

STATE OF NORTH CAROLINA
DIVISION OF HIGHWAYS

STATE PROJECT REFERENCE NO.	SHEET NO.
R-4405	TCP-1

**PLAN FOR PROPOSED
TRAFFIC CONTROL, MARKING & DELINEATION**

**BLADEN-BRUNSWICK-COLUMBUS
CUMBERLAND-DUPLIN-SAMPSON
ONSLOW-ROBESON COUNTIES**

R-4405

LEGEND

GENERAL

- DIRECTION OF TRAFFIC FLOW
- NORTH ARROW
- PROPOSED PVMT. EXIST. PVMT.
- WORK AREA
- REMOVAL OF EXISTING PAVEMENT

TRAFFIC CONTROL DEVICES

- TYPE I BARRICADE
- TYPE II BARRICADE
- TYPE III BARRICADE
- CONE
- DRUM SKINNY DRUM
- FLASHING ARROW PANEL (TYPE C)
- STATIONARY SIGN
- PORTABLE SIGN
- STATIONARY OR PORTABLE SIGN
- CRASH CUSHION
- CHANGEABLE MESSAGE SIGN
- TRUCK MOUNTED IMPACT ATTENUATOR (TMIA)
- POLICE
- FLAGGER

PAVEMENT MARKINGS

- CRYSTAL/CRYSTAL PAVEMENT MARKER
- YELLOW/YELLOW PAVEMENT MARKER
- CRYSTAL/RED PAVEMENT MARKER
- PAVEMENT MARKING SYMBOLS

ROADWAY STANDARD DRAWINGS

THE FOLLOWING ROADWAY STANDARDS AS APPEAR IN "ROADWAY STANDARD DRAWINGS"-PROJECT SERVICES UNIT-N.C. DEPARTMENT OF TRANSPORTATION-RALEIGH, N.C., DATED JULY 2006 ARE APPLICABLE TO THIS PROJECT AND BY REFERENCE HEREBY ARE CONSIDERED A PART OF THESE PLANS:

STD. NO.	TITLE
1101.02	TEMPORARY LANE CLOSURES
1101.04	TEMPORARY SHOULDER CLOSURES
1101.05	WORK ZONE VEHICLE ACCESSES
1101.11	TRAFFIC CONTROL DESIGN TABLES
1110.02	PORTABLE WORK ZONE SIGNS
1115.01	FLASHING ARROW PANELS
1130.01	DRUM
1135.01	CONES
1150.01	FLAGGING DEVICES
1165.01	TRUCK MOUNTED IMPACT ATTENUATOR
1180.01	SKINNY-DRUM
1261.01	GUARDRAIL & BARRIER DELINEATOR SPACING
1261.02	GUARDRAIL & BARRIER DELINEATOR TYPES
1262.01	GUARDRAIL END DELINEATION

INDEX OF SHEETS

SHEET NO.	TITLE
TCP-1	LIST OF APPLICABLE ROADWAY STANDARD DRAWINGS, LEGEND, AND INDEX OF SHEETS
TCP-2	GENERAL NOTES AND PHASING
TCP-3	TEMPORARY LANE CLOSURES: TWO-LANE, TWO-WAY ROADWAY
TCP-4	TEMPORARY LANE CLOSURES: DIVIDED MULTI-LANE ROADWAY
TCP-5	TEMPORARY LANE CLOSURES: RIGHT LANE CLOSURES THRU ENTRANCE RAMPS
TCP-6	TEMPORARY LANE CLOSURES: RIGHT LANE CLOSURES THRU EXIT RAMPS
TCP-7	TEMPORARY SHOULDER CLOSURES

TIP PROJECT:

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APPROVED: _____ DATE: _____	PLAN PREPARED BY: N.C.D.O.T. WORK ZONE TRAFFIC CONTROL UNIT
	J. S. BOURNE, PE TRAFFIC CONTROL ENGINEER
	G. L. GETTIER, PE TRAFFIC CONTROL PROJECT ENGINEER
	J. W. GILSTRAP TRAFFIC CONTROL PROJECT DESIGN ENGINEER
	L. K. DONALDSON TRAFFIC CONTROL DESIGN ENGINEER / TECHNICIAN

GENERAL NOTES

PHASING

DRAWINGS, STANDARD DETAILS AND ROADWAY DETAILS ARE NOT ATTAINABLE TO MEET FIELD CONDITIONS, OR RESULT IN DUPLICATE, OR UNDESIRE OVERLAPPING OF DEVICES. MODIFICATION MAY INCLUDE: MOVING, SUPPLEMENTING, COVERING OR REMOVAL OF DEVICES, AS DIRECTED BY THE ENGINEER.

THE FOLLOWING GENERAL NOTES APPLY AT ALL TIMES FOR THE DURATION OF THE CONSTRUCTION PROJECT EXCEPT WHEN OTHERWISE NOTED IN THE PLAN OR AS DIRECTED BY THE ENGINEER.

TIME RESTRICTIONS

A) DO NOT CLOSE OR NARROW TRAVEL LANES AS FOLLOWS (OR AS DIRECTED BY THE ENGINEER):

ROAD NAME	DAY AND TIME RESTRICTIONS
1. US 17/74/76 EAST OF US 17 & US 74/76 SPLIT IN BRUNSWICK COUNTY	MONDAY THRU SUNDAY 6:00 AM TO 7:00 PM
2. US 24 IN ONSLOW COUNTY	MONDAY THRU FRIDAY 6:00 AM TO 9:00 AM AND 3:00 PM TO 7:00 PM
3. ALL OTHERS	MONDAY THRU FRIDAY 6:00 AM TO 9:00 AM AND 4:00 PM TO 7:00 PM

B) DO NOT CLOSE OR NARROW TRAVEL LANES DURING HOLIDAYS AND SPECIAL EVENTS AS FOLLOWS (OR AS DIRECTED BY THE ENGINEER):

