURE AND SIGN DETAIL |
| TCP-16 | BRIDGE #207 PHASING, ROAD CLOSURE, SIGN DETAIL AND DETOUR MAP |
| TCP-17 | STANDARD ROAD CLOSURE AND SIGN DETAIL |



VICINITY MAP

PROJECT:

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| | |
|-------------------------------------|---|
| APPROVED: <i>Meredith McDiarmid</i> | PLAN PREPARED BY: N.C.D.O.T. WORK ZONE TRAFFIC CONTROL UNIT |
| DATE: 8/5/08 | |
| SEAL | J. S. BOURNE, P.E. TRAFFIC CONTROL ENGINEER |
| | M. McDIARMID, P.E. TRAFFIC CONTROL PROJECT ENGINEER |
| | C. B. HOWARD TRAFFIC CONTROL PROJECT DESIGN ENGINEER |
| | M. D. STOKES TRAFFIC CONTROL DESIGN ENGINEER / TECHNICIAN |



PROJECT NOTES

| | |
|---------------------|-----------|
| PROJ. REFERENCE NO. | SHEET NO. |
| B-4700F | TCP-2 |

GENERAL NOTES

CHANGES MAY BE REQUIRED WHEN PHYSICAL DIMENSIONS IN THE DETAIL DRAWINGS, STANDARD DETAILS AND ROADWAY DETAILS ARE NOT ATTAINABLE TO MEET FIELD CONDITIONS, OR RESULT IN DUPLICATE, OR UNDESIRE OVERLAPPING OF DEVICES. MODIFICATION MAY INCLUDE: MOVING, SUPPLEMENTING, COVERING OR REMOVAL OF DEVICES, AS DIRECTED BY THE ENGINEER.

THE FOLLOWING GENERAL NOTES APPLY AT ALL TIMES FOR THE DURATION OF THE CONSTRUCTION PROJECT, EXCEPT WHEN OTHERWISE NOTED IN THE PLAN, OR DIRECTED BY THE ENGINEER.

TIME RESTRICTIONS

A) DO NOT CLOSE ROAD, CLOSE LANES OR NARROW TRAVEL LANES AS FOLLOWS:

| ROAD NAME | BRIDGE | DAY AND TIME RESTRICTIONS |
|------------|---|---|
| 1. US 220 | #9, #17, #135, #157 #168, #177, #196, #197, #205 AND #207 | MONDAY THRU SUNDAY 6:00 A.M. TO 8:00 P.M. |
| 2. SR 3255 | #182 | MONDAY THRU SUNDAY 11:00 P.M. TO 6:00 A.M. |

B) DO NOT CLOSE ROAD, CLOSE LANES OR NARROW TRAVEL LANES DURING HOLIDAYS AND SPECIAL EVENTS AS FOLLOWS:

| ROAD NAME | BRIDGE |
|------------|--|
| 1. US 220 | #9, #17, #135, #157, #168, #177, #196, #197, #205, AND #207 |
| 2. SR 3255 | #182 |

HOLIDAY

1. FOR ANY UNEXPECTED OCCURRENCE THAT CREATES UNUSUALLY HIGH TRAFFIC VOLUMES, AS DIRECTED BY THE ENGINEER.
2. FOR NEW YEAR'S, BETWEEN THE HOURS OF 8:00 P.M. DECEMBER 31ST TO 6:00 A.M. JANUARY 2ND. IF NEW YEAR'S DAY IS ON A FRIDAY, SATURDAY, SUNDAY, OR MONDAY THEN UNTIL 6:00 A.M. THE FOLLOWING TUESDAY.
3. FOR EASTER, BETWEEN THE HOURS OF 8:00 P.M. THURSDAY AND 6:00 A.M. MONDAY.
4. FOR MEMORIAL DAY, BETWEEN THE HOURS OF 8:00 P.M. FRIDAY TO 6:00 A.M. TUESDAY.
5. FOR INDEPENDENCE DAY, BETWEEN THE HOURS OF 8:00 P.M. THE DAY BEFORE INDEPENDENCE DAY AND 6:00 A.M. THE DAY AFTER INDEPENDENCE DAY. IF INDEPENDENCE DAY IS ON A FRIDAY, SATURDAY, SUNDAY OR MONDAY, THEN BETWEEN THE HOURS OF 8:00 P.M. THE THURSDAY BEFORE INDEPENDENCE DAY AND 6:00 A.M. THE TUESDAY AFTER INDEPENDENCE DAY.
6. FOR LABOR DAY, BETWEEN THE HOURS OF 8:00 P.M. FRIDAY AND 6:00 A.M. TUESDAY.
7. FOR THANKSGIVING DAY, BETWEEN THE HOURS OF 8:00 P.M. TUESDAY TO 6:00 A.M. MONDAY.
8. FOR CHRISTMAS, BETWEEN THE HOURS OF 8:00 P.M. THE FRIDAY BEFORE THE WEEK OF CHRISTMAS DAY AND 6:00 A.M. THE FOLLOWING TUESDAY AFTER THE WEEK OF CHRISTMAS.

LANE AND SHOULDER CLOSURE REQUIREMENTS

- C) REMOVE LANE CLOSURE DEVICES FROM THE LANE WHEN WORK IS NOT BEING PERFORMED BEHIND THE LANE CLOSURE OR WHEN A LANE CLOSURE IS NO LONGER NEEDED, OR AS DIRECTED BY THE ENGINEER.
- D) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN 15 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN SHOULDER USING ROADWAY STANDARD DRAWING NO. 1101.04 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL OR A LANE CLOSURE IS INSTALLED.
- E) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING ON THE SHOULDER ADJACENT TO AN UNDIVIDED FACILITY AND WITHIN 5 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN TRAVEL LANE USING ROADWAY STANDARD DRAWING NO. 1101.02 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.
- F) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING ON THE SHOULDER ADJACENT TO A DIVIDED FACILITY AND WITHIN 10 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN TRAVEL LANE USING ROADWAY STANDARD DRAWING NO. 1101.02 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.
- G) DO NOT WORK SIMULTANEOUSLY WITHIN A LANE OF TRAVEL OF AN UNDIVIDED OR DIVIDED FACILITY, CLOSE THE LANE ACCORDING TO THE TRAFFIC CONTROL PLANS, ROADWAY STANDARD DRAWINGS OR AS DIRECTED BY THE ENGINEER. CONDUCT THE WORK SO THAT ALL PERSONNEL AND/OR EQUIPMENT REMAIN WITHIN THE CLOSED TRAVEL LANE.
- H) DO NOT WORK SIMULTANEOUSLY WITHIN 15 FT ON BOTH SIDES OF AN OPEN TRAVELWAY, RAMP OR LOOP WITHIN THE SAME LOCATION UNLESS PROTECTED WITH GUARDRAIL OR BARRIER.
- I) DO NOT INSTALL MORE THAN ONE ROAD CLOSURE AT A TIME ON US 220.

TRAFFIC PATTERN ALTERATIONS

- I) NOTIFY THE ENGINEER TWENTY ONE (21) CALENDAR DAYS PRIOR TO ANY TRAFFIC PATTERN ALTERATION.

SIGNING

- J) INSTALL ADVANCE WORK ZONE WARNING SIGNS WHEN WORK IS WITHIN 40 FT FROM THE EDGE OF TRAVEL LANE AND NO MORE THAN THREE (3) DAYS PRIOR TO THE BEGINNING OF CONSTRUCTION.
- K) PROVIDE DETOUR SIGNING WITHIN AND OFF THE PROJECT LIMITS.
- L) COVER OR REMOVE ALL DETOUR SIGNS WITHIN AND OFF THE PROJECT LIMITS WHEN A DETOUR IS NOT IN OPERATION.
- M) ENSURE ALL NECESSARY SIGNING IS IN PLACE PRIOR TO ALTERING ANY TRAFFIC PATTERN.
- N) INSTALL SIGNS WITH ROAD NAME AND DIRECTION IF THERE ARE NO EXISTING SIGNS AT OR NEAR THE "END DETOUR" SIGN AT INTERCHANGES.


TRAFFIC CONTROL DEVICES

- O) SPACE CHANNELIZING DEVICES IN WORK AREAS NO GREATER THAN TWICE THE POSTED SPEED LIMIT (MPH), EXCEPT 10 FT ON-CENTER IN RADII, AND 3 FT OFF THE EDGE OF AN OPEN TRAVELWAY, WHEN LANE CLOSURES ARE NOT IN EFFECT. WHEN SKINNY DRUMS ARE ALLOWED, REFER TO SECTION 1180 OF STANDARD SPECIFICATIONS FOR ROADS AND STRUCTURES OR AS SHOWN IN THE PLANS.
- P) PLACE TYPE III BARRICADES, WITH "ROAD CLOSED" SIGN R11-2 ATTACHED, OF SUFFICIENT LENGTH TO CLOSE ENTIRE ROADWAY.

MISCELLANEOUS

- Q) POLICE MAY BE USED TO MAINTAIN TRAFFIC THROUGH THE WORK AREA AND/OR INTERSECTIONS, AS DIRECTED BY THE ENGINEER.

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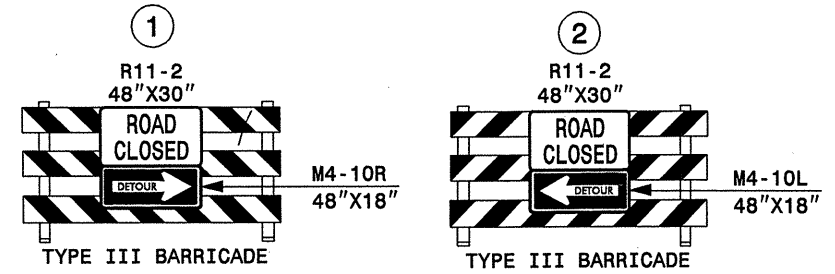
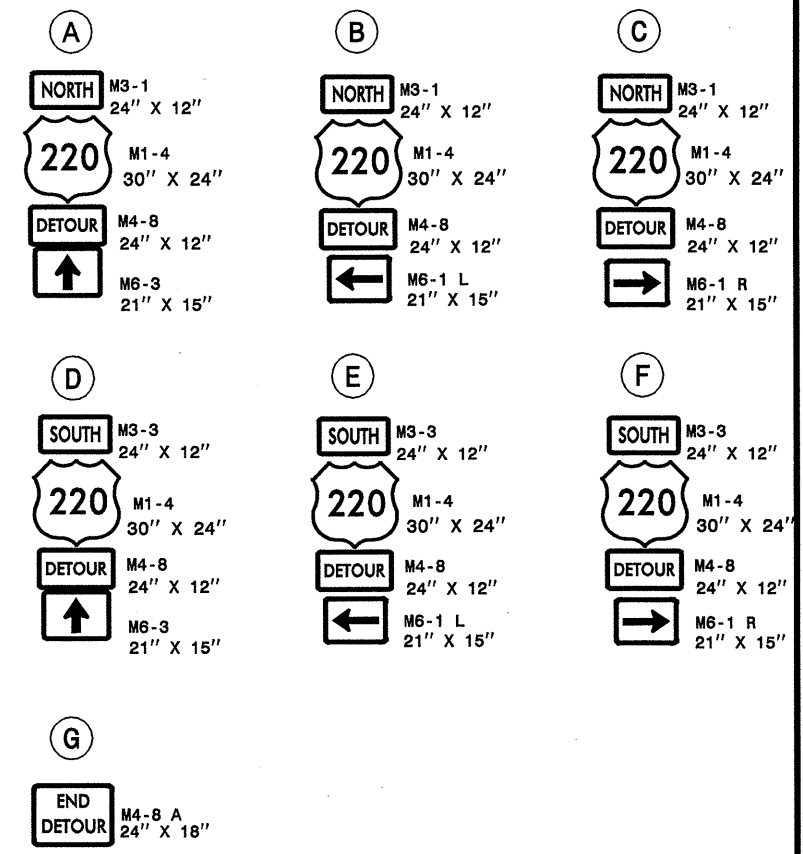
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| SCALE: | NONE | | | | | | | | | | | |
| DATE: | 6-08 | | | | | | | | | | | |
| DWG. BY: | MDS | | | | | | | | | | | |
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| REVIEWED BY: | CBH | | | | | | | | | | | |
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| NO. | DESCRIPTION | | | | | | | | | | | |
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PHASING- BRIDGE #135 & #157

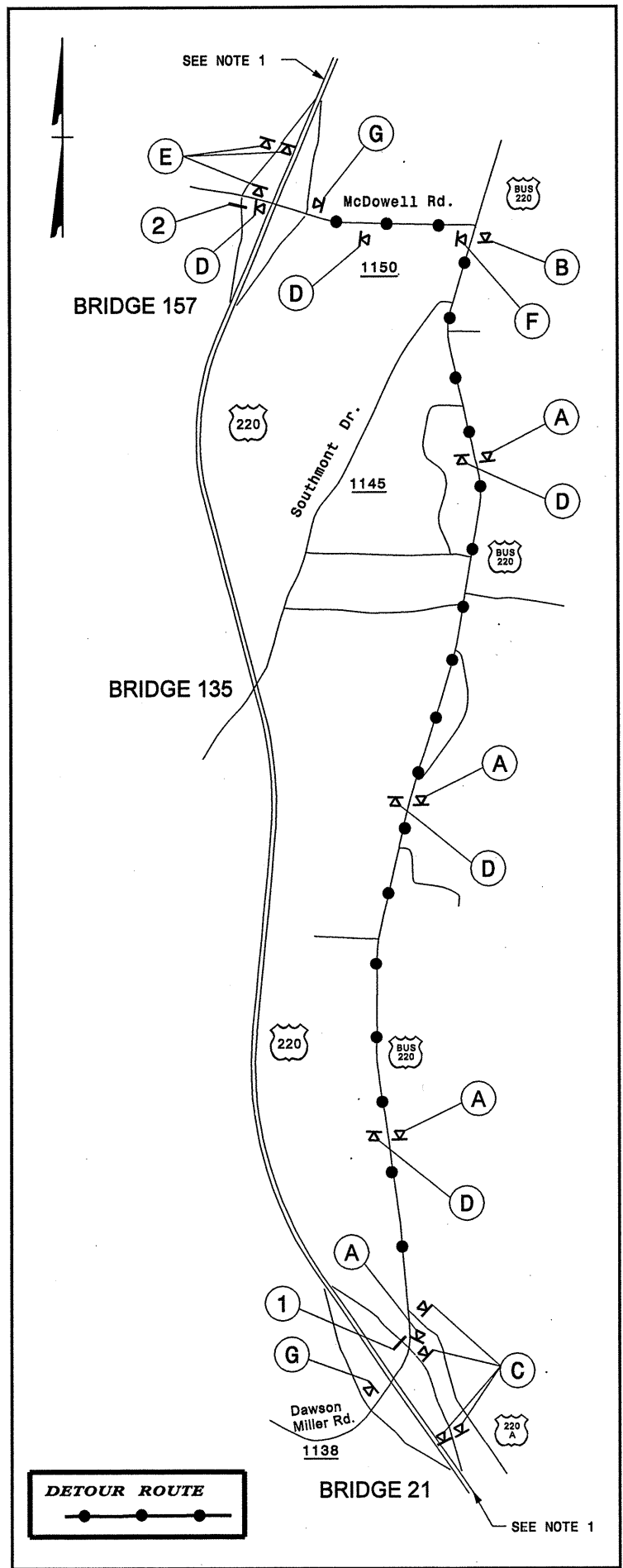
NOTE: REMOVE ALL TRAFFIC CONTROL DEVICES AT THE END OF EACH WORK DAY AND RETURN TRAFFIC TO THE EXISTING PATTERN.

