

**Project Special Provisions  
Structures**

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*Quang H. Nguyen* 12-06-07

**PROJECT SPECIAL PROVISIONS**  
**STRUCTURES AND CULVERTS**

**PROJECT B-4696**

**SWAIN COUNTY**

**DRILLED PIERS**

**(11-17-06)**

**1.0 GENERAL**

**A. Description**

This special provision governs the construction of Drilled Piers, also known as "Drilled Shafts" and "Caissons". Drilled piers are a reinforced concrete section, cast-in-place against in situ material or permanent steel casing. Drilled piers are a straight shaft type and vertical. Construct drilled piers in accordance with the details and dimensions shown on the plans and this provision.

**B. Prequalification and Experience Requirements**

Use a Drilled Pier Contractor prequalified by the Contractual Services Unit of the Department for drilled pier work (work code 3090).

Submit documentation that the Drilled Pier Contractor has successfully completed at least 5 drilled pier projects within the last 3 years with diameters, lengths and subsurface conditions similar to those anticipated for this project. Documentation should include the General Contractor and Owner's name and current contact information with descriptions of each past project. Also, submit documentation of experience with dry and wet placement of concrete and the use of temporary casing or slurry.

Provide a list of the Drilling Superintendent, Drill Rig Operators and Project Manager that will be assigned to this project. Submit documentation for these personnel verifying employment with the Drilled Pier Contractor and a minimum of 5 years experience in drilled pier construction with past projects of scope and complexity similar to that anticipated for this project. Documentation should include resumes, references, certifications, project lists, experience descriptions and details, etc. Perform work with the personnel submitted and accepted. If personnel changes are required during construction, suspend drilled pier construction until replacement personnel are submitted and accepted.

### C. Construction Sequence Plan

Submit two hard copies and an electronic copy (pdf or jpeg format on CD or DVD) of a drilled pier construction sequence plan for all the drilled piers 30 days before beginning drilled pier construction. Provide detailed project specific information in this plan including:

1. Experience documentation in accordance with Section 1.0, Item B
2. List and size of equipment including: cranes, kelly bars, drill rigs, vibratory hammers, augers, core barrels, cleanout buckets, airlifts and/or submersible pumps, tremies and/or concrete pumps, casing (diameters, thicknesses and lengths), desanding equipment (for slurry construction), etc.
3. Order of drilled pier construction
4. Casing installation and temporary casing removal methods including the order of telescoped casing removal
5. Drilled pier excavation and bottom cleaning methods
6. Reinforcement placement methods including how the cage will be supported and centered in the excavation
7. Concrete placement procedures including how the tremie or pump will be controlled and what type of discharge control will be used to prevent concrete contamination when the tremie or pump is initially placed in the excavation
8. Concrete mix design in accordance with Section 1000 of the Standard Specifications
9. Slurry details (if applicable) including intended purpose, product information, manufacturer's recommendations for use, slurry equipment information and written approval from the slurry supplier that the mixing water is acceptable
10. Procedures for handling drilling spoils and slurry overflow including environmental controls to prevent the loss of concrete, slurry and spoils
11. Methods of how the slurry level will be maintained above the highest piezometric head (if applicable)
12. Crosshole sonic logging (CSL) submittals (if applicable) in accordance with the Crosshole Sonic Logging Special Provision
13. Other information shown on the plans or requested by the Engineer

Do not begin drilled pier construction until the construction sequence plan is accepted. If alternate drilled pier construction procedures are proposed or necessary, a revised submittal may be required. If the work deviates from the accepted submittal without prior approval, the Engineer may suspend drilled pier construction until a revised drilled pier construction sequence plan is submitted and accepted.

#### D. Preconstruction Meeting

Conduct a drilled pier preconstruction meeting with the Project Manager, Drilling Superintendent, the Resident or Bridge Maintenance Engineer and/or his or her representatives, the Bridge Construction Engineer and the Geotechnical Operations Engineer to discuss construction and inspection of the drilled piers. This meeting should occur after the Drilled Pier Contractor has mobilized to the site and the construction sequence plan has been reviewed and accepted.

#### E. Definition of Rock

For the purposes of this provision, "Rock" is defined as a continuous intact natural material in which the penetration rate with a rock auger is less than 2 in (50 mm) per 5 minutes of drilling at full crowd force. This definition excludes discontinuous loose natural materials such as boulders and man-made materials such as concrete, steel, timber, etc. This definition of rock is not for pay purposes; see Section 8.0 for method of measurement of "Drilled Piers Not in Soil".

#### F. Rock Socket

When required by a note on plans, provide a minimum penetration into rock as directed by the Engineer.

### 2.0 EXCAVATION

Perform the excavations required for the drilled piers to the dimensions and elevations shown on the plans or otherwise required by the Engineer, including any miscellaneous grading or excavation to install the pier.

Excavate with a drill rig of adequate capacity. Use a rig that is capable of drilling through soil and non-soil including rock, boulders, timbers, man-made objects and any other materials encountered. Blasting is not permitted to advance the excavation. Blasting for core removal is only permitted when approved by the Engineer.

Use a drill rig capable of drilling a minimum of 25% deeper than the deepest drilled pier shown on the plans. Use drilling tools equipped with vents designed to stabilize the hydrostatic pressure above and below the tool during extraction from the excavation. For drilled piers constructed with slurry, monitor the rate at which the drilling tools are inserted and extracted so as to minimize sidewall suction action in the excavation. Drilling below the tip elevations shown on the plans may be required to achieve adequate bearing.

A drilling log signed by the Drilled Pier Contractor that includes material descriptions and depths and drilling times and tools used for each material is required for each pier.

Dispose of drilling spoils in accordance with Section 802 of the Standard Specifications and as directed by the Engineer. Drilling spoils consist of all excavated material including water removed from the excavation either by pumping or drilling tools. Construct drilled piers at the locations shown on the plans and within the tolerances specified herein. If tolerances are exceeded, the Engineer may require corrective measures to meet the tolerances specified. Construct the drilled piers such that the axis at the top of the piers is no more than 3 in (75 mm) in any direction from the position shown in the plans. Build drilled piers within 2% of the plumb deviation for the total length of the piers. Verify the plumbness of the drilled pier excavations by an accurate procedure, such as an inclinometer on the kelly bar or other approved techniques. Unless a plan note requires the construction joint to be moved below the ground line, construct the finished top of pier elevation between 1 in (25 mm) above and 3 in (75 mm) below the top of pier elevation shown on the plans.

When drilling from a barge, use a fixed template that maintains pier position and alignment during all excavation and concrete placement operations. Floating templates (attached to a barge) are not allowed.

Stabilize all drilled pier excavations with steel casing and/or slurry except, as approved by the Engineer, the portions of the excavations in rock as defined by Section 1.0, Item E. Stabilize excavations at all times from the beginning of drilling through concrete placement. When using multiple casings in a telescoped arrangement, overlap subsequent casings a minimum of 2 ft (600 mm). Provide casing or slurry in rock if unstable material is anticipated or encountered during drilling. When slurry is used, a partially excavated pier is subject to the time requirements in Section 2.0, Item C, Number 1. When slurry is not used, do not leave a drilled pier excavation open overnight unless it is cased to rock.

If a note on plans does not prohibit dewatering and the tip of the drilled pier excavation is in rock as defined by Section 1.0, Item E, dewater the excavation to the satisfaction of the Engineer. The minimum diameter of a drilled pier excavation in rock or an excavation constructed with slurry may be 2 in (50 mm) less than the design drilled pier diameter shown on the plans.

In order to remove a casing and substitute a larger diameter or longer casing through unstable or caving material, either backfill the excavation, stabilize the excavation with slurry before removing the casing to be replaced or insert the larger casing around the casing to be replaced before removal.

#### A. Permanent Steel Casing

Use permanent steel casings as directed by the Engineer and/or as required by a note on plans. Use permanent casings that are clean smooth non-corrugated watertight steel of ample strength to withstand handling and driving stresses and the pressures imposed by

concrete, earth or backfill. Provide permanent steel casings conforming to ASTM A252, Grade 2 and the following minimum wall thickness requirements.

#### CASING WALL THICKNESS

Casing Diameter	Minimum Wall Thickness
Less than or equal to 48 in (1220 mm)	3/8 in (9 mm)
Greater than 48 in (1220 mm) and less than or equal to 78 in (1982 mm)	1/2 in (12 mm)
Greater than 78 in (1982 mm)	5/8 in (16 mm)

Provide permanent casings with an outside diameter not less than the specified size of the drilled pier. If approved by the Engineer, a permanent casing larger in diameter than the drilled pier design diameter is permitted. However, no payment will be made for any costs associated with larger permanent casings. Extend the permanent casings from the top of pier elevation or top of permanent casing elevation, if shown on the plans, to a depth no deeper than the permanent casing tip elevation shown on the plans or the revised permanent casing tip elevation approved by the Engineer. Do not extend permanent casings below the permanent casing tip elevation shown on the plans without prior approval from the Engineer. Additional drilled pier length and reinforcement may be required if permanent casings are extended below the permanent casing tip elevation shown on the plans. No payment will be made for the resulting additional drilled pier length, reinforcement and permanent casing unless the Engineer approves the revised permanent casing tip elevation. Install permanent casings in one continuous unit. If splices are necessary for the casing, use an approved method of splicing. Splices are considered incidental and no additional compensation will be made.

Remove any portion of the permanent steel casing that extends above the top of the drilled pier after the Drilled Pier Concrete has achieved a compressive strength of 4500 psi (31.0 MPa). The cost of casing removal will be considered incidental to the cost of the permanent steel casing.

#### B. Temporary Steel Casing

Provide temporary steel casing to stabilize drilled pier excavations, protect personnel and prevent caving or sloughing, that is clean smooth non-corrugated watertight steel of ample strength to withstand handling and driving stresses and the pressures imposed by concrete, earth or backfill. Use temporary steel casings with a minimum wall thickness of 3/8 in (9 mm) and an outside diameter not less than the specified size of the drilled pier.

Temporary steel casings that become bound or fouled during construction and cannot be practically removed may constitute a defect in the drilled pier. Improve defective piers to the satisfaction of the Engineer by removing the concrete and extending the pier deeper, providing a replacement drilled pier or other acceptable means. Complete

all corrective measures including any additional design work to the satisfaction of the Engineer without additional compensation or an extension of the completion date of the project.

C. Slurry

When slurry use is not noted on the plans, slurry construction is an option. If slurry use is required or an option, polymer slurry use may either be required or prohibited as noted on the plans. If slurry use is required or an option and polymer slurry use is not noted on the plans, polymer slurry use is an option.

If polymer slurry is required or an option, use one of the following polymers listed in the table below:

PRODUCT	MANUFACTURER
SlurryPro CDP	KB Technologies Ltd. 3648 FM 1960 West, Suite 107 Houston, TX 77068 (800) 525-5237
Super Mud	PDS Company 105 West Sharp Street El Dorado, AR 71730 (800) 243-7455
Shore Pac GCV	CETCO Drilling Products Group 1500 West Shure Drive Arlington Heights, IL 60004 (800) 527-9948
Novagel Polymer	Geo-Tech Drilling Fluids 220 North Zapata Hwy, Suite 11A Laredo, TX 78043 (210) 587-4758

Use polymer slurry and associated additives in accordance with the manufacturer's guidelines and recommendations unless otherwise approved by the Engineer. The Drilled Pier Contractor should be aware that polymer slurry might not be appropriate for a given site. Polymer slurry should not be used for excavations in soft or loose soils as determined by the Engineer. When using polymer slurry, a representative of the manufacturer must be on-site to assist and guide the Contractor during the construction of the first three drilled piers unless otherwise approved by the Engineer. This representative must also be available for on-site assistance to the Contractor if problems are encountered during the construction of the remaining drilled piers as requested by the Engineer. The cost of all on-site assistance and representation will be considered incidental to the cost of the drilled piers.

If mineral slurry is required or an option, use mineral slurry composed of bentonite having a mineral grain size that remains in suspension and sufficient viscosity and gel characteristics to transport excavated material to a suitable screening system to minimize bottom sedimentation. Provide bentonite slurry to maintain the stability of the excavation and allow for proper concrete placement. The Drilled Pier Contractor should be aware that salt water with salt concentrations in excess of 500 ppm may adversely affect bentonite slurry.

When slurry is used and permanent steel casing is not required, use temporary casing a minimum of 10 ft (3 m) long at the top of the excavation. Maintain the top of the temporary casing a minimum of 1 ft (300 mm) above the ground surface surrounding the casing.

Maintain the slurry in the pier excavation at a level not less than 5 ft (1.5 m) or the drilled pier diameter (whichever is greater) above the highest piezometric head along the depth of the pier. It is anticipated that the highest piezometric head is the static water or groundwater elevation (elevation head). However, the Drilled Pier Contractor is responsible for determining the highest piezometric head. The use of steel casing to maintain the required slurry level is permitted; however, no payment will be made for casing that is used for this purpose. If the slurry level in the excavation suddenly changes or cannot be practically maintained, or the slurry construction method does not produce the desired result, stop the pier construction until an alternate construction procedure is accepted by the Engineer.

Thoroughly premix the slurry with water in tanks before introducing the slurry into the excavation. Submit written approval from the slurry supplier that the mixing water is acceptable. Allow bentonite slurry to hydrate 24 hours in tanks before use. Slurry tanks of adequate capacity are required for slurry circulation, storage and treatment. Excavated slurry pits are not allowed in lieu of slurry tanks without prior approval from the Engineer. Take all steps necessary to prevent the slurry from "setting up" in the excavation. Such methods include, but are not limited to agitation, circulation and/or adjusting the properties of the slurry. Perform desanding operations as necessary to achieve the acceptable sand contents before placing reinforcing steel.

#### 1. Time

Adjust the excavation operations so that the maximum time the slurry is in contact with the sidewalls of the uncased portions of the drilled pier excavation (from time of drilling to concrete placement) does not exceed 36 hours. Do not work on more than two drilled piers per drill rig below the steel casing at any time.

Agitate bentonite slurry in the drilled pier excavations a minimum of every 4 hours. If the bentonite slurry is not agitated a minimum of every 4 hours or the 36 hour time limit is exceeded, the Engineer may require the excavation to be overreamed beneath the steel casing a minimum of 1 in (25 mm) and a maximum of 3 in (75 mm) before performing any other operations in the excavation. Overream with a grooving tool, overreaming bucket or other approved equipment at a minimum



spacing of 12 in (300 mm). All costs associated with both overreaming and the resulting additional concrete placement will be considered incidental to the cost of the drilled piers.

If concrete placement is not completed within three days of beginning drilling, enlarge the design drilled pier diameter by a minimum of 6 in (150 mm), or as required by the Engineer, the entire length of the pier at no additional cost to the Department. Enlarging the drilled pier includes replacing the steel casing with steel casing the same size to which the drilled pier is enlarged at no additional cost to the Department.

## 2. Sampling

Collect all slurry samples using an approved sampling tool. Test slurry samples to determine density, viscosity, pH and sand content to establish an acceptable working pattern during slurry use. Test a minimum of 4 samples during each 8 hours of slurry use for each drilled pier. Take the first sample for the first 8 hours from the slurry tank before introducing slurry into the excavation. Collect the remaining samples from the bottom of the pier excavation. When the test results are acceptable and consistent, a decrease in the testing frequency to one sample per 4 hours of slurry use is permitted.

Before placing reinforcing steel in the drilled pier excavation, extract slurry samples from the bottom of each excavation and at intervals not exceeding 10 ft (3 m) up the excavation, until two consecutive samples produce acceptable values for density, viscosity, pH and sand content.

## 3. Testing

Have qualified personnel conduct slurry tests to determine density, viscosity, pH and sand content. The following tables show the acceptable range of values for the slurry properties:

<b>BENTONITE SLURRY</b> Sodium Montmorillonite (Commercial Bentonite) Acceptable Range of Values			
<b>Property (units)</b>	<b>At Time of Slurry Introduction</b>	<b>In Excavation Immediately Before Concrete Placement</b>	<b>Test Method</b>
Density, pcf (kg/m <sup>3</sup> )	64.3 – 69.1 (1030- 1107)	64.3 – 75.0 (1030- 1201)	Mud Weight (Density) API 13B-1 Section 1
Viscosity, sec./quart (sec./0.95 liters)	28 – 45	28 – 45	Marsh Funnel and Cup API 13B-1 Section 2.2
pH	8 – 11	8 – 11	pH Paper or Glass Electrode pH Meter
Sand Content (percent)	Less than or equal to 4	Less than or equal to 2	Sand API 13B-1 Section 5
<b>Notes:</b>			
1. Perform tests when the slurry temperature is above 40°F (4.4°C).			
2. Increase density by 2 pcf (32 kg/m <sup>3</sup> ) in saltwater.			

<b>SLURRYPRO CDP</b> KB Technologies Ltd. Acceptable Range of Values			
<b>Property (units)</b>	<b>At Time of Slurry Introduction</b>	<b>In Excavation Immediately Before Concrete Placement</b>	<b>Test Method</b>
Density, pcf (kg/m <sup>3</sup> )	Less than or equal to 67 (1073)	Less than or equal to 64 (1025)	Mud Weight (Density) API 13B-1 Section 1
Viscosity, sec./quart (sec./0.95 liters)	50 – 120	Less than or equal to 70	Marsh Funnel and Cup API 13B-1 Section 2.2
pH	6 – 11.5	6 – 11.5	pH Paper or Glass Electrode pH Meter
Sand Content (percent)	Less than or equal to 0.5	Less than or equal to 0.5	Sand API 13B-1 Section 5
Notes:			
1. Perform tests when the slurry temperature is above 40°F (4.4°C).			
2. Increase density by 2 pcf (32 kg/m <sup>3</sup> ) in saltwater.			