ROAD NAME

1. ALL

HOLIDAY

- FOR ANY UNEXPECTED OCCURRENCE THAT CREATE UNUSUALLY HIGH TRAFFIC VOLUMES, AS DIRECTED BY THE ENGINEER.
- FOR NEW YEAR'S, BETWEEN THE HOURS OF 9:00 AM DECEMBER 31st TO 6:00 AM JANUARY 2ND. IF NEW YEAR'S DAY IS ON A FRIDAY, SATURDAY, SUNDAY, OR MONDAY THEN UNTIL 6:00 AM THE FOLLOWING TUESDAY.
- FOR EASTER, BETWEEN THE HOURS OF 9:00 AM THURSDAY AND 6:00 AM MONDAY.
- FOR MEMORIAL DAY, BETWEEN THE HOURS OF 9:00 AM FRIDAY TO 6:00 AM TUESDAY.
- FOR INDEPENDENCE DAY, BETWEEN THE HOURS OF 9:00 AM THE DAY BEFORE INDEPENDENCE DAY AND 6:00 AM THE DAY AFTER INDEPENDENCE DAY.

IF INDEPENDENCE DAY IS ON A FRIDAY, SATURDAY, SUNDAY, OR MONDAY THEN BETWEEN THE HOURS OF 9:00 AM THE THURSDAY BEFORE INDEPENDENCE DAY AND 6:00 AM THE TUESDAY AFTER INDEPENDENCE DAY.
- FOR LABOR DAY, BETWEEN THE HOURS OF 9:00 AM FRIDAY AND 6:00 AM TUESDAY.
- FOR THANKSGIVING DAY, BETWEEN THE HOURS OF 9:00 AM TUESDAY TO 6:00 AM MONDAY.
- FOR CHRISTMAS, BETWEEN THE HOURS OF 9:00 AM THE FRIDAY BEFORE THE WEEK OF CHRISTMAS DAY AND 6:00 AM THE FOLLOWING TUESDAY AFTER THE WEEK OF CHRISTMAS.

C) DO NOT CONDUCT ANY HAULING OPERATIONS AGAINST THE FLOW OF TRAFFIC OF AN OPEN TRAVELWAY UNLESS THE HAULING OPERATION IS PROTECTED BY BARRIER OR GUARDRAIL OR AS DIRECTED BY THE ENGINEER.

LANE AND SHOULDER CLOSURE REQUIREMENTS

- REMOVE LANE CLOSURE DEVICES FROM THE LANE WHEN WORK IS NOT BEING PERFORMED BEHIND THE LANE CLOSURE OR WHEN A LANE CLOSURE IS NO LONGER NEEDED, OR AS DIRECTED BY THE ENGINEER.
- WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN 15 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN SHOULDER USING SHEET TCP-7 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL OR A LANE CLOSURE IS INSTALLED.
- WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING ON THE SHOULDER ADJACENT TO AN UNDIVIDED FACILITY AND WITHIN 5 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN TRAVEL LANE USING SHEET TCP-3 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.

WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING ON THE SHOULDER ADJACENT TO A DIVIDED FACILITY AND WITHIN 10 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN TRAVEL LANE USING SHEETS TCP-4 THRU SHEET TCP-6 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.
- WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN A LANE OF TRAVEL OF AN UNDIVIDED OR DIVIDED FACILITY, CLOSE THE LANE ACCORDING TO THE TRAFFIC CONTROL PLANS, ROADWAY STANDARD DRAWINGS OR AS DIRECTED BY THE ENGINEER. CONDUCT THE WORK SO THAT ALL PERSONNEL AND/OR EQUIPMENT REMAIN WITHIN THE CLOSED TRAVEL LANE.
- DO NOT WORK SIMULTANEOUSLY WITHIN 15 FT ON BOTH SIDES OF AN OPEN TRAVELWAY, RAMP OR LOOP WITHIN THE SAME LOCATION UNLESS PROTECTED WITH GUARDRAIL OR BARRIER.
- DO NOT INSTALL MORE THAN ONE (1) MILE OF LANE CLOSURE ON ALL ROADS MEASURED FROM THE BEGINNING OF THE MERGE TAPER TO THE END OF THE LANE CLOSURE.
- DO NOT INSTALL MORE THAN TWO (2) SIMULTANEOUS LANE CLOSURES, IN ANY ONE DIRECTION, ON ALL ROADS.
- PROVIDE A MINIMUM OF TWO (2) MILES BETWEEN LANE CLOSURES, MEASURED FROM THE END OF ONE CLOSURE TO THE FIRST SIGN OF THE NEXT LANE CLOSURE.

TRAFFIC PATTERN ALTERATIONS

- NOTIFY THE ENGINEER TWENTY ONE (21) CALENDAR DAYS PRIOR TO ANY TRAFFIC PATTERN ALTERATION.

SIGNING

- ENSURE ALL NECESSARY SIGNING IS IN PLACE PRIOR TO ALTERING ANY TRAFFIC PATTERN.

TRAFFIC CONTROL DEVICES

- SPACE CHANNELIZING DEVICES IN WORK AREAS NO GREATER THAN TWICE THE POSTED SPEED LIMIT (MPH), EXCEPT 10 FT ON-CENTER IN RADII, AND 3 FT OFF THE EDGE OF AN OPEN TRAVELWAY, WHEN LANE CLOSURES ARE NOT IN EFFECT. WHEN SKINNY DRUMS ARE ALLOWED REFER TO SECTION 1180 OF STANDARD SPECIFICATIONS FOR ROADS AND STRUCTURES OR AS SHOWN IN THE PLANS.
- PLACE ADDITIONAL SETS OF THREE CHANNELIZING DEVICES (DRUMS, CONES OR SKINNY DRUMS) PERPENDICULAR TO THE EDGE OF TRAVELWAY ON 500 FT CENTERS WHEN UNOPENED LANES ARE CLOSED TO TRAFFIC.