US 220 SHALL BE CLOSED AND TRAFFIC DETOURED TO PERFORM PAINTING OPERATIONS ON BRIDGES #135 AND #157. BRIDGES #135 AND #157 ARE PHASED BY WORKING ON BOTH BRIDGES SIMULTANEOUSLY. HOWEVER, WORK IS TO BE PERFORMED IN ONE DIRECTION AT A TIME. PAINTING OPERATIONS FOR ONE DIRECTION MUST BE COMPLETED BEFORE STARTING WORK IN THE OTHER DIRECTION.

- STEP 1- USING THIS SHEET, INSTALL DETOUR SIGNING FOR THE US 220 SOUTH BOUND ROAD CLOSURE.
- STEP 2- USING THIS SHEET AND SHEET TCP-17, CLOSE BOTH US 220 SOUTH BOUND LANES AT BRIDGE #157 AND THE US 220 SOUTH BOUND ENTRANCE RAMP AT BRIDGE #157 AND SHIFT TRAFFIC TO THE EXIT RAMP TO DETOUR TRAFFIC AROUND THE WORK AREA.
- STEP 3- PLACE A CONTAINMENT SYSTEM TO COLLECT DEBRIS AT EACH BRIDGE.
- STEP 4- WHEN THE PAINT HAS BEEN REMOVED AND CONTAINED AT EACH BRIDGE, PERFORM PAINTING OPERATIONS. WHEN PAINTING IS COMPLETE, REMOVE ALL EQUIPMENT AND TRAFFIC CONTROL DEVICES.
- STEP 5- USING THIS SHEET, INSTALL DETOUR SIGNING FOR THE US 220 NORTH BOUND ROAD CLOSURE.
- STEP 6- USING THIS SHEET AND SHEET TCP-17, CLOSE BOTH US 220 NORTH BOUND LANES AT BRIDGE #21 AND THE US 220 NORTH BOUND ENTRANCE RAMP AT BRIDGE #21 AND SHIFT TRAFFIC TO THE EXIT RAMP TO DETOUR TRAFFIC AROUND THE WORK AREA.
- STEP 7- REPEAT STEPS 3 AND 4 TO COMPLETE BRIDGE PAINTING OPERATIONS.



- NOTES:
- 1- USE SHEET TCP-17 TO COMPLETE THE INSTALLATION OF THE TRAFFIC CONTROL DEVICES NECESSARY FOR THE WORK TO BE PERFORMED. CLOSE BOTH US 220 LANES AS DIRECTED IN THE PHASING AND INSTALL CHANNELIZING DEVICES FOR DIRECTING TRAFFIC TO THE EXIT RAMP. INSTALL ADVANCE WARNING SIGNING, EXIT SIGNING AND ARROW PANEL AS SHOWN PER THE SHEET'S DETAILS. INSTALL THE TRAFFIC CONTROL DEVICES AS INSTRUCTED PER THE SHEET'S GENERAL NOTES.
 - 2- ALL SIGN LOCATIONS SHOWN ON DETOUR MAP ARE APPROXIMATE AND SHOULD BE FIELD ADJUSTED AS NECESSARY TO INSURE MOTORIST CAN EASILY NAVIGATE DETOUR. USE ADDITIONAL SIGNS AS NEEDED OR AS DIRECTED BY THE ENGINEER.



| | | | | | | | | |
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| APPROVED: <i>[Signature]</i> | BRIDGE #135 & #157 PHASING, ROAD CLOSURE, SIGN DETAIL AND DETOUR MAP | | | | | | | |
| | SCALE: NONE DATE: 6-08 DWG. BY: MDS DESIGN BY: MDS REVIEWED BY: CBH | | REVISIONS <table border="1"> <tr><td> </td><td> </td></tr> <tr><td> </td><td> </td></tr> <tr><td> </td><td> </td></tr> </table> | | | | | |
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PHASING- BRIDGE #168 & #177 SOUTH BOUND ROAD CLOSURE

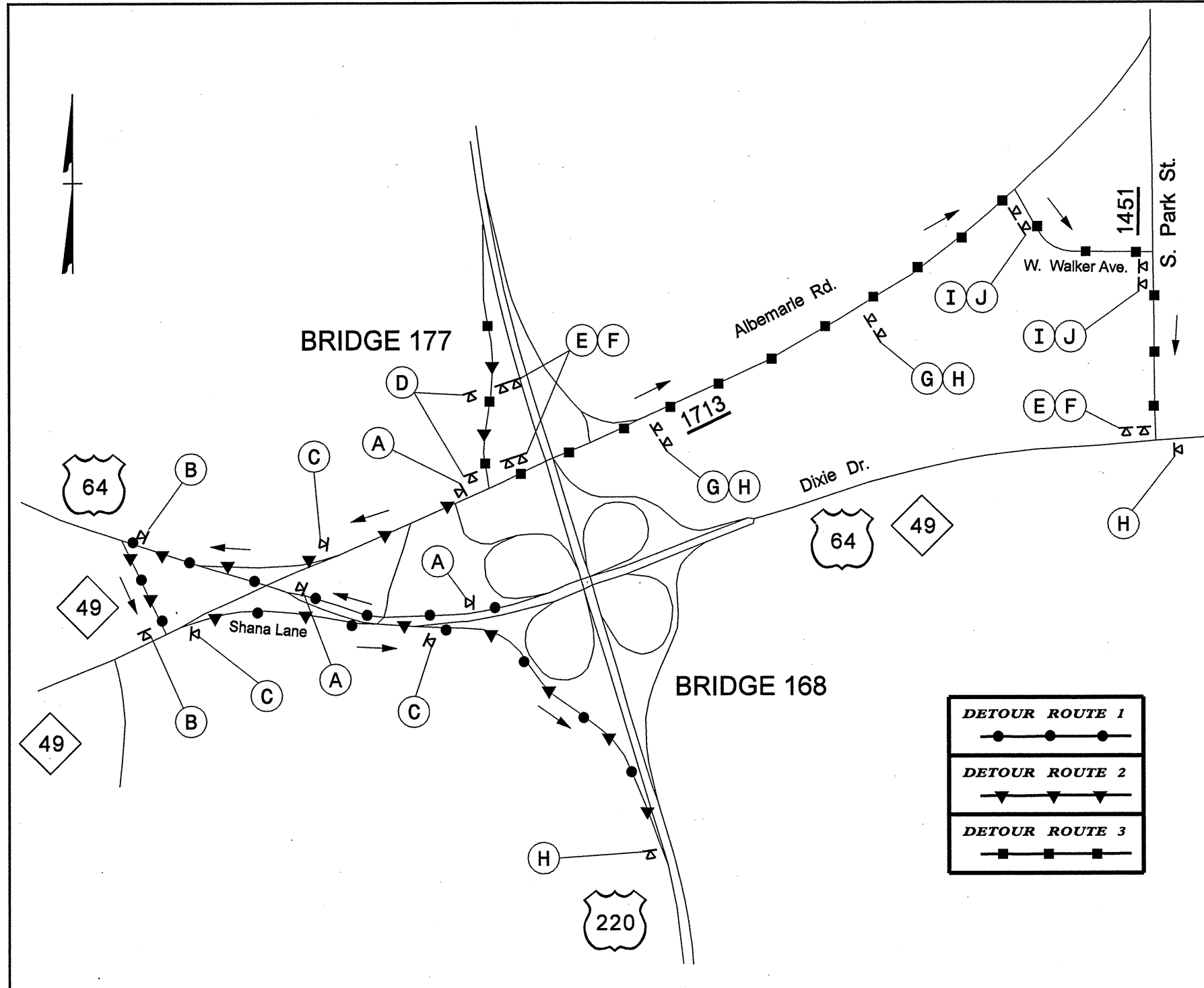
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| PROJ. REFERENCE NO. B-4700F | SHEET NO. TCP-4 |
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NOTE: REMOVE ALL TRAFFIC CONTROL DEVICES AT THE END OF EACH WORK DAY AND RETURN TRAFFIC TO THE EXISTING PATTERN.

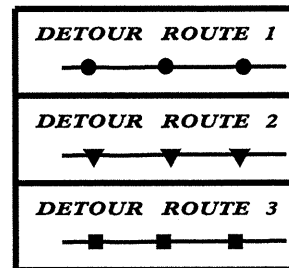
US 220 SHALL BE CLOSED AND TRAFFIC DETOURED TO PERFORM PAINTING OPERATIONS ON BRIDGES #168 AND #177. BRIDGES #168 AND #177 ARE PHASED BY WORKING ON BOTH BRIDGES SIMULTANEOUSLY. HOWEVER, WORK IS TO BE PERFORMED IN ONE DIRECTION AT A TIME. PAINTING OPERATIONS FOR ONE DIRECTION MUST BE COMPLETED BEFORE STARTING WORK IN THE OTHER DIRECTION.

- STEP 2- USING THIS SHEET, SHEET TCP-5 AND SHEET TCP-17, CLOSE BOTH US 220 SOUTH BOUND LANES AT BRIDGE #177, THE US 220 SOUTH BOUND ENTRANCE RAMP AT BRIDGE #168 AND THE US 220 SOUTH BOUND ENTRANCE RAMP WEST OF BRIDGE #177 OFF OF ALBEMARLE ROAD. SHIFT TRAFFIC TO THE EXIT RAMP AT BRIDGE #177 TO DETOUR TRAFFIC AROUND THE WORK AREA.
- STEP 3- PLACE A CONTAINMENT SYSTEM TO COLLECT DEBRIS AT EACH BRIDGE.
- STEP 4- WHEN THE PAINT HAS BEEN REMOVED AND CONTAINED AT EACH BRIDGE, PERFORM PAINTING OPERATIONS. WHEN PAINTING IS COMPLETE, REMOVE ALL EQUIPMENT AND TRAFFIC CONTROL DEVICES.

STEP 1- USING THIS SHEET, INSTALL DETOUR SIGNING FOR THE US 220 SOUTH BOUND ROAD CLOSURE.



| | |
|---|---|
| <p>(A) SOUTH 220 24" X 12" M3-3 M1-4 30" X 24" DETOUR M4-B 24" X 12" M6-3 21" X 15"</p> | <p>(B) SOUTH 220 24" X 12" M3-3 M1-4 30" X 24" DETOUR M4-B 24" X 12" M6-1 L 21" X 15"</p> |
| <p>(C) SOUTH 220 24" X 12" M3-3 M1-4 30" X 24" DETOUR M4-B 24" X 12" M6-2 21" X 15"</p> | <p>(D) SOUTH 220 24" X 12" M3-3 M1-4 30" X 24" DETOUR M4-B 24" X 12" M6-1 R 21" X 15"</p> |
| <p>(E) EAST 64 24" X 12" M3-2 M1-4 24" X 24" DETOUR M4-B 24" X 12" M6-1 L 21" X 15"</p> | <p>(F) NORTH 49 24" X 12" M3-1 M1-5 24" X 24" DETOUR M4-B 24" X 12" M6-1 L 21" X 15"</p> |
| <p>(G) EAST 64 24" X 12" M3-2 M1-4 24" X 24" DETOUR M4-B 24" X 12" M6-3 21" X 15"</p> | <p>(H) NORTH 49 24" X 12" M3-1 M1-5 24" X 24" DETOUR M4-B 24" X 12" M6-3 21" X 15"</p> |
| <p>(I) EAST 64 24" X 12" M3-2 M1-4 24" X 24" DETOUR M4-B 24" X 12" M6-1 R 21" X 15"</p> | <p>(J) NORTH 49 24" X 12" M3-1 M1-5 24" X 24" DETOUR M4-B 24" X 12" M6-1 R 21" X 15"</p> |
| <p>(H) END DETOUR M4-B A 24" X 18"</p> | |








NOTES:

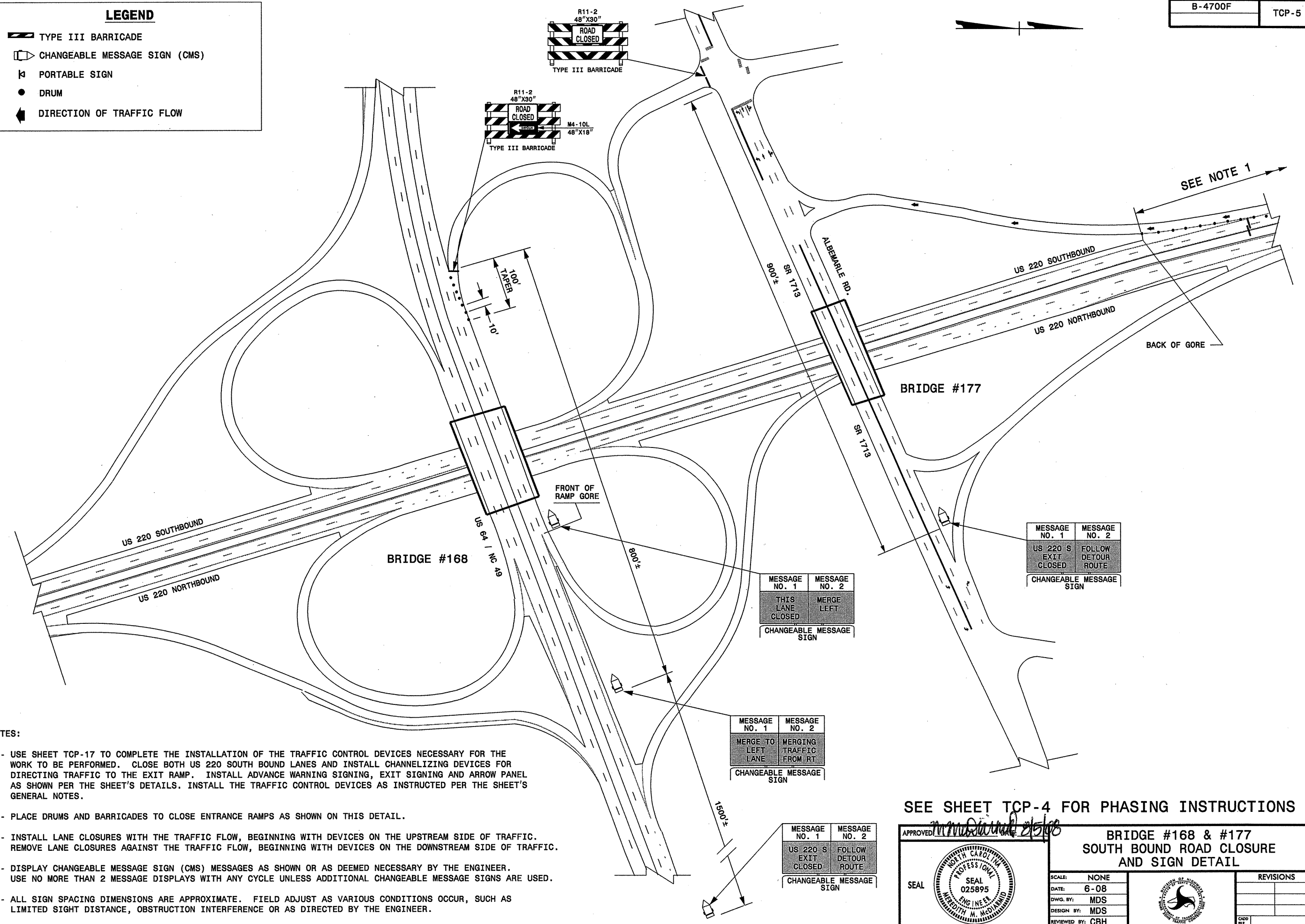
- 1- ALL SIGN LOCATIONS SHOWN ON DETOUR MAP ARE APPROXIMATE AND SHOULD BE FIELD ADJUSTED AS NECESSARY TO INSURE MOTORIST CAN EASILY NAVIGATE DETOUR. USE ADDITIONAL SIGNS AS NEEDED OR AS DIRECTED BY THE ENGINEER.