<b>SUPER MUD</b> PDS Company Acceptable Range of Values			
<b>Property (units)</b>	<b>At Time of Slurry Introduction</b>	<b>In Excavation Immediately Before Concrete Placement</b>	<b>Test Method</b>
Density, pcf (kg/m <sup>3</sup> )	Less than or equal to 64 (1025)	Less than or equal to 64 (1025)	Mud Weight (Density) API 13B-1 Section 1
Viscosity, sec./quart (sec./0.95 liters)	32 – 60	Less than or equal to 60	Marsh Funnel and Cup API 13B-1 Section 2.2
pH	8 – 10	8 – 10	pH Paper or Glass Electrode pH Meter
Sand Content (percent)	Less than or equal to 0.5	Less than or equal to 0.5	Sand API 13B-1 Section 5
Notes:			
1. Perform tests when the slurry temperature is above 40°F (4.4°C).			
2. Increase density by 2 pcf (32 kg/m <sup>3</sup> ) in saltwater.			

**SHORE PAC GCV**  
 CETCO Drilling Products Group  
 Acceptable Range of Values

<b>Property (units)</b>	<b>At Time of Slurry Introduction</b>	<b>In Excavation Immediately Before Concrete Placement</b>	<b>Test Method</b>
Density, pcf (kg/m <sup>3</sup> )	Less than or equal to 64 (1025)	Less than or equal to 64 (1025)	Mud Weight (Density) API 13B-1 Section 1
Viscosity, sec./quart (sec./0.95 liters)	33 – 74	Less than or equal to 57	Marsh Funnel and Cup API 13B-1 Section 2.2
pH	8 – 11	8 – 11	pH Paper or Glass Electrode pH Meter
Sand Content (percent)	Less than or equal to 0.5	Less than or equal to 0.5	Sand API 13B-1 Section 5

Notes:

1. Perform tests when the slurry temperature is above 40°F (4.4°C).
2. Increase density by 2 pcf (32 kg/m<sup>3</sup>) in saltwater.

<b>NOVAGEL POLYMER</b> Geo-Tech Drilling Fluids Acceptable Range of Values			
<b>Property (units)</b>	<b>At Time of Slurry Introduction</b>	<b>In Excavation Immediately Before Concrete Placement</b>	<b>Test Method</b>
Density, pcf (kg/m <sup>3</sup> )	Less than or equal to 67 (1073)	Less than or equal to 64 (1025)	Mud Weight (Density) API 13B-1 Section 1
Viscosity, sec./quart (sec./0.95 liters)	45 – 104	Less than or equal to 104	Marsh Funnel and Cup API 13B-1 Section 2.2
pH	6.5 – 11.5	6.5 – 11.5	pH Paper or Glass Electrode pH Meter
Sand Content (percent)	Less than or equal to 0.5	Less than or equal to 0.5	Sand API 13B-1 Section 5
Notes:			
1. Perform tests when the slurry temperature is above 40°F (4.4°C).			
2. Increase density by 2 pcf (32 kg/m <sup>3</sup> ) in saltwater.			

When any slurry samples are found to be unacceptable, take whatever action is necessary to bring the slurry within specification requirements. Do not place reinforcement steel until resampling and testing produce acceptable results.

Sign and submit reports of all slurry tests required above to the Engineer upon completion of each drilled pier. The Department reserves the right to perform comparison slurry tests at any time.

4. Slurry Disposal

Comply with all applicable local, state and federal regulations, as well as with the environmental permits of the project when disposing of excavated materials contaminated with slurry. Keep all excavated materials, spoils from the desanding unit and slurry out of the water and contain them at all times. The cost of the containment, removal and disposal of excavated materials contaminated with slurry, as well as the slurry itself, is incidental to the cost of the drilled piers.

### 3.0 CLEANING

Excavate the bottom of the drilled pier to a level plane or stepped with a maximum step height of 12 in (300 mm). Clean the bottom of the excavation of loose material using a technique accepted in the construction sequence plan. When the drilled pier excavation is not hand cleaned, clean the bottom of the excavation with a cleanout bucket and an airlift or submersible pump.

### 4.0 INSPECTION METHODS AND REQUIREMENTS

After the drilled pier excavation is complete and immediately before placing reinforcing steel and concrete, demonstrate the proper condition of the drilled pier excavation to the Engineer. Provide bosun chairs, gas meters, safety equipment, lights, mirrors, weighted tape measures, steel probes, personnel and all assistance required for the Engineer to inspect the drilled pier excavations.

#### A. Bearing Capacity

If the required end bearing capacity is not satisfied, increase the drilled pier length as directed by the Engineer. Payment for the additional drilled pier length to achieve adequate bearing will be made per the drilled pier pay items.

One or more of the following tests may be used to verify the conditions and continuity of the bearing material before placing reinforcing steel.

##### 1. Visual Inspection

The end bearing of the drilled pier excavation may be inspected either by entering the excavation or visually from the top of the excavation.

##### 2. Test Hole

If the tip of the drilled pier excavation is in rock as defined by Section 1.0, Item E, drill a 1-1/2 in (38 mm) diameter test hole in each drilled pier to a depth at least 6 ft (1.8 m) below the tip elevation.

##### 3. Standard Penetration Test (SPT)

When noted on the plans that a SPT is required, drive a split barrel sampler a minimum of 18 in (450 mm) below the drilled pier tip elevation or to refusal in accordance with ASTM D1586, "Penetration Test and Split-Barrel Sampling of Soils". Complete the SPT using NW rods through casing or another stabilizing method as approved by the Engineer. Extend the SPT rods from the top of the drilled pier excavation to the drilled pier tip elevation. Firmly support the SPT casing at the top of the drilled pier excavation and rest it on the bottom of the excavation. Conduct the SPT a minimum of 12 in (300 mm) away from the sidewalls of the excavation and be sure not to scrape the sidewalls of the excavation while inserting or withdrawing the SPT equipment. Have the SPT device on-site

before reaching the drilled pier tip elevation. Report the number of blows for each 6 in (150 mm) increment driven and a description of the recovered soil sample to the Engineer. The Engineer determines the number of blows required for bearing.

## B. Bottom Cleanliness

The pier excavation bottom is considered clean if a minimum of 50% of the bottom area has less than 1/2 in (13 mm) of sediment and no portion of the bottom area has more than 1-1/2 in (38 mm) of sediment as determined by the Engineer.

One or more of the following inspection procedures may be used to inspect the cleanliness of the pier excavation bottom before placing the reinforcing steel and concrete.

### 1. Visual Inspection

The cleanliness of the drilled pier excavation bottom may be observed either by entering the excavation or from the top of the excavation.

### 2. Steel Probe

If the excavation is not dewatered or if the Engineer requires it, lower a steel probe to the bottom of the drilled pier excavation to ensure that cleaning has been satisfactorily completed. Supply a steel probe that is 2 ft (0.6 m) long with a flat tip on the sounding end, weighs approximately 9 lbs, #10 rebar (4 kg, #32 rebar), and is suspended from the opposite end with a non-stretch cable.

### 3. Shaft Inspection Device (SID)

The Department may use the SID to take sediment measurements and observe the bottom conditions of the drilled pier excavation at a minimum of five locations selected by the Engineer. The SID is a remotely operated camera capable of observing bottom conditions and measuring sediment underwater and slurry. Each SID inspection (including all 5 locations) takes approximately 1 hour after the equipment has been set up. The Engineer provides the SID and the personnel to operate the device. Notify the Engineer a minimum of 2 days before beginning the drilled pier excavation so the Engineer can arrange for the transportation of the SID to the site and the personnel to perform the inspections. SID inspections are required until the cleanliness of the drilled pier excavation bottom is acceptable in accordance with Section 4.0, Item B of this provision. Do not conduct operations that interfere with the SID inspections. Remove all cleaning and drilling equipment from the drilled pier excavation during any SID inspection. Provide a working area large enough for the SID equipment and within reach of the cabling supplied and clear sight distance of the drilled pier excavation. Assist the Engineer in the transportation and handling of the SID and all the associated equipment and in supporting the electric hoist and/or hoisting tripod for the SID. If required, provide a safe and secure location to park the trailer for the SID while it is unattended on the project site. If any of the SID equipment is damaged due to the Contractor's



negligence, then replace the equipment at no additional cost to the Department. Provide replacement equipment that exactly matches the damaged equipment as directed by the Engineer. All costs involved with the initial SID inspection of each drilled pier excavation will be made per the SID pay item. No additional payment will be made for subsequent or repeated SID inspections of the same drilled pier excavation. No claims for either lost time or actual expense of any SID inspections that do not find the cleanliness of the drilled pier excavation bottom in compliance with this provision will be paid.

## 5.0 REINFORCING STEEL

Reinforcing steel shall conform to Section 1070 of the Standard Specifications. Completely assemble a cage of reinforcing steel, consisting of longitudinal and spiral bars and place it in the drilled pier excavation as a unit immediately after the proper condition of the excavation is demonstrated to the Engineer. When concrete placement does not follow immediately after cage placement, remove the steel from the pier excavation unless the Engineer directs otherwise. If the cage is removed, recheck pier excavation cleanliness in accordance with this provision before reinstalling the cage.

If the drilled pier excavation is cased down to rock, immediate placement of the reinforcing steel and the concrete is not required. If electing to delay placement of the reinforcing steel and concrete due to the presence of rock, recheck the excavation for proper cleanliness immediately before placing reinforcing steel.

### A. Construction, Placement, Support and Alignment

If it is determined in the field that the drilled pier must be longer, adequate reinforcement may be required in the extended length as directed by the Engineer. Lift the cage so racking and cage distortion does not occur. Keep the cage plumb during concrete operations and casing extraction. Check the position of the cage before and after placing the concrete. Position the splice length of the drilled pier cage so that the column or footing has the minimum concrete cover shown on the plans.

Securely cross-tie the vertical and spiral reinforcement at each intersection with double wire. Support or hold down the cage so that the vertical displacement during concrete placement and casing extraction does not exceed 6 in (150 mm).

### B. Bolsters and Spacers

Set the rebar cage directly on the bottom of the drilled pier excavation with plastic bolsters under each vertical reinforcing bar. Ensure that spacers are tall enough to raise the rebar cage off the bottom of the drilled pier excavation a minimum of 3 in (75 mm). If approved by the Engineer, the rebar cage may be hung in the excavation provided the mechanisms supporting the cage are left in place until the Drilled Pier Concrete strength has achieved 3000 psi (20.7 MPa).

In order to ensure the minimum required concrete cover and achieve concentric spacing of the cage within the pier, attach plastic spacer wheels at five points around the cage perimeter. Use spacer wheels that provide a minimum of 4 in (100 mm) "blocking" from the outside face of the spiral bars to the outermost surface of the drilled pier except in rock as defined by Section 1.0, Item E and when using slurry construction. Use spacer wheels for slurry construction or in rock that provide a minimum of 2 in (50 mm) "blocking". Tie spacer wheels that snap together with wire and allow them to rotate. Use spacer wheels that span at least two adjacent vertical bars. Start placing spacer wheels at the bottom of the cage and continue up along its length at maximum 10 ft (3 m) intervals.

## 6.0 CONCRETE

Drilled Pier Concrete shall conform to Section 1000 of the Standard Specifications. Begin concrete placement immediately after inserting reinforcing steel into the drilled pier excavation.

### A. Concrete Mix

As an option, use Type IP blended cement with a minimum cement content of 665 lbs/yd<sup>3</sup> (395 kg/m<sup>3</sup>) and a maximum cement content of 833 lbs/yd<sup>3</sup> (494 kg/m<sup>3</sup>). Use No. 78M coarse aggregate in the mix.

Use an approved water-reducer, water-reducing retarder, high-range water-reducer or high-range water-reducing retarder to facilitate placement of the concrete if necessary. Do not use a stabilizing admixture as a retarder in Drilled Pier Concrete without approval of the Engineer. Use admixtures that satisfy AASHTO M194 and add them at the concrete plant when the mixing water is introduced into the concrete. Redosing of admixtures is not permitted.

### B. Concrete Placement

Place concrete such that the drilled pier is a monolithic structure. Vibration is only permitted, if needed, in the top 10 ft (3 m) of the drilled pier. Remove any contaminated concrete from the top of the drilled pier at the time of concrete placement. Contain and remove all wasted concrete that spills over the casing.

Do not twist, move or otherwise disturb temporary casings until the concrete depth in the casing is in excess of 10 ft (3 m) above the bottom of the casing being disturbed. If the head is greater than 30 ft (9 m), the Engineer may require a concrete depth greater than 10 ft (3 m). The head is defined as the difference between the highest piezometric head along the depth of the pier and the static water elevation inside the excavation. If the Engineer requires a concrete depth greater than 10 ft (3 m), the Drilled Pier Contractor may choose to either place concrete with this required concrete depth or place concrete with the wet method and a minimum concrete depth of 10 ft (3 m).

Maintain the required concrete depth above the bottom of the innermost casing during casing removal, except when the concrete level is at or above the top of drilled pier elevation. Sustain a sufficient concrete depth above the bottom of casing to overcome outside soil and water pressure. As the casing is withdrawn, exercise care in maintaining an adequate concrete depth within the casing so that fluid trapped behind the casing is displaced upward and discharged at the ground surface without contaminating or displacing the Drilled Pier Concrete. Exerting downward pressure, hammering and/or vibrating the temporary casing is permitted to facilitate removal.

Use the water inflow rate to determine the concrete placement procedure after any pumps have been removed from the excavation. If the inflow rate is less than 6 in (150 mm) per half hour, the concrete placement is considered dry. If the water inflow rate is greater than 6 in (150 mm) per half hour, the concrete placement is considered wet.

Keep a record of the volume of concrete placed in each drilled pier excavation and make it available to the Engineer. For drilled piers constructed with slurry or as directed by the Engineer, record a graphical plot of the depth versus theoretical concrete volume and actual measured concrete volume for each drilled pier and provide it to the Engineer when finished placing concrete.

#### 1. Dry Placement

Before concrete placement, make certain the drilled pier excavation is dry so the flow of concrete around the reinforcing steel can be verified by visual inspection. If the concrete free fall does not exceed 60 ft (18.3 m), placing the concrete by a central drop method where the concrete is chuted directly down the center of the excavation is permitted.

For drilled piers exceeding 60 ft (18.3 m) in length, use a tremie or a pump to place concrete unless otherwise approved by the Engineer. Support the tremie or pump pipe so that the concrete free fall is less than 60 ft (18.3 m) at all times.

#### 2. Wet Placement

Maintain a static water or slurry level in the excavation before placing concrete underwater. When temporary casing is used as the method to stabilize the excavation, place concrete with a sectional tremie or pump (no continuous tremie).

Once the concrete in the excavation reaches the same elevation as the static water level, placing concrete with the dry method is permitted. Before changing to the dry method of concrete placement, remove the water above the concrete and clean the concrete surface of all scum and sediment to expose clean, uncontaminated concrete.

### 3. Pump and Tremie

Pump concrete in accordance with Article 420-5 of the Standard Specifications. Use a steel tremie with watertight joints and a minimum diameter of 10 in (250 mm). Use a discharge control to prevent concrete contamination when the tremie tube or pump pipe is initially placed in the excavation. Extend the tremie tube or pump pipe into the concrete a minimum of 5 ft (1.5 m) at all times except when the concrete is initially introduced into the pier excavation. If the tremie tube or pump pipe pulls out of the concrete for any reason after the initial concrete is placed, restart concrete placement with a steel capped tremie tube or pump pipe.

### 4. Placement Time

Place concrete within the time frames specified in Table 1000-2 of the Standard Specifications for Class AA concrete. Do not place concrete so fast as to trap air, slurry, water, fluids, soil or any other deleterious materials in the vicinity of the reinforcing steel and the annular zone between the rebar cage and the excavation walls.

## 7.0 SCHEDULING AND RESTRICTIONS

If caving or sloughing occurs, compensation will not be provided for additional concrete to fill the resulting voids.

For the first 16 hours after a drilled pier has achieved its initial concrete set (as determined by the Engineer), do not drill adjacent piers, do not install adjacent piles, and do not allow any equipment wheel loads or "excessive" vibrations within 20 ft (6 m) of the drilled pier.

In the event that the procedures described herein are performed unsatisfactorily, the Engineer may suspend drilled pier construction in accordance with Article 108-7 of the Standard Specifications. If the integrity of the drilled pier is in question, the Engineer reserves the right to reject the drilled piers and require remediation. Remedial measures are proposed by the Contractor and require approval of the Engineer. No compensation will be paid for losses or damage due to remedial work or any investigation of drilled piers found defective or not in accordance with this provision or the plans.

## 8.0 MEASUREMENT AND PAYMENT

### A. Method of Measurement

#### 1. Drilled Piers in Soil

The quantity of "Drilled Piers in Soil" to be paid for will be the linear feet (meters) of the drilled piers exclusive of the linear feet (meters) of "Drilled Piers Not in Soil" computed from elevations and dimensions as shown on the plans or from revised dimensions authorized by the Engineer. Drilled piers through air or water will be paid for as "Drilled Piers in Soil".

## 2. Drilled Piers Not in Soil

The quantity of "Drilled Piers Not in Soil" to be paid for will be the linear feet (meters) of drilled piers excavated in non-soil as determined by the Engineer. Non-soil is defined as material that can not be cut with a rock auger and requires excavation by coring, air tools, hand removal or other acceptable methods. Top of non-soil elevation is that elevation where the rock auger penetration rate is less than 2 in (50 mm) per 5 minutes of drilling at full crowd force and coring, air tools, etc. are used to advance the excavation. For pay purposes, after non-soil is encountered, earth seams, rock fragments and voids in the excavation less than 3 ft (0.9 m) in total length will be considered "Drilled Piers Not in Soil". If the non-soil is discontinuous, payment will revert to "Drilled Piers in Soil" at the elevation where non-soil is no longer encountered.

