

STATE OF NORTH CAROLINA  
DIVISION OF HIGHWAYS

STATE PROJECT REFERENCE NO.	SHEET NO.
I-2810	TCP-1

**PLAN FOR PROPOSED  
TRAFFIC CONTROL, MARKING & DELINEATION  
VANCE COUNTY**

I-2810

**ROADWAY STANDARD DRAWINGS**

THE FOLLOWING ROADWAY STANDARDS AS APPEAR IN "ROADWAY STANDARD DRAWINGS"- PROJECT SERVICES UNIT-N.C. DEPARTMENT OF TRANSPORTATION-RALEIGH, N.C., DATED JULY 2006 ARE APPLICABLE TO THIS PROJECT AND BY REFERENCE HEREBY ARE CONSIDERED A PART OF THESE PLANS:

STD. NO.	TITLE
1101.02	TEMPORARY LANE CLOSURES
1101.03	TEMPORARY ROAD CLOSURES
1101.04	TEMPORARY SHOULDER CLOSURES
1101.05	WORK ZONE VEHICLE ACCESSES
1101.11	TRAFFIC CONTROL DESIGN TABLES
1110.01	STATIONARY WORK ZONE SIGNS
1110.02	PORTABLE WORK ZONE SIGNS
1115.01	FLASHING ARROW PANELS
1130.01	DRUM
1135.01	CONES
1145.01	BARRICADES
1150.01	FLAGGING DEVICES
1165.01	TRUCK MOUNTED IMPACT ATTENUATOR
1170.01	PORTABLE CONCRETE BARRIER
1180.01	SKINNY-DRUM
1205.01	PAVEMENT MARKINGS - LINE TYPES & OFFSETS
1205.02	PAVEMENT MARKINGS - 2 LANE & MULTILANE ROADWAYS
1205.03	PAVEMENT MARKINGS - INTERCHANGES
1205.04	PAVEMENT MARKINGS - INTERSECTIONS
1205.05	PAVEMENT MARKINGS - TURN LANES
1205.08	PAVEMENT MARKINGS - SYMBOLS & WORD MESSAGES
1250.01	PAVEMENT MARKER SPACING
1251.01	RAISED PAVEMENT MARKERS (TEMPORARY & PERMANENT)
1253.01	SNOWPLOWABLE RAISED PAVEMENT MARKERS
1261.01	GUARDRAIL & BARRIER DELINEATOR SPACING
1261.02	GUARDRAIL & BARRIER DELINEATOR TYPES
1262.01	GUARDRAIL END DELINEATION
1267.01	FLEXIBLE DELINEATOR INSTALLATION
1267.02	FLEXIBLE DELINEATOR SPACING
1267.03	FLEXIBLE DELINEATOR-INTERCHANGES

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- GENERAL**
- DIRECTION OF TRAFFIC FLOW
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  - PROPOSED PVMT. EXIST. PVMT.
  - WORK AREA
  - REMOVAL OF EXISTING PAVEMENT
- TRAFFIC CONTROL DEVICES**
- TYPE I BARRICADE
  - TYPE II BARRICADE
  - TYPE III BARRICADE
  - CONE
  - DRUM SKINNY DRUM
  - FLASHING ARROW PANEL (TYPE C)
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  - PORTABLE SIGN
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  - CRASH CUSHION
  - CHANGEABLE MESSAGE SIGN
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  - POLICE
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- CRYSTAL/CRYSTAL PAVEMENT MARKER
  - YELLOW/YELLOW PAVEMENT MARKER
  - CRYSTAL/RED PAVEMENT MARKER
  - PAVEMENT MARKING SYMBOLS

TIP PROJECT:

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APPROVED:	PLAN PREPARED BY: N.C.D.O.T. WORK ZONE TRAFFIC CONTROL UNIT
DATE: 6/26/07	
SEAL	J. S. BOURNE, PE TRAFFIC CONTROL ENGINEER
	G. L. GETTIER, PE TRAFFIC CONTROL PROJECT ENGINEER
	J. W. WOOLARD, PE TRAFFIC CONTROL PROJECT DESIGN ENGINEERS
	L. K. DONALDSON TRAFFIC CONTROL DESIGN ENGINEER / TECHNICIAN

# GENERAL NOTES

PROJ. REFERENCE NO. I-2810	SHEET NO. TCP-2
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CHANGES MAY BE REQUIRED WHEN PHYSICAL DIMENSIONS IN THE DETAIL DRAWINGS, STANDARD DETAILS AND ROADWAY DETAILS ARE NOT ATTAINABLE TO MEET FIELD CONDITIONS, OR RESULT IN DUPLICATE, OR UNDESIRABLE OVERLAPPING OF DEVICES. MODIFICATION MAY INCLUDE: MOVING, SUPPLEMENTING, COVERING OR REMOVAL OF DEVICES, AS DIRECTED BY THE ENGINEER.

THE FOLLOWING GENERAL NOTES APPLY AT ALL TIMES FOR THE DURATION OF THE CONSTRUCTION PROJECT EXCEPT WHEN OTHERWISE NOTED IN THE PLAN OR AS DIRECTED BY THE ENGINEER.

### TIME RESTRICTIONS

- A) DO NOT CLOSE OR NARROW TRAVEL LANES UTILIZING PORTABLE CONCRETE BARRIER (PCB) AS FOLLOWS:

ROAD NAME	DAY AND TIME RESTRICTIONS
1. I-85	BETWEEN NOVEMBER 15 AND MARCH 15

- B) DO NOT CLOSE OR NARROW TRAVEL LANES AS FOLLOWS:

ROAD NAME	DAY AND TIME RESTRICTIONS
1. DABNEY DRIVE	MONDAY THROUGH SATURDAY 11:00 AM TO 7:00 PM

- C) DO NOT CLOSE OR NARROW TRAVEL LANES DURING HOLIDAYS AND SPECIAL EVENTS UTILIZING DRUMS AS FOLLOWS:

ROAD NAME
1. I-85 AND ALL RAMPS & LOOPS
2. ALL -Y- LINES

#### HOLIDAY

1. FOR ANY UNEXPECTED OCCURRENCE THAT CREATES UNUSUALLY HIGH TRAFFIC VOLUMES, AS DIRECTED BY THE ENGINEER.
2. FOR NEW YEAR'S, BETWEEN THE HOURS OF 4:00 PM DECEMBER 31st TO 9:00 AM JANUARY 2ND. IF NEW YEAR'S DAY IS ON A FRIDAY, SATURDAY, SUNDAY, OR MONDAY THEN UNTIL 9:00 AM THE FOLLOWING TUESDAY.
3. FOR EASTER, BETWEEN THE HOURS OF 4:00 PM THURSDAY AND 9:00 AM MONDAY.
4. FOR MEMORIAL DAY, BETWEEN THE HOURS OF 4:00 PM FRIDAY TO 9:00 AM TUESDAY.
5. FOR INDEPENDENCE DAY, BETWEEN THE HOURS OF 4:00 PM THE DAY BEFORE INDEPENDENCE DAY AND 9:00 AM THE DAY AFTER INDEPENDENCE DAY.  
  
IF INDEPENDENCE DAY IS ON A FRIDAY, SATURDAY, SUNDAY, OR MONDAY THEN BETWEEN THE HOURS OF 4:00 PM THE THURSDAY BEFORE INDEPENDENCE DAY AND 9:00 AM THE TUESDAY AFTER INDEPENDENCE DAY.
6. FOR LABOR DAY, BETWEEN THE HOURS OF 4:00 PM FRIDAY AND 9:00 AM TUESDAY.
7. FOR THANKSGIVING DAY, BETWEEN THE HOURS OF 4:00 PM TUESDAY TO 9:00 AM MONDAY.
8. FOR CHRISTMAS, BETWEEN THE HOURS OF 4:00 PM THE FRIDAY BEFORE THE WEEK OF CHRISTMAS DAY AND 9:00 AM THE FOLLOWING TUESDAY AFTER THE WEEK OF CHRISTMAS.

- D) DO NOT STOP TRAFFIC AS FOLLOWS:

ROAD NAME	DAY AND TIME RESTRICTIONS	DURATION AND OPERATION
1. I-85 AND ALL RAMPS & LOOPS	NONE	10 MINUTES/ TRAFFIC SHIFTS

- E) DO NOT CONDUCT ANY HAULING OPERATIONS AGAINST THE FLOW OF TRAFFIC OF AN OPEN TRAVELWAY UNLESS THE HAULING OPERATION IS PROTECTED BY BARRIER OR GUARDRAIL OR AS DIRECTED BY THE ENGINEER.

### LANE AND SHOULDER CLOSURE REQUIREMENTS

- F) REMOVE LANE CLOSURE DEVICES FROM THE LANE WHEN WORK IS NOT BEING PERFORMED BEHIND THE LANE CLOSURE OR WHEN A LANE CLOSURE IS NO LONGER NEEDED, OR AS DIRECTED BY THE ENGINEER.
- G) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN 15 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN SHOULDER USING ROADWAY STANDARD DRAWING NO. 1101.04 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL OR A LANE CLOSURE IS INSTALLED.
- H) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING ON THE SHOULDER ADJACENT TO AN UNDIVIDED FACILITY AND WITHIN 5 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN TRAVEL LANE USING ROADWAY STANDARD DRAWING NO. 1101.02 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.

WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING ON THE SHOULDER ADJACENT TO A DIVIDED FACILITY AND WITHIN 10 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN TRAVEL LANE USING ROADWAY STANDARD DRAWING NO. 1101.02 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.

- I) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN A LANE OF TRAVEL OF AN UNDIVIDED OR DIVIDED FACILITY, CLOSE THE LANE ACCORDING TO THE TRAFFIC CONTROL PLANS, ROADWAY STANDARD DRAWINGS OR AS DIRECTED BY THE ENGINEER. CONDUCT THE WORK SO THAT ALL PERSONNEL AND/OR EQUIPMENT REMAIN WITHIN THE CLOSED TRAVEL LANE.
- J) DO NOT WORK SIMULTANEOUSLY WITHIN 15 FT ON BOTH SIDES OF AN OPEN TRAVELWAY, RAMP OR LOOP WITHIN THE SAME LOCATION UNLESS PROTECTED WITH GUARDRAIL OR BARRIER.
- K) DO NOT INSTALL MORE THAN TWO SIMULTANEOUS LANE CLOSURES, IN ANY ONE DIRECTION, ON I-85.
- L) PROVIDE A MINIMUM OF TWO MILES BETWEEN LANE CLOSURES, MEASURED FROM THE END OF ONE CLOSURE TO THE FIRST SIGN OF THE NEXT LANE CLOSURE.
- M) PROVIDE TRAFFIC CONTROL FOR APPROPRIATE LANE CLOSURES FOR SURVEYING DONE BY THE DEPARTMENT.

### PAVEMENT EDGE DROP OFF REQUIREMENTS

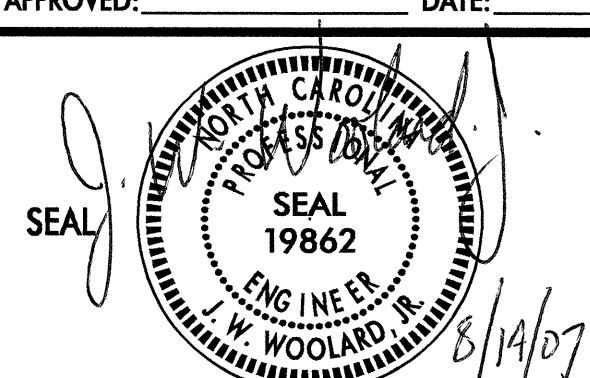
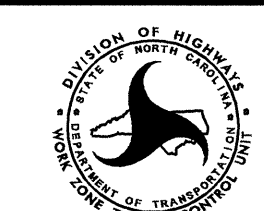
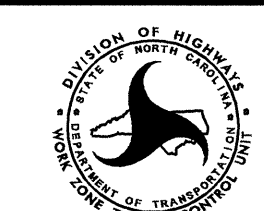
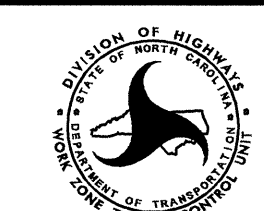
- N) BACKFILL AT A 6:1 SLOPE UP TO THE EDGE AND ELEVATION OF EXISTING PAVEMENT IN AREAS ADJACENT TO AN OPEN TRAVEL LANE THAT HAS A DROP-OFF AS FOLLOWS:

BACKFILL DROP-OFFS THAT EXCEED 2 INCHES ON ROADWAYS WITH POSTED SPEED LIMITS OF 45 MPH OR GREATER.

BACKFILL DROP-OFFS THAT EXCEED 3 INCHES ON ROADWAYS WITH POSTED SPEED LIMITS LESS THAN 45 MPH.

BACKFILL WITH SUITABLE COMPACTED MATERIAL, AS APPROVED BY THE ENGINEER, AT NO EXPENSE TO THE DEPARTMENT.

- O) DO NOT EXCEED A DIFFERENCE OF 2 INCHES IN ELEVATION BETWEEN OPEN LANES OF TRAFFIC FOR NOMINAL LIFTS OF 1.5 INCHES. INSTALL ADVANCE WARNING "UNEVEN LANES" SIGNS (W8-11) 1000 FT IN ADVANCE AND A MINIMUM OF ONCE EVERY HALF MILE THROUGHOUT THE UNEVEN AREA.

APPROVED: _____ DATE: _____  	<h2 style="margin: 0;">PROJECT NOTES</h2>										
SCALE: NONE DATE: APRIL 07 DWG. BY: LKD DESIGN BY: JWJ REVIEWED BY: JWJ	<table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <td style="width: 50%; text-align: center;">  </td> <td style="width: 50%; text-align: center;">                 REVISIONS  <table border="1" style="width: 100%; border-collapse: collapse;"> <tr><td style="width: 20px;"> </td><td> </td></tr> <tr><td> </td><td> </td></tr> <tr><td> </td><td> </td></tr> <tr><td> </td><td> </td></tr> </table> </td> </tr> </table>		REVISIONS <table border="1" style="width: 100%; border-collapse: collapse;"> <tr><td style="width: 20px;"> </td><td> </td></tr> <tr><td> </td><td> </td></tr> <tr><td> </td><td> </td></tr> <tr><td> </td><td> </td></tr> </table>								
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PHASE I

STEP 1: - INSTALL ALL WORK ZONE ADVANCE WARNING SIGNS, INCLUDING SPEED REDUCTION AND "\$250 PENALTY" SIGNS, ALONG I-85 AND ALL ON/OFF RAMP, AS SHOWN ON SHEETS TCP-18 THRU TCP-20.

- FURNISH AND INSTALL ALL DUAL MOUNTED "NO PARKING-TOW AWAY ZONE" SIGNS, AS SHOWN ON SHEET TCP-23, ON I-85 SHOULDERS EVERY 3,000 FEET, OR AS DIRECTED BY THE ENGINEER.
- PLACE DUAL CHANGEABLE MESSAGE SIGNS (CMS), IN ADVANCE OF THE "ROAD WORK AHEAD" SIGNS ON I-85, AS DIRECTED BY THE ENGINEER.
- PLACE ADDITIONAL CMS WITHIN THE PROJECT LIMITS, AS DIRECTED BY THE ENGINEER. THESE ADDITIONAL CMS WILL BE UTILIZED AS REQUIRED AND AS DIRECTED BY THE ENGINEER, TO INFORM MOTORISTS OF SPEED LIMIT REDUCTIONS, QUEUES, ACCIDENTS, RAMP/LOOP CLOSURES, ETC. WITHIN THE WORK ZONE.

CONTRACTOR SHALL REVIEW AND RECORD THE EXISTING PAVEMENT MARKINGS AND MARKERS PRIOR TO CONSTRUCTION. USE THIS RECORD OF EXISTING PAVEMENT MARKINGS AND MARKERS IN CONJUNCTION WITH THE ROADWAY STANDARD DRAWINGS TO RE-ESTABLISH THE PROPOSED PAVEMENT MARKINGS AND MARKERS UNLESS OTHERWISE DIRECTED BY THE ENGINEER.

STEP 2: - CONTRACTOR SHALL, USING ROADWAY STANDARD DRAWING NOS. 1101.02, SHEET 3 OF 9, AND 1101.04, PLACE SOUTHBOUND I-85 AND NORTHBOUND I-85 TRAFFIC IN A ONE-LANE, ONE-WAY TRAFFIC PATTERN IN THE OUTSIDE LANES OF SOUTHBOUND/NORTHBOUND I-85 RESPECTIVELY (SEE SHEET TCP-6, DETAIL-1).

- PAVE/RESURFACE/FILL-IN EXISTING RUMBLE STRIPS ON INSIDE SHOULDER OF SOUTHBOUND I-85 FROM STA. 24+00+/- -SBL1- TO STA. 106+00+/- -SBL1- AS SHOWN IN THE CONSTRUCTION PLANS.
- CONSTRUCT PROPOSED TEMPORARY MEDIAN CROSSOVERS (SEE CONSTRUCTION PLANS AND SHEETS TCP-5, AND TCP-6 DETAIL-1): -CR1N-, -CRLP1D-, -CRRP1D-, -CRRP2C-, -CRRP2D-, -CRRP3C-, AND -CR3N-.

NOTE: THE CONTRACTOR SHALL BACKFILL/SAFE-UP ALL SHOULDERS BY THE END OF EACH DAILY WORK PERIOD AS FOLLOWS:  
 BACKFILL AT A 6:1 SLOPE UP TO THE EDGE AND ELEVATION OF EXISTING PAVEMENT IN AREAS THAT EXCEED 2" ON ROADWAYS WITH POSTED SPEED LIMITS OF 45 MPH OR GREATER.  
 BACKFILL WITH SUITABLE COMPACTED MATERIAL, APPROVED BY THE ENGINEER, AT NO EXPENSE TO THE DEPARTMENT.

NOTE: INSTALL DRUMS AND TYPE III BARRICADES TO KEEP CROSSOVERS CLOSED TO TRAFFIC.

CONTRACTOR SHALL, USING ROADWAY STANDARD DRAWING NOS. 1101.02, SHEET 3, 6 & 7 OF 9, AND 1101.04, PLACE SOUTHBOUND I-85 TRAFFIC IN A ONE-LANE, ONE-WAY TRAFFIC PATTERN IN THE INSIDE LANE OF SOUTHBOUND I-85 (SEE SHEET TCP-6, DETAIL-2).

- PAVE/RESURFACE/FILL-IN EXISTING RUMBLE STRIPS ON OUTSIDE SHOULDER OF SOUTHBOUND I-85 FROM STA. 24+00+/- -SBL1- TO STA. 106+00+/- -SBL1- AS SHOWN IN THE CONSTRUCTION PLANS.
- CONSTRUCT TEMPORARY 10 FOOT OUTSIDE SHOULDER FROM STA. 106+00 +/- -SBL1- (END OF EXISTING BRIDGE NUMBER 2) TO STA. 366+04 +/- -SBL2-, INCLUDING ALL ON/OFF-RAMPS AS SHOWN IN THE CONSTRUCTION PLANS UP TO THE EDGE AND ELEVATION OF EXISTING (SEE SHEETS TCP-6, DETAIL-2).

NOTE: THE CONTRACTOR SHALL BACKFILL/SAFE-UP ALL SHOULDERS BY THE END OF EACH DAILY WORK PERIOD AS FOLLOWS:  
 BACKFILL AT A 6:1 SLOPE UP TO THE EDGE AND ELEVATION OF EXISTING PAVEMENT IN AREAS THAT EXCEED 2" ON ROADWAYS WITH POSTED SPEED LIMITS OF 45 MPH OR GREATER.  
 BACKFILL WITH SUITABLE COMPACTED MATERIAL, APPROVED BY THE ENGINEER, AT NO EXPENSE TO THE DEPARTMENT.

CONTRACTOR SHALL WORK IN A CONTINUOUS MANNER TO COMPLETE THE WORK IN PHASE I, STEPS 3 THRU 6.

STEP 3: - CONTRACTOR SHALL, USING ROADWAY STANDARD DRAWING NOS. 1101.02, SHEET 3 OF 9, PLACE SOUTHBOUND I-85 TRAFFIC INTO A ONE-LANE, ONE-WAY PATTERN IN THE INSIDE LANE OF SOUTHBOUND I-85 FROM STA. 24+00+/- -SBL1- TO STA. 366+04+/- -SBL2- (SEE SHEET TCP-6, DETAIL-2).

- REMOVE EXISTING OUTSIDE EDGE LINE, INTERCHANGE ACCELERATION/DECELERATION & GORE AREA PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKINGS (PAINT & TAPE ON BRIDGE) & TEMPORARY RAISED PAVEMENT MARKERS ON THE OUTSIDE SHOULDER/LANE OF SOUTHBOUND I-85 FOR A ONE-LANE, ONE-WAY TRAFFIC PATTERN FROM STA. 24+00+/- -SBL1- TO STA. 366+04+/- -SBL2- (SEE SHEET TCP-6, DETAIL-2 AND ROADWAY STANDARD DRAWING NUMBER 1101.03, SHEET 4 OF 9).

NOTE: INSTALL TEMPORARY RAISED PAVEMENT MARKERS ONLY IN THE ACCELERATION/DECELERATION AND GORE AREAS.

CONTRACTOR SHALL WORK IN PHASE I, STEPS 4 AND 5 SIMULTANEOUSLY.

STEP 4: - CONTRACTOR SHALL, USING ROADWAY STANDARD DRAWING NOS. 1101.02, SHEET 3 OF 9 & 1101.03, SHEET 4 OF 9, PLACE SOUTHBOUND I-85 TRAFFIC INTO A ONE-LANE, ONE-WAY PATTERN IN THE OUTSIDE LANE/SHOULDER OF SOUTHBOUND I-85 FROM STA. 24+00+/- -SBL1- TO STA. 366+04+/- -SBL2- (SEE SHEETS TCP-5 & TCP-6, DETAIL-3).

- PLACE PORTABLE CONCRETE BARRIER (PCB) ON SOUTHBOUND I-85 FROM STA. 24+00+/- -SBL1- TO STA. 364+36+/- -NBL2- (SEE SHEETS TCP-5 & TCP-6, DETAIL-3 AND ROADWAY STANDARD DRAWING NUMBER 1101.03, SHEET 4 OF 9).

NOTE: INSTALL DELINEATORS ON PORTABLE CONCRETE BARRIER EVERY 50 FEET.

- RELAP EXISTING MEDIAN GUARDRAIL, REPLACE EXISTING MEDIAN GUARDRAIL END TREATMENTS AND ADJUST CABLE GUIDE RAIL, AS REQUIRED, FOR PLACING NORTHBOUND I-85 IN THE EXISTING INSIDE LANE OF SOUTHBOUND I-85 (SEE CONSTRUCTION PLANS).

- REMOVE EXISTING INSIDE EDGE LINE & SKIP LINES, AND PLACE TEMPORARY PAVEMENT MARKINGS (PAINT & TAPE ON BRIDGE) & TEMPORARY RAISED PAVEMENT MARKERS ON THE INSIDE LANE OF SOUTHBOUND I-85 FOR A ONE-LANE, ONE-WAY NORTHBOUND I-85 TRAFFIC PATTERN FROM STA. 24+00+/- -SBL1- TO STA. 364+36+/- -NBL2- (SEE SHEETS TCP-5 & TCP-6, DETAIL-3 AND ROADWAY STANDARD DRAWING NUMBER 1101.03, SHEET 4 & 5 OF 9).

NOTE: INSTALL TEMPORARY RAISED PAVEMENT MARKERS IN THE MEDIAN CROSSOVER AREA FROM 100 FEET BEFORE THE ACCELERATION/DECELERATION LANES, THRU THE MEDIAN CROSSOVER AND 100 FEET AFTER EACH MEDIAN CROSSOVER.

STEP 5: - CONTRACTOR SHALL, USING ROADWAY STANDARD DRAWING NOS. 1101.02, SHEETS 3 OF 9 & 1101.03, SHEET 4 & 5 OF 9, PLACE NORTHBOUND I-85 TRAFFIC INTO A ONE-LANE, ONE-WAY PATTERN IN THE OUTSIDE LANE OF NORTHBOUND I-85 AND PLACE/TIE-IN TEMPORARY PAVEMENT MARKINGS (PAINT) AND TEMPORARY RAISED PAVEMENT MARKERS FOR CROSSOVERS -CR1N-, -CRLP1D-, -CRRP1D-, -CRRP2C-, -CRRP2D-, -CRRP3C-, AND -CR3N- TO INSIDE LANE OF NORTHBOUND I-85.