| | | |
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| APPROVED: <i>[Signature]</i> DATE: 8/19/08 | BRIDGE #168 & #177 SOUTH BOUND PHASING, SIGN DETAIL AND DETOUR MAP | |
| | SCALE: NONE | REVISIONS |
| | DATE: 6-08 | |
| | DWG. BY: MDS | |
| | DESIGN BY: MDS | |
| REVIEWED BY: CBH | | |

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LEGEND

-  TYPE III BARRICADE
-  CHANGEABLE MESSAGE SIGN (CMS)
-  PORTABLE SIGN
-  DRUM
-  DIRECTION OF TRAFFIC FLOW

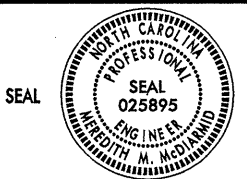



NOTES:

- 1 - USE SHEET TCP-17 TO COMPLETE THE INSTALLATION OF THE TRAFFIC CONTROL DEVICES NECESSARY FOR THE WORK TO BE PERFORMED. CLOSE BOTH US 220 SOUTH BOUND LANES AND INSTALL CHANNELIZING DEVICES FOR DIRECTING TRAFFIC TO THE EXIT RAMP. INSTALL ADVANCE WARNING SIGNING, EXIT SIGNING AND ARROW PANEL AS SHOWN PER THE SHEET'S DETAILS. INSTALL THE TRAFFIC CONTROL DEVICES AS INSTRUCTED PER THE SHEET'S GENERAL NOTES.
- 2 - PLACE DRUMS AND BARRICADES TO CLOSE ENTRANCE RAMP AS SHOWN ON THIS DETAIL.
- 3 - INSTALL LANE CLOSURES WITH THE TRAFFIC FLOW, BEGINNING WITH DEVICES ON THE UPSTREAM SIDE OF TRAFFIC. REMOVE LANE CLOSURES AGAINST THE TRAFFIC FLOW, BEGINNING WITH DEVICES ON THE DOWNSTREAM SIDE OF TRAFFIC.
- 4 - DISPLAY CHANGEABLE MESSAGE SIGN (CMS) MESSAGES AS SHOWN OR AS DEEMED NECESSARY BY THE ENGINEER. USE NO MORE THAN 2 MESSAGE DISPLAYS WITH ANY CYCLE UNLESS ADDITIONAL CHANGEABLE MESSAGE SIGNS ARE USED.
- 5 - ALL SIGN SPACING DIMENSIONS ARE APPROXIMATE. FIELD ADJUST AS VARIOUS CONDITIONS OCCUR, SUCH AS LIMITED SIGHT DISTANCE, OBSTRUCTION INTERFERENCE OR AS DIRECTED BY THE ENGINEER.

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 AT WZTC244745

SEE SHEET TCP-4 FOR PHASING INSTRUCTIONS

| APPROVED: <i>[Signature]</i> | | BRIDGE #168 & #177 SOUTH BOUND ROAD CLOSURE AND SIGN DETAIL | | | | | | | | |
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|  | SCALE: NONE |  | | | | | | | | |
| | DATE: 6-08 | | | | | | | | | |
| | DWG. BY: MDS | | | | | | | | | |
| | DESIGN BY: MDS | | | | | | | | | |
| REVIEWED BY: CBH | <table border="1"> <tr> <th colspan="2">REVISIONS</th> </tr> <tr> <td> </td> <td> </td> </tr> <tr> <td> </td> <td> </td> </tr> <tr> <td> </td> <td> </td> </tr> </table> | | REVISIONS | | | | | | | |
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PHASING- BRIDGE #168 & #177 NORTH BOUND ROAD CLOSURE

| | |
|--------------------------------|--------------------|
| PROJ. REFERENCE NO. B-4700F | SHEET NO. TCP-6 |
|--------------------------------|--------------------|

NOTE: REMOVE ALL TRAFFIC CONTROL DEVICES AT THE END OF EACH WORK DAY AND RETURN TRAFFIC TO THE EXISTING PATTERN.

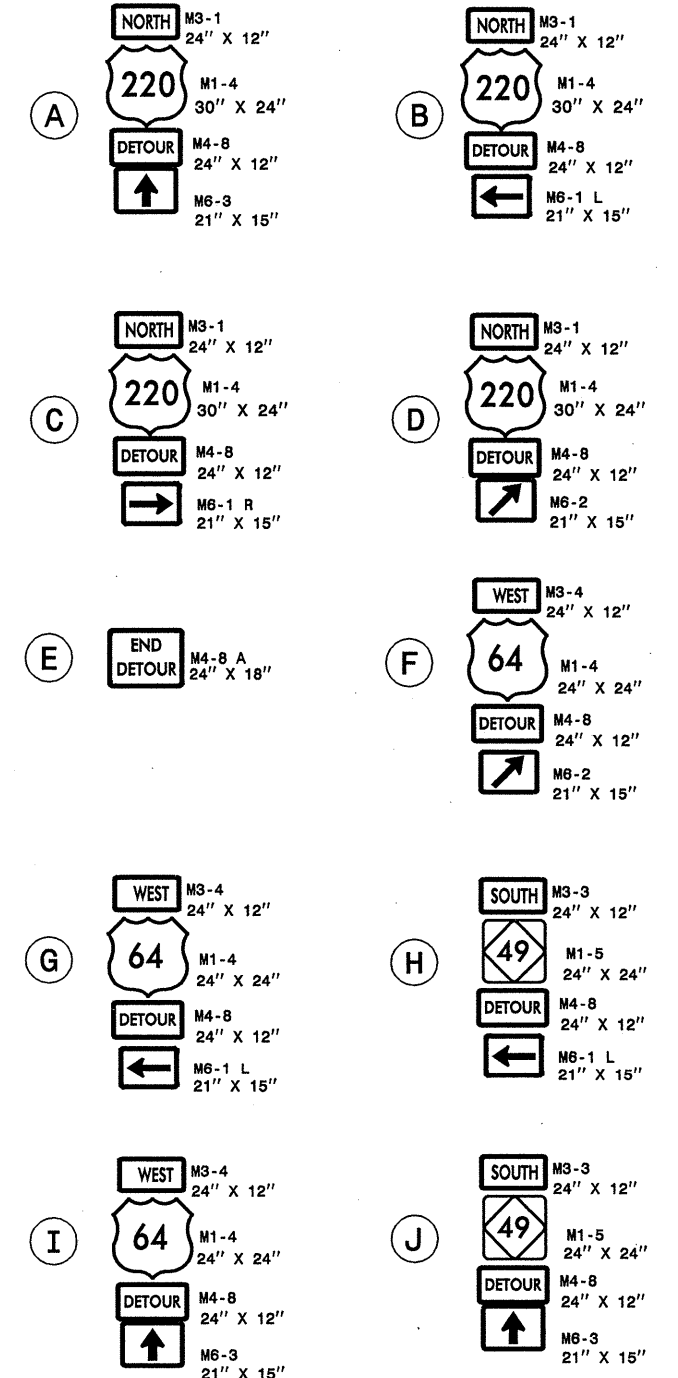
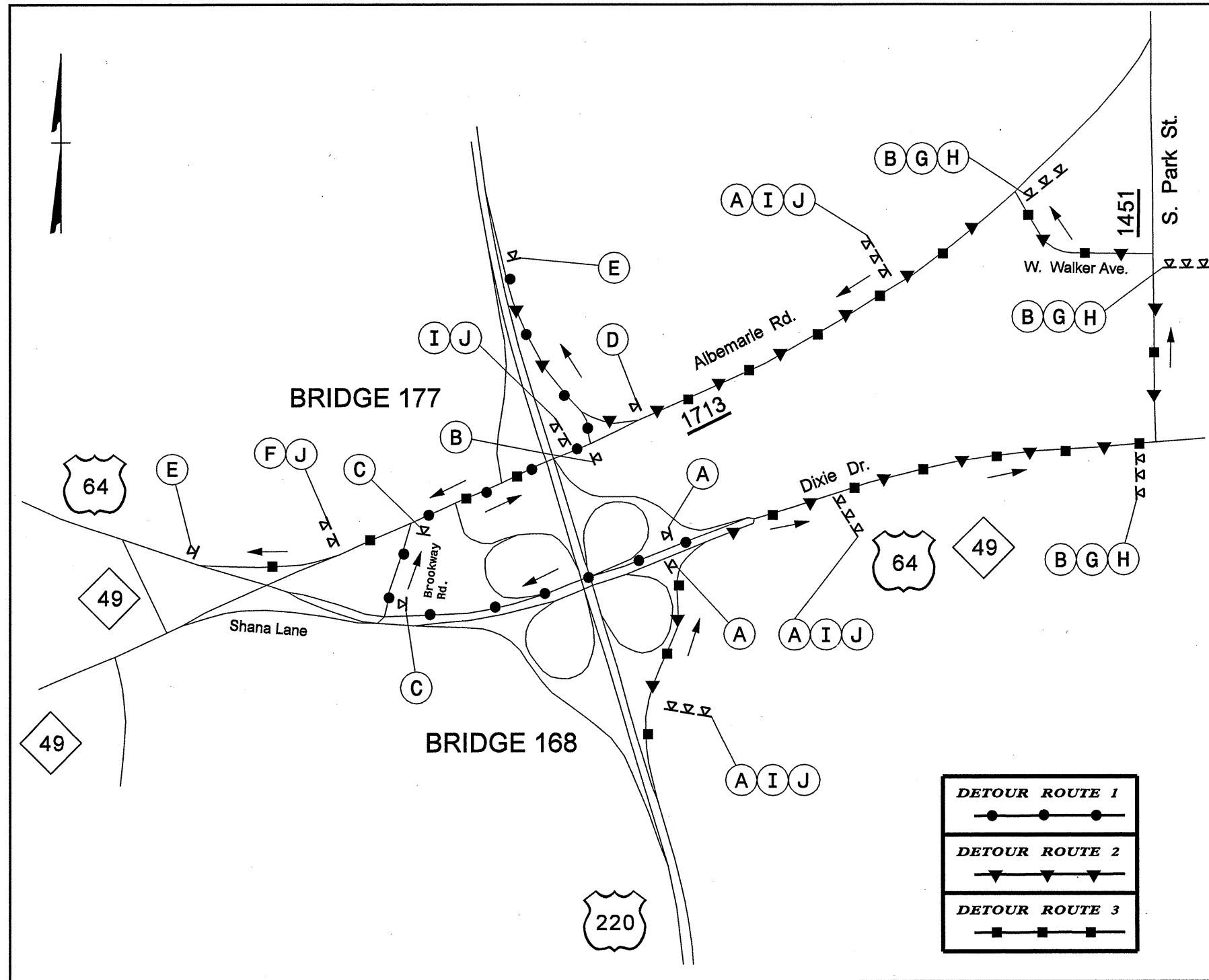
US 220 SHALL BE CLOSED AND TRAFFIC DETOURED TO PERFORM PAINTING OPERATIONS ON BRIDGES #168 AND #177. BRIDGES #168 AND #177 ARE PHASED BY WORKING ON BOTH BRIDGES SIMULTANEOUSLY. HOWEVER, WORK IS TO BE PERFORMED IN ONE DIRECTION AT A TIME. PAINTING OPERATIONS FOR ONE DIRECTION MUST BE COMPLETED BEFORE STARTING WORK IN THE OTHER DIRECTION.

STEP 2- USING THIS SHEET, SHEET TCP-7 AND SHEET TCP-17, CLOSE BOTH US 220 NORTH BOUND LANES AT BRIDGE #168, THE US 220 NORTH BOUND ENTRANCE RAMP AT BRIDGE #168 AND THE US 220 NORTH BOUND ENTRANCE RAMP EAST OF BRIDGE #168 OFF OF US 64/NC 49. SHIFT TRAFFIC TO THE EXIT RAMP AT BRIDGE #168 TO DETOUR TRAFFIC AROUND THE WORK AREA.

STEP 3- PLACE A CONTAINMENT SYSTEM TO COLLECT DEBRIS AT EACH BRIDGE.

STEP 4- WHEN THE PAINT HAS BEEN REMOVED AND CONTAINED AT EACH BRIDGE, PERFORM PAINTING OPERATIONS. WHEN PAINTING IS COMPLETE, REMOVE ALL EQUIPMENT AND TRAFFIC CONTROL DEVICES.

STEP 1- USING THIS SHEET, INSTALL DETOUR SIGNING FOR THE US 220 NORTH BOUND ROAD CLOSURE.



NOTES:

1- ALL SIGN LOCATIONS SHOWN ON DETOUR MAP ARE APPROXIMATE AND SHOULD BE FIELD ADJUSTED AS NECESSARY TO INSURE MOTORIST CAN EASILY NAVIGATE DETOUR. USE ADDITIONAL SIGNS AS NEEDED OR AS DIRECTED BY THE ENGINEER.