MISCELLANEOUS

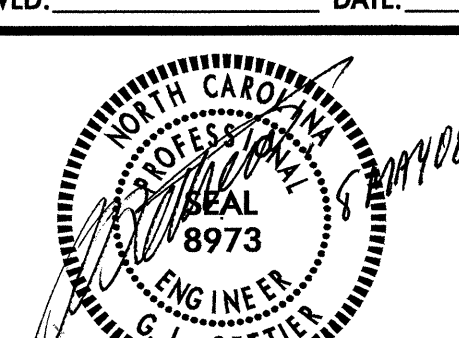
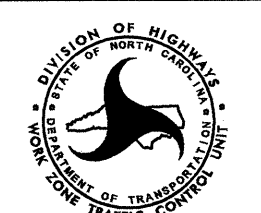
- POLICE MAY BE USED TO MAINTAIN TRAFFIC THROUGH THE WORK AREA AND/OR INTERSECTIONS, AS DIRECTED BY THE ENGINEER.

STEP 1: - INSTALL/REPAIR: GUARDRAIL, END TREATMENTS AND BRIDGE ANCHOR UNITS IN ACCORDANCE WITH CONSTRUCTION PLANS USING SHEETS TCP-3 THRU TCP-7 AS REQUIRED AND/OR AS DIRECTED BY THE ENGINEER.

NOTE: AT THE END OF EACH WORK PERIOD REMOVE ALL TEMPORARY TRAFFIC CONTROL DEVICES AND RETURN TRAFFIC TO ITS EXISTING TRAFFIC PATTERN.

STEP 2: - UPON COMPLETION OF THE PROJECT REMOVE ALL TRAFFIC CONTROL DEVICES.

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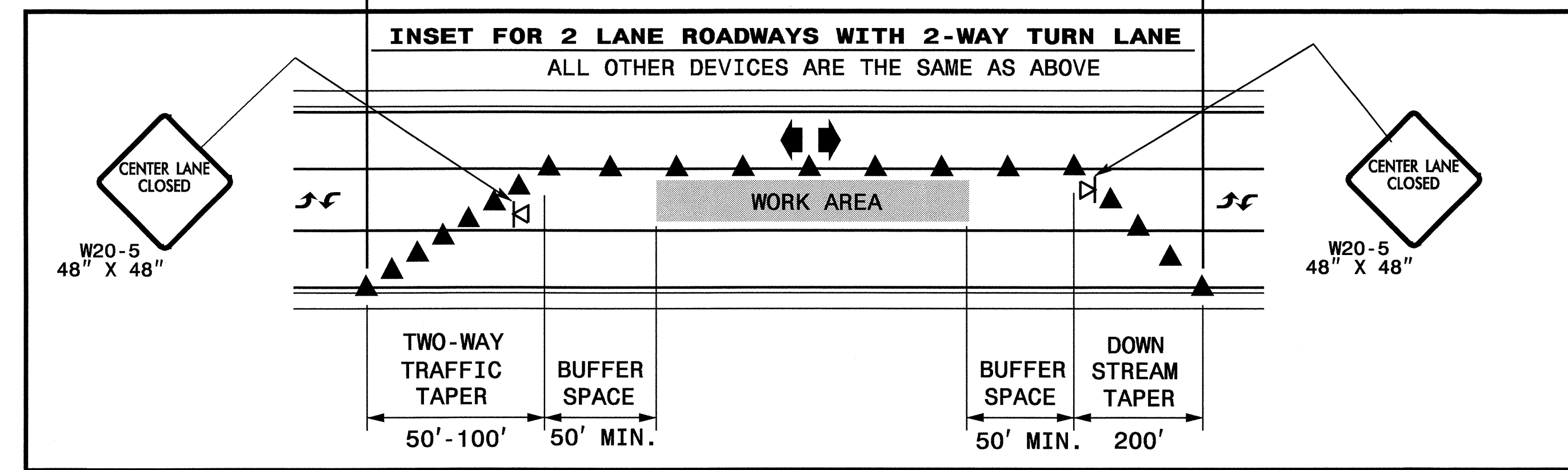
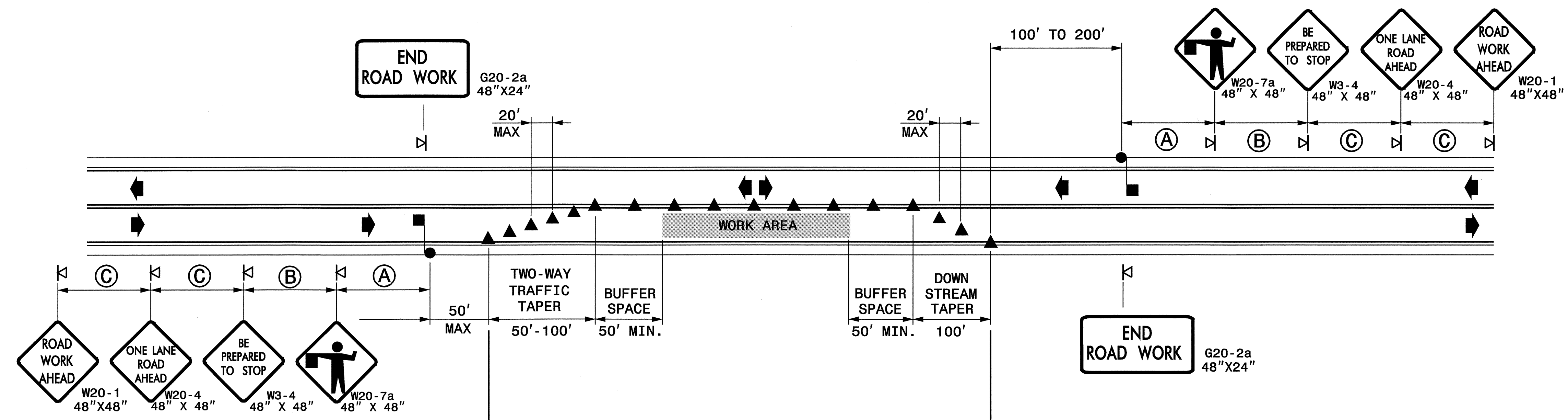
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STATE OF NORTH CAROLINA
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RALEIGH, N.C.