NOTE: INSTALL TEMPORARY RAISED PAVEMENT MARKERS IN THE MEDIAN CROSSOVER AREA FROM 100 FEET BEFORE THE ACCELERATION/DECELERATION LANES, THRU THE MEDIAN CROSSOVER AND 100 FEET AFTER EACH MEDIAN CROSSOVER.

STEP 6: - CONTRACTOR SHALL, USING SHEET TCP-5A (FOR PLACEMENT OF DRUMS AND WARNING SIGNS TO KEEP EASTBOUND US-158 ON-RAMP TO NORTHBOUND I-85 OPEN TO TRAFFIC), ROADWAY STANDARD DRAWING NOS. 1101.02, SHEETS 3 OF 9, & 1101.03, SHEETS 4 & 5 OF 9, SHIFT NORTHBOUND I-85 TRAFFIC TO THE INSIDE LANE OF SOUTHBOUND I-85. PLACE/TIE-IN TEMPORARY PAVEMENT MARKINGS (PAINT) AND TEMPORARY RAISED PAVEMENT MARKERS FOR MEDIAN CROSSOVERS -CRLP1D-, -CRRP1D-, -CRRP2C-, -CRRP2D-, AND -CRRP3C- TO EXISTING RAMPS/LOOP (SEE SHEETS TCP-5, TCP-5A & TCP-7, DETAIL-4).

NOTE: INSTALL TEMPORARY RAISED PAVEMENT MARKERS IN THE MEDIAN CROSSOVER AREA FROM 100 FEET BEFORE THE ACCELERATION/DECELERATION LANES, THRU THE MEDIAN CROSSOVER AND 100 FEET AFTER EACH MEDIAN CROSSOVER.

NOTE: CONTRACTOR SHALL PLACE DRUMS TO DELINEATE TRAVELWAY FOR MEDIAN CROSSOVERS AND TEMPORARY RAMPS/LOOP TRAFFIC PATTERNS.

NOTE: TRAFFIC IS NOW IN A TWO-LANE, TWO-WAY PATTERN ON SOUTHBOUND I-85 WITH EASTBOUND US-158 ON-RAMP TO NORTHBOUND I-85 OPEN AND ALL RAMPS/LOOP OPEN UTILIZING MEDIAN CROSSOVERS (SEE SHEETS TCP-5, TCP-5A & TCP-7, DETAIL-4).

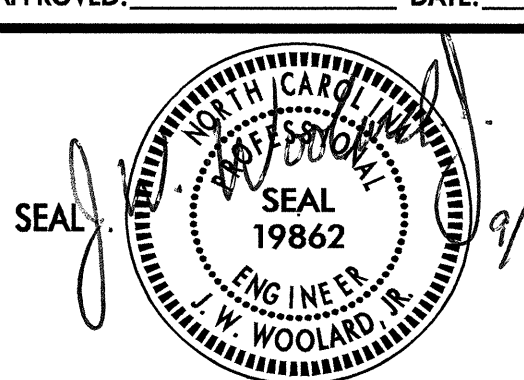
CONTRACTOR SHALL WORK IN PHASE I, STEPS 7 AND 8 SIMULTANEOUSLY.

STEP 7: - CONTRACTOR SHALL CONSTRUCT PROPOSED NORTHBOUND I-85, INCLUDING BRIDGE REHAB., FROM STA. 94+00+/- -NBL1- TO STA. 336+07+/- -NBL2- (NOT INCLUDING RUMBLE STRIPS), LEAVING SECTIONS OF NORTHBOUND I-85 OPEN FOR TRAFFIC TO ACCESS EXISTING RAMPS/LOOP FROM MEDIAN CROSSOVERS (SEE CONSTRUCTION PLANS AND SHEETS TCP-5 AND TCP-7, DETAIL-4).

NOTE: CONTRACTOR SHALL INSTALL PROPOSED GUARDRAIL/CABLE GUIDE RAIL AS MUCH AS POSSIBLE TO THE OUTSIDE & INSIDE SHOULDERS OF NORTHBOUND I-85 SO AS NOT TO INTERFERE WITH EXISTING/FUTURE TEMPORARY TRAFFIC PATTERNS, AS DIRECTED BY THE ENGINEER (SEE CONSTRUCTION PLANS).

CONTRACTOR SHALL PAVE/RESURFACE/FILL-IN EXISTING RUMBLE STRIPS ON INSIDE/OUTSIDE SHOULDERS OF NORTHBOUND I-85 FROM STA. 26+00+/- -NBL1- TO STA. 94+00+/- -NBL1- AS SHOWN IN THE CONSTRUCTION PLANS.

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REVIEWED BY: JWW		



STEP 8: - CONTRACTOR MAY CONSTRUCT ANY INTERCHANGE LOCATION (SEE STEP A, B, OR C BELOW) AS HE CHOOSES, HOWEVER ONLY ONE INTERCHANGE LOCATION (SEE STEP A, B, OR C BELOW) MAY BE CLOSED AT ONE TIME.

NOTE: CONTRACTOR SHALL PLACE CHANGEABLE MESSAGE SIGNS ON I-85 AND RAMPS/LOOP, AS DIRECTED BY THE ENGINEER, ONE WEEK PRIOR TO THE INTERCHANGE BEING CLOSED TO INFORM THE TRAVELLING PUBLIC OF THE IMPENDING CLOSURE.

CONTRACTOR SHALL WORK IN A CONTINUOUS MANNER TO COMPLETE THE WORK IN PHASE I, STEP 8A IN 60 CONSECUTIVE DAYS (SEE INTERMEDIATE CONTRACT TIME AND SPECIAL PROVISIONS).

A. NORTHBOUND I-85, OFF-LOOP TO AND ON-RAMP FROM POPLAR CREEK ROAD (EXIT #209) (SEE CONSTRUCTION PLANS AND SHEET TCP-8 FOR DETOUR 1).

1. CONTRACTOR SHALL INSTALL THE APPROPRIATE CHANGEABLE MESSAGE SIGNS & DETOUR 1 SIGNING TO CLOSE APPROPRIATE RAMP/LOOP TO TRAFFIC (SEE SHEET TCP-8 FOR DETOUR 1).
2. CONTRACTOR SHALL CONSTRUCT PROPOSED NORTHBOUND I-85, OFF-LOOP, ON-RAMP AND RE-CONSTRUCT MEDIAN CROSSOVERS TO MEET THE NEW GRADE OF THE PROPOSED NORTHBOUND I-85 AS SHOWN IN THE CONSTRUCTION PLANS.
3. CONTRACTOR SHALL PLACE INTERMEDIATE PAVEMENT MARKINGS (PAINT ON CROSSOVER & LOOP/RAMP AND TAPE ON I-85) AND TEMPORARY RAISED PAVEMENT MARKERS IN THE SAME INTERMEDIATE OFF-LOOP & ON-RAMP TRAFFIC PATTERN PRIOR TO THIS CONSTRUCTION.

NOTE: CONTRACTOR SHALL PLACE DRUMS TO DELINEATE TRAVELWAY FOR MEDIAN CROSSOVERS AND TEMPORARY RAMP/LOOP TRAFFIC PATTERNS.

4. CONTRACTOR SHALL REMOVE DETOUR SIGNING AND OPEN THE APPROPRIATE LOOP/RAMP TO TRAFFIC.

CONTRACTOR SHALL WORK IN A CONTINUOUS MANNER TO COMPLETE THE WORK IN PHASE I, STEP 8B IN 60 CONSECUTIVE DAYS (SEE INTERMEDIATE CONTRACT TIME AND SPECIAL PROVISIONS).

B. NORTHBOUND I-85, OFF-RAMP TO AND ON-RAMP FROM RUIN CREEK ROAD (EXIT #212) (SEE CONSTRUCTION PLANS AND SHEET TCP-9 FOR DETOUR 2).

1. CONTRACTOR SHALL INSTALL THE APPROPRIATE CHANGEABLE MESSAGE SIGNS & DETOUR 2 SIGNING TO CLOSE APPROPRIATE RAMPS TO TRAFFIC (SEE SHEETS TCP-9 FOR DETOUR 2).
2. CONTRACTOR SHALL CONSTRUCT PROPOSED NORTHBOUND I-85, OFF-RAMP, ON-RAMP AND RE-CONSTRUCT MEDIAN CROSSOVERS TO MEET THE NEW GRADE OF THE PROPOSED NORTHBOUND I-85 AS SHOWN IN THE CONSTRUCTION PLANS.
3. CONTRACTOR SHALL PLACE INTERMEDIATE PAVEMENT MARKINGS (PAINT ON CROSSOVER & RAMPS AND TAPE ON I-85) AND TEMPORARY RAISED PAVEMENT MARKERS IN THE SAME INTERMEDIATE OFF-RAMP & ON-RAMP TRAFFIC PATTERN PRIOR TO THIS CONSTRUCTION.

NOTE: CONTRACTOR SHALL PLACE DRUMS TO DELINEATE TRAVELWAY FOR MEDIAN CROSSOVERS AND TEMPORARY RAMPS TRAFFIC PATTERNS.

4. CONTRACTOR SHALL REMOVE DETOUR SIGNING AND OPEN THE APPROPRIATE RAMPS TO TRAFFIC.

CONTRACTOR SHALL WORK IN A CONTINUOUS MANNER TO COMPLETE THE WORK IN PHASE I, STEP 8C IN 60 CONSECUTIVE DAYS (SEE INTERMEDIATE CONTRACT TIME AND SPECIAL PROVISIONS).

C. NORTHBOUND I-85, OFF-RAMP TO DABNEY DRIVE (EXIT #213) (SEE CONSTRUCTION PLANS AND SHEET TCP-10 FOR DETOUR 3).

1. CONTRACTOR SHALL INSTALL THE APPROPRIATE CHANGEABLE MESSAGE SIGNS & DETOUR 3 SIGNING TO CLOSE APPROPRIATE RAMP TO TRAFFIC (SEE SHEET TCP-10 FOR DETOUR 3).
2. CONTRACTOR SHALL CONSTRUCT PROPOSED NORTHBOUND I-85, OFF-RAMP, AND RE-CONSTRUCT MEDIAN CROSSOVER TO MEET THE NEW GRADE OF THE PROPOSED NORTHBOUND I-85 AS SHOWN IN THE CONSTRUCTION PLANS.

NOTE: REVISE SIGNAL AS SHOWN IN THE SIGNAL PLANS.

3. CONTRACTOR SHALL PLACE INTERMEDIATE PAVEMENT MARKINGS (PAINT ON CROSSOVER & RAMP AND TAPE ON I-85) AND TEMPORARY RAISED PAVEMENT MARKERS IN THE SAME INTERMEDIATE OFF-RAMP TRAFFIC PATTERN PRIOR TO THIS CONSTRUCTION.

NOTE: CONTRACTOR SHALL PLACE DRUMS TO DELINEATE TRAVELWAY FOR MEDIAN CROSSOVER AND TEMPORARY RAMP TRAFFIC PATTERN.

4. CONTRACTOR SHALL REMOVE DETOUR SIGNING AND OPEN THE APPROPRIATE RAMP TO TRAFFIC.

STEP 9: - CONTRACTOR SHALL PLACE TEMPORARY PAVEMENT MARKINGS (PAINT) AND TEMPORARY RAISED PAVEMENT MARKERS ON NORTHBOUND I-85 IN THE EXISTING TRAFFIC PATTERN (AS RECORDED PRIOR TO CONSTRUCTION) WITHOUT INTERFERING WITH TRAFFIC FROM TEMPORARY MEDIAN CROSSOVERS TO/FROM LOOP/RAMPS AREAS FROM STA. 94+00+/- -NBL1- TO STA. 336+07+/- -NBL2- (SEE SHEET TCP-7, DETAIL-4).

CONTRACTOR SHALL WORK IN A CONTINUOUS MANNER AND SIMULTANEOUSLY TO COMPLETE THE WORK IN PHASE I, STEPS 10 AND 11.

STEP 10: - CONTRACTOR SHALL, USING ROADWAY STANDARD DRAWING NO. 1101.02, SHEETS 3 OF 9:

- SHIFT TEMPORARY NORTHBOUND I-85 TRAFFIC TO THE PROPOSED NORTHBOUND I-85 TRAFFIC PATTERN (SEE SHEET TCP-7, DETAIL-5).
- REMOVE TEMPORARY PAVEMENT MARKINGS & MARKERS ON NORTHBOUND I-85 IN TEMPORARY MEDIAN CROSSOVER AREAS AND PLACE TEMPORARY PAVEMENT MARKINGS (PAINT) & TEMPORARY RAISED PAVEMENT MARKERS FOR EXISTING NORTHBOUND I-85 AND LOOP/RAMPS TRAFFIC PATTERNS (AS RECORDED PRIOR TO CONSTRUCTION).
- OPEN BOTH LANES OF NORTHBOUND I-85 TO EXISTING TRAFFIC PATTERN (SEE SHEET TCP-7, DETAIL-6).

NOTE: INSTALL DRUMS AND TYPE III BARRICADES TO KEEP CROSSOVERS CLOSED TO TRAFFIC.

STEP 11: - WITH SOUTHBOUND I-85 TRAFFIC IN A ONE-LANE, ONE-WAY TRAFFIC PATTERN IN THE OUTSIDE SHOULDER/LANE (SEE SHEET TCP-7, DETAIL 5):

- REMOVE TEMPORARY PAVEMENT MARKINGS & TEMPORARY RAISED PAVEMENT MARKERS ON THE INSIDE LANE OF SOUTHBOUND I-85 AND PLACE TEMPORARY PAVEMENT MARKINGS (PAINT & TAPE ON BRIDGE) AND TEMPORARY RAISED PAVEMENT MARKERS FOR EXISTING SOUTHBOUND I-85 INSIDE LANE TRAFFIC PATTERN (AS RECORDED PRIOR TO CONSTRUCTION) (SEE SHEET TCP-7, DETAIL-5).

- RELAP EXISTING MEDIAN GUARDRAIL, REPLACE EXISTING MEDIAN GUARDRAIL END TREATMENTS AND ADJUST CABLE GUIDE RAIL, AS REQUIRED, FOR PLACING SOUTHBOUND I-85 IN THE EXISTING TRAFFIC PATTERN LANE OF SOUTHBOUND I-85 (SEE CONSTRUCTION PLANS).

- CONTRACTOR SHALL, USING ROADWAY STANDARD DRAWING NO. 1101.02, SHEETS 3 OF 9:

- REMOVE PORTABLE CONCRETE BARRIER AND SHIFT SOUTHBOUND I-85 TRAFFIC TO THE EXISTING INSIDE LANE IN A ONE-LANE, ONE-WAY TRAFFIC PATTERN (SEE SHEET TCP-7, DETAIL-6).

NOTE: INSTALL DRUMS AND TYPE III BARRICADES TO KEEP CROSSOVERS CLOSED TO TRAFFIC.

- REMOVE TEMPORARY OUTSIDE EDGE LINE, INTERCHANGE ACCELERATION/DECELERATION & GORE AREA PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKINGS (PAINT & TAPE ON BRIDGE) AND TEMPORARY RAISED PAVEMENT MARKERS FOR EXISTING SOUTHBOUND I-85 OUTSIDE LANE TRAFFIC PATTERN (AS RECORDED PRIOR TO CONSTRUCTION) (SEE SHEET TCP-7, DETAIL-6).

- OPEN BOTH LANES OF SOUTHBOUND I-85 TO EXISTING TRAFFIC PATTERN.

PHASE II

STEP 1: - CONTRACTOR SHALL, USING ROADWAY STANDARD DRAWING NOS. 1101.02, SHEET 3 OF 9, AND 1101.04, PLACE SOUTHBOUND I-85 AND NORTHBOUND I-85 TRAFFIC IN A ONE-LANE, ONE-WAY TRAFFIC PATTERN IN THE OUTSIDE LANES OF SOUTHBOUND/NORTHBOUND I-85 RESPECTIVELY (SEE SHEET TCP-12, DETAIL-7).

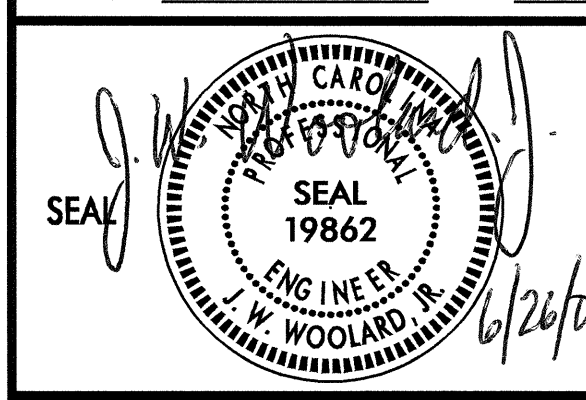
- REMOVE EXISTING TEMPORARY MEDIAN CROSSOVERS AND CONSTRUCT PROPOSED TEMPORARY MEDIAN CROSSOVERS (SEE CONSTRUCTION PLANS AND SHEETS TCP-11 & TCP-12, DETAIL-7): -CR1S-, -CRRP1B-, -CRRP1A-, -CRRP2B-, -CRRP2A-, -CR2S-, AND -CR3S-.

NOTE: THE CONTRACTOR SHALL BACKFILL/SAFE-UP ALL SHOULDERS BY THE END OF EACH DAILY WORK PERIOD AS FOLLOWS:

BACKFILL AT A 6:1 SLOPE UP TO THE EDGE AND ELEVATION OF EXISTING PAVEMENT IN AREAS THAT EXCEED 2' ON ROADWAYS WITH POSTED SPEED LIMITS OF 45 MPH OR GREATER.

BACKFILL WITH SUITABLE COMPACTED MATERIAL, APPROVED BY THE ENGINEER, AT NO EXPENSE TO THE DEPARTMENT.

NOTE: INSTALL DRUMS AND TYPE III BARRICADES TO KEEP CROSSOVERS CLOSED TO TRAFFIC.

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CONTRACTOR SHALL WORK IN A CONTINUOUS MANNER TO COMPLETE THE WORK IN PHASE II, STEPS 2 AND 3.

STEP 2: - CONTRACTOR SHALL, USING ROADWAY STANDARD DRAWING NO. 1101.02, SHEET 3 OF 9, PLACE NORTHBOUND I-85 TRAFFIC INTO A ONE-LANE, ONE-WAY PATTERN IN THE INSIDE LANE OF NORTHBOUND I-85 FROM STA. 24+00+/- -NBL1- TO STA. 336+07+/- -NBL2- (SEE SHEET TCP-12, DETAIL-8).

-- REMOVE EXISTING OUTSIDE EDGE LINE, INTERCHANGE ACCELERATION/DECELERATION & GORE AREA PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKINGS (PAINT & TAPE ON BRIDGE) & TEMPORARY RAISED PAVEMENT MARKERS ON THE OUTSIDE SHOULDER/LANE OF NORTHBOUND I-85 FOR A ONE-LANE, ONE-WAY TRAFFIC PATTERN FROM STA. 24+00+/- -NBL1- TO STA. 336+07+/- -NBL2- (SEE SHEET TCP-12, DETAIL-8 AND ROADWAY STANDARD DRAWING NUMBER 1101.03, SHEET 4 OF 9).

NOTE: INSTALL TEMPORARY RAISED PAVEMENT MARKERS ONLY IN THE ACCELERATION/DECELERATION AND GORE AREAS.

STEP 3: - CONTRACTOR SHALL, USING ROADWAY STANDARD DRAWING NOS. 1101.02, SHEET 3 OF 9, PLACE NORTHBOUND I-85 TRAFFIC INTO A ONE-LANE, ONE-WAY PATTERN IN THE OUTSIDE LANE/SHOULDER OF NORTHBOUND I-85 FROM STA. 24+00+/- -NBL1- TO STA. 336+07+/- -NBL2- (SEE SHEETS TCP-11 & TCP-12, DETAIL-9).

-- PLACE PORTABLE CONCRETE BARRIER (PCB) ON NORTHBOUND I-85 FROM STA. 24+00+/- -SBL1- TO STA. 336+07+/- -NBL2- (SEE SHEETS TCP-11 & TCP-12, DETAIL-9 AND ROADWAY STANDARD DRAWING NUMBER 1101.03, SHEET 4 OF 9).

NOTE: INSTALL DELINEATORS ON PORTABLE CONCRETE BARRIER EVERY 50 FEET.

-- RELAP EXISTING MEDIAN GUARDRAIL, REPLACE EXISTING MEDIAN GUARDRAIL END TREATMENTS AND ADJUST CABLE GUIDE RAIL, AS REQUIRED, FOR PLACING SOUTHBOUND I-85 IN THE EXISTING INSIDE LANE OF NORTHBOUND I-85 (SEE CONSTRUCTION PLANS).

-- REMOVE EXISTING INSIDE EDGE LINE & SKIP LINES, AND PLACE TEMPORARY PAVEMENT MARKINGS (PAINT & TAPE ON BRIDGE) & TEMPORARY RAISED PAVEMENT MARKERS ON THE INSIDE LANE OF NORTHBOUND I-85 FOR A ONE-LANE, ONE-WAY SOUTHBOUND I-85 TRAFFIC PATTERN FROM STA. 24+00+/- -SBL1- TO STA. 336+07+/- -NBL2- (SEE SHEETS TCP-11 & TCP-12, DETAIL-9 AND ROADWAY STANDARD DRAWING NUMBER 1101.03, SHEETS 4 & 5 OF 9).

NOTE: INSTALL TEMPORARY RAISED PAVEMENT MARKERS IN THE MEDIAN CROSSOVER AREA FROM 100 FEET BEFORE THE ACCELERATION/DECELERATION LANES, THRU THE MEDIAN CROSSOVER AND 100 FEET AFTER EACH MEDIAN CROSSOVER.

NOTE: NORTHBOUND I-85 TRAFFIC IS NOW IN A ONE-LANE, ONE-WAY PATTERN ON OUTSIDE LANE/SHOULDER OF NORTHBOUND I-85 FROM STA. 24+00+/- -NBL1- (BEGINNING OF PROJECT) TO STA. 336+07+/- -NBL2- (GORE AREA OF EASTBOUND US 158 ON-RAMP TO NORTHBOUND I-85) AND TWO-LANE, TWO-WAY PATTERN FROM STA. 336+07+/- -NBL2- (GORE AREA OF EASTBOUND US 158 ON-RAMP TO NORTHBOUND I-85) TO END OF PROJECT (SEE SHEET TCP-11).

CONTRACTOR SHALL WORK IN A CONTINUOUS MANNER TO COMPLETE THE WORK IN PHASE II, STEPS 4 THRU 8B IN 90 CONSECUTIVE DAYS (SEE INTERMEDIATE CONTRACT TIME AND SPECIAL PROVISIONS).

STEP 4: - CONTRACTOR SHALL, INSTALL DETOUR 4 SIGNING AND USING ROADWAY STANDARD DRAWING 1101.03, SHEET 3 OF 9, CLOSE EASTBOUND US 158 ON-RAMP TO NORTHBOUND I-85 TO TRAFFIC (SEE SHEET TCP-14 FOR DETOUR 4).

NOTE: CONTRACTOR SHALL PLACE CHANGEABLE MESSAGE SIGN(S) ON THE ON-RAMP TO I-85 NORTHBOUND, AS DIRECTED BY THE ENGINEER, ONE WEEK PRIOR TO THE RAMP BEING CLOSED TO INFORM THE TRAVELLING PUBLIC OF THE IMPENDING CLOSURE.

CONTRACTOR SHALL WORK IN PHASE II, STEPS 5 AND 6 SIMULTANEOUSLY.

STEP 5: - CONTRACTOR SHALL, USING ROADWAY STANDARD DRAWING NO. 1101.02, SHEET 3 OF 9, PLACE NORTHBOUND I-85 TRAFFIC INTO A ONE-LANE, ONE-WAY PATTERN IN THE INSIDE LANE OF NORTHBOUND I-85 FROM STA. 336+07+/- -NBL2- TO STA. 364+36+/- -NBL2-.

-- REMOVE EXISTING OUTSIDE EDGE LINE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKINGS (PAINT) & TEMPORARY RAISED PAVEMENT MARKERS ON THE OUTSIDE SHOULDER/LANE OF NORTHBOUND I-85 FOR A ONE-LANE, ONE-WAY TRAFFIC PATTERN FROM STA. 336+07+/- -NBL2- TO STA. 364+36+/- -NBL2- (SEE SHEET TCP-12, DETAIL-8).

NOTE: INSTALL TEMPORARY RAISED PAVEMENT MARKERS ONLY IN THE ACCELERATION/DECELERATION AND GORE AREAS.