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| | | | |
|---|---|--|-----------|
| APPROVED: <i>[Signature]</i> DATE: 6/5/08 | BRIDGE #168 & #177 NORTH BOUND PHASING, SIGN DETAIL AND DETOUR MAP | | |
| | SCALE: NONE | | |
| | DATE: 6-08 | | REVISIONS |
| | DWG. BY: MDS | | |
| | DESIGN BY: MDS | | |
| REVIEWED BY: CBH | | | |

LEGEND

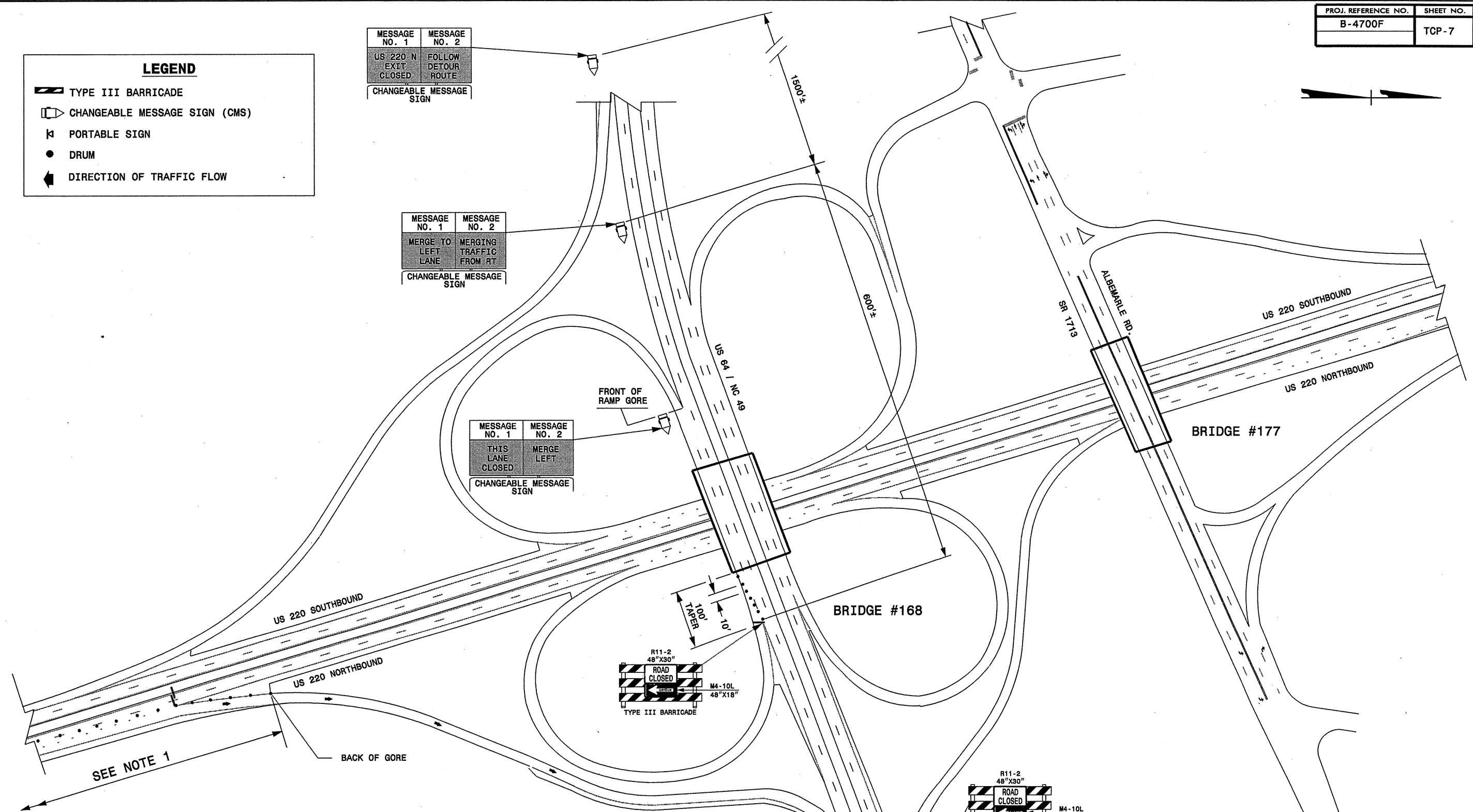
- TYPE III BARRICADE
- CHANGEABLE MESSAGE SIGN (CMS)
- PORTABLE SIGN
- DRUM
- DIRECTION OF TRAFFIC FLOW

| MESSAGE NO. 1 | MESSAGE NO. 2 |
|----------------------------|---------------------------|
| US 220 N EXIT CLOSED | FOLLOW DETOUR ROUTE |
| CHANGEABLE MESSAGE SIGN | |

| MESSAGE NO. 1 | MESSAGE NO. 2 |
|--------------------------|-------------------------------|
| MERGE TO LEFT LANE | MERGING TRAFFIC FROM RT |
| CHANGEABLE MESSAGE SIGN | |

| MESSAGE NO. 1 | MESSAGE NO. 2 |
|-------------------------|---------------|
| THIS LANE CLOSED | MERGE LEFT |
| CHANGEABLE MESSAGE SIGN | |

| MESSAGE NO. 1 | MESSAGE NO. 2 |
|----------------------------|---------------------------|
| US 220 N EXIT CLOSED | FOLLOW DETOUR ROUTE |
| CHANGEABLE MESSAGE SIGN | |



NOTES:

- 1 - USE SHEET TCP-17 TO COMPLETE THE INSTALLATION OF THE TRAFFIC CONTROL DEVICES NECESSARY FOR THE WORK TO BE PERFORMED. CLOSE BOTH US 220 NORTH BOUND LANES AND INSTALL CHANNELIZING DEVICES FOR DIRECTING TRAFFIC TO THE EXIT RAMP. INSTALL ADVANCE WARNING SIGNING, EXIT SIGNING AND ARROW PANEL AS SHOWN PER THE SHEET'S DETAILS. INSTALL THE TRAFFIC CONTROL DEVICES AS INSTRUCTED PER THE SHEET'S GENERAL NOTES.
- 2 - PLACE DRUMS AND BARRICADES TO CLOSE ENTRANCE RAMP AS SHOWN ON THIS DETAIL.
- 3 - INSTALL LANE CLOSURES WITH THE TRAFFIC FLOW, BEGINNING WITH DEVICES ON THE UPSTREAM SIDE OF TRAFFIC. REMOVE LANE CLOSURES AGAINST THE TRAFFIC FLOW, BEGINNING WITH DEVICES ON THE DOWNSTREAM SIDE OF TRAFFIC.
- 4 - DISPLAY CHANGEABLE MESSAGE SIGN (CMS) MESSAGES AS SHOWN OR AS DEEMED NECESSARY BY THE ENGINEER. USE NO MORE THAN 2 MESSAGE DISPLAYS WITH ANY CYCLE UNLESS ADDITIONAL CHANGEABLE MESSAGE SIGNS ARE USED.
- 5 - ALL SIGN SPACING DIMENSIONS ARE APPROXIMATE. FIELD ADJUST AS VARIOUS CONDITIONS OCCUR, SUCH AS LIMITED SIGHT DISTANCE, OBSTRUCTION INTERFERENCE OR AS DIRECTED BY THE ENGINEER.

SEE SHEET TCP-6 FOR PHASING INSTRUCTIONS

| | | | |
|--|----------------|--|-----------|
| APPROVED: <i>[Signature]</i> DATE: <i>[Date]</i> | | BRIDGE #168 & #177 NORTH BOUND ROAD CLOSURE AND SIGN DETAIL | |
| | | | |
| SCALE: NONE | DATE: 6-08 | | REVISIONS |
| DWG. BY: MDS | DESIGN BY: MDS | | |
| REVIEWED BY: CBH | | | |

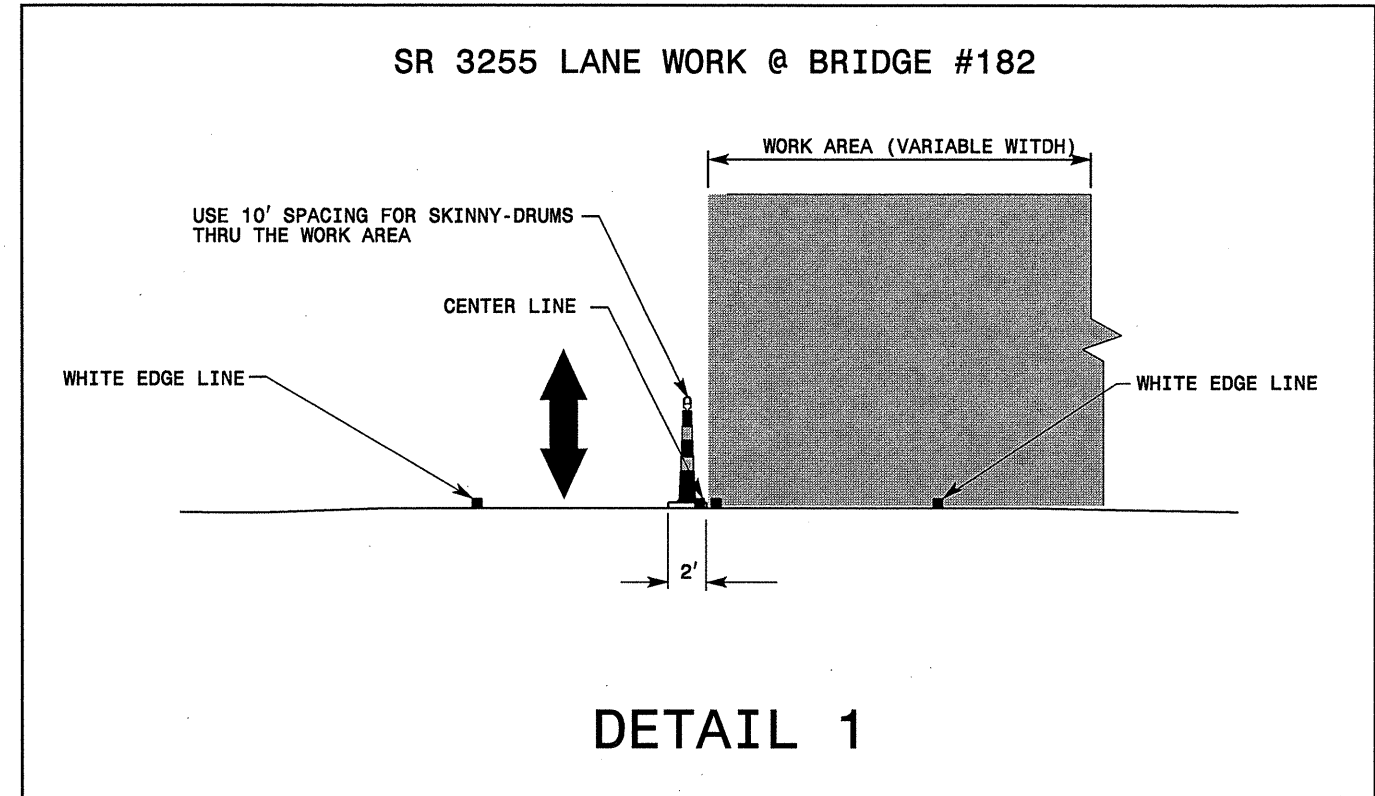
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 AT WZTC24745

PHASING- BRIDGE #182

NOTE: REMOVE ALL TRAFFIC CONTROL DEVICES AT THE END OF EACH WORK DAY AND RETURN TRAFFIC TO THE EXISTING PATTERN.

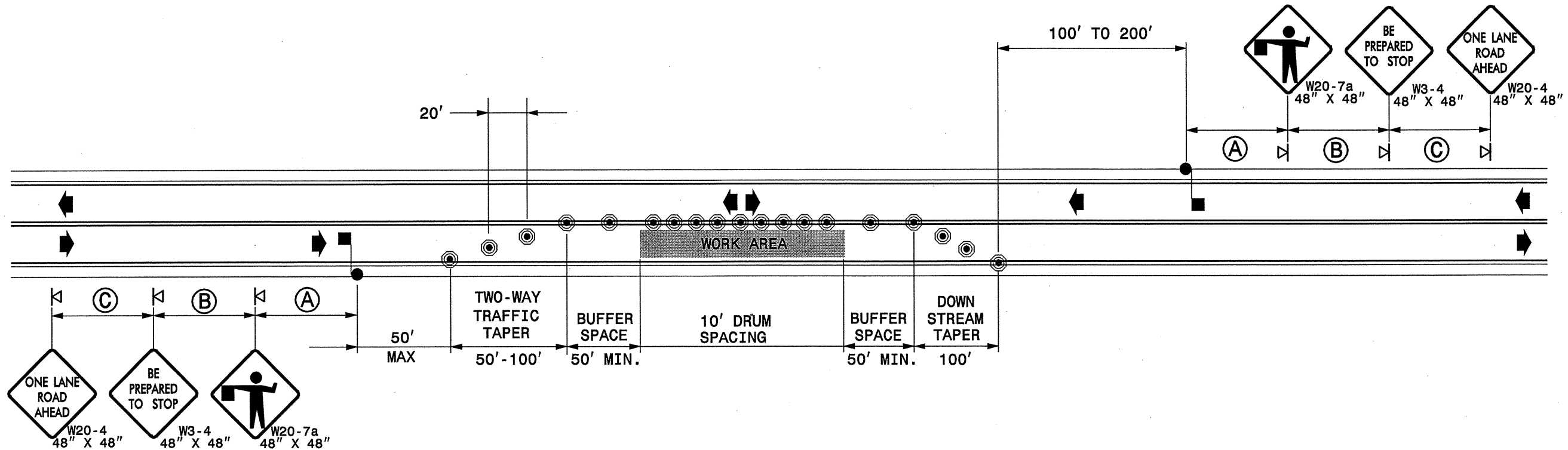
BRIDGE #182 IS PHASED BY CLOSING EACH LANE AND UTILIZING FLAGGERS TO DIRECT TRAFFIC AROUND THE WORK AREA.

- STEP 1- USING DETAIL 1 ON THIS SHEET AND THE "FLAGGING OPERATION" DETAIL ON SHEET TCP-9, CLOSE THE EAST BOUND LANE TO TRAFFIC. LANE CLOSURE DEVICES SHALL ENCROACH TWO FEET INTO THE WEST BOUND LANE.
- STEP 2- PLACE A CONTAINMENT SYSTEM IN THE CLOSED LANE TO COLLECT DEBRIS. PROVIDE CONTINUOUS SURVEILLANCE TO ENSURE THE CONTAINMENT SYSTEM DOES NOT ENCROACH INTO THE OPEN TRAVEL LANE.
- STEP 3- WHEN THE PAINT HAS BEEN REMOVED AND CONTAINED, PERFORM PAINTING OPERATIONS BEHIND THE LANE CLOSURE. WHEN PAINTING IS COMPLETE, REMOVE ALL EQUIPMENT AND TRAFFIC CONTROL DEVICES AND RETURN TRAFFIC TO THE EXISTING PATTERN.
- STEP 4- USING DETAIL 1 ON THIS SHEET AND THE "FLAGGING OPERATION" DETAIL ON SHEET TCP-9, CLOSE THE WEST BOUND LANE TO TRAFFIC. LANE CLOSURE DEVICES SHALL ENCROACH TWO FEET INTO THE EAST BOUND LANE.
- STEP 5- REPEAT STEPS 2 AND 3 TO COMPLETE BRIDGE PAINTING OPERATIONS.