## 3. Permanent Steel Casing

The quantity of "Permanent Steel Casing" to be paid for will be the linear feet (meters) of permanent steel casing as directed or required to be used. The length to be paid for will be measured along the permanent casing from the top of the casing elevation or top of the pier elevation, whichever is lower, to the permanent casing tip elevation. The Department will also pay for up to an additional 3 ft (1 m) of permanent casing cut off if the casing can not be installed to the permanent casing tip elevation shown on the plans. Permanent casing will be paid for only when permanent casing is authorized or when the Engineer directs the Contractor to leave a casing in place such that it becomes a permanent part of the pier. No payment will be made for temporary steel casings that become bound or fouled during pier construction and cannot be practically removed.

## 4. Shaft Inspection Device (SID)

The quantity of "SID Inspection" to be paid for will be per drilled pier as noted on the plans and/or directed by the Engineer. SID inspections are performed until the bottom cleanliness of the drilled pier excavation is acceptable by this provision; however, payment will only be made for the initial SID inspection of each drilled pier excavation.

## 5. Standard Penetration Test (SPT)

The quantity of "SPT Testing" to be paid for will be the actual number of SPT tests performed as noted on the plans and/or directed by the Engineer.

# B. Basis of Payment

## 1. Drilled Piers in Soil

Payment will be made at the contract unit price per linear foot (meter) for "\_\_\_\_ Dia. Drilled Piers in Soil". Such payment will include, but is not limited to, furnishing all labor, tools, equipment, materials including concrete complete and in

place and all incidentals necessary to excavate the drilled piers and complete the work as described in this provision. No additional payment will be made for slurry use. No additional payment will be made for any miscellaneous grading or excavation to install the drilled pier. "Reinforcing Steel" and "Spiral Column Reinforcing Steel" will be paid for separately and will not be part of the unit bid price for "Drilled Piers in Soil".

2. Drilled Piers Not in Soil

Payment will be made at the contract unit price per linear foot (meter) for "\_\_\_\_ Dia. Drilled Piers Not in Soil". Such payment will include, but is not limited to, furnishing all labor, tools, equipment, materials including concrete complete and in place and all incidentals necessary to excavate the drilled piers and complete the work as described in this provision. No additional payment will be made for slurry use. No additional payment will be made for any miscellaneous grading or excavation to install the drilled pier. "Reinforcing Steel" and "Spiral Column Reinforcing Steel" will be paid for separately and will not be part of the unit bid price for "Drilled Piers Not in Soil".

3. Permanent Steel Casing

Payment will be made at the contract unit price per linear foot (meter) for "Permanent Steel Casing for \_\_\_\_ Dia. Drilled Pier". Such payment will include, but is not limited to, furnishing all material, labor, tools, equipment and all incidentals necessary to install the casing in the pier excavation.

4. Shaft Inspection Device (SID)

Payment for SID will be at the contract unit price per each for "SID Inspection". Such payment will include, but is not limited to, furnishing all materials, labor, tools, equipment and all incidentals necessary to complete the SID inspection as described in this provision.

5. Standard Penetration Test (SPT)

Payment for SPT will be at the contract unit price per each for "SPT Testing". Such payment will include, but is not limited to, furnishing all materials, labor, tools, equipment and all incidentals necessary to complete the SPT at each test location.

**CROSSHOLE SONIC LOGGING**

(11-17-06)

**1.0 GENERAL**

Use the non-destructive testing method crosshole sonic logging (CSL) to verify the integrity of the drilled pier and quality of concrete. The CSL test method is described in ASTM D6760, "Integrity Testing of Concrete Deep Foundations by Ultrasonic Crosshole

Testing”. The Engineer will determine the number of CSL tests and which drilled piers will be CSL tested. Drilled piers are referred to as piers in this special provision.

The CSL test measures the time for an ultrasonic pulse to travel from a signal source in one tube to a receiver in another tube. In uniform, good quality concrete, the travel time between equidistant tubes should yield relatively consistent arrival times and correspond to a reasonable pulse velocity, signal amplitude and energy from the bottom to the top of the pier. Longer travel times, decrease in pulse velocity and lower amplitude/energy signals indicate the presence of irregularities such as poor quality concrete, voids, honeycombing, cracking and soil intrusions. The signal may be completely lost by the receiver and CSL recording system for severe defects such as voids.

Retain a CSL Consultant to perform CSL testing on the selected drilled piers. The CSL Consultant shall supply the Contractor with technical assistance and guidance during preparation and testing. Provide suitable access to the site and to the top of piers to be tested. Follow instructions from the CSL Consultant unless directed otherwise by the Engineer.

Place CSL tubes in all drilled piers. Perform CSL testing after concrete achieves a minimum compressive strength of 3000 psi (20.7 MPa) and within 7 to 30 days after concrete placement. After CSL test results have been reviewed and the Engineer has accepted the drilled pier, dewater the tubes and core holes, if any, and fill with an approved grout. If the Engineer elects not to CSL test a pier, obtain approval from the Engineer to dewater the tubes and fill them with an approved grout. Provide, mix and place grout in accordance with the Grout for Structures Special Provision.

## **2.0 PREQUALIFICATION AND EXPERIENCE REQUIREMENTS**

Use a CSL Consultant prequalified by the Contractual Services Unit of the Department for Non-Destructive Foundation Testing work (work code 3070).

Submit documentation that the CSL Consultant has successfully completed at least 5 CSL testing projects within the last 3 years of a scope and complexity similar to that anticipated for this project. Documentation should include the General Contractor and Owner’s name and current contact information with descriptions of each past project.

Provide the name of the Project Engineer that will be assigned to this project. Submit documentation for the Project Engineer verifying employment with the CSL Consultant, registration as professional engineer in North Carolina and a minimum of 5 years experience in CSL testing with past projects of scope and complexity similar to that anticipated for this project. Documentation should include resumes, references, certifications, project lists, experience descriptions and details, etc.

### 3.0 PREPARATION FOR CSL

Submit grout mix design or packaged grout type, CSL Consultant experience documentation, CSL tube size and Type 7 Contractor's Certification, cap details, couplings or joint details and the method for attaching the tubes. Provide this information with the drilled pier construction sequence plan.

Install 4 tubes in each drilled pier with a diameter of 5 ft (1524 mm) or less and 6 tubes in each pier with a diameter of greater than 5 ft (1524 mm). Provide 2 in (50 mm) inside diameter Schedule 40 steel pipe conforming to ASTM A53, Grade A or B, Type E, F or S. The tubes shall have a round, regular internal diameter free of defects or obstructions, including any at tube joints, in order to permit the free, unobstructed passage of source and receiver probes. The tubes shall provide a good bond with the concrete and be watertight.

Fit the tubes with a watertight threaded cap on the bottom and a removable threaded cap on the top. Securely attach the tubes to the interior of the reinforcing cage. Install the tubes in each drilled pier in a regular, symmetric pattern such that each tube is equally spaced from the others around the perimeter of the cage. Place tubes such that large vertical reinforcing bars do not block the direct line between adjacent tubes. The tubes are typically wire-tied to the reinforcing cage every 3 ft (1 m) or otherwise secured such that the tubes remain in position during placement of the cage and concrete. Install tubes as near to vertical and as parallel as possible, **as non-vertical tubes can adversely affect data analysis**. Extend the tubes from 6 in (150 mm) above the pier tip to at least 3 ft (1 m) above the top of the pier. If the pier top elevation is below ground elevation, extend tubes at least 2 ft (610 mm) above ground surface. If the drilled pier tip elevation is excavated more than 1 ft (305 mm) below the tip elevation shown on the plans, extend the tubes using proper threaded mechanical couplings to within 6 in (150 mm) of the revised pier tip elevation.

Before placing the reinforcing cage, record the tube lengths and positions along the length of the cage. After concrete placement, measure the stickup of the tubes above the top of the drilled piers and verify tube spacing.

After placing reinforcement and before placing concrete, fill the CSL tubes with clean water and cap them to keep out debris. CSL tubes that are not filled with water and capped will be rejected. When removing the caps, use care not to apply excess torque, force or stress, which could break the bond between the tubes and the concrete.

### 4.0 CSL EQUIPMENT

The minimum requirements of the CSL equipment are as follows:

- A microprocessor based CSL system for display of individual CSL records, analog-digital conversion and recording of CSL data, analysis of receiver responses and printing of report quality CSL logs
- Ultrasonic source and receiver probes which can travel through 2 in (50 mm) I.D. steel pipe



- An ultrasonic voltage pulser to excite the source with a synchronized triggering system to start the recording system
- A depth measurement device to electronically measure and record the source and receiver depths associated with each CSL signal
- Appropriate filter/amplification and cable systems for CSL testing
- An acquisition system that stores each log in digital format, with drilled pier identification, date, time and test details, including the source and receiver gain and displays arrival time data graphically during data acquisition
- 3D tomographic imaging software or source for completing the work

## **5.0 CSL TEST PROCEDURE**

Perform CSL testing between each adjacent perimeter CSL tube pair and opposite tube pairs along the cross section diameter. Maintain the source and receiver probes in the same horizontal plane unless test results indicate defects or poor concrete zones, in which case, further evaluate the defect zones with angle tests (source and receiver vertically offset at greater than 1.5 ft (460 mm) in the tubes). Report any defects indicated by decreased signal velocity and lower amplitude/energy signals at the time of testing and conduct angle tests in the zones of the defects as defined by the Concrete Condition Rating Criteria (CCRC) in Section 6.0 of this provision. Make CSL measurements at depth intervals of 2.5 in (65 mm) or less from the bottom of the tubes to the top of each pier. Pull the probes simultaneously, starting from the bottom of the tubes, using a depth-measuring device to electronically measure and record the depths associated with each CSL signal. Remove any slack from the cables before pulling to provide for accurate depth measurements of the CSL records. In the event defects are detected, conduct additional logs at no additional cost to the Department.

If CSL probes will not pass through the entire length of the CSL tubes, core a 2 in (50 mm) diameter hole through the concrete the full length of the drilled pier for each inaccessible tube. If the CSL tubes debond from the concrete, core a 2 in (50 mm) diameter hole through the concrete to the depth of the debonding for each debonded tube. Locate core holes approximately 9 in (230 mm) inside the reinforcement as directed by the Engineer. Fill core holes with clean water and cover to keep out debris. No additional payment will be made for coring due to inaccessible or debonded tubes.

## **6.0 CSL RESULTS AND REPORTING**

Submit two hard copies and an electronic copy (pdf or jpeg format on CD or DVD) of a CSL report sealed by the Project Engineer within 5 calendar days after field testing is complete. The CSL report should include but not limited to the following:

C. Title Sheet

- NCDOT TIP number and WBS element number
- Project description
- County
- Bridge station number
- Pier location
- Personnel
- Report date

D. Introduction

E. Site and Subsurface Conditions (including water table elevation)

F. Pier Details

- Pier and casing diameters, lengths and elevations
- Concrete compressive strength
- Installation methods and details including use of casing, slurry, pump, tremie, dry or wet placement of concrete, etc.

G. CSL Logs

H. Results/Conclusions

I. Attachments

- Boring log(s)
- Field Drilled Pier Inspection Forms, Drilling Logs, SID Inspection Forms and Concrete Curves (from Engineer)
- CSL tube locations, elevations, lengths and identifications
- CSL hardware model
- Electronic copy of all CSL raw data

Include CSL logs for each tube pair tested with analysis of the initial pulse arrival time, velocity, relative pulse energy/amplitude and stacked waveform plotted versus depth. List all zones defined by the CCRC in a tabular format including the percent velocity reduction and the velocity values used from the nearby zone of good quality concrete. Discuss each zone defined by the CCRC in the CSL report as appropriate. Base the results on the percent reduction in velocity value from a nearby zone of good quality concrete with good signal amplitude and energy as correlated to the following:

<b>Concrete Condition Rating Criteria (CCRC)</b>			
<b>CCRC</b>	<b>Rating Symbol</b>	<b>Velocity Reduction</b>	<b>Indicative Results</b>
Good	G	≤ 10 %	Good quality concrete
Questionable Defect	Q	>10 % & < 20 %	Minor concrete contamination or intrusion and questionable quality concrete
Poor	P/D	≥ 20 %	Defects exist, possible water/slurry contamination, soil intrusion and/or poor quality concrete
No Signal	NS	No Signal received	Soil intrusion or other severe defect absorbed the signal (assumes good bond of the tube-concrete interface)
Water	W	V = 4750 fps (1450 mps) to 5000 fps (1525 mps)	Water intrusion or water filled gravel intrusion with few or no fines present

The following are a few examples of types and causes of defects:

- Necking or arching of the concrete on withdrawal of the temporary casing.
- Necking or contamination of the concrete due to collapse of the side walls.
- Soft toe due to incomplete cleaning or collapse of the side walls.
- Horizontal lenses of silt\mud\slurry due to the tremie pipe rising above the concrete.
- Voids due to the use of low-slump concrete.
- Honeycombing due to washout of fines.
- Trapping of contaminants due to pumping concrete to fast.

The Engineer will require 5 working days to evaluate the CSL test results and determine whether or not the drilled pier is acceptable. Evaluation of CSL test results, with ratings other than good (G) per the CCRC may require further investigation and additional time for review and analysis of the data. Do not grout the CSL tubes or perform any further work on the CSL tested drilled pier until the Engineer determines whether the drilled pier is acceptable.

Perform tomography in order to further investigate and delineate the boundaries of any defective/unconsolidated zones with 20% or more reduction in velocity value as correlated to the CCRC. Process CSL data to construct easy to understand 2D/3D (2D cross-sections between tubes and 3D volumetric images for the entire pier) *color-coded* tomographic images indicating velocity variations along the pier. Identify the location and geometry of defective/unconsolidated zones in 3D color images with detailed discussion in the CSL report. Any further tests deemed necessary by the Engineer in order to determine the acceptability of the drilled pier will be determined after reviewing the CSL report. Additional test or analysis options include 3D tomographic imaging, single-hole sonic testing, sonic echo or impact response tests and concrete coring.

The Engineer determines the depth, location, diameter (PQ or NQ size) and number of core holes when concrete coring is required. If the Engineer is concerned about concrete strength or requires the use of a borehole camera for inspection, large diameter cores (PQ size) are required. Drill a minimum of 2 core holes to intercept the suspected defect zones. Use a coring method that provides maximum core recovery and minimizes abrasion and erosion. Provide concrete cores properly marked in a wooden crate labeled with the drilled pier depth at each interval of core recovery to the NCDOT Materials and Test Unit for evaluation and testing. Submit coring records, signed by the Contractor that include NCDOT project number, name of the Drilling Contractor, date cored and percent core recovery. Allow 5 working days after submitting the core records for the Department's review.

#### **7.0 CORRECTION OF UNACCEPTABLE DRILLED PIER**

When the Engineer determines a drilled pier is unacceptable, submit remedial measures to the Department for approval. No compensation will be made for remedial work or losses or damage due to remedial work of drilled piers found defective or not in accordance with the Drilled Piers Special Provision or the plans. Modifications to the drilled pier design or any load transfer mechanisms required by the remedial action shall be designed by a Registered North Carolina Professional Engineer. Include supporting calculations and drawings sealed by a Registered North Carolina Professional Engineer for all foundation elements affected. Do not begin remedial action work until the Department has reviewed and accepted the remedial action plan. Allow 5 working days after submitting the remedial work plan for the Department's review and acceptance. Furnish all materials and work necessary to correct defective drilled piers.

## 8.0 MEASUREMENT AND PAYMENT

The complete and accepted CSL will be paid for at the unit bid price for “Crosshole Sonic Logging” per each. The Department will only pay for the initial CSL test on a drilled pier; no additional payment will be made for subsequent CSL tests performed on the same drilled pier. Include in this unit bid price all costs incurred for procurements, conducting the CSL testing, reporting of results and incidentals necessary to complete the work including any other test required to determine the acceptability of the drilled pier.

Include the cost of the crosshole sonic logging tubes in the unit bid price for drilled piers. No separate payment will be made for the CSL tubes. The unit bid price for the drilled piers will include full compensation for furnishing, installing, extending tubes, dewatering and grouting of all CSL tubes and core holes, if applicable, and all materials, labor, tools, equipment and incidentals necessary to complete the work.

## POT BEARINGS

(6-07-05)

### 1.0 GENERAL

This item consists of furnishing, fabrication and installation of pot bearings in accordance with AASHTO Standard Specifications, the Standard Specifications, the recommendations of the manufacturer and the details shown on the plans and as specified herein.

Fixed pot bearings consist of a sole plate, a disc of elastomer in a steel cylinder with a snug fitting steel piston, masonry plate, anchor bolts, nuts and washers. Expansion pot bearings consist of a sole plate, a top steel plate with a polished stainless steel sheet facing bearing on a fixed pot bearing with a layer of virgin polytetrafluoroethylene (PTFE) material on its top, masonry plate, anchor bolt assembly which includes anchor bolts, nuts, washers, pipe sleeves, a closure plate, grout and various sizes of standard pipe and any other necessary material as detailed on the plans.

### 2.0 MATERIALS

Use pot bearings produced by the same manufacturer.

Use AASHTO M270 Grade 50W (345W) for all steel in the pot bearings. Clean, coat, and seal the plates in the pot bearing assemblies except for the areas with special facings and the internal surfaces of pot, in accordance with the Special Provision for “Thermal Sprayed Coatings (Metallization)”. Metallization of the internal surfaces of the pot is permitted provided these surfaces are then polished to a surface smoother than 63 micro inches (0.0016 mm) root mean square. Coat surfaces to a thickness of 8 mils (0.200 mm) minimum on all external parts. Repair surfaces that are abraded or damaged after the application of metallizing in accordance with the Special Provision for “Thermal Sprayed Coatings (Metallization)”.