- CONTRACTOR SHALL, USING ROADWAY STANDARD DRAWING NOS. 1101.02, SHEET 3 OF 9 & 1101.03, SHEET 4 OF 9, PLACE NORTHBOUND I-85 TRAFFIC INTO A ONE-LANE, ONE-WAY PATTERN IN THE OUTSIDE LANE/SHOULDER OF NORTHBOUND I-85 FROM STA. 336+07+/- -NBL2- TO STA. 364+36+/- -NBL2- (SEE SHEETS TCP-11 & TCP-12, DETAIL-9).

-- PLACE PORTABLE CONCRETE BARRIER (PCB) ON NORTHBOUND I-85 FROM STA. 336+07+/- -NBL2- TO STA. 364+36+/- -NBL2- (SEE SHEETS TCP-11 & TCP-12, DETAIL-9 AND ROADWAY STANDARD DRAWING NUMBER 1101.03, SHEET 4 OF 9).

NOTE: INSTALL DELINEATORS ON PORTABLE CONCRETE BARRIER EVERY 50 FEET.

-- REMOVE EXISTING INSIDE EDGE LINE & SKIP LINES, INTERCHANGE ACCELERATION & GORE AREA PAVEMENT MARKINGS (AS REQUIRED), AND PLACE TEMPORARY PAVEMENT MARKINGS (PAINT & TAPE ON BRIDGE) & TEMPORARY RAISED PAVEMENT MARKERS ON THE INSIDE LANE OF NORTHBOUND I-85 FOR A ONE-LANE, ONE-WAY SOUTHBOUND I-85 TRAFFIC PATTERN FROM STA. 24+00+/- -NBL1- TO STA. 364+36+/- -NBL2- (SEE SHEETS TCP-11 & TCP-12, DETAIL-9 AND ROADWAY STANDARD DRAWING NUMBER 1101.03, SHEET 4 & 5 OF 9).

NOTE: INSTALL TEMPORARY RAISED PAVEMENT MARKERS IN THE MEDIAN CROSSOVER AREA FROM 100 FEET BEFORE THE ACCELERATION/DECELERATION LANES, THRU THE MEDIAN CROSSOVER AND 100 FEET AFTER EACH MEDIAN CROSSOVER.

STEP 6: - CONTRACTOR SHALL, USING SHEET TCP-11A (FOR PLACEMENT OF PAVEMENT MARKINGS & MARKERS), ROADWAY STANDARD DRAWING NOS. 1101.02, SHEETS 3 OF 9 & 1101.03, SHEET 4 & 5 OF 9, PLACE SOUTHBOUND I-85 TRAFFIC INTO A ONE-LANE, ONE-WAY PATTERN IN THE OUTSIDE LANE OF SOUTHBOUND I-85 AND PLACE/TIE-IN TEMPORARY PAVEMENT MARKINGS (PAINT/TAPE) AND TEMPORARY RAISED PAVEMENT MARKERS FOR CROSSOVERS -CR1S-, -CRRP1B-, -CRRP1A-, -CRRP2B-, -CRRP2A-, -CR2S-, AND -CR3S- TO INSIDE LANE OF NORTHBOUND I-85.

NOTE: INSTALL TEMPORARY RAISED PAVEMENT MARKERS IN THE MEDIAN CROSSOVER AREA FROM 100 FEET BEFORE THE ACCELERATION/DECELERATION LANES, THRU THE MEDIAN CROSSOVER AND 100 FEET AFTER EACH MEDIAN CROSSOVER.

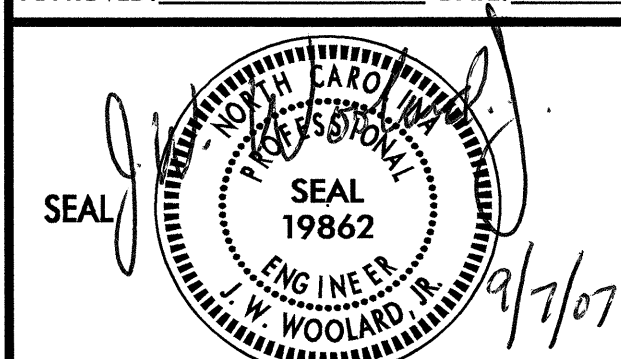

STEP 7: - CONTRACTOR SHALL INSTALL THE APPROPRIATE CHANGEABLE MESSAGE SIGNS & DETOUR 5 SIGNING TO CLOSE ON-RAMP FROM DABNEY DRIVE TO SOUTHBOUND I-85 TO TRAFFIC (SEE SHEET TCP-15 FOR DETOUR 5).

NOTE: CONTRACTOR SHALL PLACE CHANGEABLE MESSAGE SIGN(S) ON THE ON-RAMP TO I-85 SOUTHBOUND, AS DIRECTED BY THE ENGINEER, ONE WEEK PRIOR TO THE RAMP BEING CLOSED TO INFORM THE TRAVELLING PUBLIC OF THE IMPENDING CLOSURE.

- CONTRACTOR SHALL, USING SHEET TCP-11A (FOR PLACEMENT OF CHANGEABLE MESSAGE SIGNS, WARNING SIGNS AND DRUMS FOR TEMPORARY TRAFFIC PATTERN TO EXIT #213 TO KEEP WESTBOUND US 158 OFF-RAMP FROM SOUTHBOUND I-85 OPEN TO TRAFFIC), ROADWAY STANDARD DRAWING NOS. 1101.02, SHEETS 3 OF 9, AND 1101.03, SHEETS 4 & 5 OF 9, SHIFT SOUTHBOUND I-85 TRAFFIC TO THE INSIDE LANE OF NORTHBOUND I-85. PLACE/TIE-IN TEMPORARY PAVEMENT MARKINGS (PAINT ON CROSSOVERS AND TAPE ON I-85) AND TEMPORARY RAISED PAVEMENT MARKERS FOR MEDIAN CROSSOVERS -CRRP1B-, -CRRP1A-, -CRRP2B-, AND -CRRP2A- TO EXISTING RAMPS (SEE SHEETS TCP-11, TCP-11A & TCP-13, DETAIL-10).

NOTE: INSTALL TEMPORARY RAISED PAVEMENT MARKERS IN THE MEDIAN CROSSOVER AREA FROM 100 FEET BEFORE THE ACCELERATION/DECELERATION LANES, THRU THE MEDIAN CROSSOVER AND 100 FEET AFTER EACH MEDIAN CROSSOVER.

NOTE: CONTRACTOR SHALL PLACE DRUMS TO DELINEATE TRAVELWAY FOR MEDIAN CROSSOVERS AND TEMPORARY RAMPS TRAFFIC PATTERNS.

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CONTRACTOR SHALL WORK IN PHASE II, STEPS 8, 9 AND 10 SIMULTANEOUSLY.

STEP 8: - CONSTRUCTION OF SOUTHBOUND LANES FROM STA. 325+00+/- -SBL2- TO STA. 338+75+/- -SBL2-.

- A. - CONTRACTOR SHALL CONSTRUCT PROPOSED SOUTHBOUND I-85, INCLUDING BRIDGE REHAB., FROM STA. 325+00+/- -SBL2- TO STA. 338+75+/- -SBL2- (INCLUDING RUMBLE STRIPS, BUT NOT ON THE INSIDE SHOULDER OF THE -CRS2- CROSSOVER AREA) (SEE CONSTRUCTION PLANS AND SHEETS TCP-11, & TCP-13, DETAIL-10).

NOTE: CONTRACTOR SHALL INSTALL PROPOSED GUARDRAIL/CABLE GUIDE RAIL AS MUCH AS POSSIBLE TO THE OUTSIDE & INSIDE SHOULDERS OF SOUTHBOUND I-85 SO AS NOT TO INTERFERE WITH TEMPORARY TRAFFIC PATTERNS, AS DIRECTED BY THE ENGINEER (SEE CONSTRUCTION PLANS).

- CONTRACTOR SHALL PLACE TEMPORARY PAVEMENT MARKINGS (PAINT & TAPE) & TEMPORARY RAISED PAVEMENT MARKERS FOR EXISTING SOUTHBOUND I-85 TRAFFIC PATTERN (AS RECORDED PRIOR TO CONSTRUCTION) AND USING ROADWAY STANDARD DRAWING NO. 1101.03, SHEET 4 OF 9 FROM STA. 325+00+/- -SBL2- & STA. 319+58+/- -LMED- (INCLUDING CROSSOVER -CR2S-) TO STA. 342+00+/- -SBL2- AS SHOWN ON SHEETS TCP-11B, AND TCP-13, DETAIL-10.

- CONTRACTOR SHALL, USING ROADWAY STANDARD DRAWING NOS. 1101.02, SHEETS 3 & 7 OF 9, & 1101.03, SHEET 4 OF 9, OPEN SOUTHBOUND I-85 TO EXISTING PATTERN FROM STA. 342+00+/- -SBL2- TO END OF PROJECT AND SHIFT SOUTHBOUND I-85 TRAFFIC TO THE INSIDE LANE OF NORTHBOUND I-85 VIA CROSSOVER -CR2S- FROM STA. 319+58+/- -LMED- TO STA. 342+00+/- -SBL2- AS SHOWN ON SHEET TCP-11B.

NOTE: INSTALL DRUMS AND TYPE III BARRICADES TO KEEP CROSSOVER CR3S CLOSED TO TRAFFIC.

NOTE: SOUTHBOUND I-85 TRAFFIC IS NOW IN A TWO-LANE, ONE-WAY PATTERN ON SOUTHBOUND I-85 FROM STA. 338+75+/- -SBL2- (GORE AREA FOR WESTBOUND US 158 OFF-RAMP FROM SOUTHBOUND I-85, EXIT #213) TO END OF PROJECT AND ONE-LANE, ONE-WAY PATTERN ON NORTHBOUND I-85 VIA CROSSOVER -CR2S- (SEE SHEETS TCP-11 & TCP-11B).

- B. - CONTRACTOR SHALL REMOVE PORTABLE CONCRETE BARRIER (PCB) FROM NORTHBOUND I-85 FROM STA. 326+30+/- -NBL2- TO STA. 364+36+/- -NBL2- AND REPLACE WITH DRUMS (SEE SHEET TCP-13, DETAIL-11).

- CONTRACTOR SHALL PLACE TEMPORARY PAVEMENT MARKINGS (PAINT) & TEMPORARY RAISED PAVEMENT MARKERS FOR EXISTING INSIDE LANE OF NORTHBOUND I-85, INCLUDING EASTBOUND US 158 ON-RAMP TO NORTHBOUND I-85, FROM STA. 326+30+/- -NBL2- TO STA. 364+36+/- -NBL2- (AS RECORDED PRIOR TO CONSTRUCTION) (SEE SHEET TCP-13, DETAIL-11).

- CONTRACTOR SHALL, USING ROADWAY STANDARD DRAWING NO. 1101.02, SHEET 3 OF 9, SHIFT NORTHBOUND I-85 TRAFFIC TO INSIDE LANE OF NORTHBOUND I-85 FROM STA. 326+30+/- -NBL2- TO STA. 364+36+/- -NBL2- AND PLACE REMAINDER OF TEMPORARY PAVEMENT MARKINGS (PAINT) & TEMPORARY RAISED PAVEMENT MARKERS FOR EXISTING OUTSIDE LANE OF NORTHBOUND I-85, FROM STA. 326+30+/- -NBL2- TO STA. 364+36+/- -NBL2- (AS RECORDED PRIOR TO CONSTRUCTION) (SEE SHEET TCP-13, DETAIL-12).

- CONTRACTOR SHALL REMOVE DETOUR SIGNING AND OPEN BOTH LANES OF NORTHBOUND I-85, INCLUDING EASTBOUND US 158 ON-RAMP TO NORTHBOUND I-85, FROM STA. 326+30+/- -NBL2- TO STA. 364+36+/- -NBL2- TO TRAFFIC.

NOTE: NORTHBOUND I-85 TRAFFIC IS NOW IN A TWO-LANE, ONE-WAY PATTERN ON NORTHBOUND I-85 FROM STA. 326+30+/- -NBL2- (GORE AREA FOR CROSSOVER \*CR2S-) TO END OF PROJECT WITH EASTBOUND US 158 ON-RAMP TO NORTHBOUND I-85 OPEN. REMAINDER OF PROJECT IS IN A TWO-LANE, TWO-WAY PATTERN ON NORTHBOUND I-85 FROM STA. 24+00+/- -NBL1- TO STA. 326+30+/- -NBL2- (SEE SHEETS TCP-11 & TCP-13, DETAIL-11).

- STEP 9: - CONTRACTOR SHALL CONSTRUCT PROPOSED SOUTHBOUND I-85, INCLUDING BRIDGE REHAB., (INCLUDING RUMBLE STRIPS), FROM STA. 106+00+/- -SBL1- TO STA. 325+00+/- -SBL2- (INCLUDING PROPOSED ON-RAMP FROM DABNEY DRIVE EXIT #213) LEAVING SECTIONS OF SOUTHBOUND I-85 OPEN FOR TRAFFIC TO ACCESS EXISTING RAMPS FROM MEDIAN CROSSOVERS (SEE CONSTRUCTION PLANS AND SHEETS TCP-11 & TCP-13, DETAIL-10).

NOTE: CONTRACTOR SHALL INSTALL PROPOSED GUARDRAIL/CABLE GUIDE RAIL AS MUCH AS POSSIBLE SO AS NOT TO INTERFERE WITH TEMPORARY TRAFFIC PATTERNS, AS DIRECTED BY THE ENGINEER (SEE CONSTRUCTION PLANS).

NOTE: CONSTRUCT/REPLACE RUMBLE STRIPS ON BOTH SOUTHBOUND I-85 SHOULDERS THAT WERE REMOVED FROM STA. 24+00+/- -SBL1- TO STA. 106+00+/- -SBL1- (SEE CONSTRUCTION PLANS).

- STEP 10: - CONTRACTOR MAY CONSTRUCT ANY INTERCHANGE LOCATION (SEE STEP A, OR B BELOW) AS HE CHOOSES, HOWEVER ONLY ONE INTERCHANGE LOCATION (SEE STEP A OR B BELOW) MAY BE CLOSED AT ONE TIME.

NOTE: CONTRACTOR SHALL PLACE CHANGEABLE MESSAGE SIGNS ON I-85 AND RAMPS, AS DIRECTED BY THE ENGINEER, ONE WEEK PRIOR TO THE INTERCHANGE BEING CLOSED TO INFORM THE TRAVELLING PUBLIC OF THE IMPENDING CLOSURE.

CONTRACTOR SHALL WORK IN A CONTINUOUS MANNER TO COMPLETE THE WORK IN PHASE II, STEP 10A IN 60 CONSECUTIVE DAYS (SEE INTERMEDIATE CONTRACT TIME AND SPECIAL PROVISIONS).

- A. SOUTHBOUND I-85, OFF-RAMP TO AND ON-RAMP FROM POPLAR CREEK ROAD (EXIT #209) (SEE CONSTRUCTION PLANS AND SHEET TCP-16 FOR DETOUR 6).

- 1. CONTRACTOR SHALL INSTALL THE APPROPRIATE CHANGEABLE MESSAGE SIGNS & DETOUR 6 SIGNING TO CLOSE APPROPRIATE RAMPS TO TRAFFIC (SEE SHEET TCP-16 FOR DETOUR 6).

- 2. CONTRACTOR SHALL CONSTRUCT PROPOSED SOUTHBOUND I-85, OFF-RAMP, ON-RAMP AND RE-CONSTRUCT MEDIAN CROSSOVERS TO MEET THE NEW GRADE OF THE PROPOSED SOUTHBOUND I-85 AS SHOWN IN THE CONSTRUCTION PLANS.

- 3. CONTRACTOR SHALL PLACE INTERMEDIATE PAVEMENT MARKINGS (PAINT ON CROSSOVER & RAMPS AND TAPE ON I-85) AND TEMPORARY RAISED PAVEMENT MARKERS IN THE SAME INTERMEDIATE OFF-RAMP & ON-RAMP TRAFFIC PATTERN PRIOR TO THIS CONSTRUCTION.

NOTE: CONTRACTOR SHALL PLACE DRUMS TO DELINEATE TRAVELWAY FOR MEDIAN CROSSOVERS AND TEMPORARY RAMP TRAFFIC PATTERNS.

- 4. CONTRACTOR SHALL REMOVE DETOUR SIGNING AND OPEN THE APPROPRIATE RAMPS TO TRAFFIC.

CONTRACTOR SHALL WORK IN A CONTINUOUS MANNER TO COMPLETE THE WORK IN PHASE II, STEP 10B IN 60 CONSECUTIVE DAYS (SEE INTERMEDIATE CONTRACT TIME AND SPECIAL PROVISIONS).

NOTE: RUIN CREEK ROAD (EXIT #212) CAN NOT BE CLOSED UNTIL EASTBOUND US 158 ON-RAMP TO NORTHBOUND I-85 IS OPEN TO TRAFFIC IN PHASE II, STEP 8B.

- B. SOUTHBOUND I-85, OFF-RAMP TO AND ON-RAMP FROM RUIN CREEK ROAD (EXIT #212) (SEE CONSTRUCTION PLANS AND SHEET TCP-17 FOR DETOUR 7).

- 1. CONTRACTOR SHALL INSTALL THE APPROPRIATE CHANGEABLE MESSAGE SIGNS & DETOUR 7 SIGNING TO CLOSE APPROPRIATE RAMPS TO TRAFFIC (SEE SHEET TCP-17 FOR DETOUR 7).

- 2. CONTRACTOR SHALL CONSTRUCT PROPOSED SOUTHBOUND I-85, OFF-RAMP, ON-RAMP AND RE-CONSTRUCT MEDIAN CROSSOVERS TO MEET THE NEW GRADE OF THE PROPOSED NORTHBOUND I-85 AS SHOWN IN THE CONSTRUCTION PLANS.

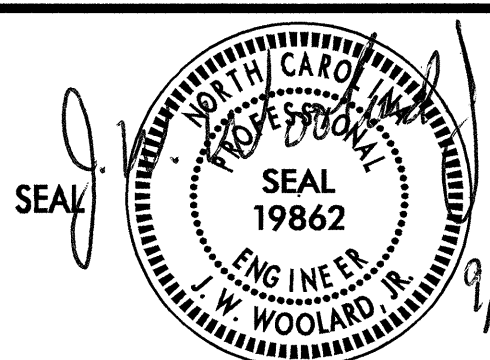
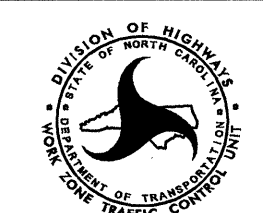
- 3. CONTRACTOR SHALL PLACE INTERMEDIATE PAVEMENT MARKINGS (PAINT ON CROSSOVER & RAMPS AND TAPE ON I-85) AND TEMPORARY RAISED PAVEMENT MARKERS IN THE SAME INTERMEDIATE OFF-RAMP & ON-RAMP TRAFFIC PATTERN PRIOR TO THIS CONSTRUCTION.

NOTE: CONTRACTOR SHALL PLACE DRUMS TO DELINEATE TRAVELWAY FOR MEDIAN CROSSOVERS AND TEMPORARY RAMPS TRAFFIC PATTERNS.

- 4. CONTRACTOR SHALL REMOVE DETOUR SIGNING AND OPEN THE APPROPRIATE RAMPS TO TRAFFIC.

- STEP 11: - CONTRACTOR SHALL PLACE TEMPORARY PAVEMENT MARKINGS (PAINT) AND TEMPORARY RAISED PAVEMENT MARKERS ON SOUTHBOUND I-85 IN THE EXISTING TRAFFIC PATTERN (AS RECORDED PRIOR TO CONSTRUCTION) WITHOUT INTERFERING WITH TRAFFIC FROM TEMPORARY MEDIAN CROSSOVERS TO/FROM RAMPS AREAS FROM STA. 106+00+/- -SBL1- TO STA. 325+00+/- -SBL2- (SEE SHEET TCP-13, DETAIL-10).

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	DESIGN BY: LKD		
	REVIEWED BY: JWW		

CONTRACTOR SHALL WORK IN A CONTINUOUS MANNER AND SIMULTANEOUSLY TO COMPLETE THE WORK IN PHASE II, STEPS 12 AND 13.

STEP 12: - CONTRACTOR SHALL REMOVE DETOUR #5 SIGNING AND USING ROADWAY STANDARD DRAWING NO. 1101.02, SHEETS 3 OF 9:

- SHIFT TEMPORARY SOUTHBOUND I-85 TRAFFIC TO THE PROPOSED SOUTHBOUND I-85 TRAFFIC PATTERN (SEE SHEET TCP-13, DETAIL-11).

NOTE: INSTALL DRUMS AND TYPE III BARRICADES TO KEEP CROSSOVERS CLOSED TO TRAFFIC.

- REMOVE TEMPORARY PAVEMENT MARKINGS & MARKERS ON SOUTHBOUND I-85 IN TEMPORARY MEDIAN CROSSOVER AREAS AND PLACE TEMPORARY PAVEMENT MARKINGS (PAINT) & TEMPORARY RAISED PAVEMENT MARKERS FOR EXISTING SOUTHBOUND I-85 AND RAMPS TRAFFIC PATTERN (AS RECORDED PRIOR TO CONSTRUCTION).
- OPEN BOTH LANES OF SOUTHBOUND I-85 TO EXISTING TRAFFIC PATTERN (SEE SHEET TCP-13, DETAIL-12).

STEP 13: - CONTRACTOR SHALL, USING ROADWAY STANDARD DRAWING NO. 1101.02, SHEETS 3 OF 9, PLACE SOUTHBOUND TRAFFIC IN A ONE-LANE, ONE-WAY TRAFFIC PATTERN IN THE OUTSIDE LANE OF SOUTHBOUND I-85 AND WITH NORTHBOUND I-85 TRAFFIC IN A ONE-LANE, ONE-WAY TRAFFIC PATTERN IN THE OUTSIDE SHOULDER/LANE (BEHIND PORTABLE CONCRETE BARRIER) REMOVE EXISTING TEMPORARY MEDIAN CROSSOVERS AND INSTALL REMAINDER OF PROPOSED GUARDRAIL/CABLE GUIDE RAIL (SEE CONSTRUCTION PLANS AND SHEET TCP-13, DETAIL-11).

NOTE: CONTRACTOR SHALL USE ROADWAY STANDARD DRAWING NO. 1101.02, SHEETS 3 OF 9, FOR SOUTHBOUND & NORTHBOUND I-85 TO REMOVE EXISTING CROSSOVER CR3S AT THE END OF THE PROJECT.

NOTE: CONTRACTOR MAY CONSTRUCT REMAINDER OF PROPOSED RUMBLE STRIPS ON THE INSIDE SHOULDERS OF SOUTHBOUND I-85, PROPOSED RUMBLE STRIPS ON THE INSIDE SHOULDER OF NORTHBOUND I-85 AND CONSTRUCT/REPLACE RUMBLE STRIPS ON INSIDE SHOULDER OF NORTHBOUND I-85 THAT WERE REMOVED FROM STA. 26+00+/- -NBL1- TO STA. 94+00+/- -NBL1-, AS DIRECTED BY THE ENGINEER (SEE CONSTRUCTION PLANS).

- REMOVE TEMPORARY PAVEMENT MARKINGS & TEMPORARY RAISED PAVEMENT MARKERS ON THE INSIDE LANE OF NORTHBOUND I-85 AND PLACE TEMPORARY PAVEMENT MARKINGS (PAINT & TAPE ON BRIDGE) AND TEMPORARY RAISED PAVEMENT MARKERS FOR EXISTING NORTHBOUND I-85 INSIDE LANE TRAFFIC PATTERN (AS RECORDED PRIOR TO CONSTRUCTION) FROM STA. 24+00+/- -NBL1- TO STA. 326+30+/- -NBL2- (SEE SHEET TCP-13, DETAIL-11).

- CONTRACTOR SHALL OPEN BOTH LANES OF SOUTHBOUND I-85 TO EXISTING TRAFFIC PATTERN WHEN ABOVE CONSTRUCTION IS COMPLETED.

STEP 14: - CONTRACTOR SHALL, USING ROADWAY STANDARD DRAWING NO. 1101.02, SHEETS 3 OF 9:

- REMOVE PORTABLE CONCRETE BARRIER AND SHIFT NORTHBOUND I-85 TRAFFIC TO THE EXISTING INSIDE LANE IN A ONE-LANE, ONE-WAY TRAFFIC PATTERN (SEE SHEET TCP-13, DETAIL-12).
- REMOVE TEMPORARY OUTSIDE EDGE LINE, INTERCHANGE ACCELERATION/DECELERATION & GORE AREA PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKINGS (PAINT & TAPE ON BRIDGE) AND TEMPORARY RAISED PAVEMENT MARKERS FOR EXISTING NORTHBOUND I-85 OUTSIDE LANE TRAFFIC PATTERN (AS RECORDED PRIOR TO CONSTRUCTION) (SEE SHEET TCP-13, DETAIL-12).