7-06

ENGLISH STANDARD DRAWING FOR
TEMPORARY LANE CLOSURES
2-LANE, 2-WAY ROADWAY - 1 LANE CLOSED

SHEET 1 OF 9
1101.02



GENERAL NOTES FOR FLAGGER OPERATIONS

- REFER TO STD. 1101.11 SHEET 4 FOR SIGN SPACING.
- INSTALL LANE CLOSURES WITH THE TRAFFIC FLOW, BEGINNING WITH DEVICES ON THE UPSTREAM SIDE OF TRAFFIC.
- REMOVE LANE CLOSURES AGAINST THE TRAFFIC FLOW, BEGINNING WITH DEVICES ON THE DOWNSTREAM SIDE OF TRAFFIC.
- PLACE CONES THRU THE WORK AREA AT THE MAXIMUM SPACING EQUAL IN FEET TO 2 TIMES THE POSTED SPEED LIMIT.
- EXTEND LANE CLOSURES AT THE BUFFER SPACE SUCH THAT STOPPING SIGHT DISTANCE IS PROVIDED TO THE FLAGGER. (REFER TO STD. 1101.11 SHEET 2)
- DO NOT STOP TRAFFIC IN ANY ONE DIRECTION FOR MORE THAN 5 MINUTES AT A TIME.
- DRUMS OR SKINNY-DRUMS MAY BE USED IN LIEU OF CONES.
- USE FLAGGERS TO CONTROL TRAFFIC AT INTERSECTIONS AFFECTED BY THE LANE CLOSURE. SUPPLEMENT FLAGGERS LOCATED AT INTERSECTIONS WITH FLAGGER AHEAD SIGNS (W20-7a) PLACED APPROXIMATELY 250 FT. IN ADVANCE OF THE FLAGGER WHERE INTERSECTIONS ARE SIGNALIZED AND PLACE SIGNALS IN THE FLASH MODE.
- FLAGGERS SHALL NOT STAND IN THE ROADWAY.

GENERAL NOTES FOR PILOT CAR OPERATIONS

- USE PILOT CARS WHEN DIRECTED BY THE ENGINEER.
- IF ROADWAY WIDTH IS LESS THAN 22 FEET (EOP TO EOP), CONES MAY NOT BE REQUIRED ALONG WORK AREA, AND AT THE DISCRETION OF THE ENGINEER, CONES MAY BE OMITTED ALONG THE WORK AREA IF USING A PILOT CAR. (NOTE: CONES ARE ALWAYS REQUIRED IN THE UPSTREAM AND DOWNSTREAM TAPERS).
- MOUNT SIGN G20-4 "PILOT CAR FOLLOW ME" AT A VISIBLE LOCATION ON THE REAR OF THE PILOT VEHICLE.
- DO NOT INSTALL MORE THAN ONE (1) MILE OF LANE CLOSURE, MEASURED FROM THE BEGINNING OF THE MERGE TAPER TO THE END OF THE LANE CLOSURE.
- ADVISE RESIDENTS AND BUSINESSES WITHIN THE LANE CLOSURE LIMITS ABOUT METHODS OF SAFE EGRESS AND INGRESS FROM DRIVEWAYS DURING FLAGGING AND PILOT CAR OPERATIONS.

LEGEND	
	CONES
	PORTABLE SIGN
	FLAGGER
	DIRECTION OF TRAFFIC FLOW

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RALEIGH, N.C.

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ENGLISH STANDARD DRAWING FOR
TEMPORARY LANE CLOSURES
2-LANE, 2-WAY ROADWAY - 1 LANE CLOSED

SHEET 1 OF 9
1101.02

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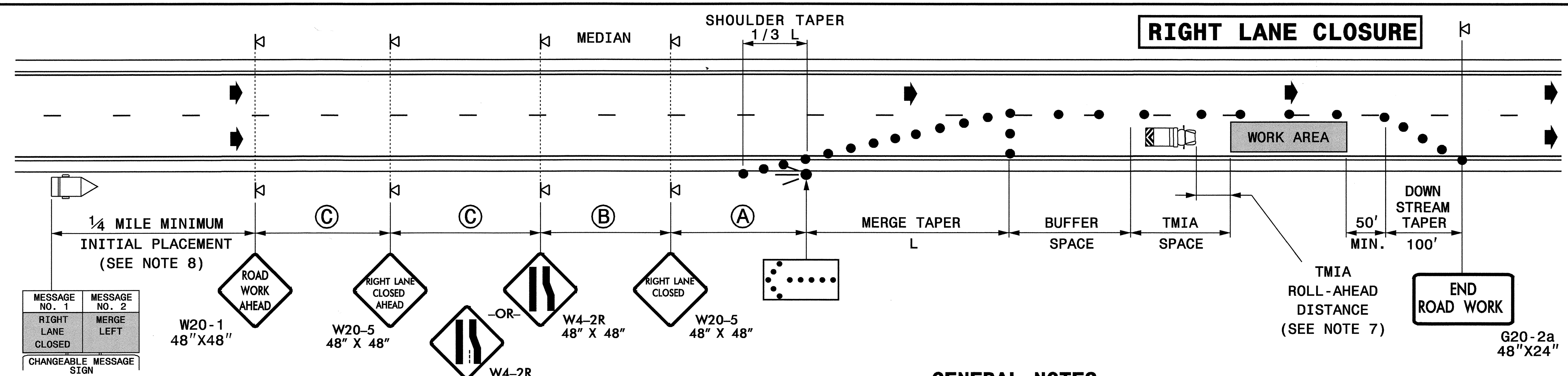
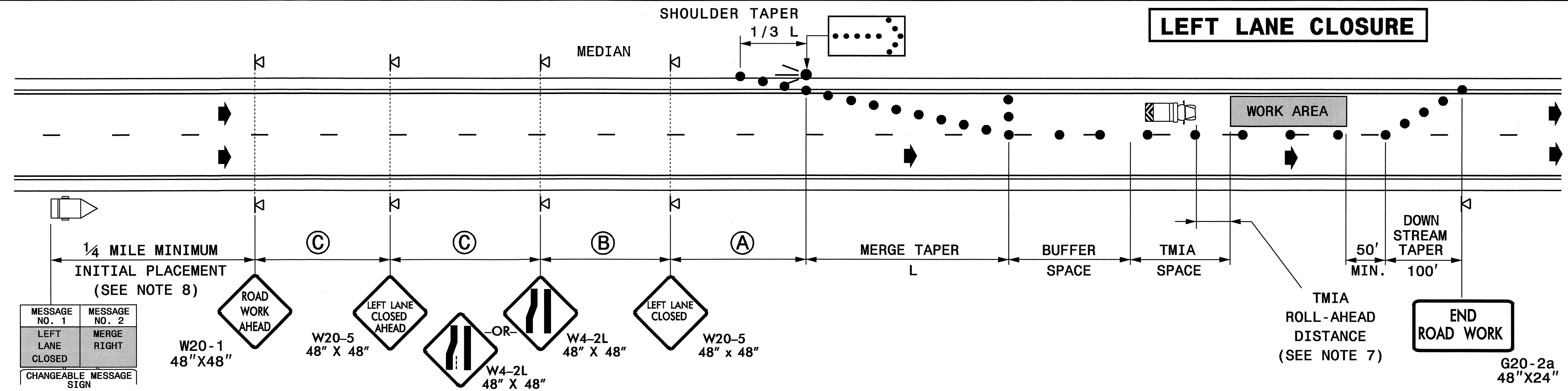
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7-06