Galvanize all fill plates specified on the plans. Provide anchor bolts and nuts in accordance with the Standard Specifications.

When the maximum plan dimension of the sheet is 12" (300 mm) or less, provide a stainless steel sheet in expansion pot bearings that is at least 16 gage or 1/16" (1.6 mm). When the maximum plan dimension is greater than 12" (300 mm), provide a stainless steel sheet that is at least 11 gage or 1/8" (3 mm). Ensure that all stainless steel sheets are in conformance with ASTM A240/A167 Type 304 and polished to a minimum #8 mirror surface finish.

Blast clean the surface of the plate that will be attached to the stainless sheet to a near white condition in accordance with the Standard Specifications. Position and clamp the back of the stainless sheet that is to be in contact with the steel plate on the steel plate. Apply the stainless steel to the blast cleaned surface of the steel plate as soon as possible after blasting and before any visible oxidation of the blast cleaned surface occurs. Weld the stainless sheet continuously around its perimeter using a tungsten inert gas, wire-fed welder.

For the PTFE sheet, used as a mating surface for the stainless sheet, provide an unfilled virgin PTFE Sheet (Recessed) or a glass-fiber filled PTFE sheet, resulting from skiving billets formed under hydraulic pressure and heat. Provide resin that conforms to the requirements of ASTM D4894 or D4895.

To bond the PTFE and the piston, use heat cured high temperature epoxy capable of withstanding temperature of -320°F to 500°F (-195°C to 260°C).

Provide a neoprene or natural rubber elastomer with a durometer hardness of 50 that allows for a minimum rotation of 0.02 radians. Place a 1/64" (0.4 mm) thick unfilled PTFE disc on either side of the elastomer inside the bearing. Use a brass sealing ring with the neoprene or natural rubber elastomer.

### 3.0 DESIGN

Have the manufacturer design the pot bearings for the loads and movements shown on the contract plans. However, use the anchor bolt size, length, spacing and masonry plate thickness as shown on the contract plans and provide an overall height of the bearing assembly that is at least the height shown on the contract plans, but no more than 1/2 inch (13 mm) greater than this height. Either combine, cast as a single piece, or weld together the sole plate and top plate/piston and the cylinder with the masonry plate.

When designing the bearings, use the following allowable bearing stresses:

- On confined elastomer: 3500 psi (24.1 MPa)
- On PTFE Sliding Surface, filled or unfilled PTFE (recessed): 3500 psi (24.1 MPa)

Submit eight sets of shop drawings and one set of design calculations for review, comments and acceptance. Have a North Carolina Registered Professional Engineer check and seal the shop drawings and design calculations.

After the Engineer reviews the drawings and, if necessary, corrections are made, submit one 22" x 34" reproducible set of the working drawings.

#### 4.0 SAMPLING AND TESTING

##### A. Sampling

The manufacturer is responsible for randomly selecting and testing sample bearings from completed lots of bearings. The manufacturer is also responsible for certifying that the completed bearings and their components have been tested and are in compliance with the requirements of this Special Provision. Have the manufacturer furnish the results of the tests to the Materials and Tests Engineer.

##### B. Testing

###### 1. Proof Load Test

Load a test bearing to 150% of the bearing's rated design capacity and simultaneously subject it to a rotational range of 0.02 radians (1.146°) for a period of 1 hour.

Have the bearing visually examined both during the test and upon disassembly after the test. Any resultant visual defects, such as extruded or deformed elastomer or PTFE, damaged seals or rings, or cracked steel is cause for rejection.

Keep the steel bearing plate and steel piston in continuous and uniform contact for the duration of the test. Any observed lift-off is cause for rejection.

###### 2. Sliding Coefficient of Friction

For all guided and non-guided expansion type bearings, measure the sliding coefficient of friction at the bearing's design capacity in accordance with the test method described below, and on the fifth and fiftieth cycles, at a sliding speed of 1 in/min (25 mm/min).

Calculate the sliding coefficient of friction as the horizontal load required to maintain continuous sliding of one bearing, divided by the bearing's vertical design capacity.

The test results are evaluated as follows:

- A maximum measured sliding coefficients of friction of 3%.
- A visual examination both during and after the test. Any resultant visual defects, such as bond failure, physical destruction, cold flow of PTFE to the point of debonding, or damaged components is cause for rejection of the lot.

Using undamaged test bearings in the work is permitted.

### 3. Test Method

For the test method and equipment, meet the following requirements:

- a. Arrange the test to determine the coefficient of friction on the first movement of the manufactured bearing.
- b. Clean the bearing surface prior to testing.
- c. Conduct the test at maximum working stress for the PTFE surface with the test load applied continuously for 12 hours prior to measuring friction.
- d. Determine the first movement static and dynamic coefficient of friction of the test bearing at a sliding speed of less than 1 in/min (25 mm/min), not to exceed:
 

0.04	unfilled PTFE
0.08	filled PTFE
- e. Subject the bearing specimen to 100 movements of at least 1 inch (25 mm) of relative movement and, if the test facility permits, the full design movement at a speed of less than 1 ft/min (300 mm/min). Following this test determine the static and kinetic coefficient of friction again. The specimen is considered a failure if it exceeds the values measured in (d) above or if it shows any signs of bond failure or other defects.

Bearings represented by test specimens passing the above requirements are approved for use in the structure subject to on-site inspection for visible defects.

## 5.0 INSTALLATION

Prior to shipment, seal the joint between the steel piston and the steel cylinder with a bead of caulk. Store pot bearings delivered to the bridge site under cover on a platform above the ground surface. Protect the bearings from injury at all times and, before placing the bearings, dry and clean all dirt, oil, grease or other foreign substances from the bearing. Do not disassemble the bearings during installation, except at the manufacturer's direction. Place the bearings in accordance with the recommendations of the manufacturer, Contract Drawings, and as directed by the Engineer. If there is any discrepancy between the recommendations of the manufacturer, Special Provisions, and Contract Drawings, the Engineer is the sole judge in reconciling any such discrepancy.

Provide preformed bearing pads under the masonry plates in accordance with Article 1079-1 of the Standard Specifications.

Do not install any bearing before the Engineer approves it.



## 6.0 BASIS OF PAYMENT

Payment will be at the lump sum contract price bid for "Pot Bearings" which price will be full compensation for furnishing all labor, materials, tools, equipment and incidentals required to complete the work in accordance with the Standard Specifications, this Special Provision, the manufacturer's requirements and as directed by the Engineer.

## THERMAL SPRAYED COATINGS (METALLIZATION)

(6-07-05)

### 1.0 DESCRIPTION

Apply a thermal sprayed coating (TSC) and sealer to metal surfaces as specified herein when called for on the plans or by other Special Provisions, or when otherwise approved by the Engineer in accordance with the SSPC-CS 23.00/AWS C2.23/NACE No. 12 Specification. Only Arc Sprayed application methods are used to apply TSC coatings, the Engineer must approve other methods of application.

### 2.0 QUALIFICATIONS

Only use NCDOT approved TSC Contractors meeting the following requirements:

1. Who have the capability of blast cleaning steel surfaces to SSPC SP-5 and SP-10 Finishes.
2. Who employ a Spray Operator(s) qualified in accordance with AWS C.16/C2.16M2002 and a Quality Control Inspector(s) who have documented training in the applicable test procedures of ASTM D-3276 and SSPC-CS 23.00.

A summary of the contractor's related work experience and the documents verifying each Spray Operator's and Quality Control Inspector's qualifications are submitted to the Engineer before any work is performed.

### 3.0 MATERIALS

Provide wire in accordance with the metallizing equipment manufacturer's recommendations. Use the wire alloy specified on the plans which meets the requirements in Annex C of the SSPC-CS 23.00 Specification. Have the contractor provide a certified analysis (NCDOT Type 2 Certification) for each lot of wire material.

Apply an approved sealer to all metallized surfaces in accordance with Section 9 of SSPC-CS 23. The sealer must either meet SSPC Paint 27 or is an alternate approved by the Engineer.

**4.0 SURFACE PREPARATION AND TSC APPLICATION**

Grind flame cut edges to remove the carbonized surface prior to blasting. Bevel all flame cut edges in accordance with Article 442-10(D) regardless of included angle. Blast clean surfaces to be metallized with grit or mineral abrasive in accordance with Steel Structures Painting Council SSPC SP-5/10(as specified) to impart an angular surface profile of 2.5 - 4.0 mils (0.063 – 0.100 mm). Surface preparation hold times are in accordance with Section 7.32 of SSPC-CS 23. If flash rusting occurs prior to metallizing, blast clean the metal surface again. Apply the thermal sprayed coating only when the surface temperature of the steel is at least 5°F (3°C) above the dew point.

At the beginning of each work period or shift, conduct bend tests in accordance with Section 6.5 of SSPC-CS 23.00. Any disbonding or delamination of the coating that exposes the substrate requires corrective action, additional testing, and the Engineer’s approval before resuming the metallizing process.

Apply TSC with the alloy to the thickness specified on the plans or as provided in the table below. All spot results (the average of 3 to 5 readings) must meet the minimum requirement. No additional tolerance (as allowed by SSPC PA-2) is permitted. (For Steel Beams: For pieces with less than 200 ft<sup>2</sup> (18.6m<sup>2</sup>) measure 2 spots/surface per piece and for pieces greater than 200 ft<sup>2</sup> (18.6m<sup>2</sup>) add 1 additional spots/surface for each 500 ft<sup>2</sup> (46.5m<sup>2</sup>)).

<b>Application</b>	<b>Thickness</b>	<b>Alloy</b>	<b>Seal Coat</b>
Pot Bearings	8 mil	85/15 Zinc (W-Zn-Al-2)	0.5 mil
Armored Joint Angles	8 mil	85/15 Zinc (W-Zn-Al-2)	0.5 mil
Modular Joints	8 mil	99.99% Zn (W-Zn-1)	0.5 mil
Expansion Joint Seals	8 mil	99.99% Zn (W-Zn-1)	0.5 mil
Optional Disc Bearings	8 mil	85/15 Zinc (W-Zn-Al-2)	0.5 mil

When noted on the plans or as specified in the above chart, apply the sealer to all metallized surfaces in accordance with the manufacturer’s recommendations and these provisions. Apply the seal coat only when the air temperature is above 40°F (4°C) and the surface temperature of the steel is at least 5°F (3°C) above the dew point. If the sealer is not applied within eight hours after the final application of TSC, the applicator verifies acceptable TSC surfaces and obtains approval from the Engineer before applying the sealer.

## 5.0 INSPECTION FREQUENCY

The TSC Contractor must conduct the following tests at the specified frequency and the results documented in a format approved by the Engineer.

Test/Standard	Location	Frequency	Specification
Ambient Conditions	Site	Each Process	5°F (3°C) above the dew point
Abrasive Properties	Site	Each Day	Size, angularity, cleanliness
Surface Cleanliness SSPC Vis 1	All Surfaces	Visual All Surfaces	SSPC-SP-10 Atmospheric Service SSPC-SP - 5 Immersion Service
Surface Profile ASTM D-4417 Method C	Random Surfaces	3 per 500 ft <sup>2</sup>	2.5 - 4.0 mils
Bend Test SSPC-CS 23.00	Site	5 per shift	Pass Visual
Thickness SSPC PA-2R SSPC-CS 23.00	Each Surface	Use the method in PA-2 Appendix 3 for Girders and Appendix 4 for frames and miscellaneous steel. See Note 1.	Zn - 8 mils minimum Al - 8 mils minimum Zn Al - 8 mils minimum  Areas with more than twice the minimum thickness are inspected for compliance to the adhesion and cut testing requirements of this specification.
Adhesion ASTM 4541	Random Surfaces Splice Areas	1 set of 3 per 500 ft <sup>2</sup>	Zn > 500 psi Al > 1000 psi Zn Al > 750 psi
Cut Test - SSPC-CS 23.00	Random Surfaces	3 sets of 3 per 500 ft <sup>2</sup>	No peeling or delamination
Job Reference Std. SSPC-CS 23.00	Site	1 per job	Meets all the above requirements

## 6.0 REPAIRS

All Repairs are to be performed in accordance with the procedures below, depending on whether the repair surface is hidden or exposed. As an exception to the following, field welded splices on joint angles and field welding bearing plates to girders may be repaired in accordance with the procedures for hidden surfaces.

**For hidden surfaces (including but not limited to interior girders, interior faces of exterior girders, and below-grade sections of piles):**

1. Welding of metallized surfaces may be performed only if specifically permitted by the Engineer. Remove metallizing at the location of field welds by blast cleaning (SSPC SP-6 finish), or hand (SSPC SP-2 finish) or power tool cleaning (SSPC SP-3 finish) just prior to welding. Clean sufficiently to prevent contamination of the weld. All repairs to welded connections are metallized in accordance with SSPC CS 23.00.
2. Minor areas less than or equal to  $0.1 \text{ ft}^2$  ( $9300\text{mm}^2$ ) exposing the substrate are metallized in accordance with SSPC CS 23.00 or painted in accordance with ASTM A780, "Repair of Damaged and Uncoated Areas of Hot Dip Galvanized Coatings."
3. Large areas greater than  $0.1 \text{ ft}^2$  ( $9300\text{mm}^2$ ) exposing the substrate are metallized in accordance with SSPC CS 23.00.
4. Damaged (burnished) areas not exposing the substrate with less than the specified coating thickness are metallized in accordance with SSPC CS 23.00 or painted in accordance with ASTM A780, "Repair of Damaged and Uncoated Areas of Hot Dip Galvanized Coatings."
5. Damaged (burnished) areas not exposing the substrate with more than the specified coating thickness are not repaired.
6. Defective coating is repaired by either method 2 or 3 depending on the area of the defect.

**For Exposed Surfaces (including but not limited to exterior faces of exterior girders and above-grade sections of piles):**

1. Welding of metallized surfaces may be performed only if specifically permitted by the Engineer. Remove metallization at the location of field welds by blast cleaning (SSPC SP-6 finish), or hand (SSPC SP-2 finish) or power tool cleaning (SSPC SP-3 finish) just prior to welding. Clean sufficiently to prevent contamination of the weld. All repairs to welded connections are metallized in accordance with SSPC CS 23.00.

2. All areas exposing the substrate are metallized in accordance with SSPC CS 23.00
3. Defective coating is repaired by either method 2 or 3 depending on the area of the defect.

## **7.0 TWELVE MONTH OBSERVATION PERIOD**

The contractor maintains responsibility for the coating system for a twelve (12) month observation period beginning upon the satisfactory completion of all the work required in the plans or as directed by the engineer. The contractor must guarantee the coating system under the payment and performance bond (refer to article 109-10). To successfully complete the observation period, the coating system must meet the following requirements after twelve(12) months service:

- No visible rust, contamination or application defect is observed in any coated area.
- Painted surfaces have a uniform color and gloss.
- Surfaces have an adhesion of no less than 500 psi (3.45 MPa) when tested in accordance with ASTM D-4541.

## **8.0 BASIS OF PAYMENT**

The contract price bid for the bridge component to which the coating is applied will be full compensation for the thermal sprayed coating.

## **EVAZOTE JOINT SEALS**

**(8-13-04)**

### **1.0 SEALS**

Use preformed seals compatible with concrete and resistant to abrasion, oxidation, oils, gasoline, salt and other materials that are spilled on or applied to the surface. Use a low-density closed cell, cross-linked ethylene vinyl acetate polyethylene copolymer nitrogen blown material for the seal.

Use seals manufactured with grooves 1/8" (3 mm) ± wide by 1/8" (3 mm) ± deep and spaced between 1/4 (6 mm) and 1/2 inch (13 mm) apart along the bond surface running the length of the joint. Use seals sized so that the depth of the seal meets the manufacturer's recommendation, but is not less than 70% of the uncompressed width. Provide a seal designed so that, when compressed, the center portion of the top does not extend upward above the original height of the seal by more than 1/4 inch (6 mm). Splice the seal using the heat welding method by placing the joint material ends against a teflon heating iron of 350°F (177°C) for 7 - 10 seconds, then pressing the ends together tightly. Do not test the welding until the material has completely cooled. Use material that resists weathering and

ultraviolet rays. Provide a seal that has a working range of 30% tension and 60% compression and is watertight along its entire length including the ends.

Provide seals that meet the requirements given below.

TEST	TEST METHOD	REQUIREMENT
Elongation at break	ASTM D3575	210 ± 15%
Tensile strength, psi (kPa)	ASTM D3575	110 ± 15 (755 ± 100)
Compression Recovery (% of original width)	AASHTO T42 50% compr. for 22 hr. @ 73°F (23°C) 1/2 hr. recovery	87 ± 3
Weather/Deterioration	AASHTO T42 Accelerated Weathering	No deterioration for 10 years min.
Compression/Deflection	@ 50% deflection of original width @ 50% deflection of original width	10 psi (69 kPa) min. 60 psi (414 kPa) max.
Tear Strength, psi (kPa)	ASTM D624	16 ± 3 (110 ± 20)
Density	ASTM D545	2.8 to 3.4
Water Absorption (% vol/vol)	ASTM D3575 Total immersion for 3 months	3

Have the top of the evazote seal clearly shop marked. Inspect the evazote seals upon receipt to ensure that the marks are clearly visible upon installation.

**2.0 ADHESIVES**

Use a two component, 100% solid, modified epoxy adhesive with the seal that meets the requirements of ASTM C881, Type 1, Grade 3, Class B & C and has the following physical properties:

- Tensile strength..... 3500 psi (24.1 MPa) min.
- Compressive strength..... 7000 psi (48.3 MPa) min.
- Shore D Hardness ..... 75 psi (0.5 MPa) min.
- Water Absorption..... 0.25% by weight

Use an adhesive that is workable to 40°F (4°C). When installing in temperatures below 40°F (4°C) or for application on moist, difficult to dry concrete surfaces, use an adhesive specified by the manufacturer of the joint material.