NOTE: CONTRACTOR MAY CONSTRUCT PROPOSED RUMBLE STRIPS ON THE OUTSIDE SHOULDER OF NORTHBOUND I-85 AND CONSTRUCT/REPLACE RUMBLE STRIPS ON OUTSIDE SHOULDER OF NORTHBOUND I-85 THAT WERE REMOVED FROM STA. 26+00+/- -NBL1- TO STA. 94+00+/- -NBL1-, AS DIRECTED BY THE ENGINEER (SEE CONSTRUCTION PLANS).

- OPEN BOTH LANES OF NORTHBOUND I-85 TO EXISTING TRAFFIC PATTERN.

PHASE III

STEP 1: - CONTRACTOR SHALL, USING ROADWAY STANDARD DRAWING NOS. 1101.02, SHEET 3, 6 & 7 OF 9, AND 1101.04:

- INSTALL REMAINDER OF PROPOSED GUARDRAIL & CABLE GUIDE RAIL AND CONSTRUCT REMAINDER OF RUMBLE STRIPS, INCLUDING THOSE REMOVED ON BOTH SHOULDERS OF NORTHBOUND I-85 FROM STA. 26+00+/- -NBL1- TO STA. 94+00+/- -NBL1- AS SHOWN IN THE CONSTRUCTION PLANS.
- CONDUCT RESURFACING OPERATIONS ON BOTH NORTHBOUND AND SOUTHBOUND I-85 AS SHOWN IN THE CONSTRUCTION PLANS.
- PLACE THE FINAL PAVEMENT MARKINGS (POLYUREA) AND INSTALL THE PERMANENT/SNOWPLOWABLE RAISED PAVEMENT MARKERS FOR THE FINAL TRAFFIC PATTERN (AS RECORDED PRIOR TO CONSTRUCTION) ON THE ENTIRE PROJECT.
- INSTALL FLEXIBLE DELINEATORS IN THE INTERCHANGE AREAS (SEE ROADWAY STANDARD DRAWINGS NOS. 1267.01, 1267.02, & 1267.03).

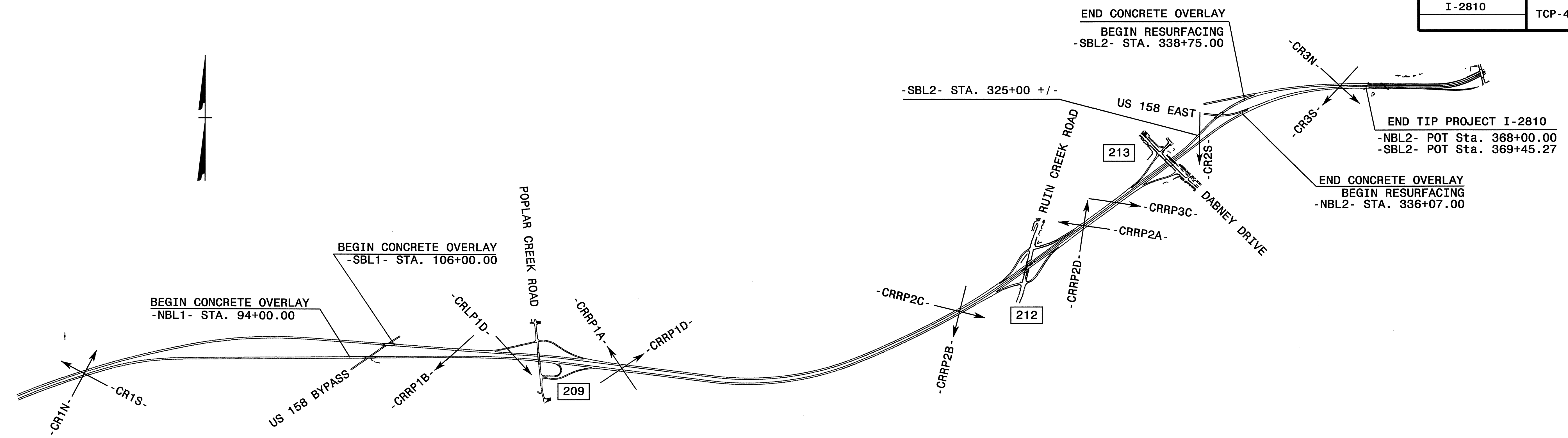
STEP 2: - REMOVE ALL TRAFFIC CONTROL DEVICES AND OPEN I-85 TO THE FINAL PATTERN.

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PROJ. REFERENCE NO.	SHEET NO.
I-2810	TCP-4



**GENERAL PHASING OUTLINE**

THE FOLLOWING IS ONLY A GENERAL PHASING OUTLINE. IT SHALL NOT IN ANY WAY REPLACE OR SUPERCEDE THE PROJECT NOTES AND PROJECT PHASING CONTAINED IN THIS TRAFFIC CONTROL PLAN.

**PHASE I**

- INSTALL WORK ZONE ADVANCE WARNING SIGNS, SPEED REDUCTION SIGNS, "\$250 SPEEDING PLENTY" SIGNS, "NO PARKING TOW-AWAY ZONE" SIGNS AND CHANGEABLE MESSAGE SIGNS.
- REVIEW AND RECORD THE EXISTING PAVEMENT MARKINGS AND MARKERS PRIOR TO CONSTRUCTION FOR BOTH NORTHBOUND AND SOUTHBOUND I-85.
- NOTE: THIS RECORD WILL BE UTILIZED TO PLACE THE INTERMEDIATE/FINAL PAVEMENT MARKINGS AND MARKERS THROUGHOUT THE CONSTRUCTION OF THIS PROJECT.
- CONSTRUCT 10' TEMPORARY OUTSIDE SHOULDER ON SOUTHBOUND I-85 AND PAVE/RESURFACE/FILL-IN EXISTING RUMBLE STRIPS.
- CONSTRUCT TEMPORARY MEDIAN CROSSOVERS -CR1N-, -CRLP1D-, -CRRP1D-, -CRRP2C-, -CRRP2D-, -CRRP3C-, AND -CR3N-.
- SHIFT SOUTHBOUND I-85 TRAFFIC TO OUTSIDE SHOULDER/OUTSIDE LANE, INSTALL PCB AND SHIFT NORTHBOUND I-85 TRAFFIC TO THE INSIDE LANE OF SOUTHBOUND I-85. UTILIZE MEDIAN CROSSOVERS TO MAINTAIN TRAFFIC TO ALL THREE INTERCHANGE AREAS.
- NOTE: TRAFFIC IS NOW IN A TWO-LANE, TWO-WAY TRAFFIC PATTERN ON SOUTHBOUND I-85 FROM MEDIAN CROSSOVER -CR1N- TO MEDIAN CROSSOVER -CR3N-.
- CONSTRUCT PROPOSED NORTHBOUND I-85, INCLUDING BRIDGE REHAB., AND PAVE/RESURFACE/FILL-IN EXISTING RUMBLE STRIPS ON INSIDE/OUTSIDE SHOULDERS OF NORTHBOUND I-85 AT THE BEGINNING OF THE PROJECT.

**INTERMEDIATE CONTRACT TIMES WITH LIQUIDATED DAMAGES (SEE SPECIAL PROVISIONS)**

- CLOSE SELECTED INTERCHANGE AREA AND CONSTRUCT PROPOSED ON/OFF RAMP/LOOP, ASSOCIATED SECTION OF NORTHBOUND I-85, AND RE-CONSTRUCTED MEDIAN CROSSOVERS.
- NOTE: REPEAT THIS OPERATION UNTIL ALL INTERCHANGE AREAS ARE CONSTRUCTED.
- PLACE NORTHBOUND I-85 TRAFFIC ON THE NEWLY COMPLETED NORTHBOUND I-85 LANES AND OPEN SOUTHBOUND I-85 TO ORIGINAL TRAFFIC PATTERN.
- NOTE: TRAFFIC IS NOW IN THE ORIGINAL FOUR-LANE, TWO-WAY TRAFFIC PATTERN OF I-85.

**PHASE II**

- CONSTRUCT TEMPORARY MEDIAN CROSSOVERS -CR1S-, -CRRP1B-, -CRRP1A-, -CRRP2B-, -CRRP2A-, -CR2S-, AND -CR3S-.

**INTERMEDIATE CONTRACT TIMES WITH LIQUIDATED DAMAGES (SEE SPECIAL PROVISIONS)**

- SHIFT NORTHBOUND I-85 TRAFFIC TO OUTSIDE SHOULDER/OUTSIDE LANE, CLOSE EASTBOUND US 158 ON-RAMP TO NORTHBOUND I-85 TO TRAFFIC, INSTALL PCB, AND SHIFT SOUTHBOUND I-85 TRAFFIC TO THE INSIDE LANE OF NORTHBOUND I-85. UTILIZE MEDIAN CROSSOVERS TO MAINTAIN TRAFFIC TO ALL THREE INTERCHANGE AREAS.
- NOTE: TRAFFIC IS NOW IN A TWO-LANE, TWO-WAY TRAFFIC PATTERN ON NORTHBOUND I-85 FROM MEDIAN CROSSOVER -CR1S- TO MEDIAN CROSSOVER -CR3S-.
- CONSTRUCT PROPOSED SOUTHBOUND I-85, INCLUDING BRIDGE REHAB., FROM STA. 325+00 +/- -SBL2- TO STA. 338+75 +/- -SBL2- AND OPEN THIS SECTION OF SOUTHBOUND I-85 TO TWO-LANE, ONE-WAY PATTERN. REMOVE PCB FROM THIS SECTION OF NORTHBOUND I-85 AND OPEN THIS SECTION OF NORTHBOUND I-85 & EASTBOUND US 158 ON-RAMP TO TRAFFIC.
- NOTE: TRAFFIC IS NOW IS A TWO-LANE, TWO-WAY TRAFFIC PATTERN ON NORTHBOUND I-85 FROM MEDIAN CROSSOVER -CR1S- TO MEDIAN CROSSOVER -CR2S-.
- CONSTRUCT PROPOSED NORTHBOUND I-85, INCLUDING BRIDGE REHAB.

**INTERMEDIATE CONTRACT TIMES WITH LIQUIDATED DAMAGES (SEE SPECIAL PROVISIONS)**

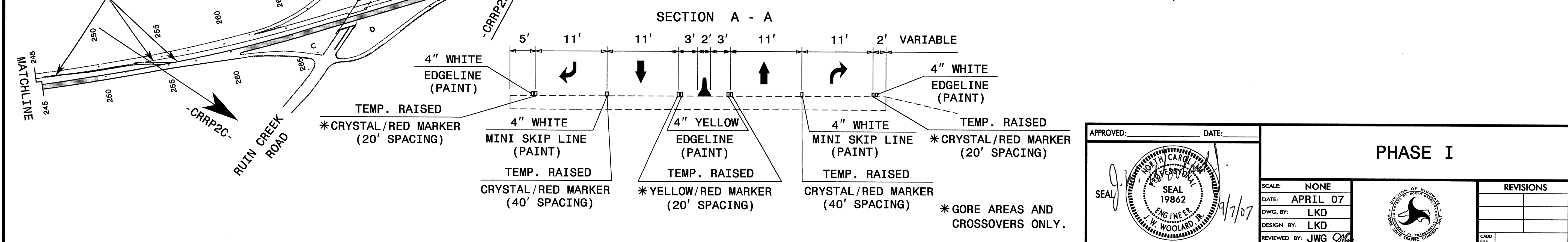
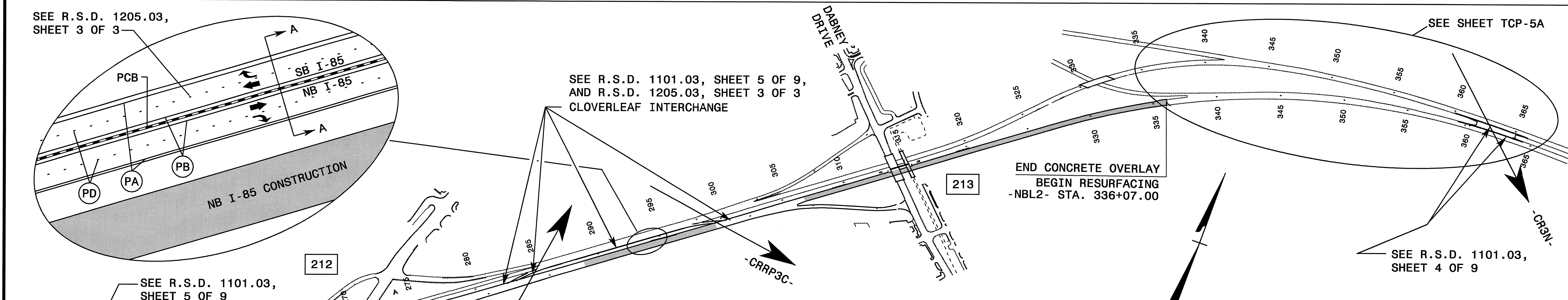
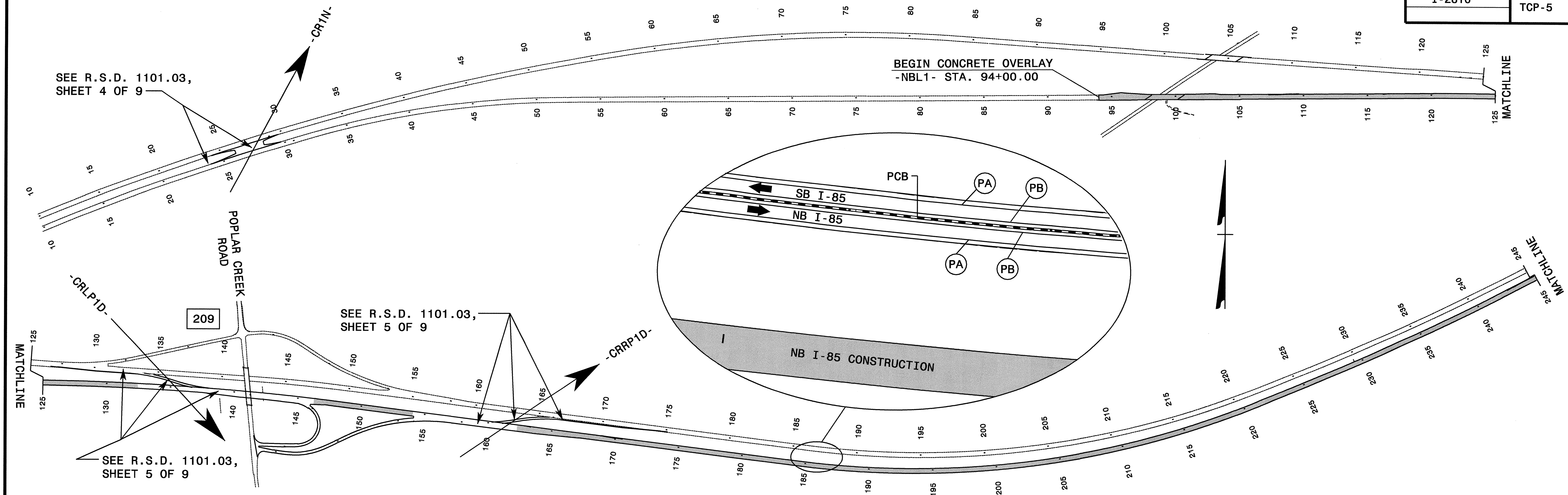
- CLOSE SELECTED INTERCHANGE AREA AND CONSTRUCT PROPOSED ON/OFF RAMP, ASSOCIATED SECTION OF SOUTHBOUND I-85, AND RE-CONSTRUCTED MEDIAN CROSSOVERS.
- NOTE: REPEAT THIS OPERATION UNTIL ALL INTERCHANGE AREAS ARE CONSTRUCTED.
- PLACE SOUTHBOUND I-85 TRAFFIC ON THE NEWLY COMPLETED SOUTHBOUND I-85 LANES AND OPEN NORTHBOUND I-85 TO ORIGINAL TRAFFIC PATTERN.
- NOTE: TRAFFIC IS NOW IN THE ORIGINAL FOUR-LANE, TWO-WAY TRAFFIC PATTERN OF I-85.

**PHASE III**

- CONDUCT RESURFACING OPERATIONS ON BOTH NORTHBOUND AND SOUTHBOUND SECTIONS OF I-85 AT THE END OF THE PROJECT.
- PLACE THE FINAL PAVEMENT MARKINGS (POLYUREA) AND INSTALL PERMANENT/SNOWPLOWABLE RAISED PAVEMENT MARKINGS FOR THE ORIGINAL TRAFFIC PATTERN ON I-85.
- INSTALL FLEXIBLE DELINEATORS IN THE INTERCHANGE AREAS.
- OPEN I-85 TO FINAL TRAFFIC PATTERN.

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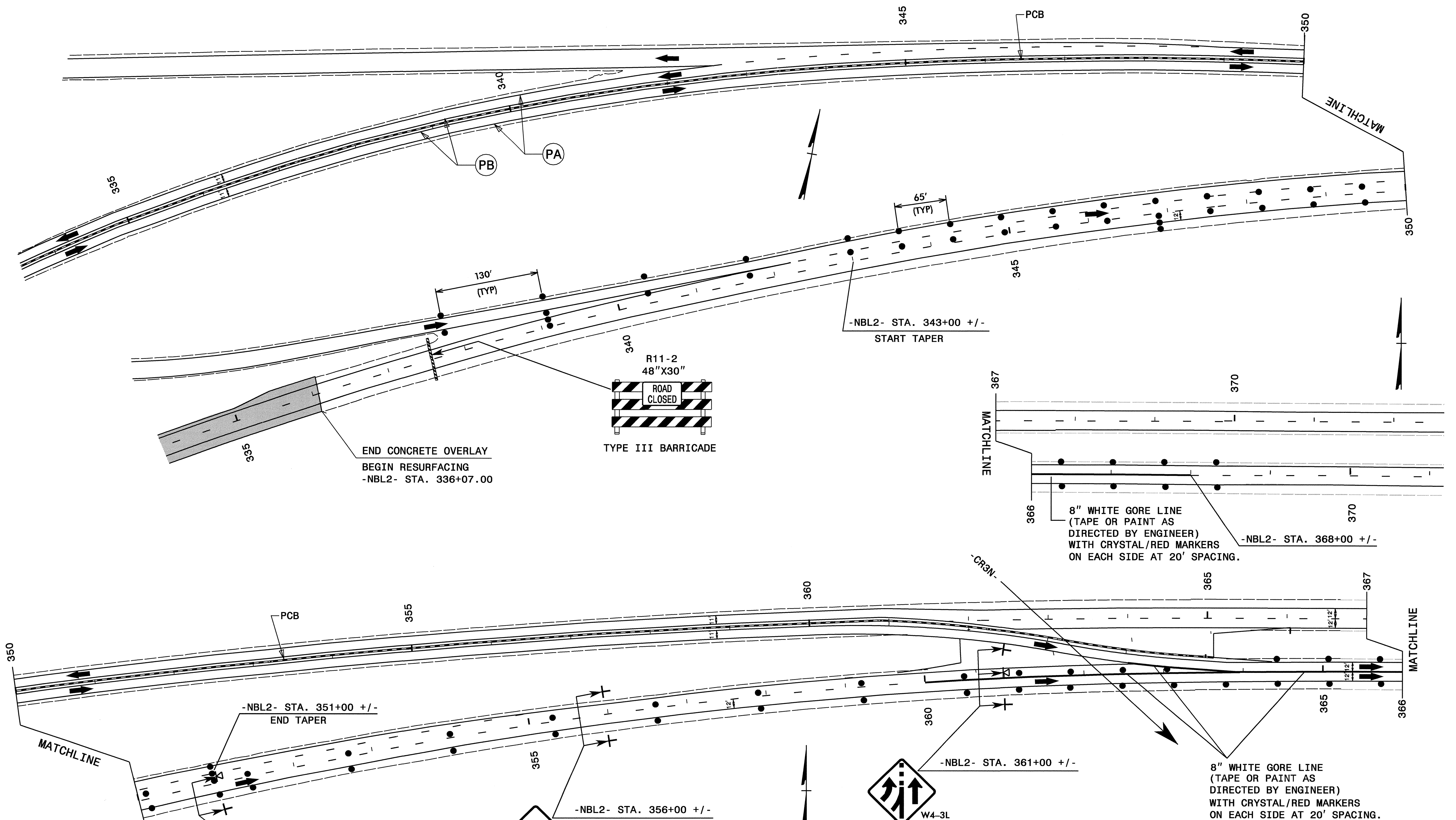
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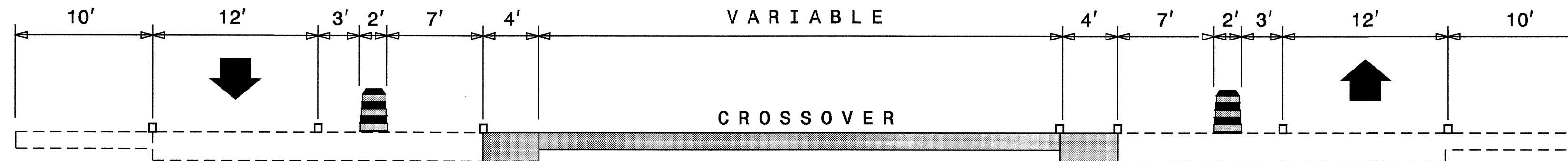


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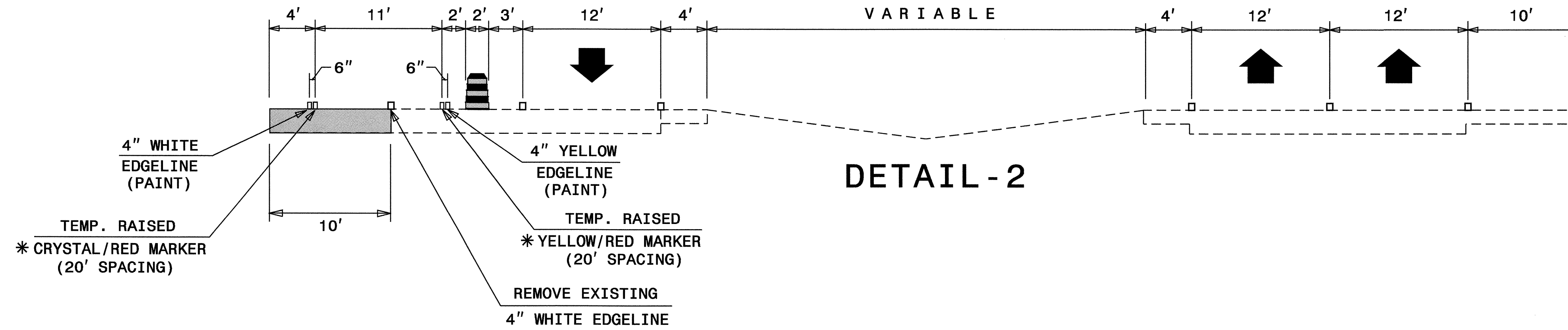
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I - 85 SBL

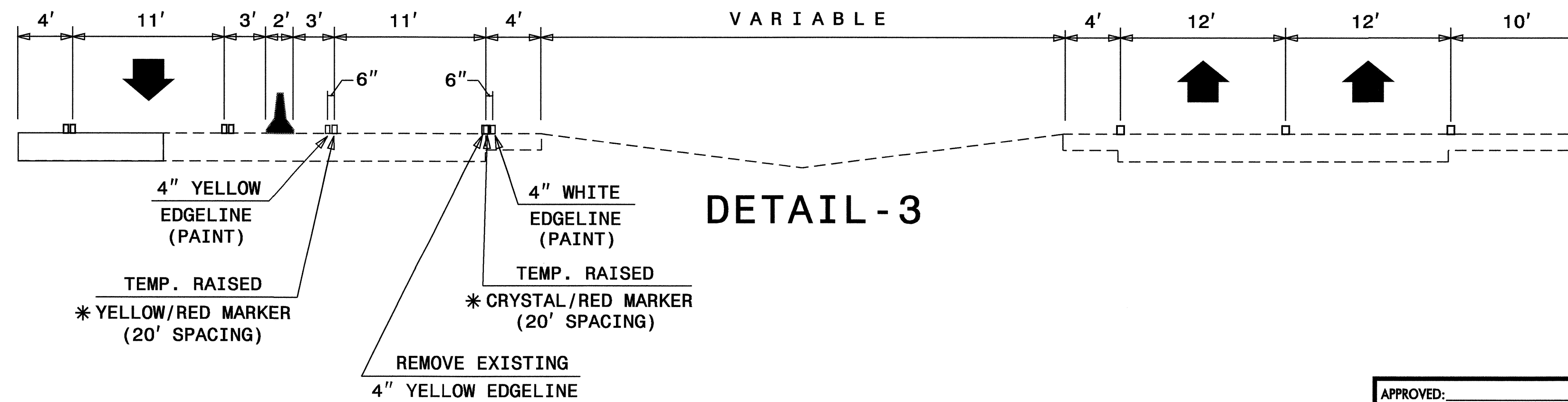
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DETAIL - 1

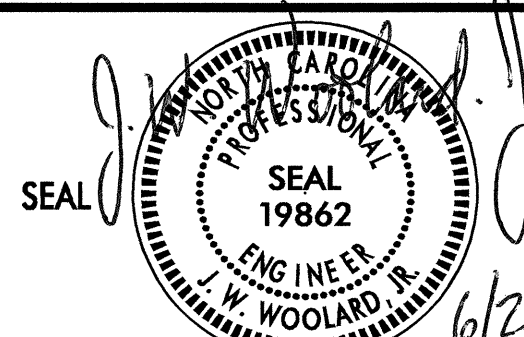
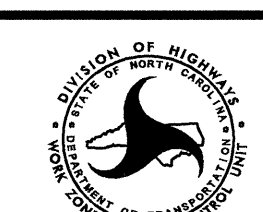


DETAIL - 2



DETAIL - 3

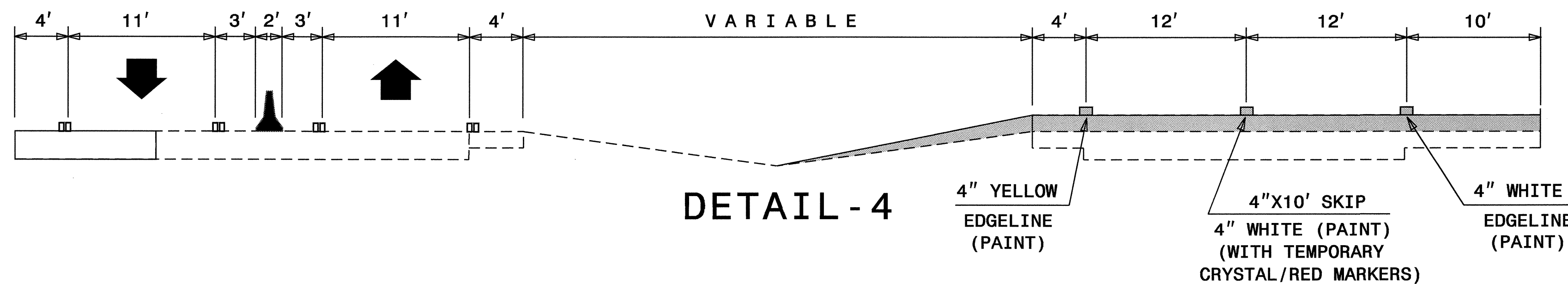
\* GORE AREAS AND CROSSOVERS ONLY.