ENGLISH STANDARD DRAWING FOR
TEMPORARY LANE CLOSURES
DIVIDED MULTI-LANE ROADWAY - 1 LANE CLOSED

SHEET 3 OF 9
1101.02



GENERAL NOTES

- 1- IF NECESSARY USE THIS STD. FOR ONE-WAY CITY TYPE STREETS WHERE SIGNS MAY BE MOUNTED ON BOTH SIDES OF THE ROADWAY.
- 2- PLACE ARROW PANELS ON THE SHOULDER (PAVED OR UNPAVED). PLACE ARROW PANELS WITHIN THE TAPER IF SHOULDERS DO NOT EXIST. MEET THE REQUIREMENTS FOR STOPPING SIGHT DISTANCE AT THE ARROW PANEL LOCATION. IF NEEDED, EXTEND LANE CLOSURES AT THE BUFFER SPACE, SUCH THAT STOPPING SIGHT DISTANCE TO THE ARROW PANEL IS MET. (SEE STD. 1101.11 SHEET 2)
- 3- PLACE DRUMS IN TAPERS AT THE MAXIMUM SPACING EQUAL IN FEET TO THE POSTED SPEED LIMIT. PLACE DRUMS ALONG THE BUFFER SPACE AND WORK AREA AT THE MAXIMUM SPACING EQUAL IN FEET TO 2 TIMES THE POSTED SPEED LIMIT.
- 4- REFER TO STD. 1101.11 SHEETS 1 & 4, FOR "L" DISTANCE AND SIGN SPACING.
- 5- REFER TO SHEET TCP-5 AND TCP-6 FOR TREATMENT OF LANE CLOSURES THRU INTERCHANGES.
- 6- INSTALL LANE CLOSURES WITH THE TRAFFIC FLOW, BEGINNING WITH DEVICES ON THE UPSTREAM SIDE OF TRAFFIC. REMOVE LANE CLOSURES AGAINST THE TRAFFIC FLOW, BEGINNING WITH DEVICES ON THE DOWNSTREAM SIDE OF TRAFFIC.
- 7- TMIA'S ARE REQUIRED ONLY WHEN A BUFFER SPACE CANNOT BE ATTAINED, OR WHEN DIRECTED BY THE ENGINEER OR THE PLANS. WHEN USED, POSITION THE TMIA TO MAINTAIN A ROLL-AHEAD DISTANCE AS RECOMMENDED BY THE MANUFACTURER.
- 8- PLACE CHANGEABLE MESSAGE SIGN (CMS) ON THE OUTSIDE OF THE TRAVELWAY AS DIRECTED BY THE ENGINEER. PLACE CMS APPROXIMATELY 1/2 MILE IN ADVANCE OF THE W20-5 SIGNS. IF TRAFFIC BACKS UP TO WHERE THE CMS IS INITIALLY PLACED, RELOCATE CMS 1/2 MILE FROM ANTICIPATED BACKUP. CONTINUE TO MONITOR TRAFFIC, MOVE CMS APPROXIMATELY 1/2 MILE IN CONJUNCTION WITH ANTICIPATED BACKUP.
- 9- MESSAGES MAY READ "LEFT LANE CLOSED" OR "RIGHT LANE CLOSED", "MERGE RIGHT" OR "MERGE LEFT" DEPENDING ON LANE BEING CLOSED.

LEGEND

- FLASHING ARROW PANEL (TYPE C)
- ☐ TRUCK MOUNTED IMPACT ATTENUATOR(TMIA)
- ☐ CHANGEABLE MESSAGE SIGN (CMS)
- DRUM
- △ PORTABLE SIGN
- ➔ DIRECTION OF TRAFFIC FLOW

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RALEIGH, N.C.

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ENGLISH STANDARD DRAWING FOR
TEMPORARY LANE CLOSURES
DIVIDED MULTI-LANE ROADWAY - 1 LANE CLOSED

SHEET 3 OF 9
1101.02

APPROVED: _____ DATE: _____

TEMPORARY LANE CLOSURES
DIVIDED MULTI-LANE ROADWAY

SCALE: NONE

DATE: _____

DWG. BY: LKD

DESIGN BY: _____

REVIEWED BY: JWG

REVISIONS

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PROFESSIONAL SEAL
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ENGINEER
G. L. GETTIE

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7-06

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RALEIGH, N.C.