### 3.0 SAWING THE JOINTS

When the plans call for sawing the joints, the joints shall be initially formed to a width as shown on the plans including the blockout for the elastomeric concrete. Complete placement of the elastomeric concrete after the reinforced concrete deck slab has cured for seven full days and reached a minimum strength of 3000 psi (20.7 Mpa).

Cure the elastomeric concrete for a minimum of 2 days prior to sawing the elastomeric concrete to the final width and depth as specified in the plans.

When sawing the joint to receive the evazote seal, always use a rigid guide to control the saw in the desired direction. To control the saw and to produce a straight line as indicated on the plans, anchor and positively connect a template or a track to the bridge deck. Do not saw the joint by visual means such as a chalk line. Fill the holes used for holding the template or track to the deck with an approved, flowable non-shrink, non-metallic grout.

Saw cut to the desired width and depth in one or two passes of the saw by placing and spacing two metal blades on the saw shaft to the desired width for compression seals.

The desired depth is the depth of the seal plus 1/4 inch (6 mm) above the top of the seal plus approximately 1 inch (25 mm) below the bottom of the seal. An irregular bottom of sawed joint is permitted as indicated on the plans. Grind exposed corners on saw cut edges to a 1/4" (6 mm) chamfer.

Remove any staining or deposited material resulting from sawing with a wet blade to the satisfaction of the Engineer.

Use extreme care to saw the joint straight to the desired width and to prevent any chipping or damage to sawed edges of the joint.

### 4.0 PREPARATIONS FOR SAWED JOINTS

When the plans call for sawing the joint, the Engineer thoroughly inspects the sawed joint opening for spalls, popouts, cracks, etc. Make all necessary repairs prior to blast cleaning and installing the seal.

Immediately before sealing, clean the joints by sandblasting with clean dry sand. Sandblast to provide a firm, clean joint surface free of curing compound, loose material and any foreign matter. Sandblast without causing pitting or uneven surfaces. The aggregate in the elastomeric concrete may be exposed after sandblasting.

After blasting, either brush the surface with clean brushes made of hair, bristle or fiber, blow the surface with compressed air, or vacuum the surface until all traces of blast products and abrasives are removed from the surface, pockets, and corners.

If nozzle blasting, use compressed air that does not contain detrimental amounts of water or oil.

Examine the blast cleaned surface and remove any traces of oil, grease or smudge deposited in the cleaning operations.

Bond the seal to the blast cleaned surface on the same day the surface is blast cleaned.

## 5.0 PREPARATIONS FOR ARMORED JOINTS

When the plans call for armored joints, form the joint and blockout openings in accordance with the plans. If preferred, wrap the temporary form with polyethylene sheets to allow for easier removal. Do not use form release agents.

### A. Submittals

Submitting detailed working drawings is not required; however, submitting catalog cuts of the proposed material is required. In addition, direct the joint supplier to provide an angle segment placing plan.

### B. Surface Preparation

Prepare the surface within the 48 hours prior to placing the elastomeric concrete. Do not place the elastomeric concrete until the surface preparation is completed and approved.

#### 1. Angle Assembly

Clean and free metallized steel of all foreign contaminants and blast the non-metallized steel surfaces to SSPC SP-10. Blast-cleaning anchor studs is not required.

#### 2. Concrete

Prior to placing the elastomeric concrete, thoroughly clean and dry all concrete surfaces. Sandblast the concrete surface in the blockout and clear the surface of all loose debris.

### C. Elastomeric Concrete Placement

Make sure that a manufacturer's representative is present when placing elastomeric concrete. Do not place elastomeric concrete if the ambient air temperature is below 45°F (7°C).

Prepare and apply a primer, as per manufacturer's recommendations, to all vertical concrete faces, all steel components to be in contact with elastomeric concrete, and to areas specified by the manufacturer. Align the angles with the joint opening.



Prepare, batch, and place the elastomeric concrete in accordance with the manufacturer's instructions. Place the elastomeric concrete in the areas specified on the plans while the primer is still tacky and within 2 hours after applying the primer. Pay careful attention to properly consolidate the concrete around the steel and anchors. Trowel the elastomeric concrete to a smooth finish.

#### D. Joint Preparation

Prior to installing the seal, the Engineer thoroughly inspects the armored joint opening for proper alignment and full consolidation of elastomeric concrete under the angle assemblies. Make all necessary repairs prior to cleaning the joint opening and installing the seal.

Clean the armored joint opening with a pressure washer rated at 3000 psi (20.7 MPa) minimum at least 24 hours after placing the elastomeric concrete. Dry the cleaned surface prior to installing the seal.

Examine the cleaned surface and remove traces of oil, grease or smudge deposited during the cleaning operations.

Bond the seal to the cleaned surface on the same day the surface is cleaned.

### 6.0 SEAL INSTALLATION

Install the joint seal according to the manufacturer's procedures and recommendations and as recommended below. Do not install the joint seal if the ambient air temperature is below 45°F (7°C). Have a manufacturer's representative present during the installation of the first seal of the project.

Begin installation at the low end of the joint after applying the mixed epoxy to the sides of both the joint material and both sides of the joint, making certain to completely fill the grooves with epoxy. With gloved hands, compress the material and with the help of a blunt probe, push it down into the joint until it is recessed approximately 1/4 inch (6 mm) below the surface. Do not push the seal at an angle that would stretch the material. Once work on a joint begins, do not stop until it is completed. Clean the excess epoxy off the surface of the joint material *quickly* and *thoroughly*. Do not use solvents to remove excess epoxy. Remove excess epoxy in accordance with the joint manufacturer's recommendations.

Install the seal so that it is watertight. Testing of the joint seal is not required, but it is observed until final inspection.

### 7.0 BASIS OF PAYMENT

Payment for all evazote joint seals will be at the lump sum contract price bid for "Evazote Joint Seals" which prices and payment will be full compensation for furnishing all material, including elastomeric concrete when required, labor, tools and equipment necessary for installing these units in place and accepted.

**ELASTOMERIC CONCRETE**

(10-12-01)

**1.0 DESCRIPTION**

Elastomeric concrete is a mixture of a two-part polymer consisting of polyurethane and/or epoxy, and kiln-dried aggregate. Have the manufacturer supply it as a unit. Use the concrete in the blocked out areas on both sides of the bridge deck joints as indicated on the plans.

**2.0 MATERIALS**

Provide materials that comply with the following minimum requirements at 14 days.

CONCRETE PROPERTIES	TEST METHOD	MINIMUM REQUIREMENT
Bond Strength to Concrete, psi (MPa)	(a) STM D638 (D638M)	450 (3.1)
Brittleness by Impact, ft-lb (kg-m)	Ball Drop	7 (0.97)
Compressive Strength, psi (MPa)	ASTM D695 (D695M)	2800 (19.3)

BINDER PROPERTIES (without aggregate)	TEST METHOD	MINIMUM REQUIREMENT
Tensile Strength, psi (MPa)	ASTM D638 (D638M)	800 (5.5)
Ultimate Elongation	ASTM D638 (D638M)	150%
Tear Resistance, lb/in (kN/m)	ASTM D624	90 (15.7)

In addition to the requirements above, use elastomeric concrete that also resists water, chemical, UV, and ozone exposure and withstands extreme temperature (freeze-thaw) changes.

Furnish a manufacturer's certification verifying that the materials satisfy the above requirements. Provide samples of elastomeric concrete to the Engineer, if requested, to independently verify conformance with the above requirements.

Require a manufacturer's representative to be present on site during the installation of the elastomeric concrete.

### 3.0 BASIS OF PAYMENT

No separate payment will be made for elastomeric concrete. The lump sum contract price bid for "Evazote Joint Seals" will be full compensation for furnishing and placing the Elastomeric Concrete.

## FALSEWORK AND FORMWORK

(7-18-06)

### 1.0 DESCRIPTION

Use this Special Provision as a guide to develop temporary works submittals required by the Standard Specifications or other provisions; no additional submittals are required herein. Such temporary works include, but are not limited to, falsework and formwork.

Falsework is any temporary construction used to support the permanent structure until it becomes self-supporting. Formwork is the temporary structure or mold used to retain plastic or fluid concrete in its designated shape until it hardens. Access scaffolding is a temporary structure that functions as a work platform that supports construction personnel, materials, and tools, but is not intended to support the structure. Scaffolding systems that are used to temporarily support permanent structures (as opposed to functioning as work platforms) are considered to be falsework under the definitions given. Shoring is a component of falsework such as horizontal, vertical, or inclined support members. Where the term "temporary works" is used, it includes all of the temporary facilities used in bridge construction that do not become part of the permanent structure.

Design and construct safe and adequate temporary works that will support all loads imposed and provide the necessary rigidity to achieve the lines and grades shown on the plans in the final structure.

### 2.0 MATERIALS

Select materials suitable for temporary works; however, select materials that also ensure the safety and quality required by the design assumptions. The Engineer has authority to reject material on the basis of its condition, inappropriate use, safety, or nonconformance with the plans. Clearly identify allowable loads or stresses for all materials or manufactured devices on the plans. Revise the plan and notify the Engineer if any change to materials or material strengths is required.

### 3.0 DESIGN REQUIREMENTS

#### A. Working Drawings

Provide working drawings for items as specified in the contract, or as required by the Engineer, with design calculations and supporting data in sufficient detail to permit a structural and safety review of the proposed design of the temporary work.

When concrete placement is involved, include data such as the drawings of proposed sequence, rate of placement, direction of placement, and location of all construction joints. Submit the number of copies as called for by the contract.

When required, have the drawings and calculations prepared under the guidance of, and sealed by, a North Carolina Registered Professional Engineer who is knowledgeable in temporary works design.

Design falsework and formwork requiring submittals in accordance with the 1995 AASHTO *Guide Design Specifications for Bridge Temporary Works* except as noted herein.

1. Wind Loads

Table 2.2 of Article 2.2.5.1 is modified to include wind velocities up to 110 mph (177 km/hr). In addition, Table 2.2A is included to provide the maximum wind speeds by county in North Carolina.

**Table 2.2 - Wind Pressure Values**

Height Zone feet (m) above ground	Pressure, lb/ft <sup>2</sup> (kPa) for Indicated Wind Velocity, mph (km/hr)				
	70 (112.7)	80 (128.7)	90 (144.8)	100 (160.9)	110 (177.0)
0 to 30 (0 to 9.1)	15 (0.72)	20 (0.96)	25 (1.20)	30 (1.44)	35 (1.68)
30 to 50 (9.1 to 15.2)	20 (0.96)	25 (1.20)	30 (1.44)	35 (1.68)	40 (1.92)
50 to 100 (15.2 to 30.5)	25 (1.20)	30 (1.44)	35 (1.68)	40 (1.92)	45 (2.15)
over 100 (30.5)	30 (1.44)	35 (1.68)	40 (1.92)	45 (2.15)	50 (2.39)

2. Time of Removal

The following requirements replace those of Article 3.4.8.2.

Do not remove forms until the concrete has attained strengths required in Article 420-16 of the Standard Specifications and these Special Provisions.

Do not remove forms until the concrete has sufficient strength to prevent damage to the surface.

Table 2.2A - Steady State Maximum Wind Speeds by Counties in North Carolina

COUNTY	25 YR (mph) (km/hr)	COUNTY	25 YR (mph) (km/hr)	COUNTY	25 YR (mph) (km/hr)
Alamance	70 (112.7)	Franklin	70 (112.7)	Pamlico	100 (160.9)
Alexander	70 (112.7)	Gaston	70 (112.7)	Pasquotank	100 (160.9)
Alleghany	70 (112.7)	Gates	90 (144.8)	Pender	100 (160.9)
Anson	70 (112.7)	Graham	80 (128.7)	Perquimans	100 (160.9)
Ashe	70 (112.7)	Granville	70 (112.7)	Person	70 (112.7)
Avery	70 (112.7)	Greene	80 (128.7)	Pitt	90 (144.8)
Beaufort	100 (160.9)	Guilford	70 (112.7)	Polk	80 (128.7)
Bertie	90 (144.8)	Halifax	80 (128.7)	Randolph	70 (112.7)
Bladen	90 (144.8)	Harnett	70 (112.7)	Richmond	70 (112.7)
Brunswick	100 (160.9)	Haywood	80 (128.7)	Robeson	80 (128.7)
Buncombe	80 (128.7)	Henderson	80 (128.7)	Rockingham	70 (112.7)
Burke	70 (112.7)	Hertford	90 (144.8)	Rowan	70 (112.7)
Cabarrus	70 (112.7)	Hoke	70 (112.7)	Rutherford	70 (112.7)
Caldwell	70 (112.7)	Hyde	110 (177.0)	Sampson	90 (144.8)
Camden	100 (160.9)	Iredell	70 (112.7)	Scotland	70 (112.7)
Carteret	110 (177.0)	Jackson	80 (128.7)	Stanley	70 (112.7)
Caswell	70 (112.7)	Johnston	80 (128.7)	Stokes	70 (112.7)
Catawba	70 (112.7)	Jones	100 (160.9)	Surry	70 (112.7)
Cherokee	80 (128.7)	Lee	70 (112.7)	Swain	80 (128.7)
Chatham	70 (112.7)	Lenoir	90 (144.8)	Transylvania	80 (128.7)
Chowan	90 (144.8)	Lincoln	70 (112.7)	Tyrell	100 (160.9)
Clay	80 (128.7)	Macon	80 (128.7)	Union	70 (112.7)
Cleveland	70 (112.7)	Madison	80 (128.7)	Vance	70 (112.7)
Columbus	90 (144.8)	Martin	90 (144.8)	Wake	70 (112.7)
Craven	100 (160.9)	McDowell	70 (112.7)	Warren	70 (112.7)
Cumberland	80 (128.7)	Mecklenburg	70 (112.7)	Washington	100 (160.9)
Currituck	100 (160.9)	Mitchell	70 (112.7)	Watauga	70 (112.7)
Dare	110 (177.0)	Montgomery	70 (112.7)	Wayne	80 (128.7)
Davidson	70 (112.7)	Moore	70 (112.7)	Wilkes	70 (112.7)
Davie	70 (112.7)	Nash	80 (128.7)	Wilson	80 (128.7)
Duplin	90 (144.8)	New Hanover	100 (160.9)	Yadkin	70 (112.7)
Durham	70 (112.7)	Northampton	80 (128.7)	Yancey	70 (112.7)
Edgecombe	80 (128.7)	Onslow	100 (160.9)		
Forsyth	70 (112.7)	Orange	70 (112.7)		

Note on the working drawings any anchorages, connectors, inserts, steel sleeves or other such devices used as part of the falsework or formwork that remains in the permanent structure. If the plan notes indicate that the structure contains the necessary corrosion protection required for a Corrosive Site, epoxy coat, galvanize, metallize or otherwise protect these devices as directed by the Engineer. Any coating required by the Engineer will be considered incidental to the various pay items requiring temporary works.

#### B. Review and Approval

The Engineer is responsible for the review and approval of temporary works' drawings.

Submit the working drawings sufficiently in advance of proposed use to allow for their review, revision (if needed), and approval without delay to the work.

Do not start construction of any temporary work for which working drawings are required until the drawings have been approved. Such approval does not relieve the Contractor of the responsibility for the accuracy and adequacy of the working drawings.

The time period for review of the working drawings does not begin until complete drawings and design calculations, when required, are received by the Engineer.

On the drawings, show all information necessary to allow the design of any component to be checked independently as determined by the Engineer.

If requested by the Engineer, submit with the working drawings manufacturer's catalog data listing the weight of all construction equipment that will be supported on the temporary work. Show anticipated total settlements and/or deflections of falsework and forms on the working drawings. Include falsework footing settlements, joint take-up, and deflection of beams or girders. Falsework hangers that support concentrated loads and are installed at the edge of thin top flange concrete girders (such as bulb tee girders) shall be spaced so as not to exceed 75% of the manufacturer's stated safe working load. Use of dual leg hangers (such as Meadow Burke HF-42 and HF-43) are not allowed. Design the falsework and forms supporting deck slabs and overhangs on girder bridges so that there will be no differential settlement between the girders and the deck forms during placement of deck concrete.

#### 4.0 CONSTRUCTION REQUIREMENTS

All requirements of Section 420 of the Standard Specifications apply.

Construct temporary works in conformance with the approved working drawings. Ensure that the quality of materials and workmanship employed is consistent with that assumed in the design of the temporary works. Do not weld falsework members to any portion of the permanent structure unless approved. Show any welding to the permanent structure on the approved construction drawings.

Provide tell-tales attached to the forms and extending to the ground, or other means, for accurate measurement of falsework settlement. Make sure that the anticipated compressive settlement and/or deflection of falsework does not exceed 1 inch (25 mm). For cast-in-place concrete structures, make sure that the calculated deflection of falsework flexural members does not exceed 1/240 of their span regardless of whether or not the deflection is compensated by camber strips.

#### A. Maintenance and Inspection

Inspect and maintain the temporary work in an acceptable condition throughout the period of its use. Certify that the manufactured devices have been maintained in a condition to allow them to safely carry their rated loads. Clearly mark each piece so that its capacity can be readily determined at the job site.

Perform an in-depth inspection of an applicable portion(s) of the temporary works, in the presence of the Engineer, not more than 24 hours prior to the beginning of each concrete placement. Inspect other temporary works at least once a month to ensure that they are functioning properly. Have a North Carolina Registered Professional Engineer inspect the cofferdams, shoring, sheathing, support of excavation structures, and support systems for load tests prior to loading.

#### B. Foundations

Determine the safe bearing capacity of the foundation material on which the supports for temporary works rest. If required by the Engineer, conduct load tests to verify proposed bearing capacity values that are marginal or in other high-risk situations.

The use of the foundation support values shown on the contract plans of the permanent structure is permitted if the foundations are on the same level and on the same soil as those of the permanent structure.