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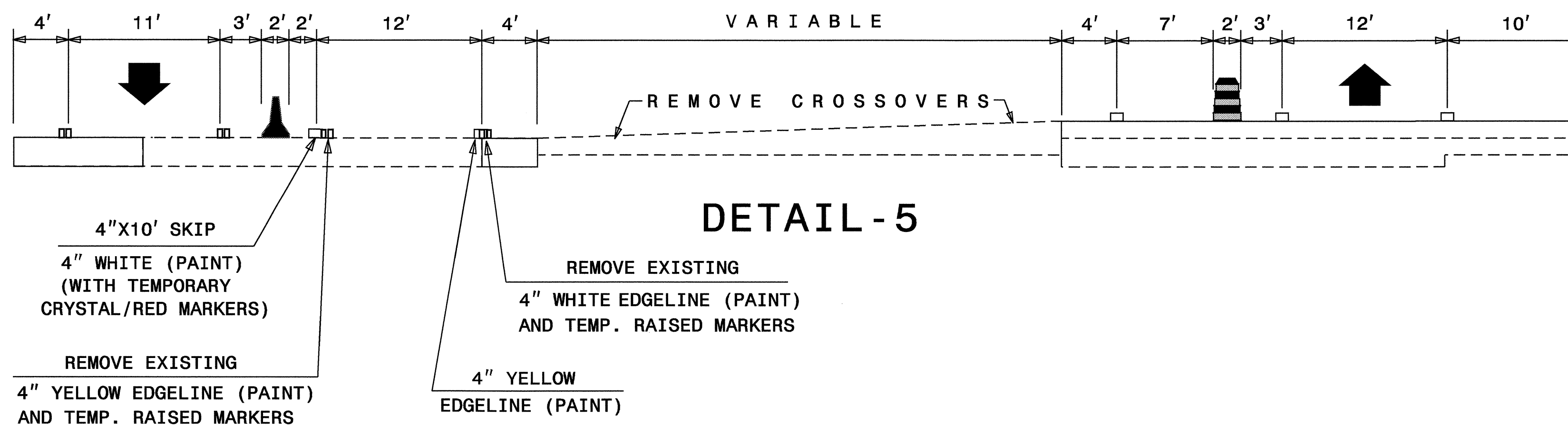


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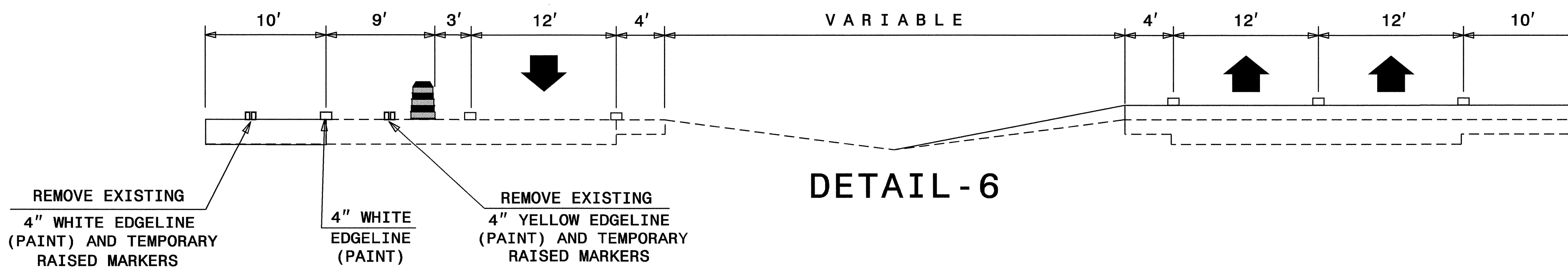
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DETAIL - 4



DETAIL - 5

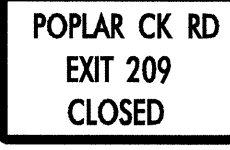



DETAIL - 6

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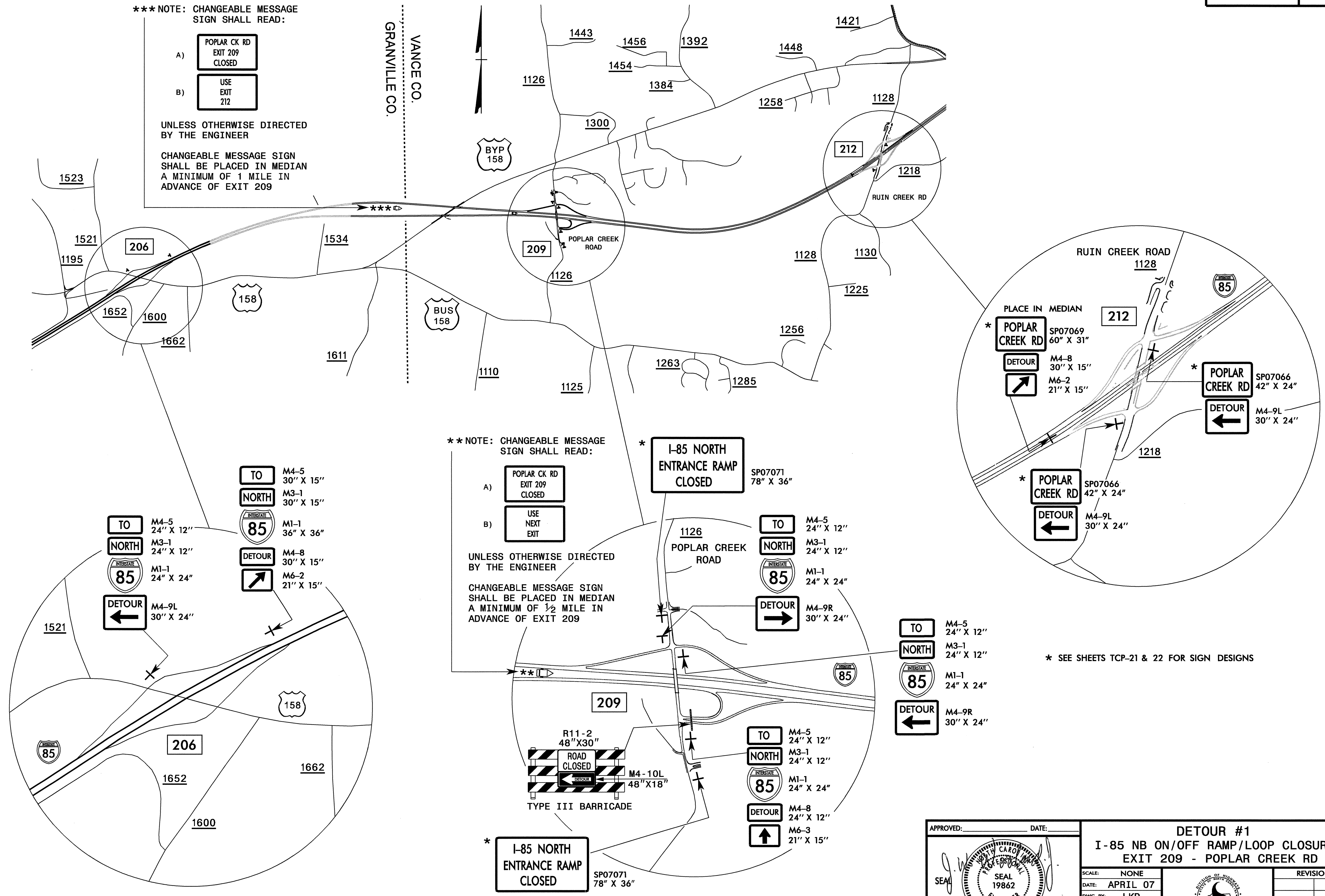
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\*\*\* NOTE: CHANGEABLE MESSAGE SIGN SHALL READ:

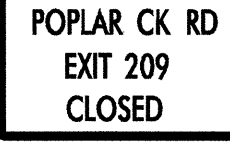

- A) 
- B) 

UNLESS OTHERWISE DIRECTED BY THE ENGINEER

CHANGEABLE MESSAGE SIGN SHALL BE PLACED IN MEDIAN A MINIMUM OF 1 MILE IN ADVANCE OF EXIT 209






\*\* NOTE: CHANGEABLE MESSAGE SIGN SHALL READ:

- A) 
- B) 

UNLESS OTHERWISE DIRECTED BY THE ENGINEER

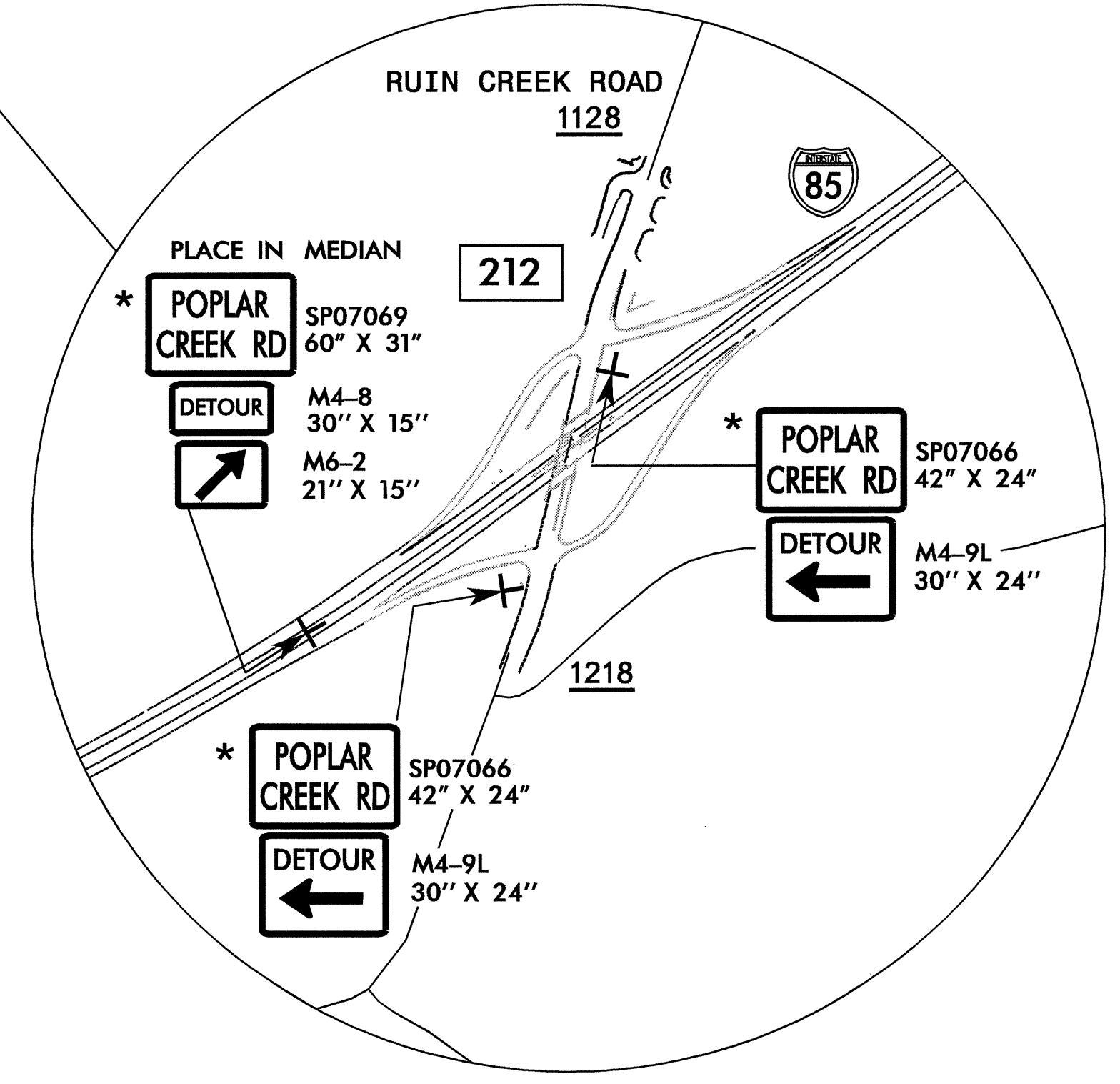
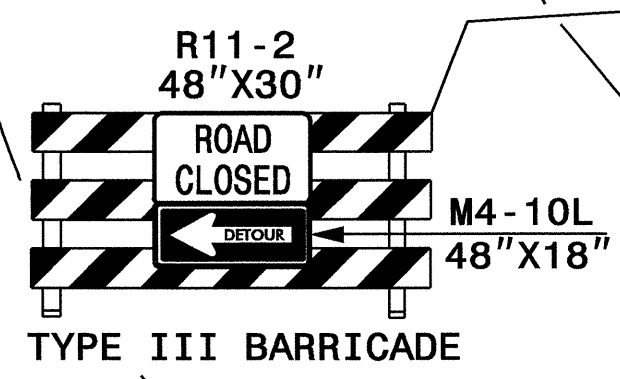
CHANGEABLE MESSAGE SIGN SHALL BE PLACED IN MEDIAN A MINIMUM OF 1/2 MILE IN ADVANCE OF EXIT 209

\* I-85 NORTH ENTRANCE RAMP CLOSED  
SP07071 78" X 36"

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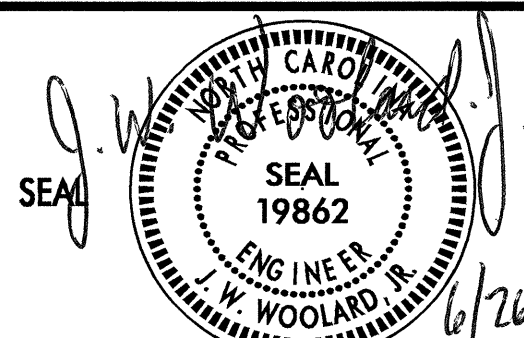
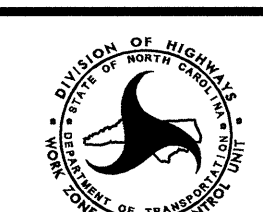
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- NORTH M3-1 24" X 12"
- I-85 M1-1 24" X 24"
- DETOUR M4-9R 30" X 24"

\* I-85 NORTH ENTRANCE RAMP CLOSED  
SP07071 78" X 36"

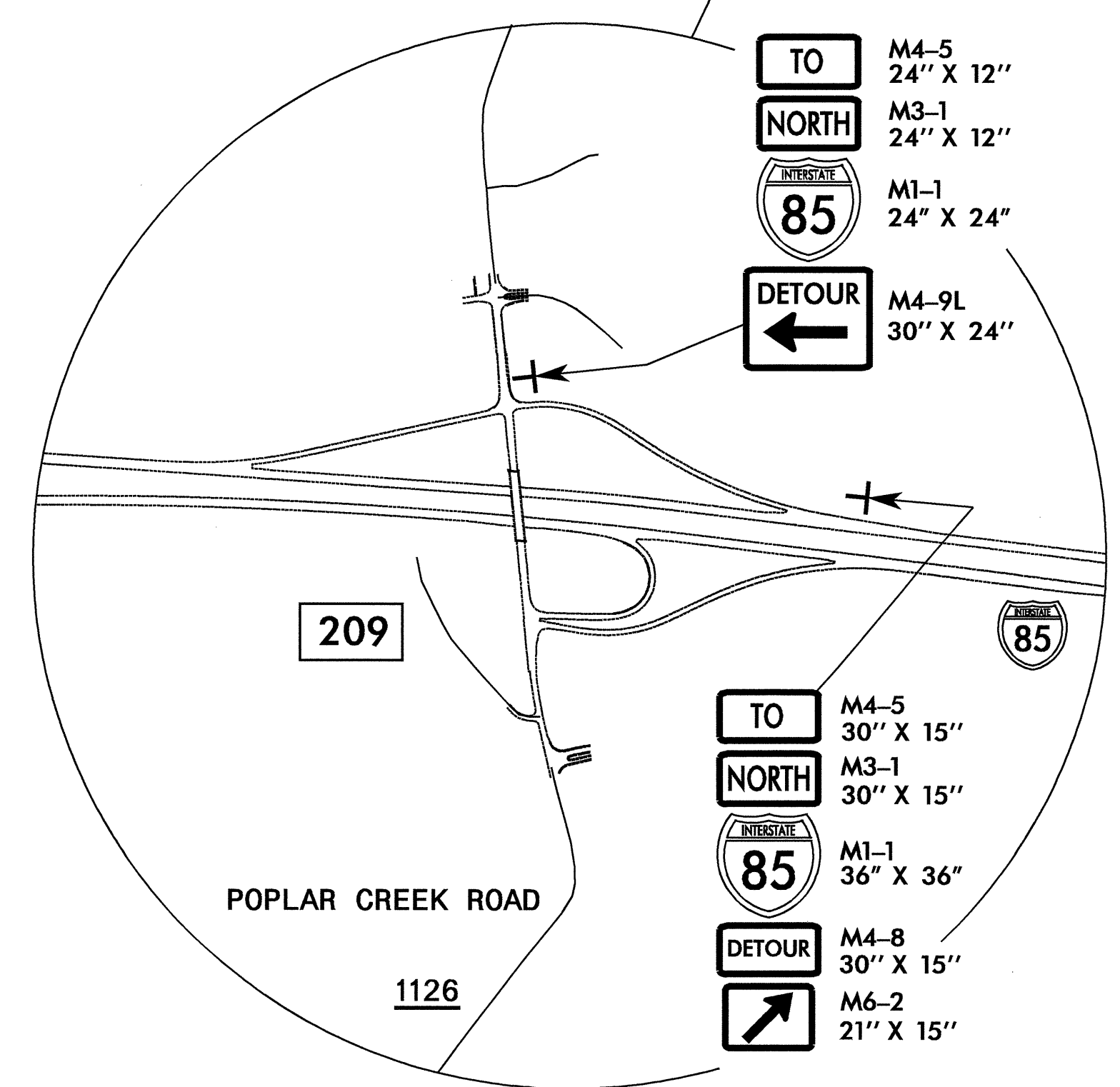
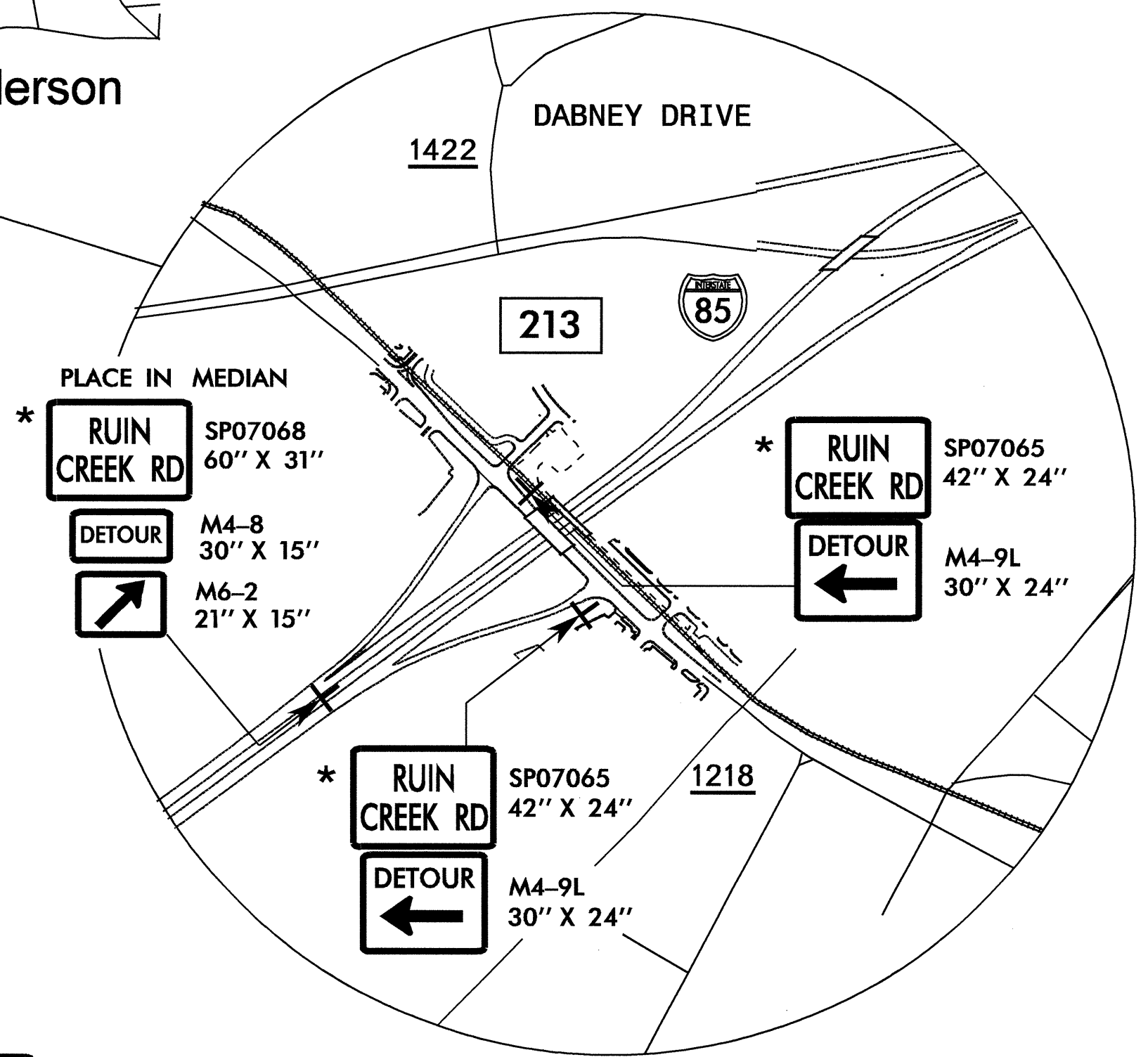
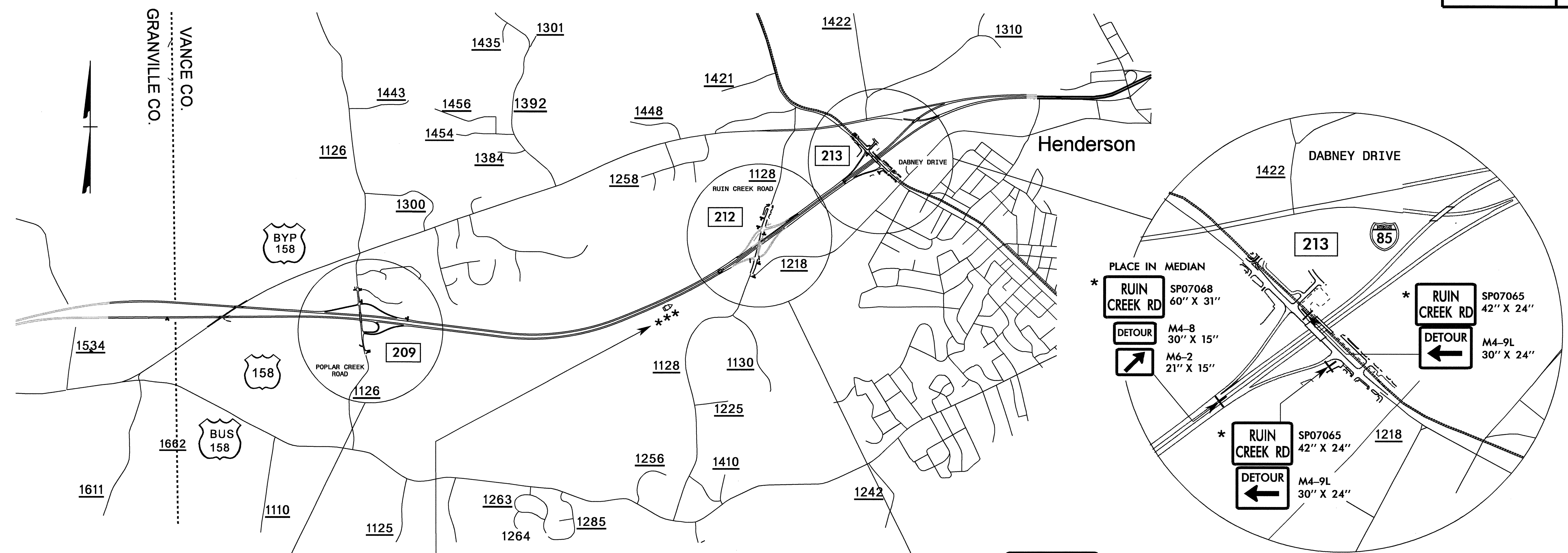