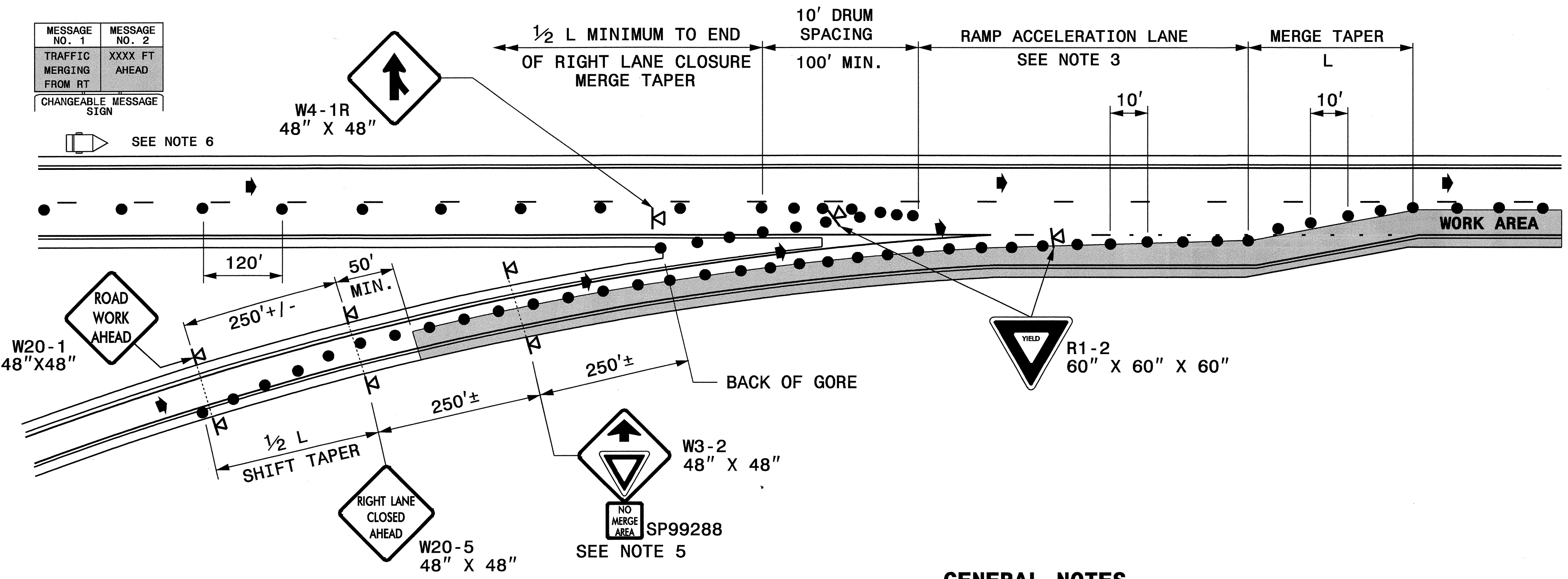
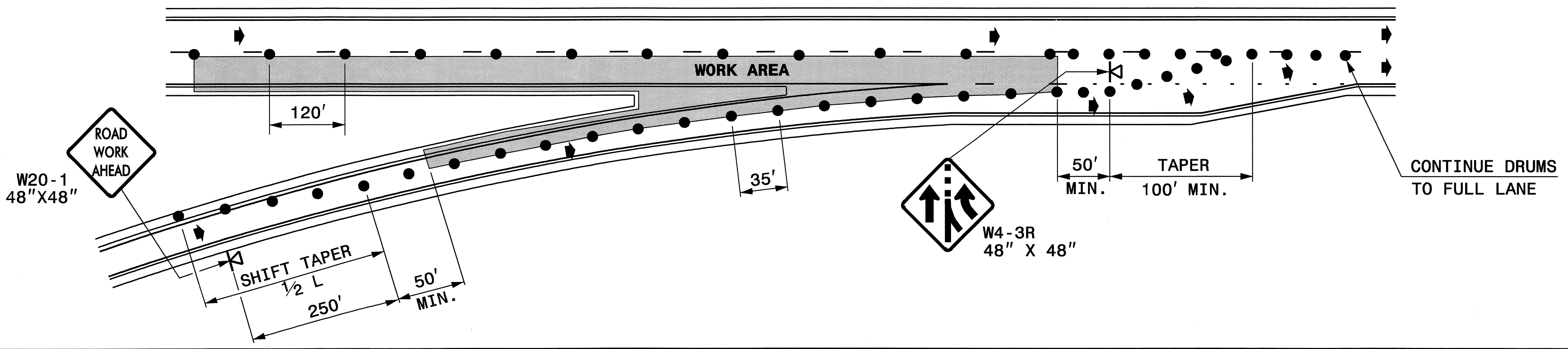
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ENGLISH STANDARD DRAWING FOR
TEMPORARY LANE CLOSURES
RIGHT LANE CLOSURES THRU ENTRANCE RAMP

SHEET 6 OF 9
1101.02

ENGLISH STANDARD DRAWING FOR
TEMPORARY LANE CLOSURES
RIGHT LANE CLOSURES THRU ENTRANCE RAMP

SHEET 6 OF 9
1101.02



MESSAGE NO. 1	MESSAGE NO. 2
TRAFFIC MERGING FROM RT	XXXX FT AHEAD
CHANGEABLE MESSAGE SIGN	

SEE NOTE 6

LEGEND	
	CHANGEABLE MESSAGE SIGN (CMS)
	DRUM
	PORTABLE SIGN
	DIRECTION OF TRAFFIC FLOW

GENERAL NOTES

- 1- USE THE ABOVE DETAILS IN CONJUNCTION WITH A RIGHT LANE CLOSURE AS SHOWN ON SHEET TCP-4.
- 2- MOUNT SIGNS SHOWN A MINIMUM OF 5 FEET ABOVE THE PAVEMENT ELEVATION.
- 3- IF EXISTING ACCELERATION DISTANCE OR A MINIMUM OF 400' ACCELERATION DISTANCE CANNOT BE PROVIDED, CONTACT THE WORK ZONE TRAFFIC CONTROL UNIT FOR FURTHER GUIDANCE.
- 4- CLOSE THE RIGHT LANE SUFFICIENTLY IN ADVANCE TO STABILIZE MOTOR VEHICLE TRAFFIC FLOW BEFORE THE MERGE AS SHOWN ON SHEET TCP-4.
- 5- INSTALL SP99288 BELOW THE YIELD AHEAD SIGN (AS SHOWN) TO ALERT MOTORISTS THAT THE ACCELERATION DISTANCE HAS BEEN REDUCED.
- 6- COORDINATE WITH THE ENGINEER FOR LOCATION OF CMS.