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dstokes AT W12244745

| APPROVED: <i>[Signature]</i> | BRIDGE #182 PHASING AND DETAILS | | | | | | | | |
|------------------------------|--|-----------|--|--|--|--|--|--|--|
| | SCALE: NONE | | | | | | | | |
| | DATE: 6-08 | | | | | | | | |
| | DWG. BY: MDS | | | | | | | | |
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| REVIEWED BY: CBH | <table border="1"> <thead> <tr> <th colspan="2">REVISIONS</th> </tr> </thead> <tbody> <tr> <td> </td> <td> </td> </tr> <tr> <td> </td> <td> </td> </tr> <tr> <td> </td> <td> </td> </tr> </tbody> </table> | REVISIONS | | | | | | | |
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| DISTANCE TABLE | | | |
|--------------------------|--------|-----|-----|
| POSTED SPEED LIMIT (MPH) | FEET ± | | |
| | (A) | (B) | (C) |
| 35 | 200 | 200 | 200 |

| LEGEND | |
|--------|---------------------------|
| ◎ | SKINNY-DRUM |
| ▽ | PORTABLE SIGN |
| ■ | FLAGGER |
| ← | DIRECTION OF TRAFFIC FLOW |

NOTES:

- 1 - THIS SHEET IS TO BE USE FOR THE EAST BOUND AND WEST BOUND BRIDGE PAINTING OPERATION.
- 2 - INSTALL LANE CLOSURES WITH THE TRAFFIC FLOW, BEGINNING WITH DEVICES ON THE UPSTREAM SIDE OF TRAFFIC.
- 3 - REMOVE LANE CLOSURES AGAINST THE TRAFFIC FLOW, BEGINNING WITH DEVICES ON THE DOWNSTREAM SIDE OF TRAFFIC.
- 4 - PLACE SKINNY-DRUMS THRU THE WORK AREA AT A SPACING OF 20 FEET UNLESS OTHERWISE SPECIFIED PER DETAILS ON THIS SHEET.
- 5 - EXTEND LANE CLOSURES AT THE BUFFER SPACE SUCH THAT STOPPING SIGHT DISTANCE IS PROVIDED TO THE FLAGGER (REFER TO STD. 1101.11 SHEET 2).
- 6 - DO NOT STOP TRAFFIC IN ANY ONE DIRECTION FOR MORE THAN 5 MINUTES AT A TIME.
- 7 - FLAGGERS SHALL NOT STAND IN THE ROADWAY.

APPROVED: *W. McDaniel 6/5/08*

**BRIDGE #182
FLAGGING OPERATION DETAIL**

| | | |
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| SCALE: NONE | | REVISIONS |
| DATE: 6-08 | | |
| DWG. BY: MM | | |
| DESIGN BY: MM | | |
| REVIEWED BY: CH | | |

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 AT WZTC244745
 dstokes

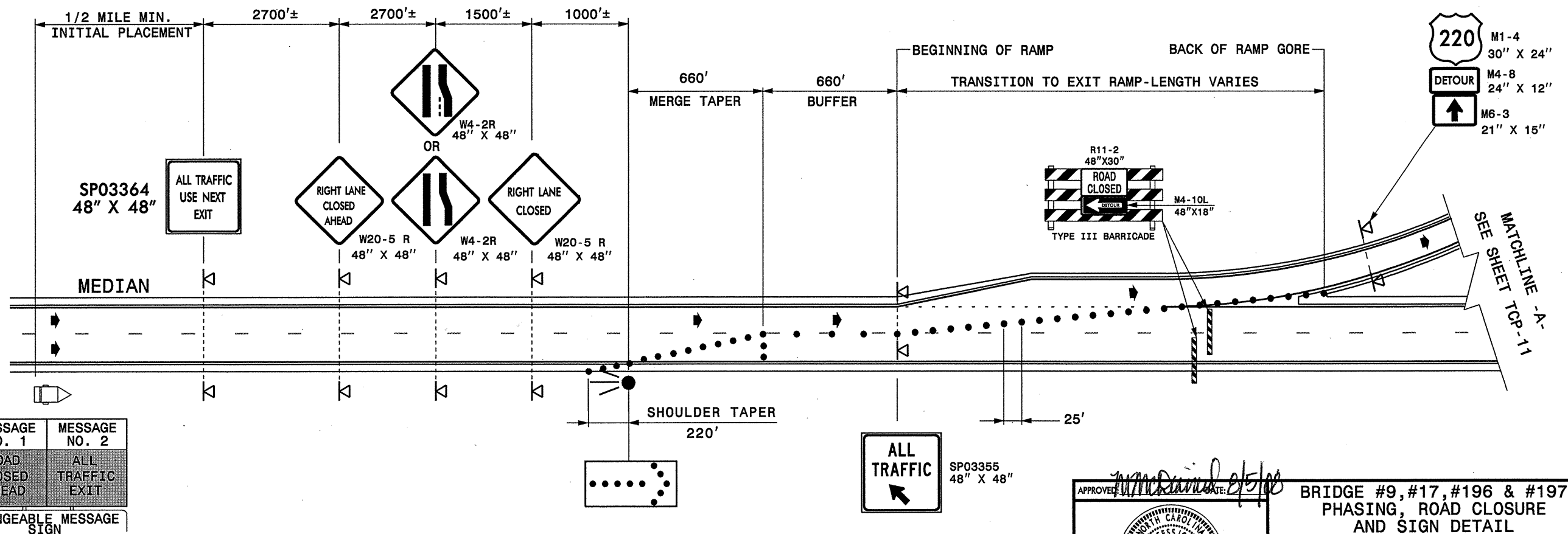
PHASING- BRIDGES #9 AND #197 BRIDGES #17 AND #196

| | |
|--------------------------------|---------------------|
| PROJ. REFERENCE NO. B-4700F | SHEET NO. TCP-10 |
|--------------------------------|---------------------|

NOTE: REMOVE ALL TRAFFIC CONTROL DEVICES AT THE END OF EACH WORK DAY AND RETURN TRAFFIC TO THE EXISTING PATTERN.

US 220 SHALL BE CLOSED AND TRAFFIC DETOURED TO PERFORM PAINTING OPERATIONS ON BRIDGES #9, #197, #17 AND #196. BRIDGES #9 AND #197 ARE PHASED BY CLOSING SOUTH BOUND US 220 AND PERFORMING WORK ON BOTH BRIDGES SIMULTANEOUSLY. BRIDGES #17 AND #196 ARE PHASED BY CLOSING NORTH BOUND US 220 AND PERFORMING WORK ON BOTH BRIDGES SIMULTANEOUSLY. HOWEVER, WORK IS TO BE PERFORMED ON ONE PAIR OF BRIDGES IN ONE DIRECTION AT A TIME. PAINTING OPERATIONS MUST BE COMPLETED ON ONE PAIR OF BRIDGES IN ONE DIRECTION BEFORE STARTING WORK ON THE OTHER PAIR.

- STEP 1- USING DETAILS ON THIS SHEET AND SHEET TCP-11, CLOSE BOTH NORTH BOUND LANES TO PERFORM WORK ON BRIDGES #17 AND #196. SHIFT TRAFFIC TO THE RAMP TO DETOUR TRAFFIC AROUND WORK AREA.
- STEP 2- PLACE A CONTAINMENT SYSTEM TO COLLECT DEBRIS AT EACH BRIDGE.
- STEP 3- WHEN THE PAINT HAS BEEN REMOVED AND CONTAINED AT EACH BRIDGE, PERFORM PAINTING OPERATIONS. WHEN PAINTING IS COMPLETE, REMOVE ALL EQUIPMENT AND TRAFFIC CONTROL DEVICES.
- STEP 4- USING DETAILS ON THIS SHEET AND SHEET TCP-11, CLOSE BOTH SOUTH BOUND LANES TO PERFORM WORK ON BRIDGES #9 AND #197. REPEAT TRAFFIC CONTROL SETUP USED FOR NORTH BOUND WORK.
- STEP 5- REPEAT STEPS 2 AND 3 TO COMPLETE BRIDGE PAINTING OPERATIONS.



| | |
|-------------------------|------------------|
| MESSAGE NO. 1 | MESSAGE NO. 2 |
| ROAD CLOSED AHEAD | ALL TRAFFIC EXIT |
| CHANGEABLE MESSAGE SIGN | |

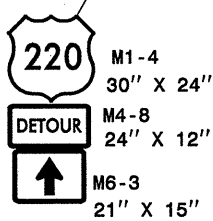
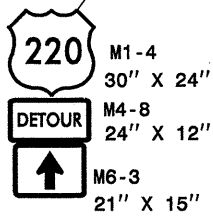
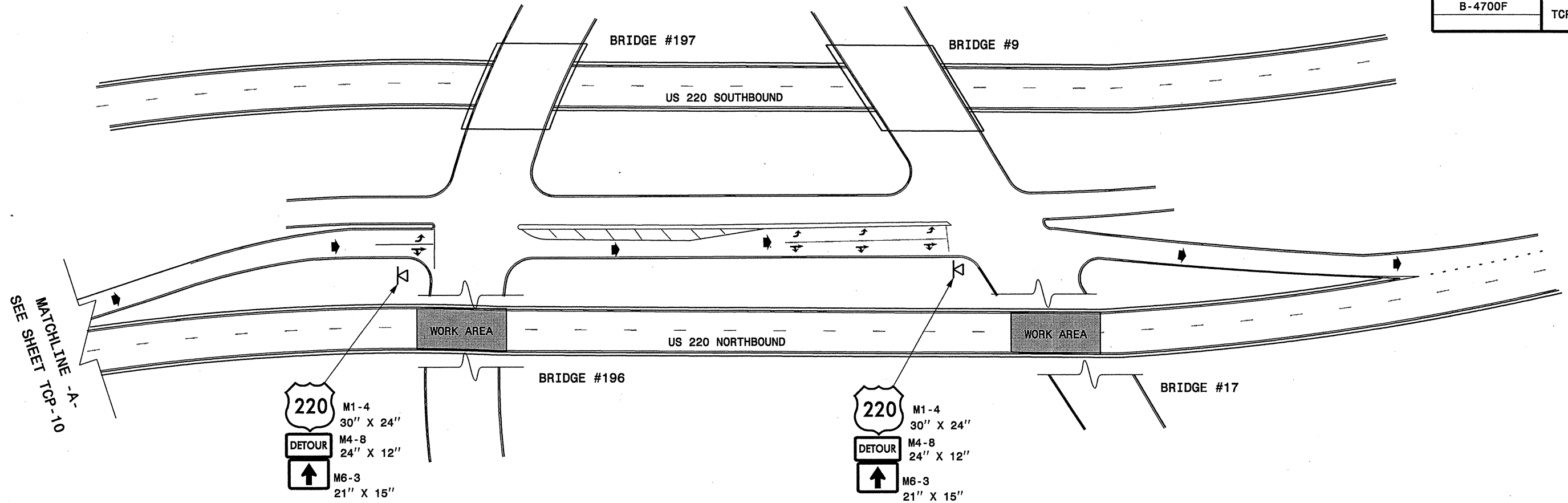
ALL TRAFFIC
←
SP03355
48" X 48"

APPROVED: *[Signature]* DATE: 6/5/08

SEAL
NORTH CAROLINA PROFESSIONAL ENGINEER
SEAL 025895
WALTER M. McDIARMID

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|---|-----------|
| BRIDGE #9, #17, #196 & #197 PHASING, ROAD CLOSURE AND SIGN DETAIL | |
| SCALE: NONE | REVISIONS |
| DATE: 6-08 | |
| DWG. BY: DSS | |
| DESIGN BY: DSS | |
| REVIEWED BY: CBH | |

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AT WZTC24745
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NOTES:

- 1 - IF NECESSARY USE POLICE OFFICERS OR FLAGGERS TO DIRECT TRAFFIC AT RAMP TERMINALS OR AS DIRECTED BY THE ENGINEER.
- 2 - IF RAMP TERMINALS ARE SIGNALIZED, SIGNALS MAY BE PLACED IN FLASH MODE AND POLICE SHALL BE USED.
- 3 - PLACE ARROW PANEL ON THE SHOULDER (PAVED OR UNPAVED). PLACE ARROW PANEL WITHIN THE TAPER IF SHOULDER DOES NOT EXIST. MEET THE REQUIREMENTS FOR STOPPING SIGHT DISTANCE AT THE ARROW PANEL LOCATION. IF NEEDED, EXTEND LANE CLOSURE AT THE BUFFER SPACE, SUCH THAT STOPPING SIGHT DISTANCE TO THE ARROW PANEL IS MET. (SEE ROADWAY STANDARD DWG.1101.11 -SHT. 2 OF 4)
- 4 - PLACE DRUMS IN TAPERS AT THE MAXIMUM SPACING EQUAL IN FEET TO THE POSTED SPEED LIMIT. PLACE DRUMS ALONG THE BUFFER SPACE AND WORK AREA AT THE MAXIMUM SPACING EQUAL IN FEET TO 2 TIMES THE POSTED SPEED LIMIT UNLESS OTHERWISE SPECIFIED PER DETAILS ON THIS SHEET.
- 5 - INSTALL LANE CLOSURES WITH THE TRAFFIC FLOW, BEGINNING WITH DEVICES ON THE UPSTREAM SIDE OF TRAFFIC. REMOVE LANE CLOSURES AGAINST THE TRAFFIC FLOW, BEGINNING WITH DEVICES ON THE DOWNSTREAM SIDE OF TRAFFIC.
- 6 - PLACE CHANGEABLE MESSAGE SIGN (CMS) ON THE OUTSIDE OF THE TRAVELWAY AS DIRECTED BY THE ENGINEER. INITIALLY LOCATE THE SIGN APPROXIMATELY 1/2 MILE IN ADVANCE OF THE SPO3364 SIGNS. IF IT IS ANTICIPATED THAT TRAFFIC WILL BACK UP TO WHERE THE SIGN IS LOCATED, PLACE THE SIGN APPROXIMATELY 1/2 MILE IN ADVANCE OF THE ANTICIPATED BACKUP. CONTINUE TO MONITOR TRAFFIC, MOVE CMS APPROXIMATELY 1/2 MILE IN CNJUNCTION WITH ANTICIPATED BACK UP.
- 7 - DISPLAY CHANGEABLE MESSAGE SIGN (CMS) MESSAGES AS SHOWN OR AS DEEMED NECESSARY BY THE ENGINEER. USE NO MORE THAN 2 MESSAGE DISPLAYS WITH ANY CYCLE UNLESS ADDITIONAL CHANGEABLE MESSAGE SIGNS ARE USED.
- 8 - ALL SIGN SPACING DIMENSIONS ARE APPROXIMATE. FIELD ADJUST AS VARIOUS CONDITIONS OCCUR, SUCH AS LIMITED SIGHT DISTANCE, OBSTRUCTION INTERFERENCE OR AS DIRECTED BY THE ENGINEER.