\* SEE SHEETS TCP-21 & 22 FOR SIGN DESIGNS

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<b>I-85 NB ON/OFF RAMP/LOOP CLOSURES</b>		<b>EXIT 209 - POPLAR CREEK RD</b>	
SCALE: NONE	DATE: APRIL 07		REVISIONS
DWG. BY: LKD	DESIGN BY: LKD		
REVIEWED BY: JWG	CADD FILE		





\*\*\* NOTE: CHANGEABLE MESSAGE SIGN SHALL READ:

- A) 

RUIN CK RD
EXIT 212
CLOSED
- B) 

USE
EXIT
213

UNLESS OTHERWISE DIRECTED BY THE ENGINEER  
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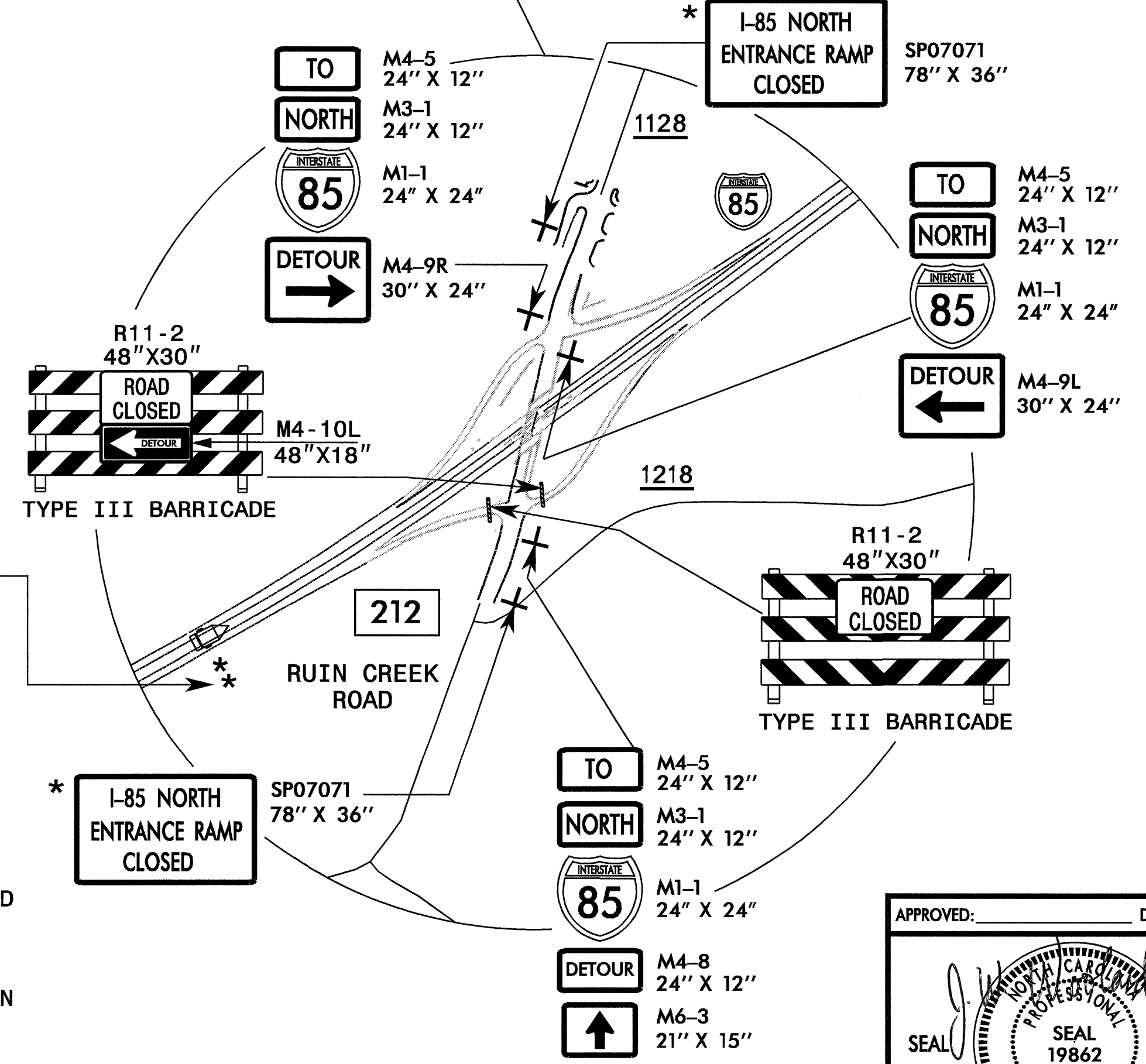
\*\* NOTE: CHANGEABLE MESSAGE SIGN SHALL READ:

- A) 

RUIN CK RD
EXIT 212
CLOSED
- B) 

USE
NEXT
EXIT

UNLESS OTHERWISE DIRECTED BY THE ENGINEER  
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\* SEE SHEETS TCP-21 & 22 FOR SIGN DESIGNS

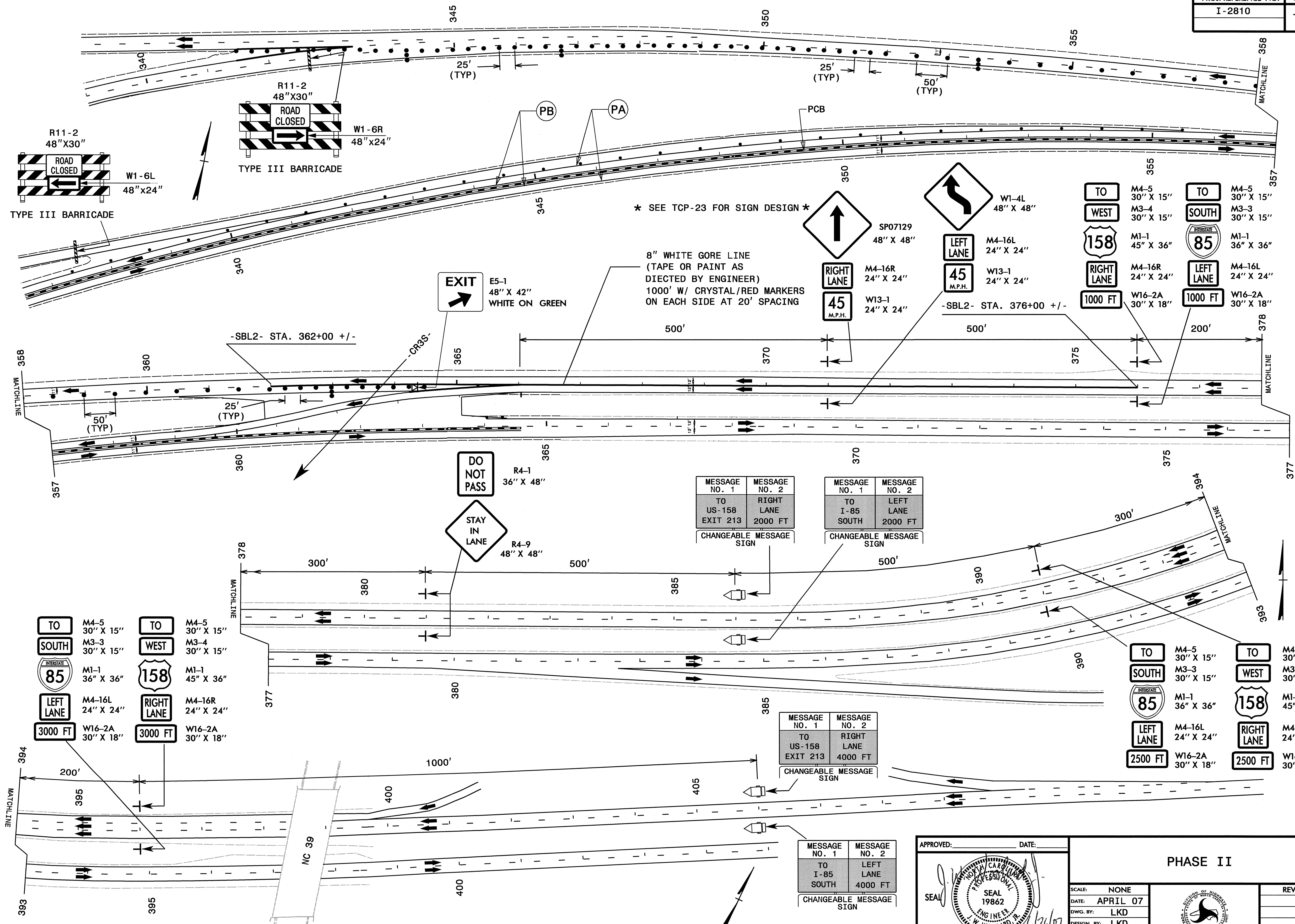
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<b>EXIT 212 - RUIN CREEK RD</b>		REVISIONS							
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 idonlison AT WZTC22231









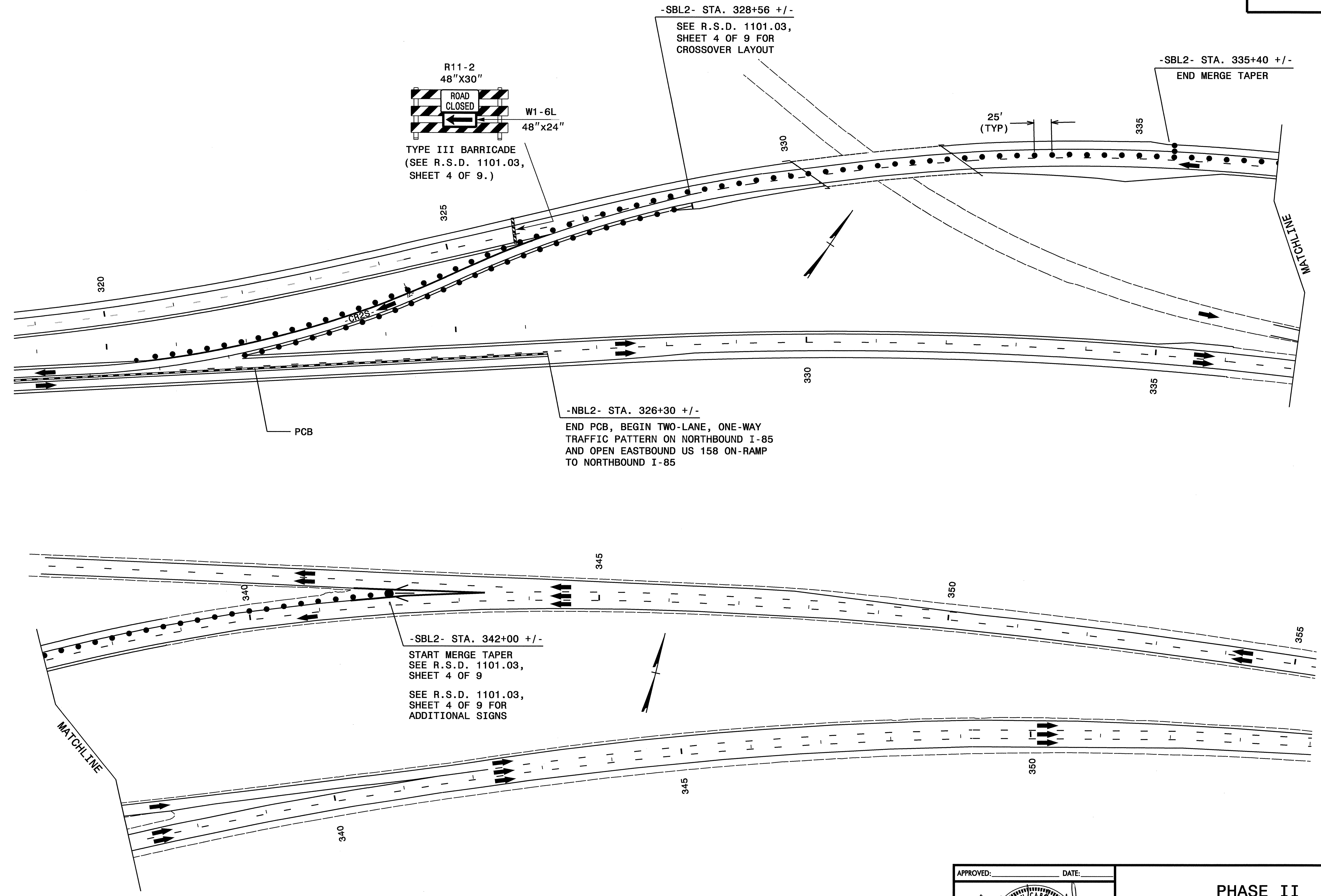
- |                            |                             |
|----------------------------|-----------------------------|
| TO M4-5 30" X 15"          | TO M4-5 30" X 15"           |
| SOUTH M3-3 30" X 15"       | WEST M3-4 30" X 15"         |
| 85 M1-1 36" X 36"          | 158 M1-1 45" X 36"          |
| LEFT LANE M4-16L 24" X 24" | RIGHT LANE M4-16R 24" X 24" |
| 3000 FT W16-2A 30" X 18"   | 3000 FT W16-2A 30" X 18"    |

- |                            |                             |
|----------------------------|-----------------------------|
| TO M4-5 30" X 15"          | TO M4-5 30" X 15"           |
| SOUTH M3-3 30" X 15"       | WEST M3-4 30" X 15"         |
| 85 M1-1 36" X 36"          | 158 M1-1 45" X 36"          |
| LEFT LANE M4-16L 24" X 24" | RIGHT LANE M4-16R 24" X 24" |
| 2500 FT W16-2A 30" X 18"   | 2500 FT W16-2A 30" X 18"    |

25-JUN-2007 08:05  
 \\dot\dfs\root\0\Proj\TIP\Projects\I2810\TrafficControl\I2810\_TC\_TCP\_11A.dgn  
 idonaldson AT WZTC2231

APPROVED: _____ DATE: _____		PHASE II					
SCALE: NONE		DATE: APRIL 07	DESIGN BY: LKD	REVIEWED BY: JWG			
		<table border="1"> <tr> <th>REVISIONS</th> </tr> <tr> <td> </td> </tr> <tr> <td> </td> </tr> <tr> <td> </td> </tr> </table>		REVISIONS			
REVISIONS							





R11-2  
48"X30"  
ROAD  
CLOSED  
W1-6L  
48"X24"  
TYPE III BARRICADE  
(SEE R.S.D. 1101.03,  
SHEET 4 OF 9.)

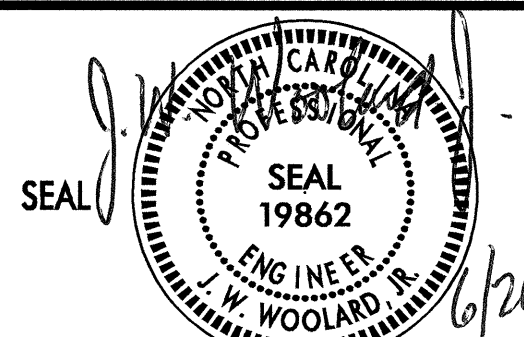
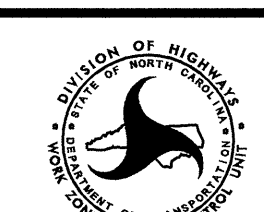
-SBL2- STA. 328+56 +/-  
SEE R.S.D. 1101.03,  
SHEET 4 OF 9 FOR  
CROSSOVER LAYOUT

-SBL2- STA. 335+40 +/-  
END MERGE TAPER

-NBL2- STA. 326+30 +/-  
END PCB, BEGIN TWO-LANE, ONE-WAY  
TRAFFIC PATTERN ON NORTHBOUND I-85  
AND OPEN EASTBOUND US 158 ON-RAMP  
TO NORTHBOUND I-85

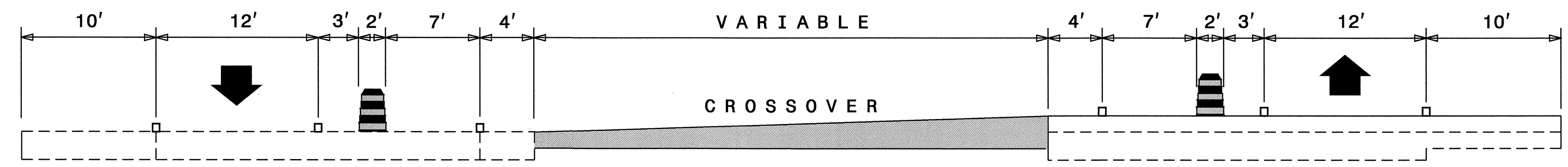
-SBL2- STA. 342+00 +/-  
START MERGE TAPER  
SEE R.S.D. 1101.03,  
SHEET 4 OF 9  
SEE R.S.D. 1101.03,  
SHEET 4 OF 9 FOR  
ADDITIONAL SIGNS

25-JUN-2007 08:05  
 \\dot\dfs\oc\10\Pro\TIP\Projects\N2810\Traffic\TrafficControl\Top\I2810\_TC\_TCP\_IB.dgn  
 DONALDSON AT WZTC21231

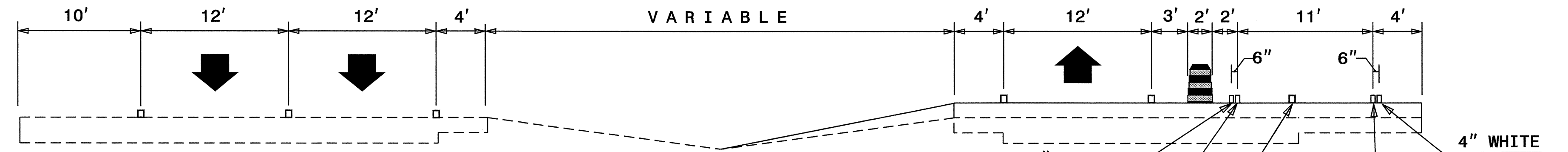
APPROVED:	DATE:	<b>PHASE II</b>	
	6/26/07		
SCALE: NONE	DATE: APRIL		REVISIONS
DWG. BY: LKD	DESIGN BY: LKD		
REVIEWED BY: JWG			

I - 85 SBL

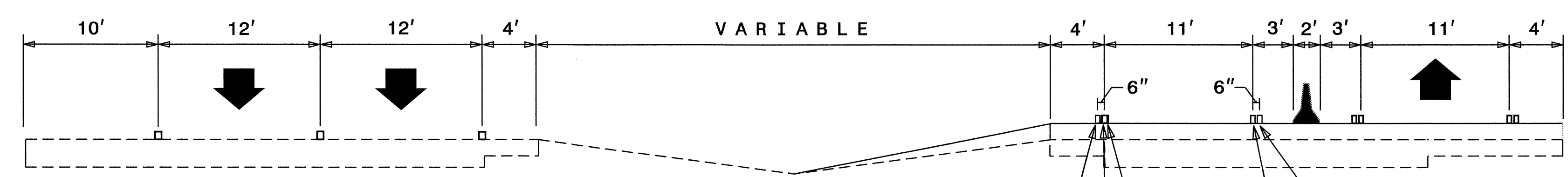
I - 85 NBL



DETAIL - 7



DETAIL - 8



DETAIL - 9

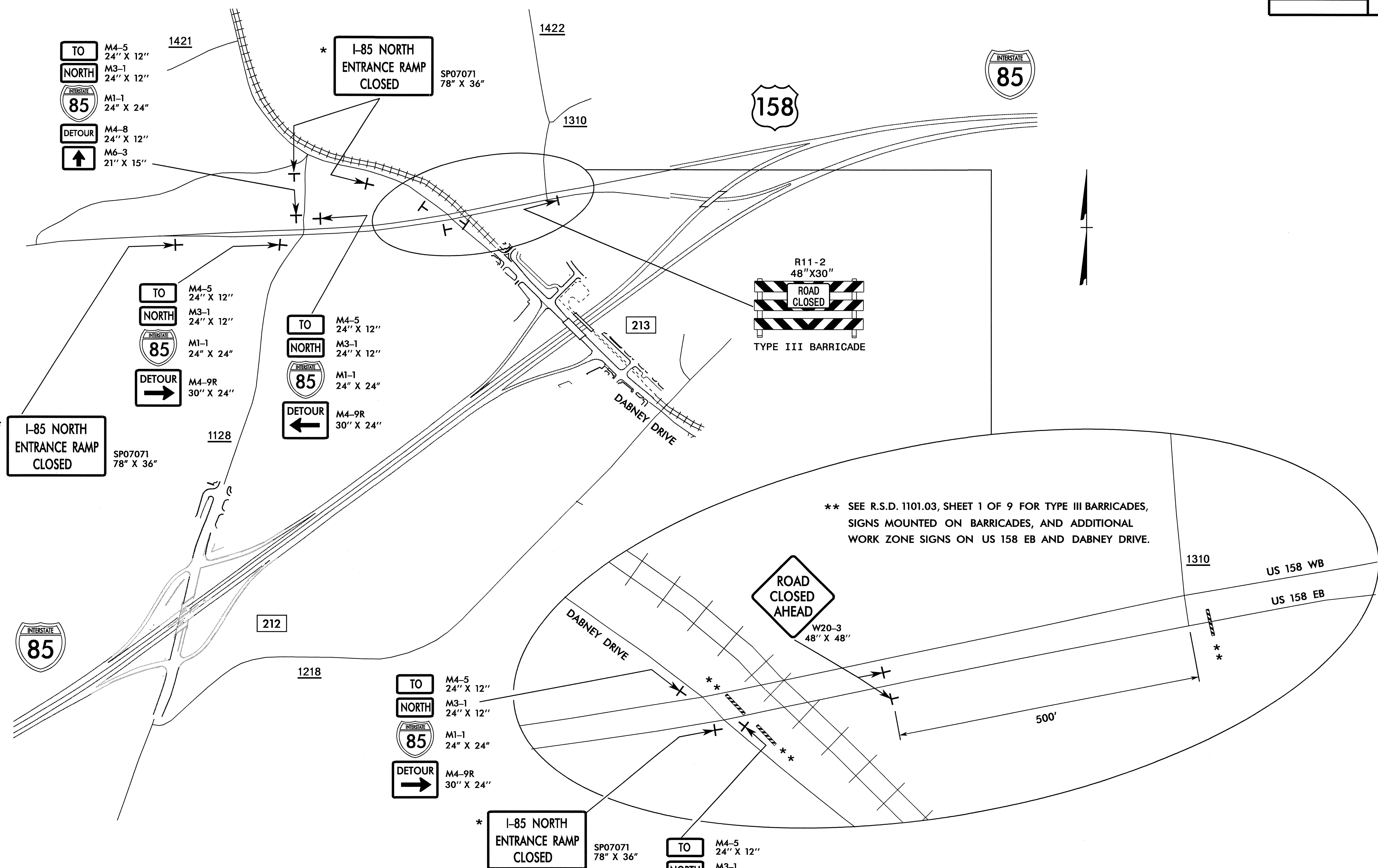
\* GORE AREAS AND CROSSOVERS ONLY.