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TEMPORARY LANE CLOSURES RIGHT LANE CLOSURES THRU ENTRANCE RAMP		REVISIONS	
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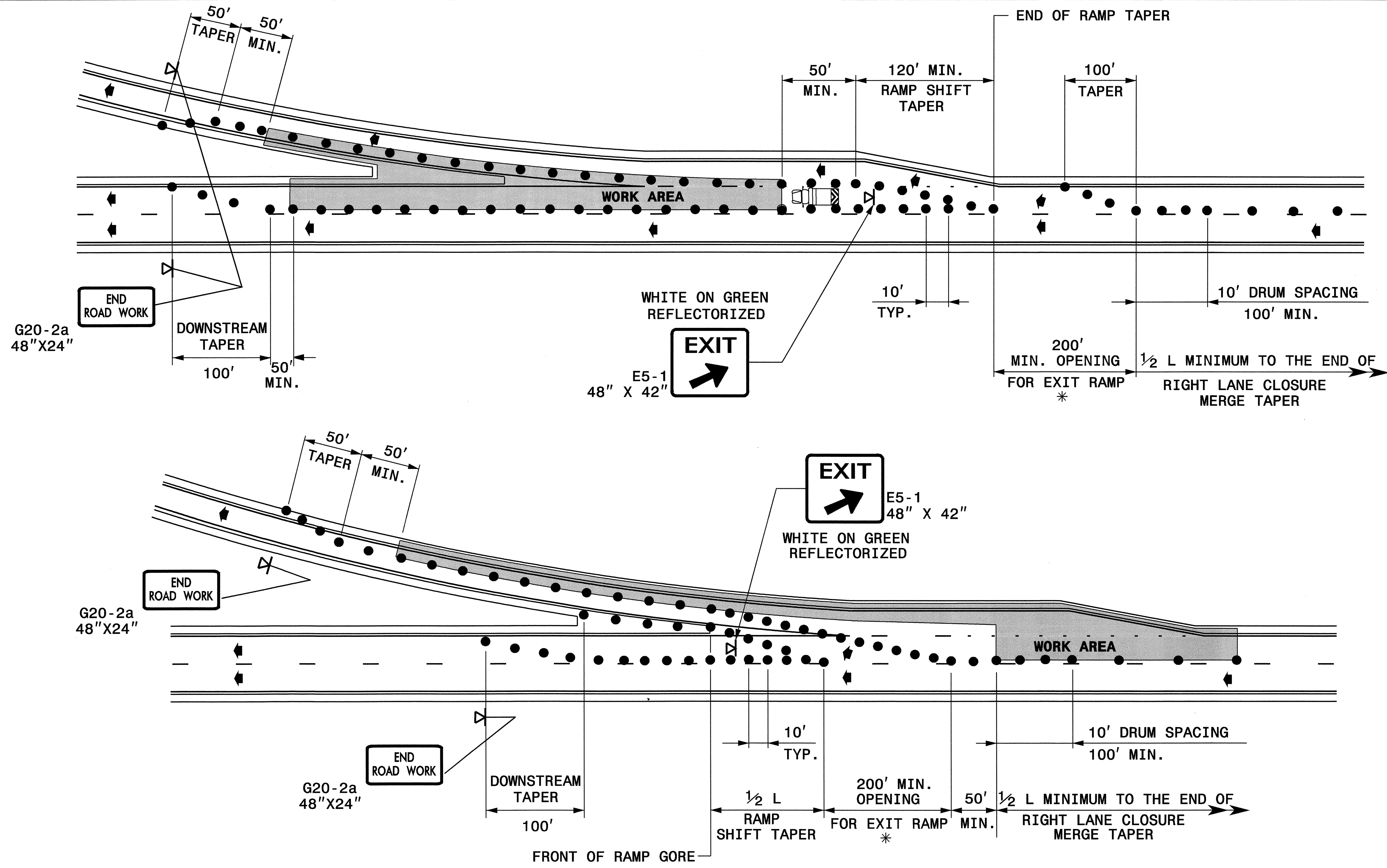
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ENGLISH STANDARD DRAWING FOR
TEMPORARY LANE CLOSURES
RIGHT LANE CLOSURES THRU EXIT RAMPS

SHEET 7 OF 9
1101.02



LEGEND

	TRUCK MOUNTED IMPACT ATTENUATOR
	DRUM
	PORTABLE SIGN
	DIRECTION OF TRAFFIC FLOW

- GENERAL NOTES**
- 1-USE THE ABOVE DETAILS IN CONJUNCTION WITH A RIGHT LANE CLOSURE AS SHOWN ON SHEET TCP-4.
 - 2-MOUNT EXIT SIGNS A MINIMUM OF 7 FEET ABOVE THE PAVEMENT ELEVATION.

*** NOTE:**

- 1-USE EXISTING RAMP OPENING LENGTH, BUT NO LESS THAN 1/2 ORIGINAL LENGTH.
- 2-CONSIDER CLOSING RAMP IF LESS THAN 1/2 ORIGINAL LENGTH CAN NOT BE OBTAINED.