Allow for adequate site drainage or soil protection to prevent soil saturation and washout of the soil supporting the temporary works supports.

If piles are used, the estimation of capacities and later confirmation during construction using standard procedures based on the driving characteristics of the pile is permitted. If preferred, use load tests to confirm the estimated capacities; or, if required by the Engineer conduct load tests to verify bearing capacity values that are marginal or in other high risk situations.

The Engineer reviews and approves the proposed pile and soil bearing capacities.

## 5.0 REMOVAL

Unless otherwise permitted, remove and keep all temporary works upon completion of the work. Do not disturb or otherwise damage the finished work.

Remove temporary works in conformance with the contract documents. Remove them in such a manner as to permit the structure to uniformly and gradually take the stresses due to its own weight.

## 6.0 METHOD OF MEASUREMENT

Unless otherwise specified, temporary works will not be directly measured.

## 7.0 BASIS OF PAYMENT

Payment at the contract unit prices for the various pay items requiring temporary works will be full compensation for the above falsework and formwork.

## SUBMITTAL OF WORKING DRAWINGS

(7-12-07)

### 1.0 GENERAL

Submit working drawings in accordance with Article 105-2 of the Standard Specifications and the requirements of this special provision. For the purposes of this provision, "submittals" refers to only those listed in this provision. The list of submittals contained herein does not represent a list of required submittals for this project. Submittals are only necessary for those items as required by the Standard Specifications, other Special Provisions or contract plans. Make submittals that are not specifically noted in this Special Provision directly to the Resident Engineer. Either the Structure Design Unit or the Geotechnical Engineering Unit or both units will jointly review submittals.

If a submittal contains variations from plan details or specifications or significantly affects project cost, field construction or operations, discuss the submittal with and submit all copies to the Resident Engineer. State the reason for the proposed variation in the submittal. To minimize review time, make sure all submittals are complete when initially submitted. Provide a contact name and information with each submittal. Direct any questions regarding submittal requirements to the Resident Engineer, Structure Design Unit contacts or the Geotechnical Engineering Unit contacts noted below.

In order to facilitate in-plant inspection by NCDOT and approval of working drawings, provide the name, address and telephone number of the facility where fabrication will actually be done if different than shown on the title block of the submitted working drawings. This includes, but is not limited to, precast concrete items, prestressed concrete items and fabricated steel or aluminum items.



2.0 ADDRESSES AND CONTACTS

For submittals to the Structure Design Unit, use the following addresses:

Via US mail:

Mr. G. R. Perfetti, P. E.  
State Bridge Design Engineer  
North Carolina Department  
of Transportation  
Structure Design Unit  
1581 Mail Service Center  
Raleigh, NC 27699-1581

Attention: Mr. P. D. Lambert, P. E.

Via other delivery service:

Mr. G. R. Perfetti, P. E.  
State Bridge Design Engineer  
North Carolina Department  
of Transportation  
Structure Design Unit  
1000 Birch Ridge Drive  
Raleigh, NC 27610

Attention: Mr. P. D. Lambert, P. E.

For submittals to the Geotechnical Engineering Unit, use the following addresses:

For projects in Divisions 1-7, use the following Eastern Regional Office address:

Via US mail:

Mr. K. J. Kim, Ph. D., P. E.  
Eastern Regional Geotechnical  
Manager  
North Carolina Department  
of Transportation  
Geotechnical Engineering Unit  
Eastern Regional Office  
1570 Mail Service Center  
Raleigh, NC 27699-1570

Via other delivery service:

Mr. K. J. Kim, Ph. D., P. E.  
Eastern Regional Geotechnical  
Manager  
North Carolina Department  
of Transportation  
Geotechnical Engineering Unit  
Eastern Regional Office  
3301 Jones Sausage Road, Suite 100  
Garner, NC 27529

For projects in Divisions 8-14, use the following Western Regional Office address:

Via US mail:

Mr. John Pilipchuk, L. G., P. E.  
Western Regional Geotechnical  
Manager  
North Carolina Department  
of Transportation  
Geotechnical Engineering Unit  
Western Regional Office  
5253 Z Max Boulevard  
Harrisburg, NC 28075

Via other delivery service:

Mr. John Pilipchuk, L. G., P. E.  
Western Region Geotechnical  
Manager  
North Carolina Department  
of Transportation  
Geotechnical Engineering Unit  
Western Regional Office  
5253 Z Max Boulevard  
Harrisburg, NC 28075

Direct any questions concerning submittal review status, review comments or drawing markups to the following contacts:

Primary Structures Contact:	Paul Lambert (919) 250 – 4041 (919) 250 – 4082 facsimile plambert@dot.state.nc.us
Secondary Structures Contacts:	James Gaither      (919) 250 – 4042 David Stark        (919) 250 – 4044
Eastern Regional Geotechnical Contact (Divisions 1-7):	K. J. Kim (919) 662 – 4710 (919) 662 – 3095 facsimile kkim@dot.state.nc.us
Western Regional Geotechnical Contact (Divisions 8-14):	John Pilipchuk (704) 455 – 8902 (704) 455 – 8912 facsimile jpilipchuk@dot.state.nc.us

### 3.0 SUBMITTAL COPIES

Furnish one complete copy of each submittal, including all attachments, to the Resident Engineer. At the same time, submit the number of hard copies shown below of the same complete submittal directly to the Structure Design Unit and/or the Geotechnical Engineering Unit.

The first table below covers “Structure Submittals”. The Resident Engineer will receive review comments and drawing markups for these submittals from the Structure Design Unit. The second table in this section covers “Geotechnical Submittals”. The Resident Engineer will receive review comments and drawing markups for these submittals from the Geotechnical Engineering Unit.

Unless otherwise required, submit one set of supporting calculations to either the Structure Design Unit or the Geotechnical Engineering Unit unless both units require submittal copies in which case submit a set of supporting calculations to each unit. Provide additional copies of any submittal as directed by the Engineer.

**STRUCTURE SUBMITTALS**

<b>Submittal</b>	<b>Copies Required by Structure Design Unit</b>	<b>Copies Required by Geotechnical Engineering Unit</b>	<b>Contract Reference Requiring Submittal <sup>1</sup></b>
Arch Culvert Falsework	5	0	Plan Note, SN Sheet & "Falsework and Formwork"
Box Culvert Falsework <sup>7</sup>	5	0	Plan Note, SN Sheet & "Falsework and Formwork"
Cofferdams	6	2	Article 410-4
Evazote Joint Seals <sup>6</sup>	9	0	"Evazote Joint Seals"
Expansion Joint Seals (hold down plate type with base angle)	9	0	"Expansion Joint Seals"
Expansion Joint Seals (modular)	2, then 9	0	"Modular Expansion Joint Seals"
Expansion Joint Seals (strip seals)	9	0	"Strip Seals"
Falsework & Forms <sup>2</sup> (substructure)	8	0	Article 420-3 & "Falsework and Formwork"
Falsework & Forms (superstructure)	8	0	Article 420-3 & "Falsework and Formwork"
Girder Erection over Railroad	5	0	Railroad Special Provisions
Maintenance and Protection of Traffic Beneath Proposed Structure	8	0	"Maintenance and Protection of Traffic Beneath Proposed Structure at Station ____"
Metal Bridge Railing	8	0	Plan Note
Metal Stay-in-Place Forms	8	0	Article 420-3
Metalwork for Elastomeric Bearings <sup>4,5</sup>	7	0	Article 1072-10
Miscellaneous Metalwork <sup>4,5</sup>	7	0	Article 1072-10
Optional Disc Bearings <sup>4</sup>	8	0	"Optional Disc Bearings"

Overhead Signs	13	0	Article 903-3(C) & Applicable Project Special Provisions
Pile Splicer	7	2	Subarticle 450-7(C)
Placement of Equipment on Structures (cranes, etc.)	7	0	Article 420-20
Pot Bearings <sup>4</sup>	8	0	“Pot Bearings”
Precast Concrete Box Culverts	2, then 1 reproducible	0	“Optional Precast Reinforced Concrete Box Culvert at Station ____”
Precast Retaining Wall Panels	10	1	Article 1077-2
Prestressed Concrete Cored Slab (detensioning sequences) <sup>3</sup>	6	0	Article 1078-11
Prestressed Concrete Deck Panels	6 and 1 reproducible	0	Article 420-3
Prestressed Concrete Girder (strand elongation and detensioning sequences)	6	0	Articles 1078-8 and 1078- 11
Removal of Existing Structure over Railroad	5	0	Railroad Special Provisions
Revised Bridge Deck Plans (adaptation to prestressed deck panels)	2, then 1 reproducible	0	Article 420-3
Revised Bridge Deck Plans (adaptation to modular expansion joint seals)	2, then 1 reproducible	0	“Modular Expansion Joint Seals”
Sound Barrier Wall Casting Plans	10	0	Article 1077-2 & “Sound Barrier Wall”
Sound Barrier Wall Steel Fabrication Plans <sup>5</sup>	7	0	Article 1072-10 & “Sound Barrier Wall”
Structural Steel <sup>4</sup>	2, then 7	0	Article 1072-10

Temporary Detour Structures	10	2	Article 400-3 & “Construction, Maintenance and Removal of Temporary Structure at Station _____”
Temporary Shoring <sup>8</sup>	7	2	“Temporary Shoring”
TFE Expansion Bearings <sup>4</sup>	8	0	Article 1072-10

**FOOTNOTES**

- References are provided to help locate the part of the contract where the submittals are required. References in quotes refer to the Project Special Provision by that name. Articles or subarticles refer to the Standard Specifications.
- Submittals for these items are necessary only when required by a note on plans.
- Submittals for these items may not be required. A list of pre-approved sequences is available from the producer or the Materials and Tests Unit.
- The fabricator may submit these items directly to the Structure Design Unit.
- The two sets of preliminary submittals required by Article 1072-10 of the Standard Specifications are not required for these items.
- Submittals for Fabrication Drawings are not required. Submittals for Catalogue Cuts of Proposed Material are required. See Section 5.A of the referenced Project Special Provision.
- Submittals are necessary only when the top slab thickness is 18 inches or greater.
- Electronic copies of submittals are required. See referenced Project Special Provision.

**GEOTECHNICAL SUBMITTALS**

<b>Submittal</b>	<b>Copies Required by Geotechnical Engineering Unit</b>	<b>Copies Required by Structure Design Unit</b>	<b>Contract Reference Requiring Submittal <sup>1</sup></b>
Crosshole Sonic Logging (CSL) Reports <sup>2</sup>	1	0	“Crosshole Sonic Logging”
Drilled Pier Construction Sequence Plans <sup>2</sup>	1	0	“Drilled Piers”
Mechanically Stabilized Earth (MSE) Retaining Walls	8	2	“MSE Retaining Walls”
Pile Driving Analyzer (PDA) Reports <sup>2</sup>	2	0	“Pile Driving Analyzer”
Pile Driving Equipment Data <sup>3</sup>	1	0	Article 450-5
Proprietary Retaining Walls	8	2	Applicable Project Special Provision
Anchored Retaining Walls	8	2	Applicable Project Special Provision
Soil Nail Retaining Walls	8	2	Applicable Project Special Provision
Temporary Mechanically Stabilized (MSE) Earth Wall <sup>2</sup>	9	0	“Temporary Shoring”

**FOOTNOTES**

- References are provided to help locate the part of the contract where the working drawing submittals are required. References in quotes refer to the Project Special Provision by that name. Articles refer to the Standard Specifications.
- Electronic copies of submittals are required. See referenced Project Special Provision.
- Download Pile Driving Equipment Data Form from following link:  
<http://www.ncdot.org/doh/preconstruct/highway/geotech/formdet/>  
Submit one hard copy of the completed form to the Resident Engineer. Submit a second copy of the completed form electronically, by facsimile or via US Mail or other delivery service to the Geotechnical Engineering Unit. Electronic submission is preferred. See second page of form for submittal instructions.

**CONSTRUCTION, MAINTENANCE AND REMOVAL  
OF TEMPORARY ACCESS AT STATION 15+15.00 -L-**

**(SPECIAL)**

**1.0 GENERAL**

Construct, maintain, and remove the temporary access required to provide the working area necessary for construction of the new bridge, construction of the temporary detour structure, or for the removal of an existing bridge, as applicable. Temporary access may include other methods than those outlined in this Special Provision; however, all types of temporary access are required to meet the requirements of all permits, the Standard Specifications, and this Special Provision.

**2.0 TEMPORARY ROCK CAUSEWAY [WORKPAD]**

Construction of a temporary rock causeway [workpad] within the limits shown on the plans is permitted. Build the causeway [workpad] with Class II riprap topped by a layer of Class A riprap or as otherwise designated on the plans or approved by the Engineer. If desired, recycle the Class II riprap used in the causeway [workpad] for placement in the final riprap slope protection as directed by the Engineer. No payment will be made for recycled riprap as this material is considered incidental to the causeway [workpad] placement and removal. If this option is exercised, no adjustment in contract bid price will be allowed due to an underrun in the quantity of "Plain Rip Rap Class II (2'-0" (600 mm) Thick)".

Completely remove all causeway [workpad] material including pipes and return the entire causeway [workpad] footprint to the original contours and elevations within 90 days of the completion of the deck slab or as otherwise required by permits.

For sites affected by moratoriums or restrictions on in-stream work: Do not construct or remove causeway [workpad] during the moratorium period shown on the permit. If the completion of the deck slab falls within the prohibitive dates for causeway [workpad] construction or removal, begin causeway [workpad] removal immediately following the prohibitive dates.

At the contractor's option, construction of a temporary work bridge in lieu of the causeway(s) [workpad] is acceptable, provided the temporary work bridge satisfies all permits. Submit details of the temporary work bridge to the Engineer prior to constructing the work bridge to ensure conformance with the plans and all permits. Completely remove the temporary bridge prior to final acceptance or as otherwise required by the permits.

### 3.0 TEMPORARY WORK BRIDGE

Construction of a temporary work bridge is permitted as shown on the plans. The temporary work bridge shall have a minimum span length of 20 feet. Submit details of the temporary work bridge to the Engineer prior to constructing the work bridge to ensure conformance with the plans and all permits. Completely remove the temporary bridge prior to final acceptance or as otherwise required by the permits.

### 4.0 BASIS OF PAYMENT

The lump sum price bid for "Construction, Maintenance and Removal of Temporary Access at Station \_\_\_\_\_" will be full compensation for the above work, or other methods of access, including all material, pipes, work bridge components, equipment, tools, labor, disposal, and incidentals necessary to complete the work.

## OPTIONAL DISC BEARINGS

(6-07-05)

### 1.0 GENERAL

This item consists of furnishing, fabrication and installation of disc bearings in accordance with AASHTO Standard Specifications, the Standard Specifications, the recommendations of the manufacturer and as specified herein. In addition, all plan notes pertaining to furnishing and installing pot bearing assemblies shall also apply to disc bearing assemblies, except as noted herein.

Disc Bearings consist of a polyether urethane structural element (disc) confined by upper and lower steel bearing plates. Equip disc bearings with a shear restriction mechanism to prevent movement of the disc. Supply disc bearings as fixed bearings and guided expansion bearings as designated by the Contract Documents.

Fixed disc bearings allow rotation but no longitudinal or transverse movement in the bearing plane. Fixed bearings consist of a sole plate, an elastomer disc, upper bearing plate, lower bearing plate, masonry plate, anchor bolts, nuts and washers.

Guided expansion disc bearings allow rotation and only longitudinal movement in the bearing plane. Guided expansion disc bearings consist of a sole plate, a top steel plate with a polished stainless steel sheet facing bearing on a fixed disc bearing with a layer of virgin polytetrafluoroethylene (PTFE) material on its top, masonry plate, anchor bolt assembly which includes anchor bolts, nuts, washers, pipe sleeves, a closure plate, grout and various sizes of standard pipe and any other necessary material as detailed on the plans. To allow longitudinal movement, bond a polytetrafluoroethylene (PTFE) sheet to the upper steel bearing plate. Support a sliding steel top bearing plate with the upper steel bearing plate. Face the mating surface of the sliding steel top bearing plate with polished stainless steel. Use either a guide bar or keyway system to restrict transverse movement. Face the sliding surfaces of the guide bar or keyway systems with either PTFE sheets or stainless steel.



## 2.0 MATERIALS

Use disc bearings produced by the same manufacturer.

Use AASHTO M270 Grade 50W (345W) for all steel in the disc bearings. Clean, coat, and seal the plates in the disc bearing assemblies except for the areas with special facings and the areas that come in contact with the elastomer disc, in accordance with the Special Provision for "Thermal Sprayed Coatings (Metallization)". Coat surfaces to a thickness of 8 mils (0.200 mm) minimum on all external parts. Repair surfaces that are abraded or damaged after the application of metallizing in accordance with the Special Provision for "Thermal Sprayed Coatings (Metallization)".

Provide anchor bolts and nuts in accordance with the Standard Specifications.

When the maximum plan dimension of the sheet is 12" (300 mm) or less, provide a stainless steel sheet in expansion disc bearings that is at least 16 gage or 1/16" (1.6 mm). When the maximum plan dimension is greater than 12" (300 mm), provide a stainless steel sheet that is at least 11 gage or 1/8" (3 mm). Ensure that all stainless steel sheets are in conformance with ASTM A167/A240 Type 304 and polished to a minimum #8 mirror surface finish.

Blast clean the surface of the plate that will be attached to the stainless sheet to a near white condition in accordance with the Standard Specifications. Position and clamp the back of the stainless sheet that is to be in contact with the steel plate on the steel plate. Apply the stainless steel to the blast cleaned surface of the steel plate as soon as possible after blasting and before any visible oxidation of the blast cleaned surface occurs. Weld the stainless sheet continuously around its perimeter using a tungsten inert gas, wire-fed welder.