LEGEND

- TYPE III BARRICADE
- CHANGEABLE MESSAGE SIGN (CMS)
- FLASHING ARROW PANEL (TYPE C)
- DRUM
- PORTABLE SIGN
- DIRECTION OF TRAFFIC FLOW

SEE SHEET TCP-10 FOR PHASING INSTRUCTIONS

| | | | | | | | | | | | | | | | | | |
|------------------------------|--|-------------|--|------------|--------------|----------------|------------------|--|--|--|--|--|--|--|--|--|--|
| APPROVED: <i>[Signature]</i> | BRIDGE #9, #17, #196 & #197 PHASING, ROAD CLOSURE AND SIGN DETAIL | REVISIONS | | | | | | | | | | | | | | | |
| SEAL | <table border="1" style="width: 100%;"> <tr> <td>SCALE: NONE</td> <td rowspan="4" style="text-align: center;"></td> </tr> <tr> <td>DATE: 6-08</td> </tr> <tr> <td>DWG. BY: DSS</td> </tr> <tr> <td>DESIGN BY: DSS</td> </tr> <tr> <td>REVIEWED BY: CBH</td> <td></td> </tr> </table> | SCALE: NONE | | DATE: 6-08 | DWG. BY: DSS | DESIGN BY: DSS | REVIEWED BY: CBH | | <table border="1" style="width: 100%;"> <tr> <td> </td> <td> </td> </tr> <tr> <td> </td> <td> </td> </tr> <tr> <td> </td> <td> </td> </tr> <tr> <td> </td> <td> </td> </tr> </table> | | | | | | | | |
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PHASING- BRIDGE #205 SOUTH BOUND

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| PROJ. REFERENCE NO. B-4700F | SHEET NO. TCP-12 |
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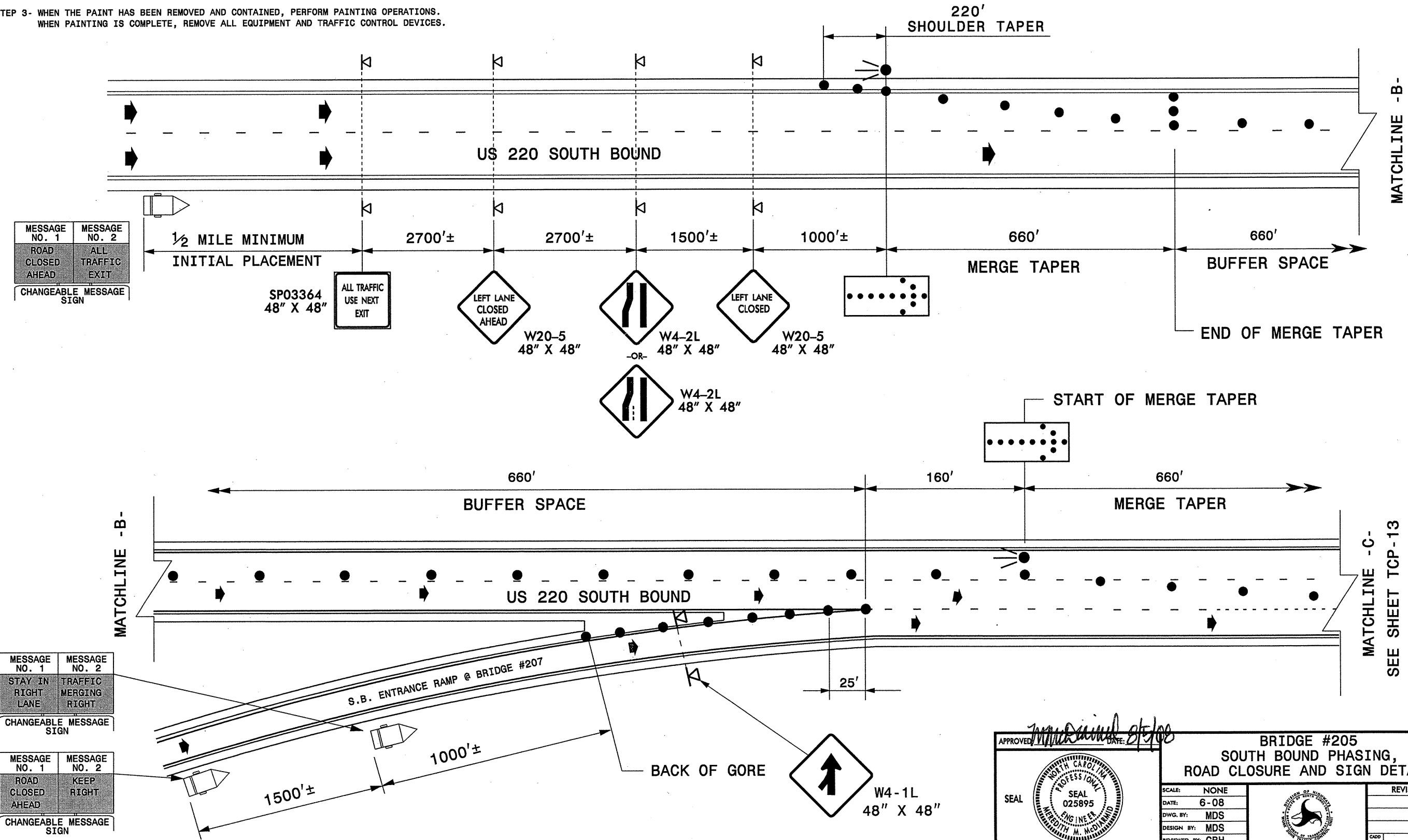
NOTE: REMOVE ALL TRAFFIC CONTROL DEVICES AT THE END OF EACH WORK DAY AND RETURN TRAFFIC TO THE EXISTING PATTERN.

US 220 SHALL BE CLOSED AND TRAFFIC DETOURED TO PERFORM PAINTING OPERATIONS ON BRIDGE #205. BRIDGE #205 IS PHASED BY CLOSING SOUTH BOUND US 220 AND NORTH BOUND US 220 AND PERFORMING WORK INDEPENDENTLY FROM EACH OTHER. HOWEVER, WORK IN EACH DIRECTION MAY BE PERFORMED AT THE SAME TIME IF PREFERRED.

STEP 1- USING DETAILS ON THIS SHEET AND SHEET TCP-13, CLOSE BOTH US 220 SOUTH BOUND LANES AND SHIFT TRAFFIC TO THE RAMP TO DETOUR TRAFFIC AROUND WORK AREA.

STEP 2- PLACE A CONTAINMENT SYSTEM TO COLLECT DEBRIS.

STEP 3- WHEN THE PAINT HAS BEEN REMOVED AND CONTAINED, PERFORM PAINTING OPERATIONS. WHEN PAINTING IS COMPLETE, REMOVE ALL EQUIPMENT AND TRAFFIC CONTROL DEVICES.



| | |
|------------------------------------|-----------------------------------|
| MESSAGE NO. 1 ROAD CLOSED AHEAD | MESSAGE NO. 2 ALL TRAFFIC EXIT |
| CHANGEABLE MESSAGE SIGN | |

| | |
|-------------------------------------|--|
| MESSAGE NO. 1 STAY IN RIGHT LANE | MESSAGE NO. 2 TRAFFIC MERGING RIGHT |
| CHANGEABLE MESSAGE SIGN | |

| | |
|------------------------------------|-----------------------------|
| MESSAGE NO. 1 ROAD CLOSED AHEAD | MESSAGE NO. 2 KEEP RIGHT |
| CHANGEABLE MESSAGE SIGN | |

APPROVED: *[Signature]* DATE: *[Date]*

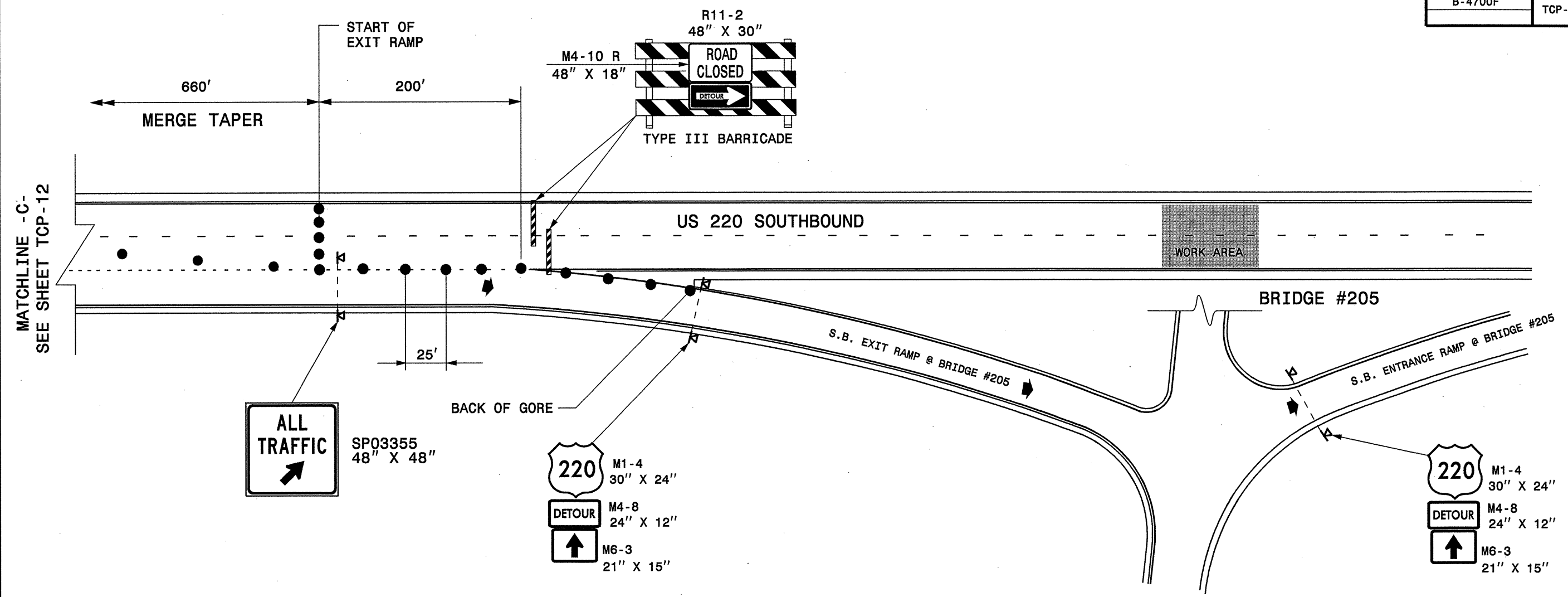
SEAL

BRIDGE #205
SOUTH BOUND PHASING,
ROAD CLOSURE AND SIGN DETAIL

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| SCALE: NONE | REVISIONS |
| DATE: 6-08 | |
| DWG. BY: MDS | |
| DESIGN BY: MDS | |
| REVIEWED BY: CBH | |

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MATCHLINE -C-
SEE SHEET TCP-13



MATCHLINE -C-
SEE SHEET TCP-12

NOTES:

- 1 - IF NECESSARY USE POLICE OFFICERS OR FLAGGERS TO DIRECT TRAFFIC AT RAMP TERMINALS OR AS DIRECTED BY THE ENGINEER.
- 2 - PLACE ARROW PANELS ON THE SHOULDER (PAVED OR UNPAVED). PLACE ARROW PANELS WITHIN THE TAPER IF SHOULDERS DO NOT EXIST. MEET THE REQUIREMENTS FOR STOPPING SIGHT DISTANCE AT THE ARROW PANEL LOCATION. IF NEEDED, EXTEND LANE CLOSURES AT THE BUFFER SPACE, SUCH THAT STOPPING SIGHT DISTANCE TO THE ARROW PANEL IS MET. (SEE ROADWAY STANDARD DWG.1101.11 -SHT. 2 OF 4)
- 3 - PLACE DRUMS IN TAPERS AT THE MAXIMUM SPACING EQUAL IN FEET TO THE POSTED SPEED LIMIT. PLACE DRUMS ALONG THE BUFFER SPACE AND WORK AREA AT THE MAXIMUM SPACING EQUAL IN FEET TO 2 TIMES THE POSTED SPEED LIMIT UNLESS OTHERWISE SPECIFIED PER DETAILS ON THE SHEETS.
- 4 - INSTALL LANE CLOSURES WITH THE TRAFFIC FLOW, BEGINNING WITH DEVICES ON THE UPSTREAM SIDE OF TRAFFIC. REMOVE LANE CLOSURES AGAINST THE TRAFFIC FLOW, BEGINNING WITH DEVICES ON THE DOWNSTREAM SIDE OF TRAFFIC.
- 5 - PLACE CHANGEABLE MESSAGE SIGN (CMS) ON THE OUTSIDE OF THE TRAVELWAY AS DIRECTED BY THE ENGINEER. INITIALLY LOCATE THE SIGN APPROXIMATELY 1/2 MILE IN ADVANCE OF THE SP03364 SIGNS. IF IT IS ANTICIPATED THAT TRAFFIC WILL BACK UP TO WHERE THE SIGN IS LOCATED, PLACE THE SIGN APPROXIMATELY 1/2 MILE IN ADVANCE OF THE ANTICIPATED BACKUP. CONTINUE TO MONITOR TRAFFIC, MOVE CMS APPROXIMATELY 1/2 MILE IN CNJUNCTION WITH ANTICIPATED BACK UP.
- 6 - DISPLAY CHANGEABLE MESSAGE SIGN (CMS) MESSAGES AS SHOWN OR AS DEEMED NECESSARY BY THE ENGINEER. USE NO MORE THAN 2 MESSAGE DISPLAYS WITH ANY CYCLE UNLESS ADDITIONAL CHANGEABLE MESSAGE SIGNS ARE USED.
- 7 - ALL SIGN SPACING DIMENSIONS ARE APPROXIMATE. FIELD ADJUST AS VARIOUS CONDITIONS OCCUR, SUCH AS LIMITED SIGHT DISTANCE, OBSTRUCTION INTERFERENCE OR AS DIRECTED BY THE ENGINEER.