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ENGLISH STANDARD DRAWING FOR
TEMPORARY LANE CLOSURES
RIGHT LANE CLOSURES THRU EXIT RAMPS

SHEET 7 OF 9
1101.02

APPROVED: _____ DATE: _____

TEMPORARY LANE CLOSURES
RIGHT LANE CLOSURES THRU EXIT RAMPS

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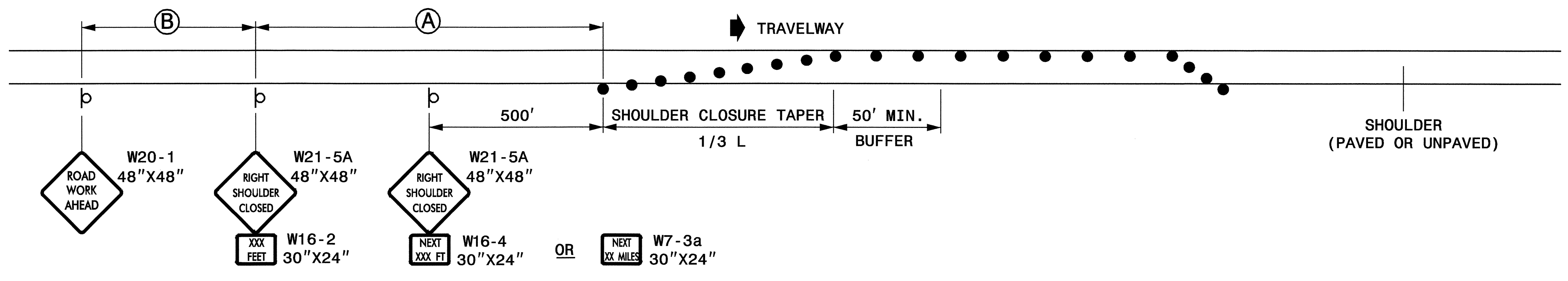
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7-06

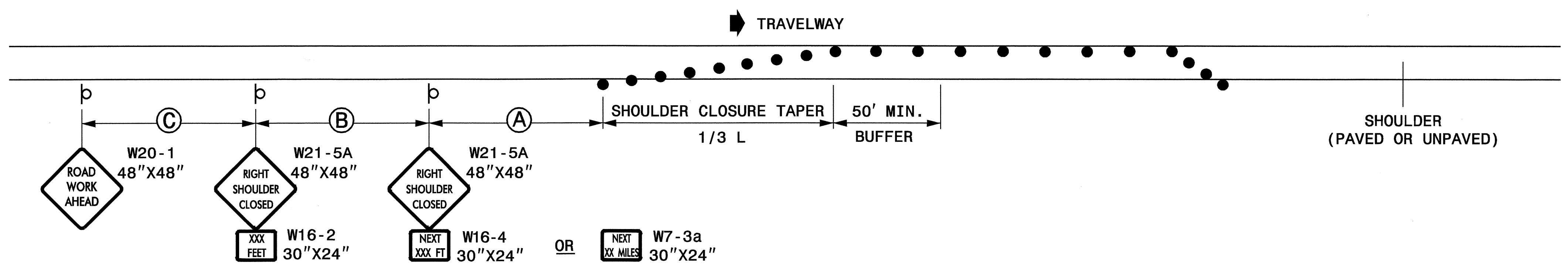
ENGLISH STANDARD DRAWING FOR
TEMPORARY SHOULDER CLOSURES

SHEET 1 OF 1
1101.04

SHOULDER CLOSURE ON CONTROLLED ACCESS FACILITIES - 55 MPH OR ABOVE

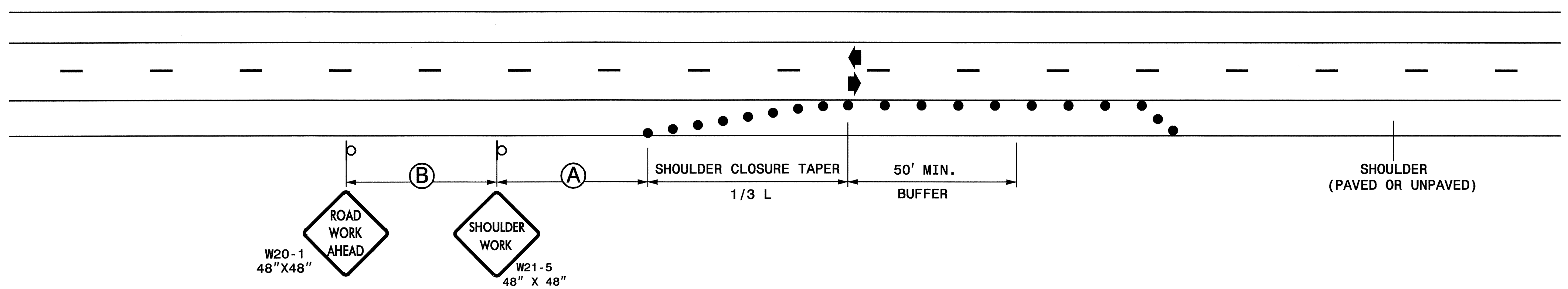


SHOULDER CLOSURE ON OTHER DIVIDED FACILITIES - 55 MPH OR LESS



SHOULDER CLOSURE ON TWO-LANE, TWO-WAY ROADWAYS

(SEE NOTE 5)



LEGEND

- DRUM
- ⊔ STATIONARY OR PORTABLE SIGN
- ➔ DIRECTION OF TRAFFIC FLOW

GENERAL NOTES

- 1- PLACE SHOULDER CLOSURE SIGNS ON THE SAME SIDE AS THE SHOULDER THAT IS CLOSED.
- 2- PLACE DRUMS IN THE SHOULDER TAPER AT THE MAXIMUM SPACING EQUAL IN FEET TO THE POSTED SPEED LIMIT. THE MAXIMUM SPACING OF DRUMS ALONG THE WORK AREA IS EQUAL IN FEET TO 2 TIMES THE POSTED SPEED LIMIT.
- 3- USE STATIONARY SIGNS FOR LONG TERM OPERATIONS (LONGER THAN 3 DAYS).
- 4- REFER TO STD. 1101.11 SHEETS 1, 3, & 4, FOR "L" DISTANCE, AND SIGN SPACING.
- 5- THE TWO-LANE, TWO-WAY DRAWING MAY BE APPLIED TO UNDIVIDED, MULTI-LANE FACILITIES.

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ENGLISH STANDARD DRAWING FOR
TEMPORARY SHOULDER CLOSURES

SHEET 1 OF 1
1101.04

APPROVED: _____ DATE: _____

TEMPORARY SHOULDER CLOSURES

SCALE: NONE		REVISIONS
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