For the PTFE sheet, used as a mating surface for the stainless sheet, provide an unfilled virgin PTFE Sheet (Recessed) or a glass-fiber filled PTFE sheet, resulting from skiving billets formed under hydraulic pressure and heat. Provide resin that conforms to the requirements of ASTM D4894 or D4895.

To bond the PTFE and the bearing plate, use heat cured high temperature epoxy capable of withstanding temperature of -320°F to 500°F (-195 °C to 260 °C).

Mold the polyether urethane structural element from a polyether urethane compound. Conform the physical properties of the polyether urethane to the following requirements:

Physical Property	ASTM Test Method	Requirements	
		Min.	Max.
Hardness, Type D Durometer	D2240	60	64
Tensile Stress psi (Mpa) At 100% elongation At 200% elongation	D412	2000 (13.8) 3700 (25.5)	----
Tensile Strength psi (Mpa)	D412	5000 (34.5)	----
Ultimate Elongation %	D412	220	----
Compression Set % 22 hrs. at 158°F (70°C)	D395	----	40

### 3.0 DESIGN

Design the disc bearings for the loads and movements shown on the contract plans. However, use the anchor bolt size, length, spacing and masonry plate thickness as shown on the contract plans and provide an overall height of the bearing assembly that is at least the height shown on the contract plans, but no more than 1/2 inch (13 mm) greater than this height. Either combine and cast the sole plate and top plate/upper bearing plate and the lower bearing plate and masonry plate as a single unit or weld together prior to the installation of the disc.

When designing the bearings, use the following allowable bearing stresses:

- On polyether urethane structural element: 5000 psi (34.5 MPa)
- On PTFE Sliding Surface, filled or unfilled PTFE (recessed): 3500 psi (24.1 MPa)

Submit eight sets of shop drawings and one set of design calculations for review, comments and acceptance. Have a North Carolina Registered Professional Engineer check and seal the shop drawings and design calculations.

After the Engineer reviews the drawings and, if necessary, corrections are made, submit one 22" x 34" reproducible set of the working drawings.

#### 4.0 SAMPLING AND TESTING

##### A. Sampling

The manufacturer is responsible for randomly selecting and testing sample bearings from completed lots of bearings. The manufacturer is also responsible for certifying that the completed bearings and their components have been tested and are in compliance with the requirements of this Special Provision. Have the manufacturer furnish the results of the tests to the Materials and Tests Engineer.

##### B. Testing

###### 1. Proof Load Test

Load a test bearing to 150% of the bearing's rated design capacity and simultaneously subject it to a rotational range of 0.02 radians (1.146°) for a period of 1 hour.

Have the bearing visually examined both during the test and upon disassembly after the test. Any resultant visual defects, such as extruded or deformed elastomer or PTFE, damaged seals or rings, or cracked steel is cause for rejection.

Keep continuous and uniform contact between the polyether urethane element and the bearing plates and between the sliding steel top plate and the upper bearing plate for the duration of the test. Any observed lift-off is cause for rejection.

###### 2. Sliding Coefficient of Friction

For all guided and non-guided expansion type bearings, measure the sliding coefficient of friction at the bearing's design capacity in accordance with the test method described below, and on the fifth and fiftieth cycles, at a sliding speed of 1 in/min (25 mm/min).

Calculate the sliding coefficient of friction as the horizontal load required to maintain continuous sliding of one bearing, divided by the bearing's vertical design capacity.

The test results are evaluated as follows:

- A maximum measured sliding coefficient of friction of 3%.
- A visual examination both during and after the test. Any resultant visual defects, such as bond failure, physical destruction, cold flow of PTFE to the point of debonding, or damaged components is cause for rejection of the lot.

Using undamaged test bearings in the work is permitted.

### 3. Test Method

The test method and equipment shall meet the following requirements:

- f. Arrange the test to determine the coefficient of friction on the first movement of the manufactured bearing.
- g. Clean the bearing surface prior to testing.
- h. Conduct the test at maximum working stress for the PTFE surface with the test load applied continuously for 12 hours prior to measuring friction.
- i. Determine the first movement static and dynamic coefficient of friction of the test bearing at a sliding speed of less than 1 in/min (25 mm/min), not to exceed:

0.04	unfilled PTFE
0.08	filled PTFE
- j. Subject the bearing specimen to 100 movements of at least 1 inch (25 mm) of relative movement and, if the test facility permits, the full design movement at a speed of less than 1 ft/min (300 mm/min). Following this test determine the static and kinetic coefficient of friction again. The specimen is considered a failure if it exceeds the values measured in (d) above or if it shows any signs of bond failure or other defects.

Bearings represented by test specimens passing the above requirements are approved for use in the structure subject to on-site inspection for visible defects.

## 5.0 INSTALLATION

Store disc bearings delivered to the bridge site under cover on a platform above the ground surface. Protect the bearings from injury at all times and, before placing the bearings, dry and clean all dirt, oil, grease or other foreign substances from the bearing. Do not disassemble the bearings during installation, except at the manufacturer's direction. Place the bearings in accordance with the recommendations of the manufacturer, Contract Drawings, and as directed by the Engineer. If there is any discrepancy between the recommendations of the manufacturer, Special Provisions, and Contract Drawings, the Engineer is the sole judge in reconciling any such discrepancy.

Provide preformed bearing pads under the masonry plates in accordance with Article 1079-1 of the Standard Specifications.

Do not install any bearing before the Engineer approves it.

## 6.0 BASIS OF PAYMENT

Payment for all optional disc bearings will be at the lump sum contract price bid for "Pot Bearings" which includes full compensation for furnishing all disc bearings, labor, materials, tools, equipment, testing and incidentals required to complete the work in accordance with the Standard Specifications, this Special Provision, the manufacturer's requirements and as directed by the Engineer.

### CRANE SAFETY

(8-15-05)

Comply with the manufacturer specifications and limitations applicable to the operation of any and all cranes and derricks. Prime contractors, sub-contractors, and fully operated rental companies shall comply with the current Occupational Safety and Health Administration regulations (OSHA).

Submit all items listed below to the Engineer prior to beginning crane operations involving critical lifts. A critical lift is defined as any lift that exceeds 75 percent of the manufacturer's crane chart capacity for the radius at which the load will be lifted or requires the use of more than one crane. Changes in personnel or equipment must be reported to the Engineer and all applicable items listed below must be updated and submitted prior to continuing with crane operations.

### CRANE SAFETY SUBMITTAL LIST

- A. **Competent Person:** Provide the name and qualifications of the "Competent Person" responsible for crane safety and lifting operations. The named competent person will have the responsibility and authority to stop any work activity due to safety concerns.
- B. **Riggers:** Provide the qualifications and experience of the persons responsible for rigging operations. Qualifications and experience should include, but not be limited to, weight calculations, center of gravity determinations, selection and inspection of sling and rigging equipment, and safe rigging practices.
- C. **Crane Inspections:** Inspection records for all cranes shall be current and readily accessible for review upon request.
- D. **Certifications:** By July 1, 2006, crane operators performing critical lifts shall be certified by NC CCO (National Commission for the Certification of Crane Operators), or satisfactorily complete the Carolinas AGC's Professional Crane Operator's Proficiency Program. Other approved nationally accredited programs will be considered upon request. All crane operators shall also have a current CDL medical card. Submit a list of anticipated critical lifts and corresponding crane operator(s). Include current certification for the type of crane operated (small hydraulic, large hydraulic, small lattice, large lattice) and medical evaluations for each operator.

**SHIPPING STEEL STRUCTURAL MEMBERS**

(7-18-06)

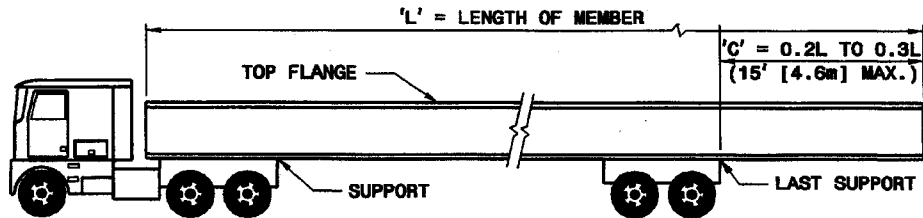
Section 1072-23 Marking and Shipping

Add the following paragraphs after the third paragraph of the Section.

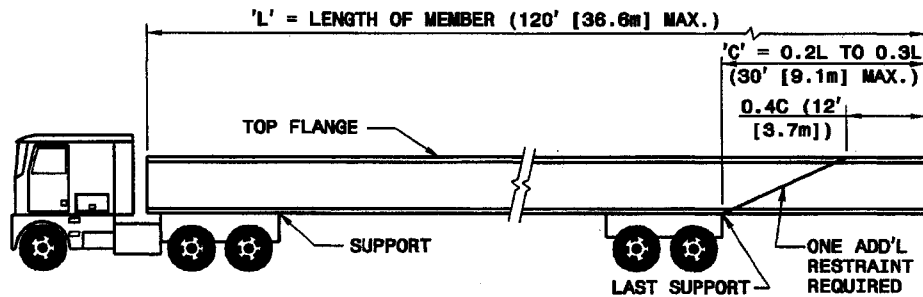
Load and ship steel beams and girders in accordance with the Figure below for all types of transportation.

Below is the sketches provided to Materials and Tests Unit on May 8, 1991. When the contractor wishes to place members on trucks not in accordance with these limits, to ship by rail, to attach shipping restraints to the members, to ship horizontally curved steel members, or to invert members, he shall submit a shipping plan prior to shipping. See also Article 1072-11.

**LIMITS FOR PLACEMENT OF BEAMS AND GIRDERS DURING SHIPMENT**



**WHEN 'C' = 15' (4.6m) OR LESS**



**WHEN 'C' = OVER 15' (4.6m) THRU 30' (9.1m)**

L	MIN. 'C'	MAX 'C'
75 (22.9m)	15 (4.6m)	22½ (6.9m)
80 (24.4m)	16 (4.9m)	24 (7.3m)
85 (25.9m)	17 (5.2m)	25½ (7.8m)
90 (27.4m)	18 (5.5m)	27 (8.2m)
95 (29.0m)	19 (5.8m)	28½ (8.7m)
100 (30.5m)	20 (6.1m)	30 (9.1m)
105 (32.0m)	21 (6.4m)	30 (9.1m)
110 (33.5m)	22 (6.7m)	30 (9.1m)
115 (35.1m)	23 (7.0m)	30 (9.1m)
120 (36.6m)	24 (7.3m)	30 (9.1m)

NOTES: ALL DIMENSIONS ARE IN FEET (METERS).  
 TRUCK LOADING SHOWN FOR SIMPLICITY  
 DIMENSIONS APPLY TO ALL TYPES OF SHIPMENTS.

**GROUT FOR STRUCTURES**

(7-12-07)

**1.0 DESCRIPTION**

This special provision addresses grout for use in structures, including continuous flight auger (CFA) piles, micropiles, soil nail and anchored retaining walls and backfilling crosshole sonic logging (CSL) tubes or grout pockets, shear keys, dowel holes and recesses for cored slabs and box beams. This provision does not apply to grout placed in post-tensioning ducts for bridge beams, girders, or decks. Provide grout composed of portland cement, water and at the Contractor’s option, fine aggregate and/or pozzolan. If necessary, use set controlling admixtures. Proportion, mix and place grout in accordance with the plans, the applicable section of the *Standard Specifications* or special provision for the application and this provision.

**2.0 MATERIALS**

Refer to Division 10 of the *Standard Specifications*:

<b>Item</b>	<b>Article</b>
Portland Cement	1024-1
Water	1024-4
Fine Aggregate	1014-1
Fly Ash	1024-5
Ground Granulated Blast Furnace Slag	1024-6
Admixtures	1024-3

At the Contractor’s option, use an approved packaged grout in lieu of the materials above with the exception of the water. Contact the Materials and Tests (M&T) Unit for a list of approved packaged grouts. Consult the manufacturer to determine if the packaged grout selected is suitable for the application and meets the compressive strength and shrinkage requirements.

**3.0 REQUIREMENTS**

Unless required elsewhere in the Contract, provide non-metallic grout with minimum compressive strengths as follows:

<b>Property</b>	<b>Requirement</b>
Compressive Strength @ 3 days	2500 psi (17.2 MPa)
Compressive Strength @ 28 days	4500 psi (31.0 MPa)

For applications other than micropiles, soil nails and ground anchors, use non-shrink grout with shrinkage of less than 0.15%.

When using approved packaged grout, a grout mix design submittal is not required. Submit grout mix designs in terms of saturated surface dry weights on M&T Form 312U in accordance with the applicable section of the *Standard Specifications* or special provision for the structure. Use an approved testing laboratory to determine the grout mix proportions. Adjust proportions to compensate for surface moisture contained in the aggregates at the time of mixing. Changes in the saturated surface dry mix proportions will not be permitted unless a revised grout mix design submittal is accepted.

For each grout mix design, provide laboratory test results for compressive strength, density, flow and if applicable, aggregate gradation and shrinkage. Submit compressive strength for at least 3 cube and 2 cylinder specimens at the age of 3, 7, 14 and 28 days for a total of at least 20 specimens tested. Perform laboratory tests in accordance with the following:

Property	Test Method
Compressive Strength	AASHTO T106 and T22
Density	AASHTO T133
Flow for Sand Cement Grout	ASTM C939 (as modified below)
Flow for Neat Cement Grout (no fine aggregate)	Marsh Funnel and Cup API RP 13B-1, Section 2.2
Aggregate Gradation for Sand Cement Grout	AASHTO T27
Shrinkage for Non-shrink Grout	ASTM C1090

When testing grout for flow in accordance with ASTM C939, modify the flow cone outlet diameter from 1/2 to 3/4 inch (13 to 19 mm).

When grout mix designs are submitted, the Engineer will review the mix designs and notify the Contractor as to their acceptability. Do not use grout mix designs until written acceptance has been received. Acceptance of grout mix designs or use of approved packaged grouts does not relieve the Contractor of responsibility to furnish a product that meets the Contract requirements.

Upon written request from the Contractor, a grout mix design accepted and used satisfactorily on a Department project may be accepted for use on other projects.



**4.0 SAMPLING AND PLACEMENT**

The Engineer will determine the locations to sample grout and the number and type of samples collected for field and laboratory testing. Use API RP 13B-1 for field testing grout flow and density of neat cement grout. The compressive strength of the grout will be considered the average compressive strength test results of 3 cube or 2 cylinder specimens at 28 days.

Do not place grout if the grout temperature is less than 50°F (10°C) or more than 90°F (32°C) or if the air temperature measured at the location of the grouting operation in the shade away from artificial heat is below 40°F (4°C).

Provide grout at a rate that permits proper handling, placing and finishing in accordance with the manufacturer’s recommendations unless directed otherwise by the Engineer. Use grout free of any lumps and undispersed cement. Agitate grout continuously before placement.

Control grout delivery so the interval between placing batches in the same component does not exceed 20 minutes. Place grout before the time between adding the mixing water and placing the grout exceeds that in the table below.

<b>ELAPSED TIME FOR PLACING GROUT</b>		
<b>(with continuous agitation)</b>		
<b>Air or Grout Temperature Whichever is Higher</b>	<b>Maximum Elapsed Time</b>	
	<b>No Set Retarding Admixture Used</b>	<b>Set Retarding Admixture Used</b>
90°F (32°C) or above	30 min.	1 hr. 15 min.
80°F (27°C) through 89°F (31°C)	45 min.	1 hr. 30 min.
79°F (26°C) or below	60 min.	1 hr. 45 min.

**5.0 MISCELLANEOUS**

Comply with Articles 1000-9 through 1000-12 of the *Standard Specifications* to the extent applicable for grout in lieu of concrete.

**DECORATIVE CONCRETE AND METAL RAIL****(SPECIAL)****1.0 GENERAL**

The work covered by this special provision consists of the construction of the decorative concrete and metal rails on the bridge and the metal rail at the retaining wall in accordance with the plans, Standard Specifications, and this Special Provision. Plans for the concrete bridge rails are detailed for cast-in-place concrete and must be placed using conventional forms.

**2.0 METAL RAIL**

The metal rail shall be in accordance with the Standard Specifications except the rail shall be made of AASHTO M270 Grade 50 steel and painted in lieu of aluminum or galvanized steel. The paint used shall be System 3 of Article 442-1 of the Standard Specifications.

**3.0 CONCRETE BRIDGE RAIL**

Concrete for the concrete bridge rail shall meet the requirements for class AA concrete of the Standard Specifications. The bridge rails shall be constructed to the established shape, line, grade, and dimensions shown on the plans. Joints in the rails shall be placed at the locations specified on the plans.

All concrete surfaces are finished as directed by the Engineer.

**4.0 ROCK FACING**

Cherokee DOT shall be responsible for the rock facing of the bridge rails.

**5.0 BASIS OF PAYMENT**

Payment for the Decorative Concrete and Metal Rail, excluding the rock facing, will be at the lump sum price bid for "Decorative Concrete and Metal Rail," which price and payment shall be full compensation for all materials, forms, falsework, fabrication and erection of all metal rails including the metal rail at the retaining wall, painting, tools, labor, equipment and incidentals necessary to complete the work.

**HEAT CURVING GIRDERS FOR BRIDGES AT  
STATIONS 15+15.00 -L-**

(10-12-01)

**1.0 TYPE OF HEATING**

With approval, use continuous or V-type heating methods to curve girders. For the continuous method, simultaneously heat a strip along the edge of the top and bottom flanges that is of sufficient width and temperature to obtain the required curvature. For V-type heating, heat the top and bottom flanges simultaneously in truncated triangular or wedge-shaped areas. Position the areas with their base along the flange edge and spaced at regular intervals along each flange. Set the spacing and temperature so as to approximate the required curvature by a series of short chords. Heat along the top and bottom flanges at approximately the same rate.