LEGEND

- ▬ TYPE III BARRICADE
- ◻ CHANGEABLE MESSAGE SIGN (CMS)
- ◀ FLASHING ARROW PANEL (TYPE C)
- DRUM
- ▬ PORTABLE SIGN
- ➔ DIRECTION OF TRAFFIC FLOW

SEE SHEET TCP-10 FOR PHASING INSTRUCTIONS

| | | | |
|---|------------------|--|-----------|
| APPROVED: <i>[Signature]</i> DATE: 6/5/08 | | BRIDGE #205 SOUTH BOUND PHASING, ROAD CLOSURE AND SIGN DETAIL | |
| SEAL | SCALE: NONE | | REVISIONS |
| | DATE: 6-08 | | |
| | DWG. BY: MDS | | |
| | DESIGN BY: MDS | | |
| | REVIEWED BY: CBH | | |

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AT WZTC24745
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PHASING- BRIDGE #205 NORTH BOUND

| | |
|---------------------|-----------|
| PROJ. REFERENCE NO. | SHEET NO. |
| B-4700F | TCP-14 |

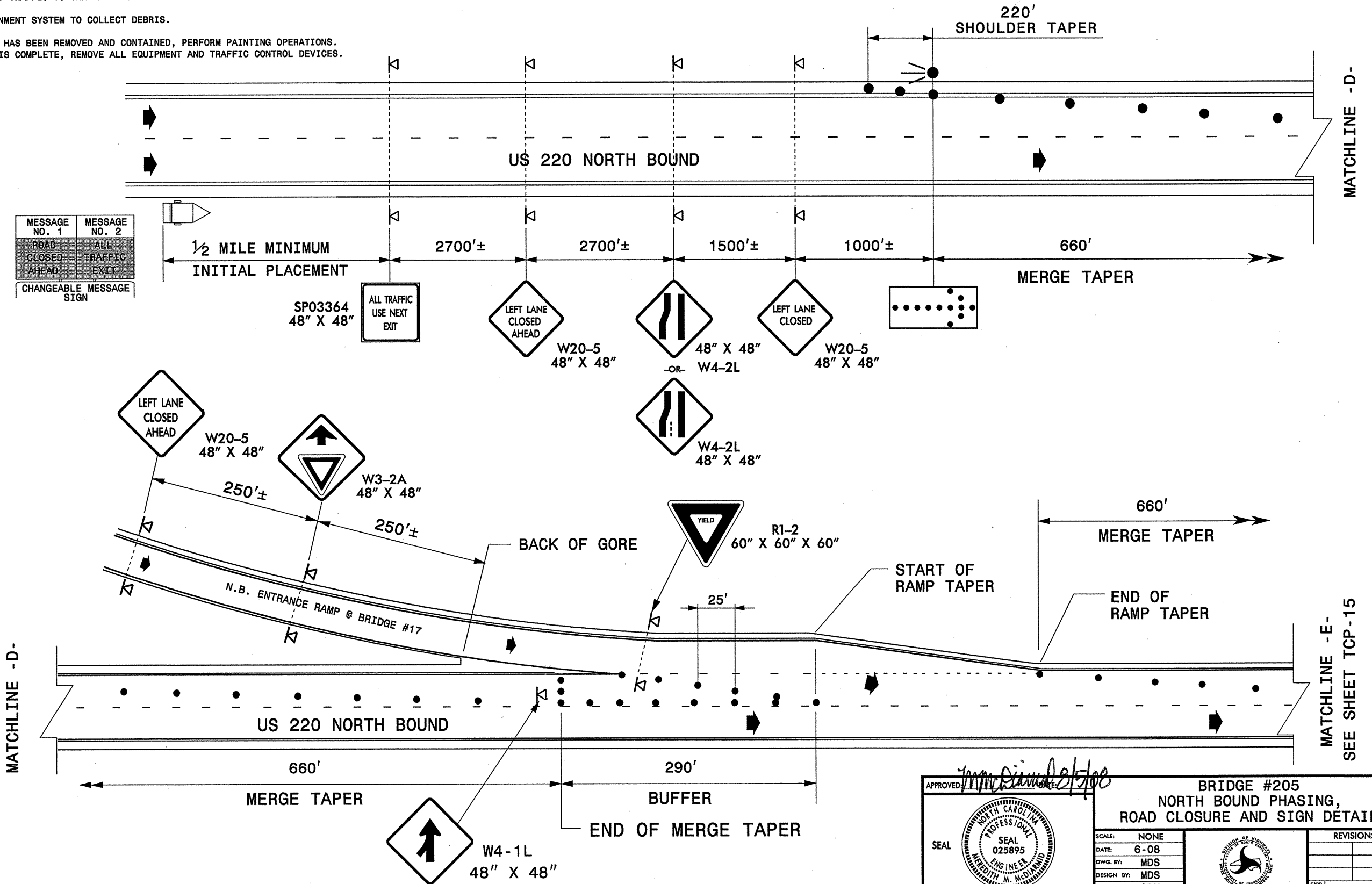
NOTE: REMOVE ALL TRAFFIC CONTROL DEVICES AT THE END OF EACH WORK DAY AND RETURN TRAFFIC TO THE EXISTING PATTERN.

US 220 SHALL BE CLOSED AND TRAFFIC DETOURED TO PERFORM PAINTING OPERATIONS ON BRIDGE #205. BRIDGE #205 IS PHASED BY CLOSING SOUTH BOUND US 220 AND NORTH BOUND US 220 AND PERFORMING WORK INDEPENDENTLY FROM EACH OTHER. HOWEVER, WORK IN EACH DIRECTION MAY BE PERFORMED AT THE SAME TIME IF PREFERRED.

STEP 1- USING DETAILS THIS SHEET AND SHEET TCP-15, CLOSE BOTH US 220 NORTH BOUND LANES AND SHIFT TRAFFIC TO THE RAMP TO DETOUR TRAFFIC AROUND WORK AREA.

STEP 2- PLACE A CONTAINMENT SYSTEM TO COLLECT DEBRIS.

STEP 3- WHEN THE PAINT HAS BEEN REMOVED AND CONTAINED, PERFORM PAINTING OPERATIONS. WHEN PAINTING IS COMPLETE, REMOVE ALL EQUIPMENT AND TRAFFIC CONTROL DEVICES.



| | |
|-------------------------|------------------|
| MESSAGE NO. 1 | MESSAGE NO. 2 |
| ROAD CLOSED AHEAD | ALL TRAFFIC EXIT |
| CHANGEABLE MESSAGE SIGN | |

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APPROVED: *[Signature]* DATE: 8/5/08

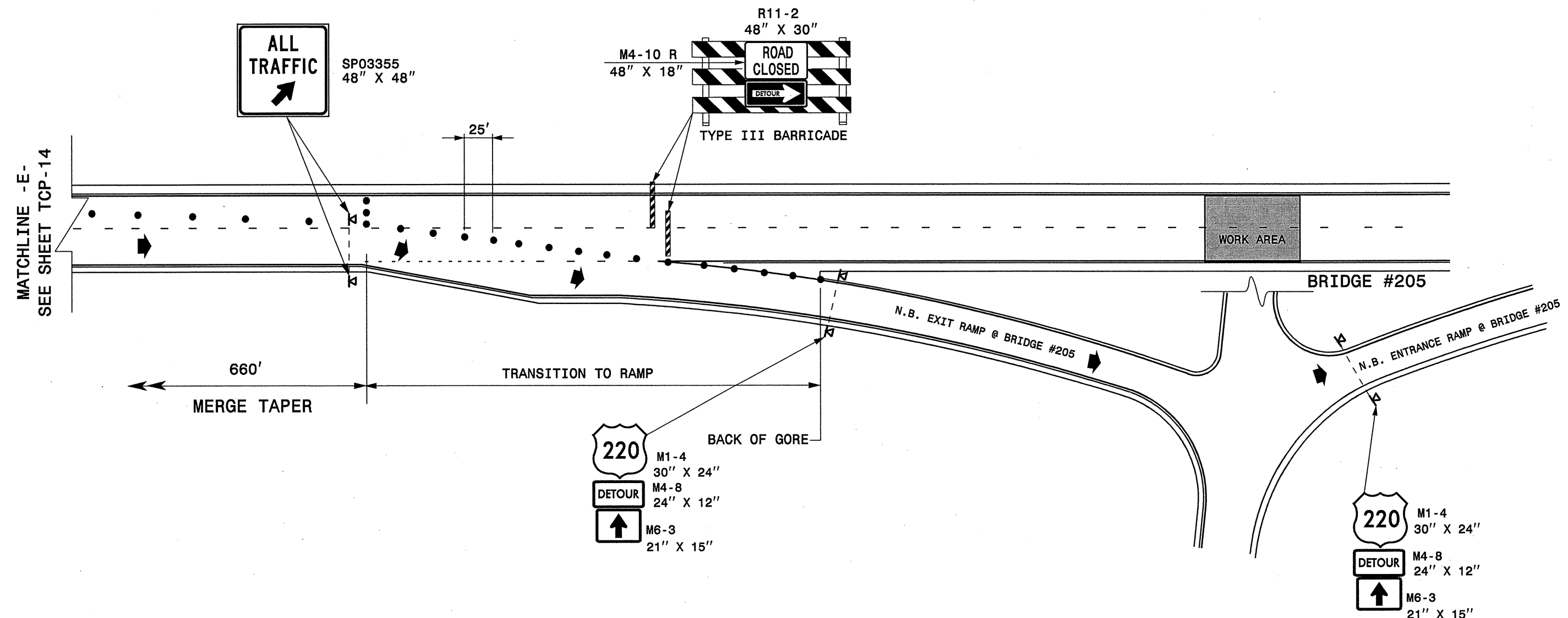
SEAL

PROFESSIONAL ENGINEER
SEAL 025895
NORTH CAROLINA
M. McDaniel

BRIDGE #205
NORTH BOUND PHASING,
ROAD CLOSURE AND SIGN DETAIL

| | | |
|----------------|--|-----------|
| SCALE: NONE | | REVISIONS |
| DATE: 6-08 | | |
| DWG. BY: MDS | | |
| DESIGN BY: MDS | | |

REVIEWED BY: CBH



- NOTES:
- 1 - IF NECESSARY USE POLICE OFFICERS OR FLAGGERS TO DIRECT TRAFFIC AT RAMP TERMINALS OR AS DIRECTED BY THE ENGINEER.
 - 2 - PLACE ARROW PANELS ON THE SHOULDER (PAVED OR UNPAVED). PLACE ARROW PANELS WITHIN THE TAPER IF SHOULDERS DO NOT EXIST. MEET THE REQUIREMENTS FOR STOPPING SIGHT DISTANCE AT THE ARROW PANEL LOCATION. IF NEEDED, EXTEND LANE CLOSURES AT THE BUFFER SPACE, SUCH THAT STOPPING SIGHT DISTANCE TO THE ARROW PANEL IS MET. (SEE ROADWAY STANDARD DWG.1101.11 -SHT. 2 OF 4)
 - 3 - PLACE DRUMS IN TAPERS AT THE MAXIMUM SPACING EQUAL IN FEET TO THE POSTED SPEED LIMIT. PLACE DRUMS ALONG THE BUFFER SPACE AND WORK AREA AT THE MAXIMUM SPACING EQUAL IN FEET TO 2 TIMES THE POSTED SPEED LIMIT UNLESS OTHERWISE SPECIFIED PER DETAILS ON THE SHEETS.
 - 4 - INSTALL LANE CLOSURES WITH THE TRAFFIC FLOW, BEGINNING WITH DEVICES ON THE UPSTREAM SIDE OF TRAFFIC. REMOVE LANE CLOSURES AGAINST THE TRAFFIC FLOW, BEGINNING WITH DEVICES ON THE DOWNSTREAM SIDE OF TRAFFIC.
 - 5 - POSITION THE TMIA TO MAINTAIN A ROLL-AHEAD DISTANCE AS RECOMMENDED BY THE MANUFACTURER.
 - 6 - PLACE CHANGEABLE MESSAGE SIGN (CMS) ON THE OUTSIDE OF THE TRAVELWAY AS DIRECTED BY THE ENGINEER. INITIALLY LOCATE THE SIGN APPROXIMATELY 1/2 MILE IN ADVANCE OF THE SP03364 SIGNS. IF IT IS ANTICIPATED THAT TRAFFIC WILL BACK UP TO WHERE THE SIGN IS LOCATED, PLACE THE SIGN APPROXIMATELY 1/2 MILE IN ADVANCE OF THE ANTICIPATED BACKUP. CONTINUE TO MONITOR TRAFFIC, MOVE CMS APPROXIMATELY 1/2 MILE IN CNJUNCTION WITH ANTICIPATED BACK UP.
 - 7 - DISPLAY CHANGEABLE MESSAGE SIGN (CMS) MESSAGES AS SHOWN OR AS DEEMED NECESSARY BY THE ENGINEER. USE NO MORE THAN 2 MESSAGE DISPLAYS WITH ANY CYCLE UNLESS ADDITIONAL CHANGEABLE MESSAGE SIGNS ARE USED.
 - 8 - ALL SIGN SPACING DIMENSIONS ARE APPROXIMATE. FIELD ADJUST AS VARIOUS CONDITIONS OCCUR, SUCH AS LIMITED SIGHT DISTANCE, OBSTRUCTION INTERFERENCE OR AS DIRECTED BY THE ENGINEER.

| LEGEND | |
|--------|-------------------------------|
| | TYPE III BARRICADE |
| | CHANGEABLE MESSAGE SIGN (CMS) |
| | FLASHING ARROW PANEL (TYPE C) |
| | DRUM |
| | PORTABLE SIGN |
| | DIRECTION OF TRAFFIC FLOW |

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|---|--|--|--|--|--|--|
| APPROVED: <i>[Signature]</i> DATE: 6/5/08 | BRIDGE #205 NORTH BOUND PHASING, ROAD CLOSURE AND SIGN DETAIL | | | | | |
| | SCALE: NONE DATE: 6-08 DWG. BY: MDS DESIGN BY: MDS REVIEWED BY: CBH | | REVISIONS <table border="1"> <tr> <td> </td> <td> </td> </tr> <tr> <td> </td> <td> </td> </tr> </table> | | | |
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 AT W:\124745