For V-type heating, terminate the apex of the truncated triangular area applied to the inside flange surface just before the juncture of the web and flange. To avoid web distortion, make certain that heat is not applied directly to the web when heating the inside flange surfaces (the surfaces that intersect the web). Extend the apex of the truncated triangular heating pattern applied to the outside flange surface to the juncture of the flange and web. Use an included angle of approximately 15 to 30 degrees in the truncated triangular pattern, but do not allow the base of the triangle to exceed 10 inches (254 mm). Vary the patterns prescribed above only with the Engineer's approval.

For both types of heating, heat the flange edges that will be on the inside of the horizontal curve after cooling. Concurrently heat both inside and outside flange surfaces for flange thickness  $1\frac{1}{4}$  inches (32 mm) and greater. Adhere to the temperature requirements presented below.

**2.0 TEMPERATURE**

Conduct the heat-curving operation so that the temperature of the steel never exceeds 1150°F (620°C) as measured by temperature indicating crayons or other suitable means. Do not artificially cool the girder until it naturally cools to 600°F (315°C); Use dry compressed air to artificially cool the girder.

**3.0 POSITION FOR HEATING**

Heat-curving the girder with the web in either a vertical or horizontal position is permitted. When curved in the vertical position, brace or support the girder so that the tendency of the girder to deflect laterally during the heat-curving process does not cause the girder to overturn.

When curved in the horizontal position, support the girder near its ends and at intermediate points, if required, to obtain a uniform curvature. Do not allow the bending stress in the flanges to exceed 27,000 psi (186.2 MPa). To prevent a sudden sag due to plastic flange buckling when the girder is positioned horizontally for heating, place intermediate safety catch blocks at the midlength of the girder within 2 inches (50 mm) of the flanges at all times during the heating process.

#### **4.0 SEQUENCE OF OPERATIONS**

Conduct the heat-curving operation either before or after completing all the required welding of transverse intermediate stiffeners to the web. However, unless provisions are made for shrinkage, position and attach connection plates and bearing stiffeners after heat-curving. In any event, weld the stiffeners, connection plates, and bearing stiffeners to the girder flanges after the member is curved. If longitudinal stiffeners are required, heat-curve or oxygen-cut these stiffeners separately prior to welding to the curved girder.

#### **5.0 CAMBER**

Camber the girders before heat-curving. Cut the web to the prescribed camber allowing for shrinkage due to cutting welding, and heat-curving. If approved, a carefully supervised application of heat is permitted to correct moderate deviations from the specified camber.

#### **6.0 MEASUREMENT OF CURVATURE AND CAMBER**

Horizontal curvature and vertical camber is measured for final acceptance after all welding and heating operations are complete and the flanges have cooled to a uniform temperature. Horizontal curvature is checked with the web in the vertical position by measuring offsets from a string line or wire attached to both flanges or by using other suitable means. Camber is checked with the web in the horizontal position. Camber the girder so that it meets the horizontal and vertical curvature ordinates without inducing stress into the girders by mechanical force.

Compensate for loss of camber in the heat-curved girders as residual stresses dissipate during service life of the structure. This anticipated loss of camber is computed in accordance with the AASHTO Standard Specifications for Highway Bridges as shown on the plans.

Clearly define the deviation (d) from curvature which should be reasonably close to what AWS allows: 1/8 inch per 10 foot (3 mm per 3.05 m) length.

#### **7.0 PROCEDURE SPECIFICATION AND SHOP DRAWINGS**

Submit structural steel shop drawings, including a detailed written procedure specification for heat curving the girders, supplemented by calculations and sketches, for review, comments and acceptance. On the shop drawings, indicate the type, location, and spacing of heat sectors, if used, supports, and catch blocking for each field section of girders. Also include suitable blocking diagrams for measuring horizontal curvature similar to those usually prepared for camber and vertical curvature.

**HIGH STRENGTH BOLTS****(11-17-06)**

In Section 440-8(A) of the Standard Specifications, revise the third paragraph and insert a new paragraph four, respectively, as follows:

“Make sure that plain bolts and washers have a thin coat of lubricant at the time of installation.”

“Use nuts that are pre-waxed by the producer/supplier prior to shipping to the project.”

**AESTHETICALLY TREATED CONCRETE SIDEWALKS****(SPECIAL)****1.0 GENERAL**

The work covered by this special provision consists of coloring and imprinting with a brick pattern the horizontal surface of the sidewalks in accordance with the plans, the Standard Specifications and this Special Provision.

All horizontal surfaces of the sidewalks shall be colored, imprinted, textured and sealed within the limit shown on the plans. The brick pattern, color and texture shall be selected and approved by the Cherokee Department of Transportation and the Engineer. The Contractor shall follow the manufacturer’s specifications for the product selected to achieve the color, texture, and imprinted pattern. The seal shall be applied in accordance with the manufacturer’s recommendations immediately after completing the imprinting process.

All work shall be installed by a contractor who shall provide a foreman or supervisor who has a minimum of three years of experience with imprinted and textured concrete, and who has successfully completed at least five imprinted concrete installations of high quality, similar in scope herein. Evidence that the contractor is qualified to comply with the requirements specified herein shall be submitted to the Engineer for approval.

The Contractor shall provide a job- site sample (10 square feet minimum) to be approved by the Cherokee DOT and the Engineer prior to construction of the sidewalks. Said sample shall be the standard for the work installed, and shall be protected against damage until final approval from the Engineer.

**2.0 MEASUREMENT AND PAYMENT**

No separate measurement or payment will be made for the above work. The entire cost of the aesthetically treated concrete sidewalks shall be included in the construction cost of the sidewalks which will be full compensation for all work covered by this special provision and Standard Specifications. The cost for the concrete, reinforcing steel, labor, equipment, tools, and incidentals necessary to place the aesthetically treated concrete sidewalks are included in the square foot cost of Reinforced Concrete Deck Slab or the lump sum cost of Bridge Approach Slabs.

**18" STEEL SHEET PILES**

**(SPECIAL)**

Complete all work in accordance with the contract plans and Section 452 of the Standard Specifications except measurement and payment for the steel sheet piles will be as described below.

Sheet piles will be measured and paid as the actual number of square feet of sheet piles completed and accepted. In determining this quantity, the sheet pile lengths used in the computation are the sheet pile lengths shown on the plans. The sheet pile heights are measured as the difference between the top elevation as shown on the plans and the bottom of the steel sheet piles.

Payment will be made under:

18" Steel Sheet Piles .....Sq. Ft.

PROJECT SPECIAL PROVISIONS  
ELECTRICAL CONDUIT SYSTEM

August 4, 2004

## DESCRIPTION:

The work covered by this section consists of furnishing and installing a conduit system embedded in concrete, for light standards to be installed by others. Perform all work in accordance with these special provisions, the plans, the National Electrical Code (NEC), and Division 14 of the North Carolina Department of Transportation "Standard Specifications for Roads and Structures."

The Contractor actually performing the work described in these special provisions shall have a license of the proper classification from the North Carolina State Board of Examiners of Electrical Contractors.

The licensed Electrical Contractor must be available on the job site when the work is being performed or when requested by the Engineer. The licensed Electrical Contractor shall have a set of plans and special provisions in his possession on the job site, and must maintain accurate "as built" plans.

## MATERIALS

Submit eight (8) copies of catalog cuts and/or drawings for all proposed materials for the Engineer's review and approval. Include the brand name, stock number, description, size, rating, manufacturing specification, and applicable contract item number(s) on each submittal. Allow forty (40) days for submittal review. The Engineer will advise the Contractor of reasons for rejected submittals and will return approved submittals to the Contractor. Do not deliver material to the project prior to submittal approval.

Conduit shall be non-metallic, rigid PVC (Polyvinyl chloride), Schedule 40, approved for above ground and underground use without concrete encasement per U.L. 651 "Rigid Non-Metallic Conduit". Use Terminations designed for PVC conduit, to seal and stub out each PVC conduit, and to provide watertight protection.

Provide expansion fittings of the appropriate size as noted in the plans. Expansion fittings shall be weatherproof, designed for Schedule 40 PVC conduit and provide 100mm (4") minimum of conduit movement.

Type SW junction box shall be cast iron, hot-dipped galvanized with external recess flange for flush mounting sized as shown on the plans. It shall have a neoprene gasketed cover with brass or stainless steel screws and shall be suitable for a watertight installation. A mounting button with a blind tapped bolt hole shall be provided on the interior for future connection of a grounding lug. The junction box shall have a replaceable flange and checkered cover made to withstand pedestrian and light vehicular traffic. The flanges and covers shall be a standardized design so that replacement can be done without disturbing the box or conduit system. Provide

pull lines specifically designed for pulling rope through conduit. Use pull lines made of 2-ply line, with a tensile strength of 109kg (240 pounds) minimum. Use rot and mildew resistant pull lines that are resistant to tangling when being dispensed.

Use mastic that is a permanent, non-hardening, water sealing compound that adheres to metal, plastic, and concrete.

Provide zinc rich paint conforming to Section 1080-9 of the Standard Specifications.

The Eastern Band of the Cherokee Indians will provide light standard anchor bolts.

### CONSTRUCTION METHODS

All conduit and boxes shall be securely fastened with ties prior to placing any concrete. After the conduit is encased in concrete, the Contractor shall clean each conduit by snaking with a steel band to which shall be attached an approved tube cleaner equipped with a mandrel of a diameter not less than 85% of the nominal inside diameter of the conduit. To ensure against corrosion in the areas where hot dipped galvanizing has been damaged, cover all raw metal surfaces with a cold galvanized, zinc rich paint. Install mastic around the flange of the junction box as indicated on the plans before placing concrete.

Stub the conduit out at an accessible location and seal with termination kits designed specifically for that purpose. Use termination kits of the same material as the conduit. Place backfill in accordance with Section 300-7 of the Standard Specifications. Conduit may enter junction boxes through field drilled holes protected with zinc rich paint before the conduit is inserted. Use threaded adapter and insulating bushing at all junction box to conduit connections. Install a pull line in each conduit for future use. Leave sufficient slack for attachment of a rope that will be used to install conductors. Coordinate electrical conduit system work with work by others, and allow installation of light standards, luminaires and circuitry as directed by the Engineer.

Install anchor bolts according to light standard manufacturer's specifications.

All work must be inspected and approved by the Engineer before concealment.

### METHOD OF MEASUREMENT

No direct measurement will be made for the conduit system, since it will be paid for on a lump sum basis.

### BASIS OF PAYMENT

Lump Sum Basis:

Payment for the conduit system will be made at the contract lump sum price for "Electrical Conduit System at Station \_\_\_\_\_".

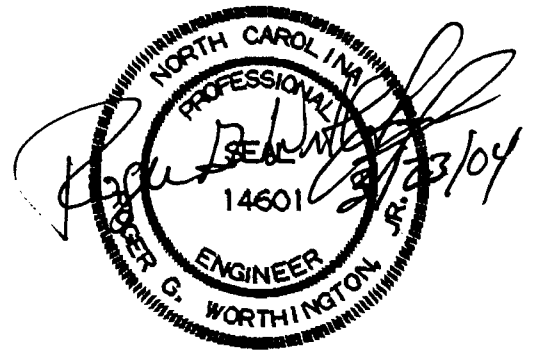
Compensation:



Such price and payment for the conduit system as provided above will be considered full compensation for all materials, equipment, and labor necessary to complete the work in accordance with the plans and these special provisions.

Payment will be made under:

Electrical Conduit System at Station \_\_\_\_\_ Lump Sum



Project: B-4696  
County: SWAIN

## PROJECT SPECIAL PROVISIONS

### Utility Construction 12" DI RJ WATER MAIN BRIDGE STATION 15+15.00

#### 1. SCOPE OF WORK

The Contractor shall be responsible for furnishing all equipment, labor, and materials necessary to install a 12" ductile iron restrained joint water main from station 13+96.88 to station 16+17.04 -L- including attachment of the water main to the proposed bridge at station 15+15.00 -L-.

All work shall be performed in accordance with the Structure Plans, these Special Provisions, the S-N sheet and as directed by the Engineer and subject to the approval of the Owner.

#### 2. GENERAL CONSTRUCTION REQUIREMENTS

##### MATERIAL SPECIFICATION

When brand names of materials have been determined, the Contractor shall obtain approval, through the engineer, from the State Design Services Engineer and the owner prior to their use and/or installation.

The Contractor shall furnish, but is not limited to furnishing, catalog cuts and/or shop drawings of the materials. Forty days shall be allowed for the engineer's review of each submittal, eight copies of each catalog cut and/or shop drawing (signed and sealed) shall be submitted.

##### PIPE SLEEVES

The pipe sleeves shall be of the thickness shown on the plans and shall conform to the requirements of ASTM A53 Grade B or API 5L Grade B.

The pipe sleeves are to be installed in the endwalls as shown on the plans. Both ends of the sleeves shall be flush with both faces of the endwalls, and vertical grade that the main will be installed. If not fabricated to fit, a torch shall be used to cut the ends of the sleeves, and protective shields are to be placed on both faces of the endwalls.

All rough edges on the ends and inside of the sleeves caused by cutting shall be removed by filing or reaming.

#### CONCRETE INSERTS

Concrete inserts shall be galvanized in accordance with Section 1076 and have a minimum working load tension capacity of 3.147 kips for 7/8" rods. Concrete Inserts shall be installed in strict compliance with the recommendations specified by the manufacturer.

#### HARDWARE

Bolts, nuts, and washers shall be high strength and galvanized in accordance with Article 1072-7 of Standard Specifications.

#### HANGER RODS

The hanger rods shall be galvanized in accordance with Section 1076, threaded on both ends or threaded continuously and conforming to ASTM-A36 or A-575. The hanger rods shall be of a proper length so as to place the main at the vertical location shown on the plans.

#### PIPE HANGERS

The pipe hangers shall be an approved adjustable steel yoke pipe hanger type capable of supporting the RJ ductile iron water main and accepting the hanger rod. The entire hanger assembly is to be galvanized except the pipe yoke, which is to be coated with a minimum of 5 mils thickness of polyvinyl chloride or neoprene.

#### DUCTILE IRON "RESTRAINED JOINT" WATER PIPE

Ductile Iron Restrained Joint Water Pipe shall be of the thickness class and pressure rating shown on the utility plans and shall conform to ANSI A21.51 (AWWA C151) Push-on joints for such pipe shall be in accordance with ANSI A21.11 (AWWA C111). Pipe thickness shall be designed in accordance with ANSI A21.50 (AWWA C150) and based on laying conditions and internal pressures as stated on the plans.

Cement mortar lining and seal coating for pipe shall be in accordance with ANSI A21.4 (AWWA C104). Bituminous outside coating shall be in accordance with ANSI A21.51 (AWWA C151).

All Ductile Iron Restrained Joint Water Pipe shall be installed in accordance with laying condition Type 2 as stated in ANSI A21.51 (AWWA C151) unless otherwise shown on the plans.

Restraining joints shall be boltless, flexible type design using an integrally cast extended bell. Restraining gaskets are not acceptable.

Ductile iron restrained joint pipe shall be manufactured to the lengths required. Cutting of ductile iron pipe by the contractor will not be allowed.

#### INSULATION AND ALUMINUM WRAP FOR PIPE ATTACHED TO BRIDGE

The insulation for the pipe attached to the bridge shall be an approved closed cell/hard foam type material with a minimum R value of 6.0. This insulation will have vapor barrier included to prevent condensation into insulating material. Any voids created by notching and fitting the insulation at braces shall be completely filled with spray Urethane Foam or an approved equal. The aluminum wrap shall be of an approved type 0.016 inch thick and shall be held firmly in place by aluminum banding tape or by an approved alternative. The insulation shall not be installed until after the Pressure Test and Leakage Test have been successfully completed.

#### ENDWALL SEAL

Seals shall be placed to fill the annular space between the carrier pipe and the endwalls, as indicated on the plans. The seal shall be of an approved link, lock or modular sleeve and casing type. Seals shall be modular mechanical type consisting of interlocking synthetic or rubber links shaped to continuously fill the annular space between the carrier pipe and the endwall. Links shall be loosely assembled with bolts to form a continuous rubber belt around the pipe with a pressure plate under each bolt head and nut. The seal shall be recessed a minimum of 2" from the fill face of the endwalls. After the seal assembly is positioned around the carrier pipe, tightening of the bolts shall cause the rubber sealing elements to expand and provide a watertight seal between the carrier pipe and the endwall. The seal shall be constructed so as to provide insulation between the pipe and the endwall, thus preventing the flow of stray currents from the main to the bridge. See Utility Attachment Detail Sheet UC-5.

#### PAINTING

The surface of all exposed metallic materials that are not galvanized or otherwise coated shall be painted in accordance with the Standard Specifications using the materials specified herein.

Surface Preparation – Surface preparation shall be in accordance with Subarticle 442-8(A) of the Standard Specifications.

Shop Paint – All steel shall have shop coat of self-curing inorganic zinc paint conforming to Article 1080-7 of the Standard Specifications.

Field paint – All surfaces shall be given two (2) coats of a minimum dry thickness of 1 ½ mils for each coat of an approved type zinc paint conforming to Section 1080-9 of the Standard Specifications.

Pressure Test, Leakage Test and Sterilization

The pressure test, leakage test and sterilization of the main shall be performed as provided for in the Standard Specifications.

**3. Method of Measurement and Basis of Payment**

Measurement and Payment shall be made at the contract lump sum price bid for "12" DI RJ Water Main Attachment" which includes furnishing and installing the water main, steel pipe sleeves, insulation, hangers, inserts, labor, materials, and incidentals. The lump sum bid price shall be full compensation for all materials, equipment and labor necessary to complete the work in accordance with the plans, Specifications, and as directed by the Engineer.