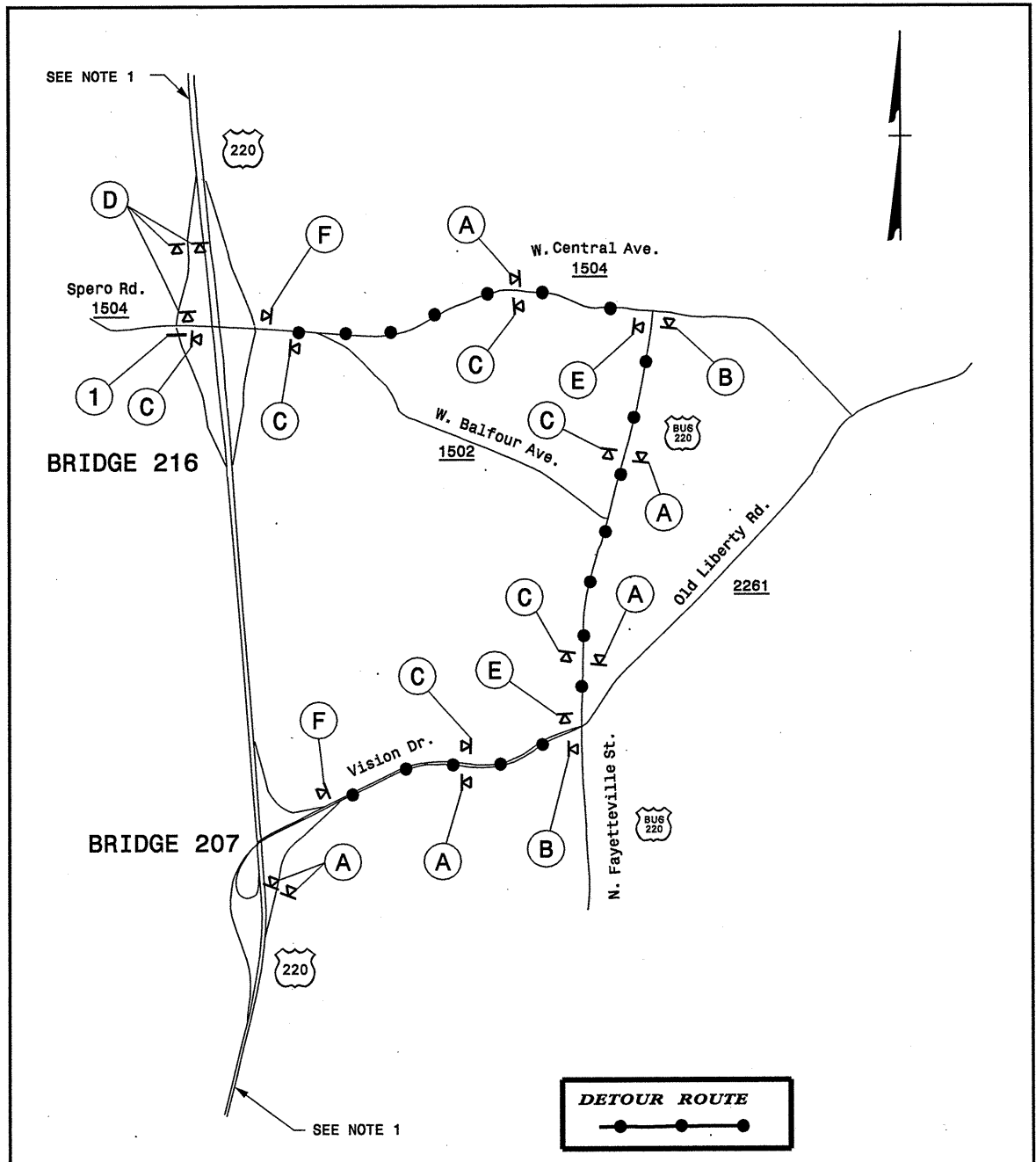
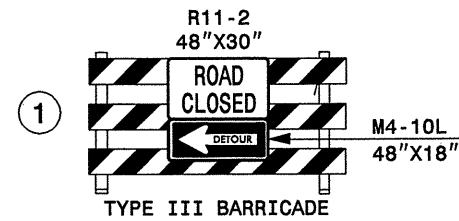
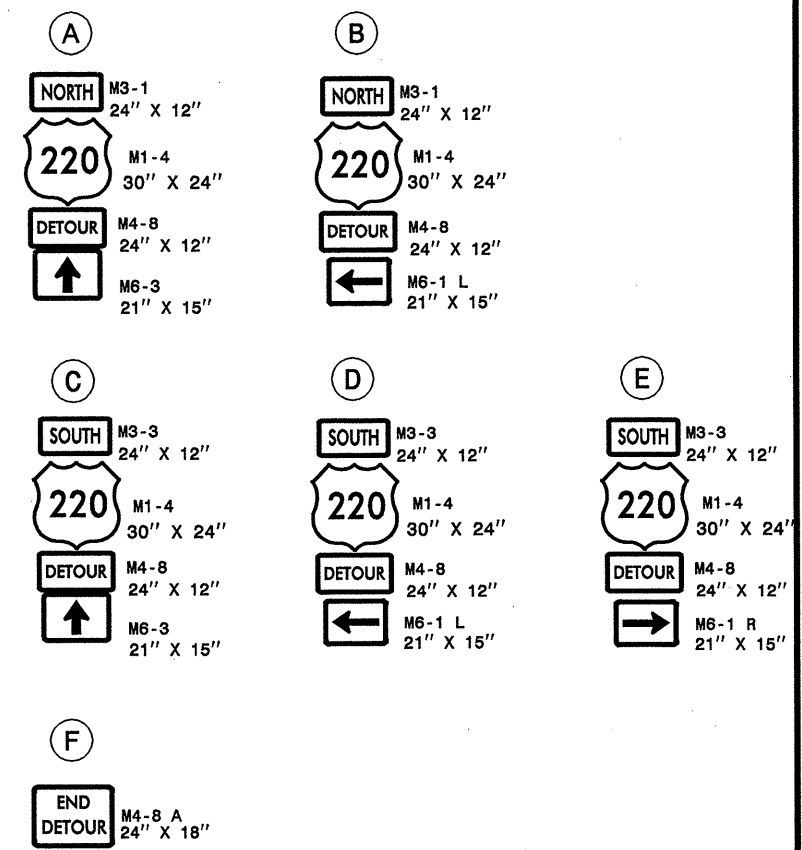
PHASING- BRIDGE #207

NOTE: REMOVE ALL TRAFFIC CONTROL DEVICES AT THE END OF EACH WORK DAY AND RETURN TRAFFIC TO THE EXISTING PATTERN.

US 220 SHALL BE CLOSED AND TRAFFIC DETOURED TO PERFORM PAINTING OPERATIONS ON BRIDGE #207. HOWEVER, WORK IS TO BE PERFORMED IN ONE DIRECTION AT A TIME. PAINTING OPERATIONS FOR ONE DIRECTION MUST BE COMPLETED BEFORE STARTING WORK IN THE OTHER DIRECTION.

- STEP 1- USING THIS SHEET, INSTALL DETOUR SIGNING FOR THE US 220 SOUTH BOUND ROAD CLOSURE.
- STEP 2- USING THIS SHEET AND SHEET TCP-17, CLOSE BOTH US 220 SOUTH BOUND LANES AT BRIDGE #216 AND THE US 220 SOUTH BOUND ENTRANCE RAMP AT BRIDGE #216 AND SHIFT TRAFFIC TO THE EXIT RAMP TO DETOUR TRAFFIC AROUND THE WORK AREA.
- STEP 3- PLACE A CONTAINMENT SYSTEM TO COLLECT DEBRIS.
- STEP 4- WHEN THE PAINT HAS BEEN REMOVED AND CONTAINED, PERFORM PAINTING OPERATIONS. WHEN PAINTING IS COMPLETE, REMOVE ALL EQUIPMENT AND TRAFFIC CONTROL DEVICES.

- STEP 5- USING THIS SHEET, INSTALL DETOUR SIGNING FOR THE US 220 NORTH BOUND ROAD CLOSURE.
- STEP 6- USING THIS SHEET AND SHEET TCP-17, CLOSE BOTH US 220 NORTH BOUND LANES AT BRIDGE #207 AND SHIFT TRAFFIC TO THE EXIT RAMP TO DETOUR TRAFFIC AROUND THE WORK AREA.
- STEP 7- REPEAT STEPS 3 AND 4 TO COMPLETE BRIDGE PAINTING OPERATIONS.

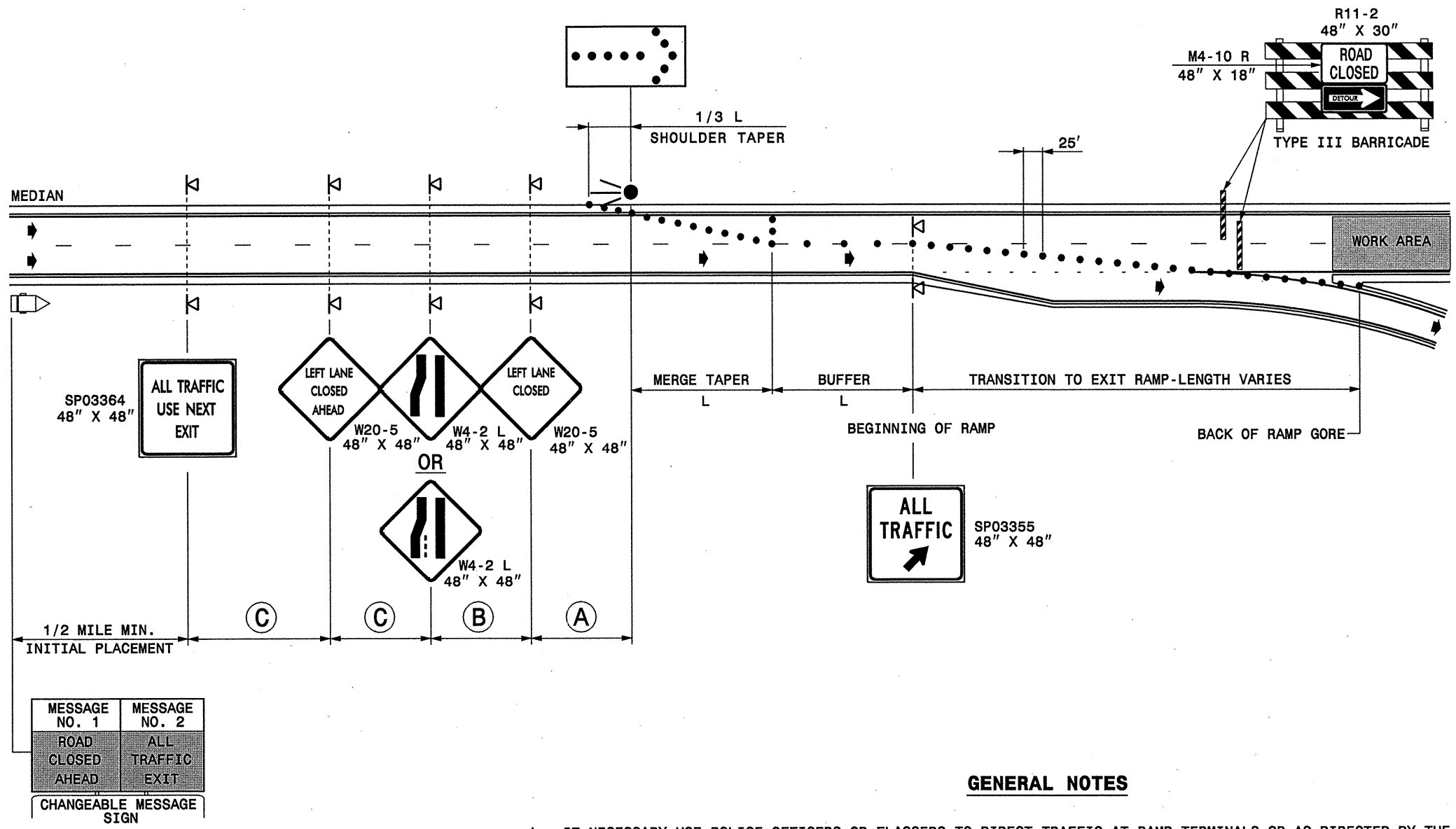


NOTES:

- 1- USE SHEET TCP-17 TO COMPLETE THE INSTALLATION OF THE TRAFFIC CONTROL DEVICES NECESSARY FOR THE WORK TO BE PERFORMED. CLOSE BOTH US 220 LANES AS DIRECTED IN THE PHASING AND INSTALL CHANNELIZING DEVICES FOR DIRECTING TRAFFIC TO THE EXIT RAMP. INSTALL ADVANCE WARNING SIGNING, EXIT SIGNING AND ARROW PANEL AS SHOWN PER THE SHEET'S DETAILS. INSTALL THE TRAFFIC CONTROL DEVICES AS INSTRUCTED PER THE SHEET'S GENERAL NOTES.
- 2- ALL SIGN LOCATIONS SHOWN ON DETOUR MAP ARE APPROXIMATE AND SHOULD BE FIELD ADJUSTED AS NECESSARY TO INSURE MOTORIST CAN EASILY NAVIGATE DETOUR. USE ADDITIONAL SIGNS AS NEEDED OR AS DIRECTED BY THE ENGINEER.

| | | |
|---|--|--|
| APPROVED: <i>[Signature]</i> DATE: 8/5/08 | BRIDGE #207 PHASING, ROAD CLOSURE, SIGN DETAIL AND DETOUR MAP | |
| SEAL | SCALE: NONE | |
| | DATE: 6-08 | |
| | DESIGN BY: MDS | |
| | REVIEWED BY: CBH | |
| | REVISIONS | |

05-AUG-2008 10:01 c:\npsr\01\special\36727.38\traffic\trafficcontrol\top\36727.38.tc-bridge35 & 157,168 & 177,207.dgn
CSTONES AT WZ12244745



| | |
|-------------------------|------------------|
| MESSAGE NO. 1 | MESSAGE NO. 2 |
| ROAD CLOSED AHEAD | ALL TRAFFIC EXIT |
| CHANGEABLE MESSAGE SIGN | |

GENERAL NOTES

- 1 - IF NECESSARY USE POLICE OFFICERS OR FLAGGERS TO DIRECT TRAFFIC AT RAMP TERMINALS OR AS DIRECTED BY THE ENGINEER.
- 2 - PLACE DRUMS IN TAPERS AT THE MAXIMUM SPACING EQUAL IN FEET TO THE POSTED SPEED LIMIT. PLACE DRUMS ALONG THE BUFFER SPACE AT THE MAXIMUM SPACING EQUAL IN FEET TO 2 TIMES THE POSTED SPEED LIMIT UNLESS OTHERWISE SPECIFIED PER DETAILS ON THE SHEET.
- 3 - INSTALL LANE CLOSURES WITH THE TRAFFIC FLOW, BEGINNING WITH DEVICES ON THE UPSTREAM SIDE OF TRAFFIC. REMOVE LANE CLOSURES AGAINST THE TRAFFIC FLOW, BEGINNING WITH DEVICES ON THE DOWNSTREAM SIDE OF TRAFFIC.
- 4 - PLACE CHANGEABLE MESSAGE SIGN (CMS) ON THE OUTSIDE OF THE TRAVELWAY AS DIRECTED BY THE ENGINEER. INITIALLY LOCATE THE SIGN APPROXIMATELY 1/2 MILE IN ADVANCE OF THE SPO3364 SIGNS. IF IT IS ANTICIPATED THAT TRAFFIC WILL BACK UP TO WHERE THE SIGN IS LOCATED, PLACE THE SIGN APPROXIMATELY 1/2 MILE IN ADVANCE OF THE ANTICIPATED BACKUP. CONTINUE TO MONITOR TRAFFIC, MOVE CMS APPROXIMATELY 1/2 MILE IN CNJUNCTION WITH ANTICIPATED BACK UP.
- 5 - DISPLAY CHANGEABLE MESSAGE SIGN (CMS) MESSAGES AS SHOWN OR AS DEEMED NECESSARY BY THE ENGINEER. USE NO MORE THAN 2 MESSAGE DISPLAYS WITH ANY CYCLE UNLESS ADDITIONAL CHANGEABLE MESSAGE SIGNS ARE USED.
- 6 - ALL SIGN SPACING DIMENSIONS ARE APPROXIMATE. FIELD ADJUST AS VARIOUS CONDITIONS OCCUR, SUCH AS LIMITED SIGHT DISTANCE, OBSTRUCTION INTERFERENCE OR AS DIRECTED BY THE ENGINEER.

| POSTED SPEED LIMIT (MPH) | FEET ± | | | |
|--------------------------|--------|------|------|-----|
| | (A) | (B) | (C) | L |
| 55 | 1000 | 1500 | 2700 | 660 |
| 65 | 1000 | 1500 | 2700 | 780 |

LEGEND

- FLASHING ARROW PANEL (TYPE C)
- CHANGEABLE MESSAGE SIGN (CMS)
- DRUM
- PORTABLE SIGN
- DIRECTION OF TRAFFIC FLOW

APPROVED: *M. McDaniel* DATE: 6/5/08

STANDARD ROAD CLOSURE AND SIGN DETAIL

SCALE: NONE
 DATE: 6-08
 DWG. BY: MDS
 DESIGN BY: MDS
 REVIEWED BY: CBH

SEAL:

REVISIONS:

| | | |
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| NO. | DATE | DESCRIPTION |
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