APPROVED: _____ DATE: _____	<b>PHASE II DETAILS 7-9</b>		
	SCALE: NONE		
	DATE: APRIL 07		REVISIONS
	DWG. BY: LKD		
	DESIGN BY: LKD		
REVIEWED BY: JWG			

25-JUN-2007 08:04  
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 idonaldson AT WZTC21231





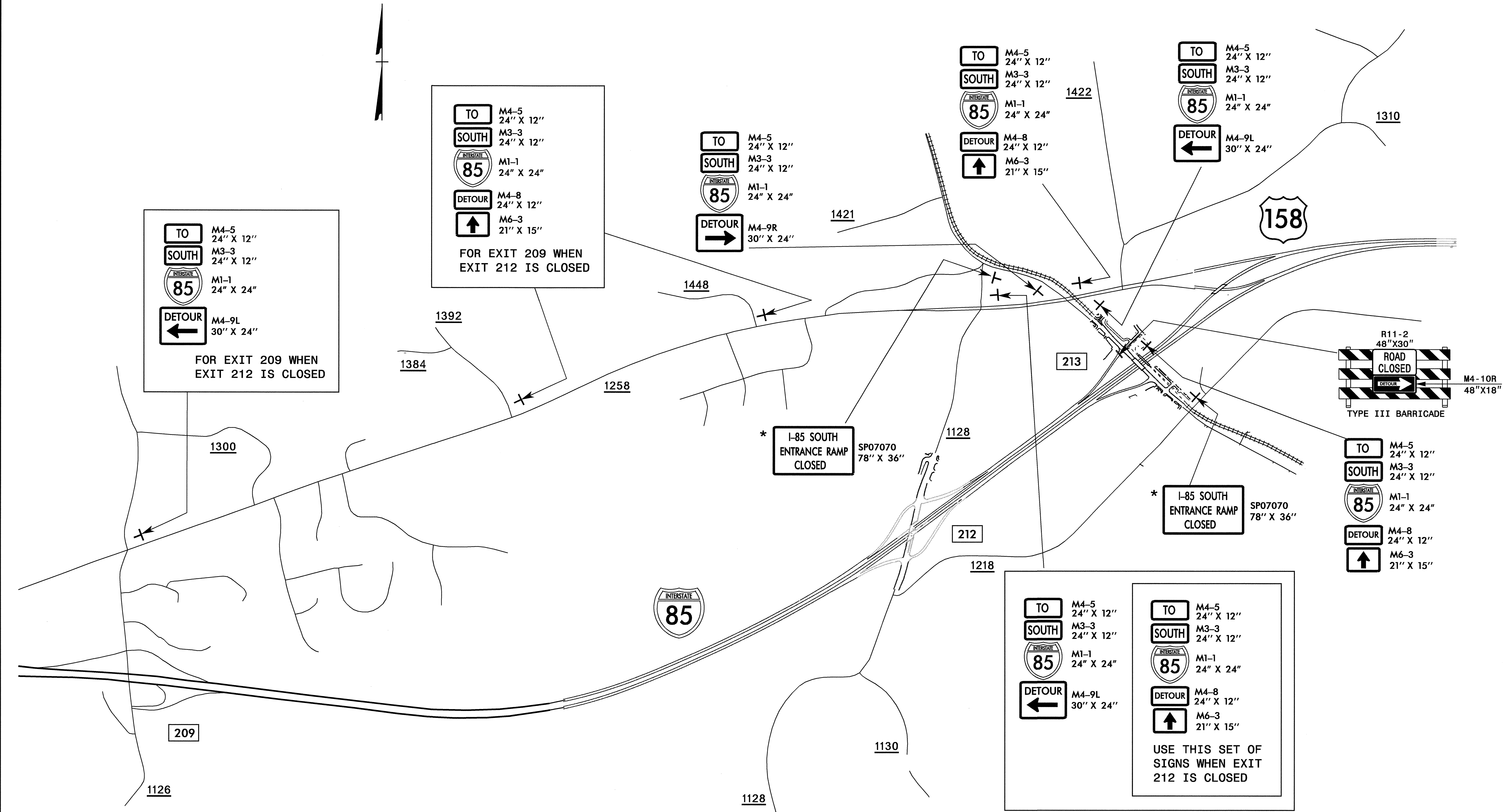


\* SEE SHEET TCP-21 FOR SIGN DESIGNS

APPROVED: _____ DATE: _____	<b>DETOUR #4</b>	
<b>US 158 EB ON-RAMP TO I-85 NB</b>		
SCALE: NONE		REVISIONS
DATE: APRIL 07		
DWG. BY: LKD		
DESIGN BY: LKD		
REVIEWED BY: JWG		CADD FILE

25-JUN-2007 08:04  
 \\dot\dfsroot\01\Proj\TIP\Projects\12810\Traffic\TrafficControl\tcp\2810\_TC\_TCP-14\_detour4.dgn  
 AT WZT212231  
 donaldson

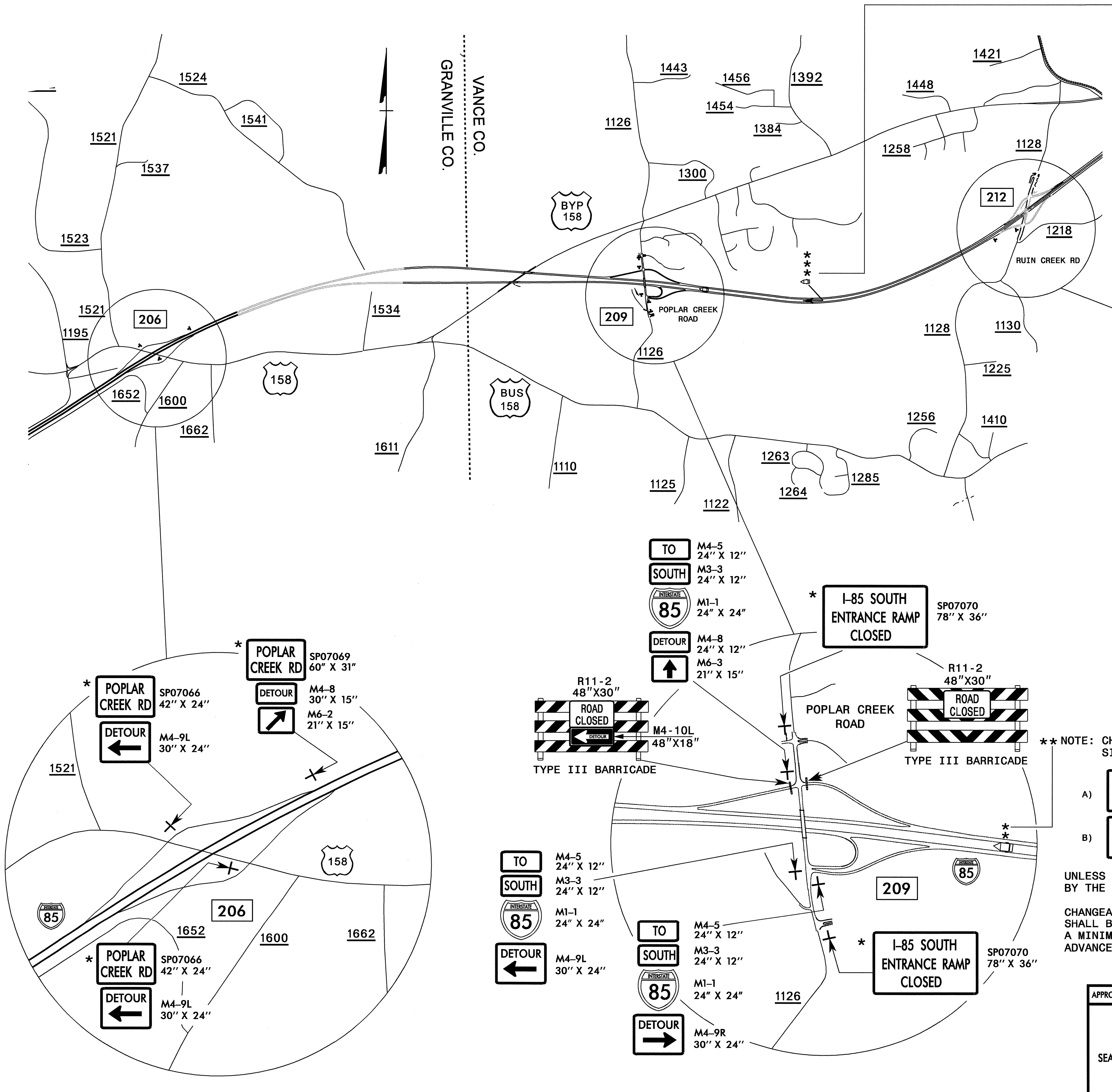




\* SEE SHEET TCP-21 FOR SIGN DESIGNS

25-JUN-2007 08:03  
 \\dot\dfs\root\Projects\N2810\Traffic\TrafficControl\top\I2810\_TC\_TCP-15\_detour5.dgn  
 IDonaldson AT WZTC21231

APPROVED: _____	DATE: _____	<b>DETOUR #5</b>	
		<b>DABNEY DR ON-RAMP TO I-85 SB</b>	
		SCALE: NONE	REVISIONS
DATE: APRIL 07			REVISIONS
DWG. BY: LKD			
DESIGN BY: LKD			
REVIEWED BY: JWG			CADD FILE

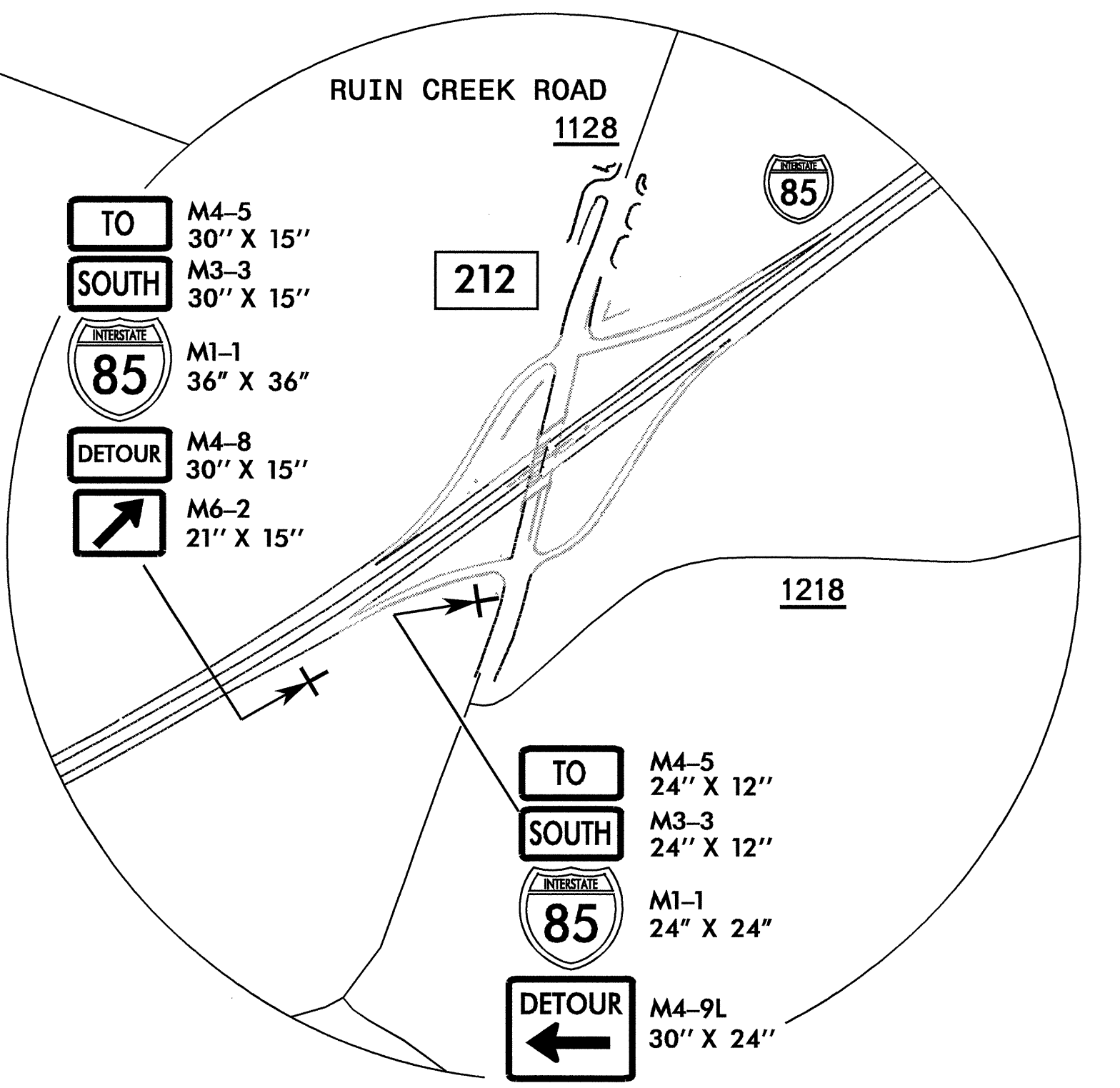


\*\*\* NOTE: CHANGEABLE MESSAGE SIGN SHALL READ:

- A) POPLAR CK RD  
EXIT 209  
CLOSED
- B) USE  
EXIT  
206

UNLESS OTHERWISE DIRECTED BY THE ENGINEER

CHANGEABLE MESSAGE SIGN SHALL BE PLACED IN MEDIAN A MINIMUM OF 1 MILE IN ADVANCE OF EXIT 209



\*\* NOTE: CHANGEABLE MESSAGE SIGN SHALL READ:

- A) POPLAR CK RD  
EXIT 209  
CLOSED
- B) USE  
NEXT  
EXIT

UNLESS OTHERWISE DIRECTED BY THE ENGINEER

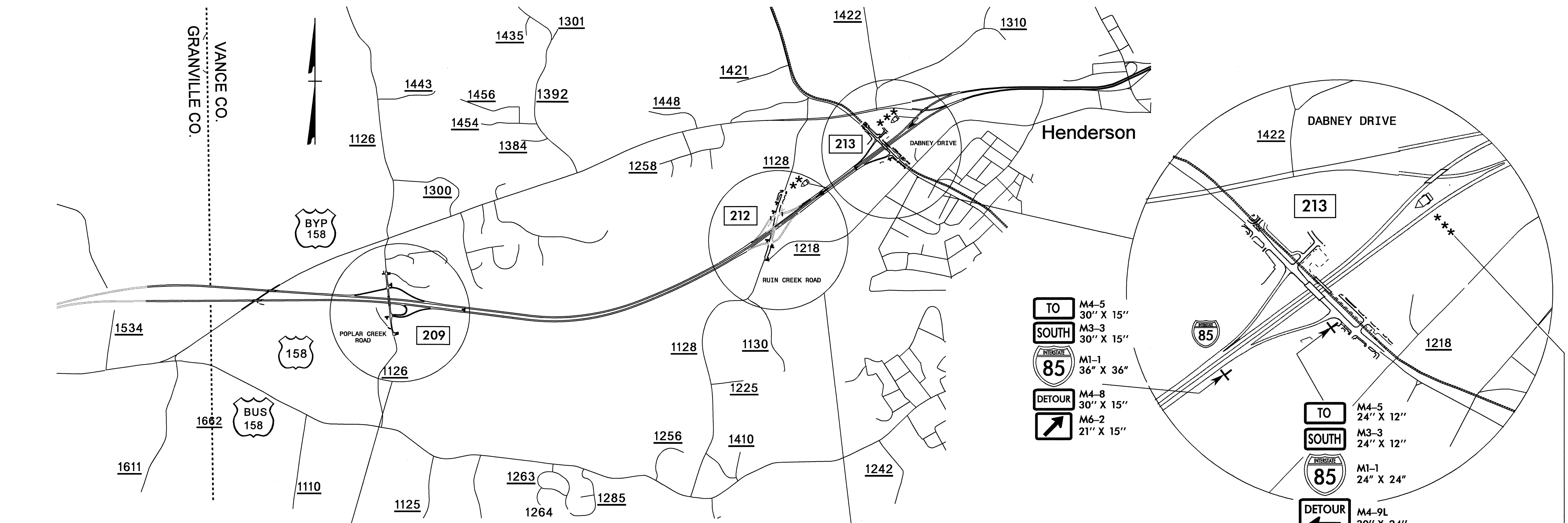
CHANGEABLE MESSAGE SIGN SHALL BE PLACED IN MEDIAN A MINIMUM OF 1/2 MILE IN ADVANCE OF EXIT 209

\* SEE SHEETS TCP-21 & 22 FOR SIGN DESIGNS

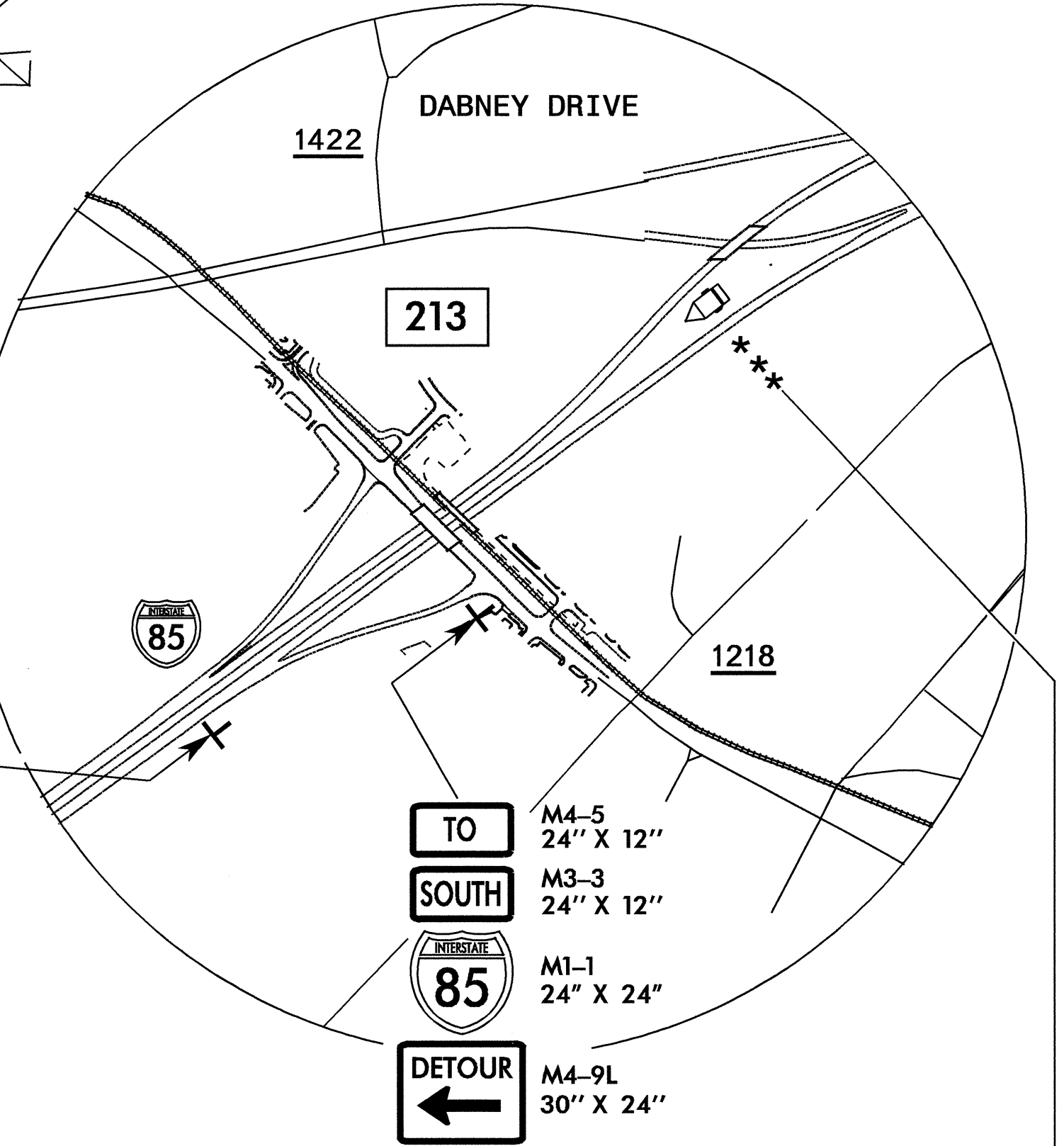
25-JUN-2007 08:40  
 \\dot\dfs\root\Projects\I-2810\Traffic\tr\aff\control\top\2810\_Tc\_TCP-16\_detour6.dgn  
 idonaldson AT WZTC21223

APPROVED: _____	DATE: _____	<b>DETOUR #6</b>	
		<b>I-85 SB ON/OFF RAMPS CLOSURE</b>	
		<b>EXIT 209 - POPLAR CREEK RD</b>	
SCALE: NONE	DATE: APRIL 07		REVISIONS
DWG. BY: LKD	DESIGN BY: LKD		
REVIEWED BY: JWG			





- TO M4-5 30" X 15"
- SOUTH M3-3 30" X 15"
- I-85 M1-1 36" X 36"
- DETOUR M4-8 30" X 15"
- M6-2 21" X 15"



- TO M4-5 24" X 12"
- SOUTH M3-3 24" X 12"
- I-85 M1-1 24" X 24"
- DETOUR M4-9L 30" X 24"

\* NOTE: SEE SHEETS TCP-21 & 22 FOR SIGN DESIGNS

\*\*\* NOTE: CHANGEABLE MESSAGE SIGN SHALL READ:

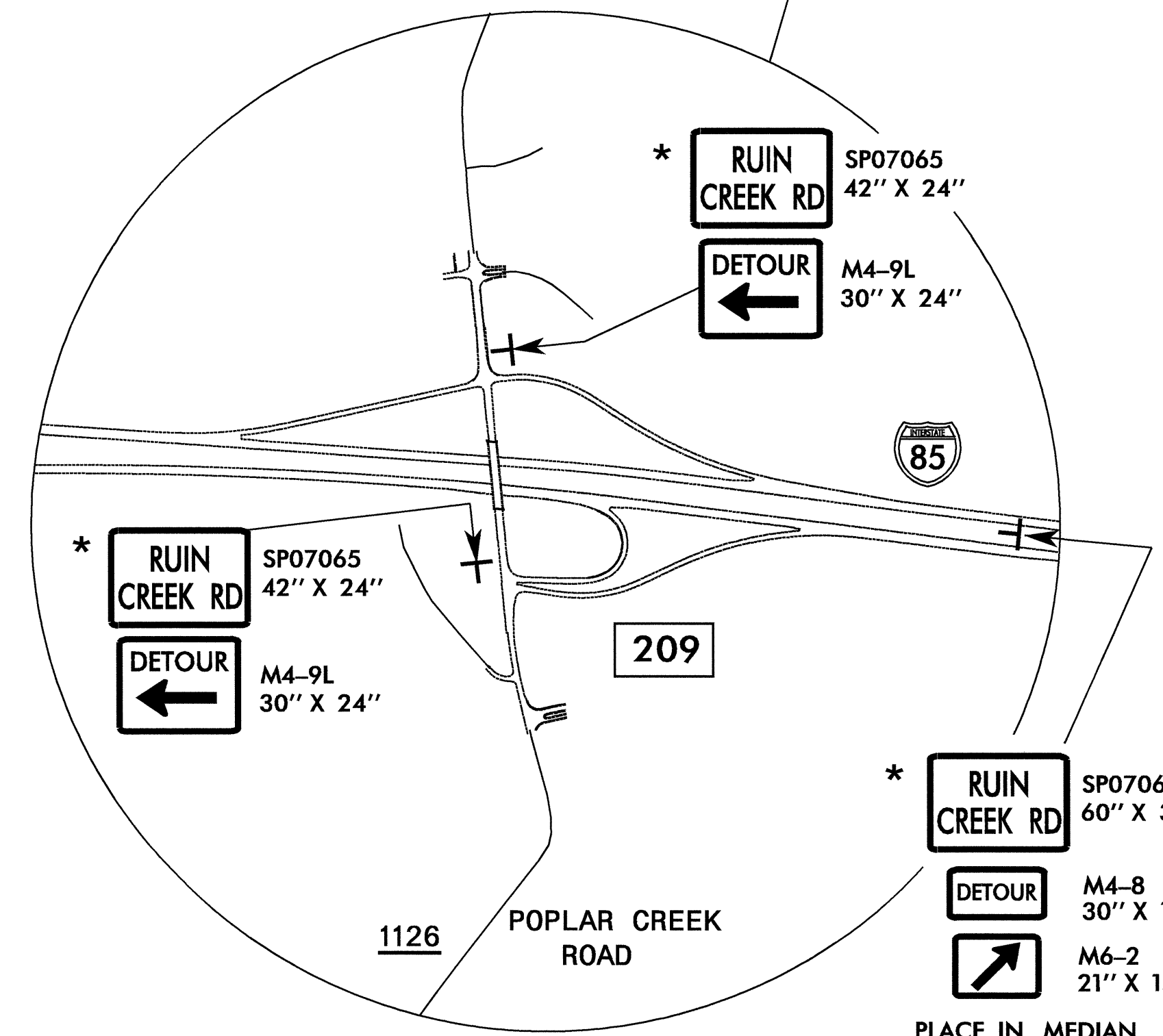
- A) RUI N CK RD  
EXIT 212  
CLOSED
- B) USE  
EXIT  
209

\*\* NOTE: CHANGEABLE MESSAGE SIGN SHALL READ:

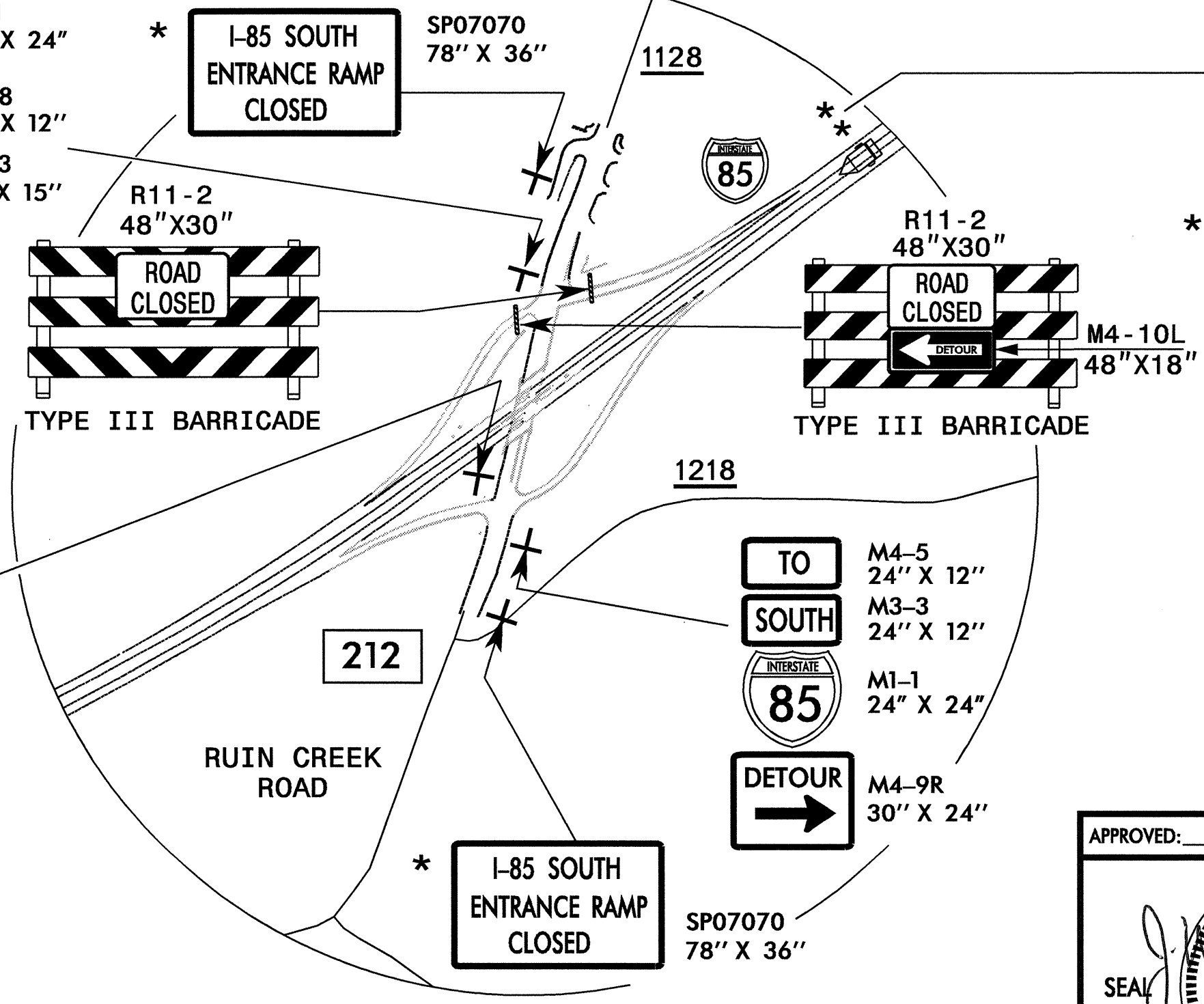
- A) RUI N CK RD  
EXIT 212  
CLOSED
- B) USE  
NEXT  
EXIT

UNLESS OTHERWISE DIRECTED BY THE ENGINEER

CHANGEABLE MESSAGE SIGN SHALL BE PLACED IN MEDIAN A MINIMUM OF 1/2 MILE IN ADVANCE OF EXIT 212



- TO M4-5 24" X 12"
- SOUTH M3-3 24" X 12"
- I-85 M1-1 24" X 24"
- DETOUR M4-8 24" X 12"
- M6-3 21" X 15"



- TO M4-5 24" X 12"
- SOUTH M3-3 24" X 12"
- I-85 M1-1 24" X 24"
- DETOUR M4-9L 30" X 24"

- TO M4-5 24" X 12"
- SOUTH M3-3 24" X 12"
- I-85 M1-1 24" X 24"
- DETOUR M4-9R 30" X 24"

- RUI N CK RD SP07068 60" X 31"
- DETOUR M4-8 30" X 15"
- M6-2 21" X 15"

PLACE IN MEDIAN

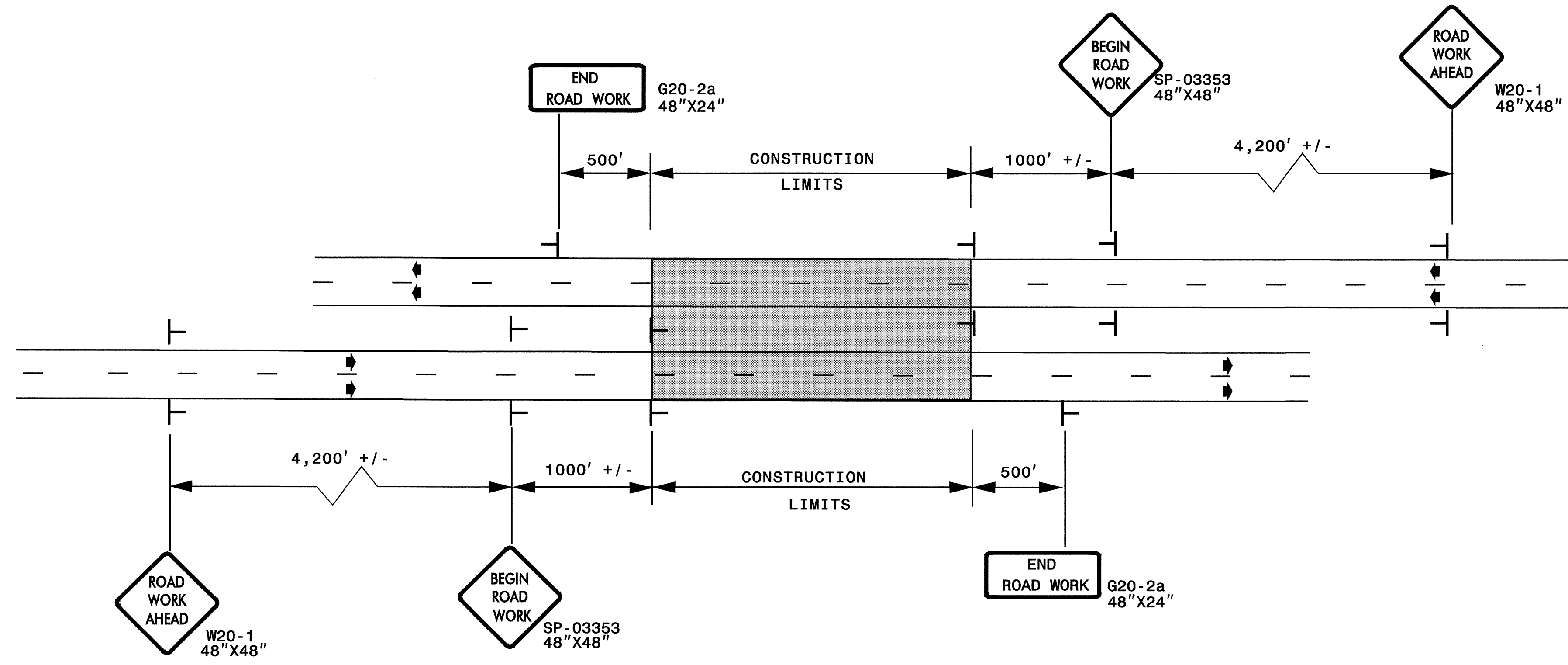
APPROVED: _____	DATE: _____	<b>DETOUR #7</b>	
		<b>I-85 SB ON/OFF RAMP CLOSURES EXIT 212 - RUI N CK RD</b>	
SCALE: NONE	DATE: APRIL		REVISIONS
DWG. BY: LKD	DESIGN BY: LKD		
REVIEWED BY: JWG			

25-JUN-2007 08:02  
 \\dot\dfs\oo\0\Pro\NTP\Projects\12810\_Traffic\TrafficControl\Top\12810\_TC\_TCP-17\_detour7.dgn  
 idonldson AT WZTC212231

# ADVANCED WORK ZONE WARNING SIGNING FOR FREEWAYS (4 LANES OR GREATER)

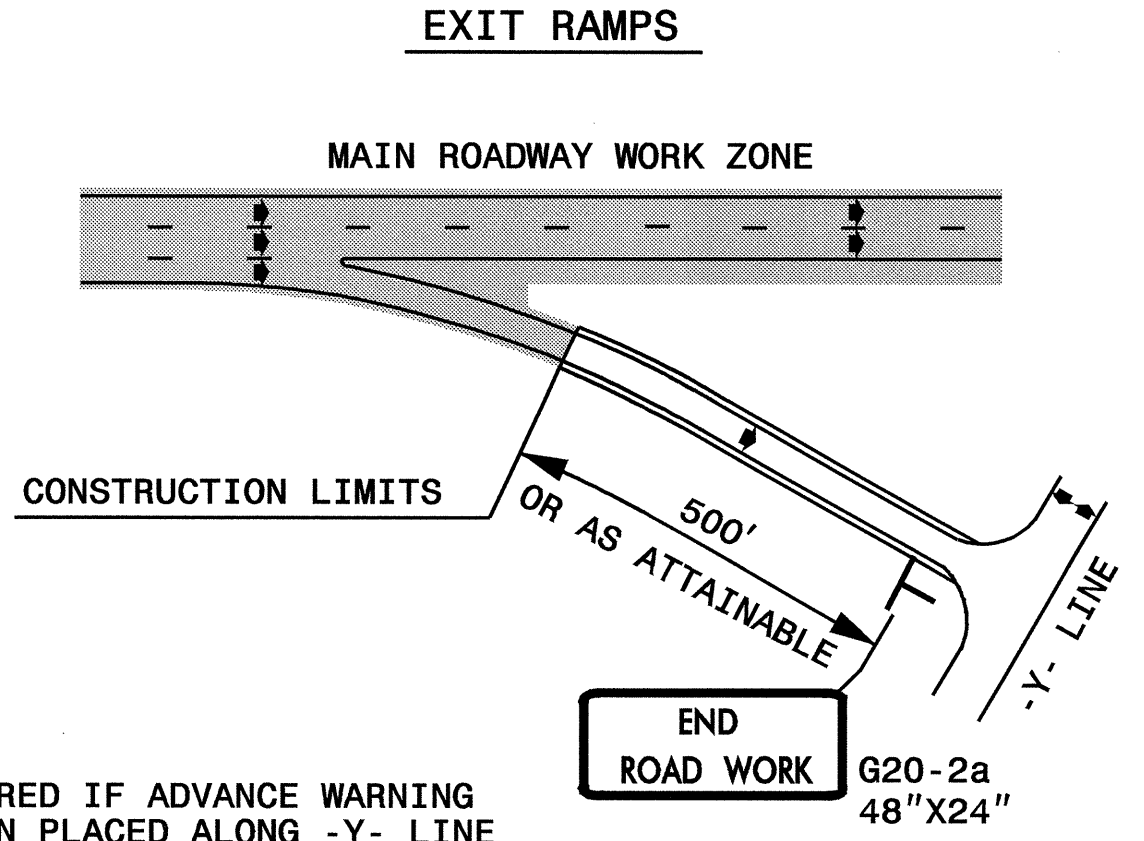
PROJ. REFERENCE NO.	SHEET NO.
I-2810	TCP-18

## DETAIL A



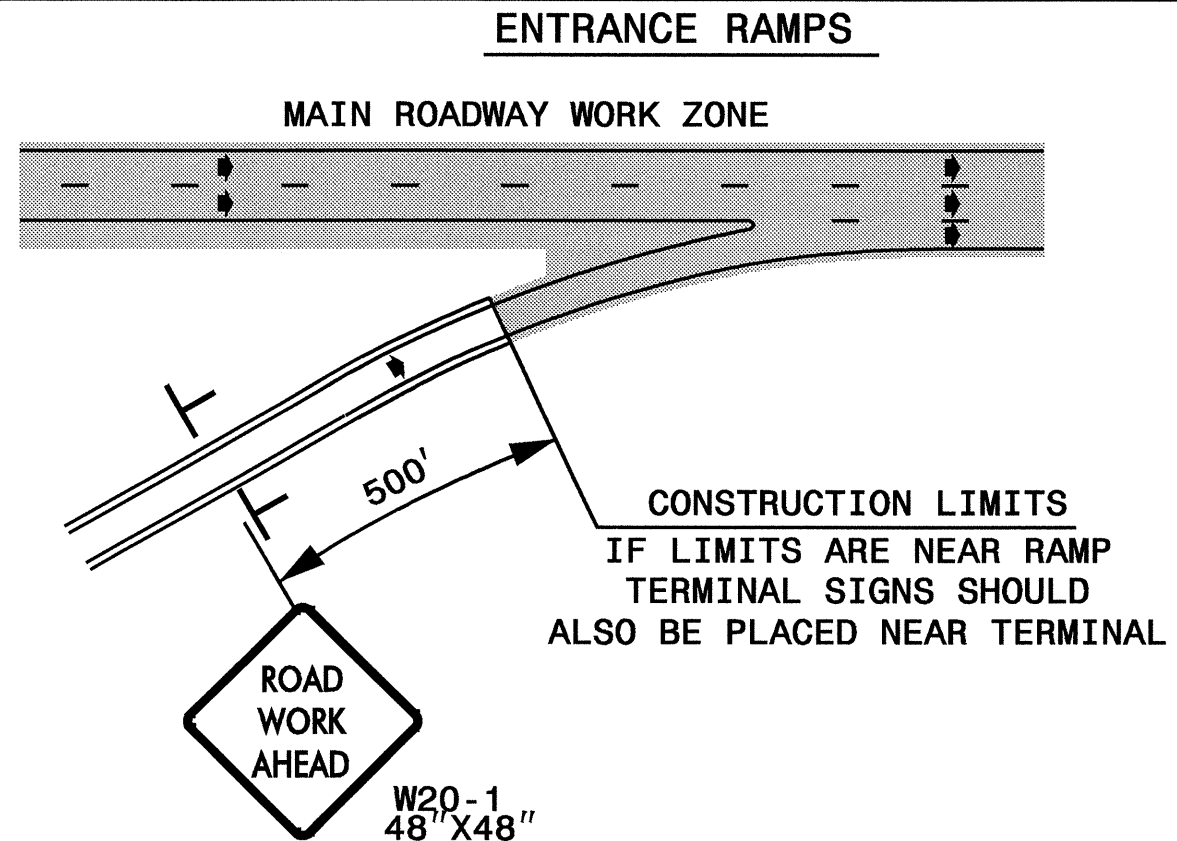
LEGEND	
	STATIONARY SIGN
▶	DIRECTION OF TRAFFIC FLOW

## DETAIL B

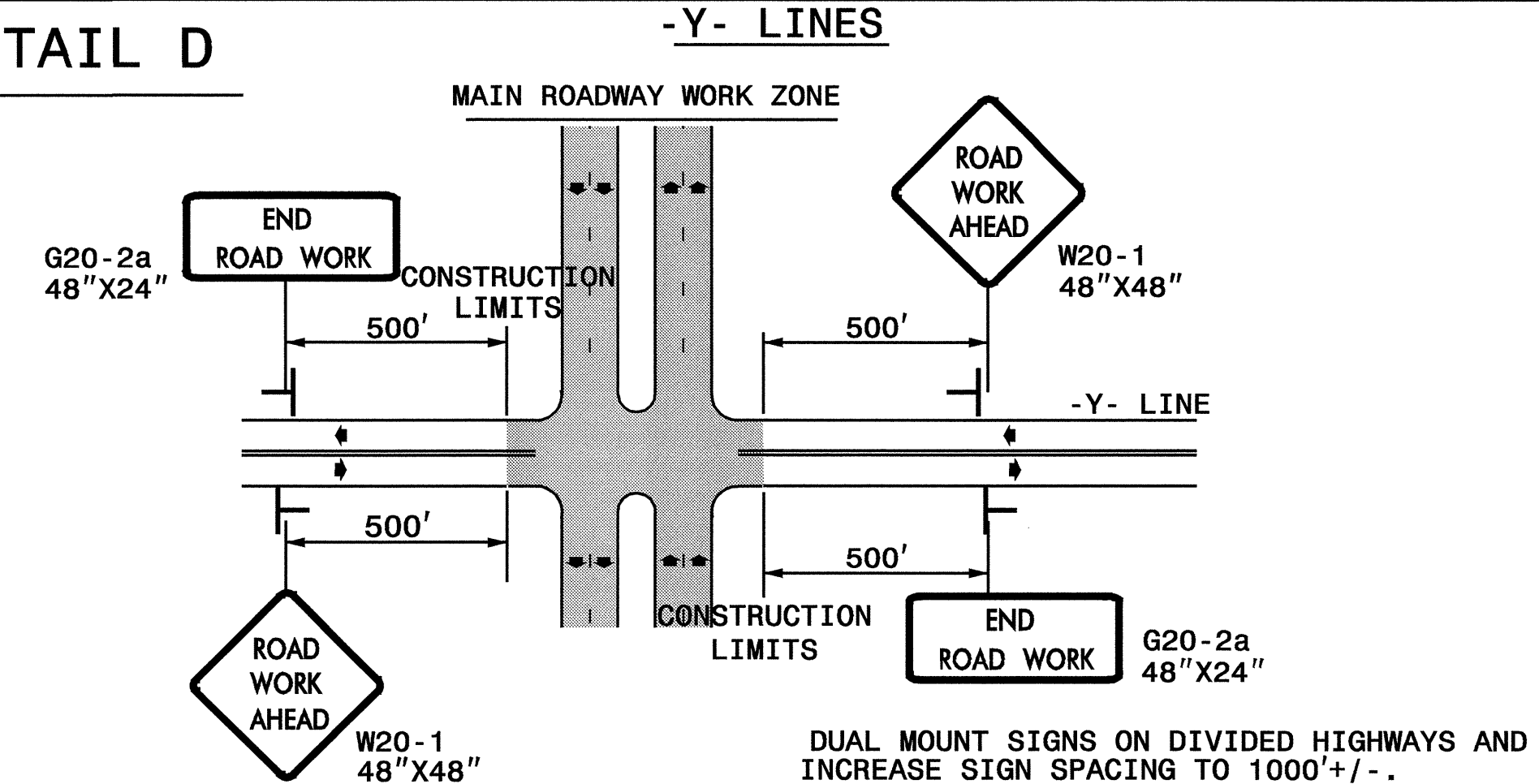


NOTE: SIGN NOT REQUIRED IF ADVANCE WARNING SIGNS HAVE BEEN PLACED ALONG -Y- LINE THAT RAMP INTERSECTS. IF CONSTRUCTION LIMITS ARE AT END OF RAMP, PLACE SIGN AT END OF RAMP.

## DETAIL C



## DETAIL D



### GENERAL NOTES

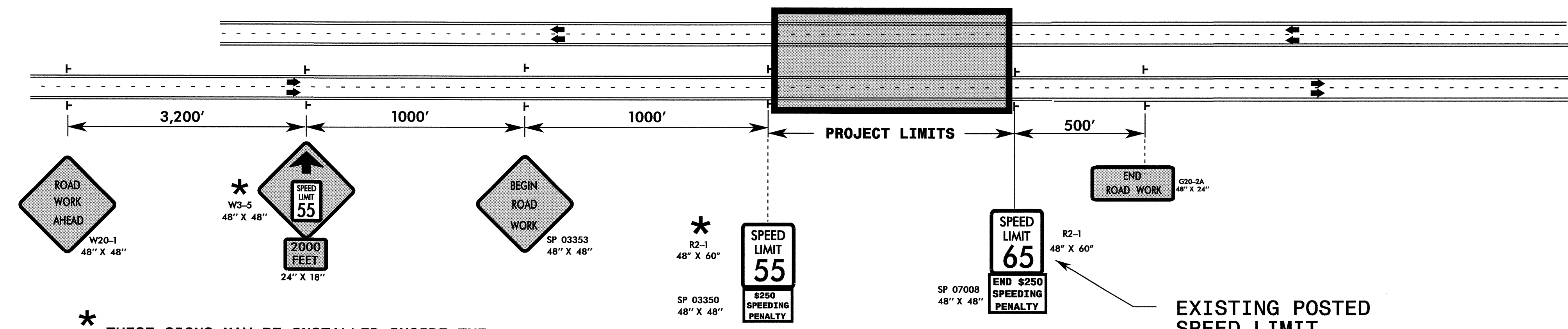
- USE FLUORESCENT ORANGE SHEETING (TYPE VII OR HIGHER) ON ALL ADVANCED WORK ZONE SIGNS.
- DO NOT INSTALL ADVANCE WARNING SIGNS MORE THAN 3 DAYS PRIOR TO BEGINNING OF WORK.
- SIGNS SHOWN ARE REQUIRED FOR WORK ZONES THAT WILL REMAIN IN EFFECT OVERNIGHT. FOR SHORT-TERM DAILY MAINTENANCE TYPE OPERATIONS, THIS SIGNING APPLICATION IS OPTIONAL; MAY USE ONLY APPLICABLE ROADWAY STANDARD DRAWINGS INSTEAD. HOWEVER, IF THIS SIGNING APPLICATION IS USED, SIGNS MAY BE PORTABLE MOUNTED.
- ALL SIGN SPACING DIMENSIONS ARE APPROXIMATE, FIELD ADJUST AS NECESSARY OR AS DIRECTED.
- USE 3LB STEEL U-CHANNEL POST OR 4" X 4" WOOD POST FOR ALL WORK ZONE SIGNS. 3LB STEEL U-CHANNEL POSTS MUST MEET THE REQUIREMENTS OF STANDARD SPECIFICATION SECTION 1094-1(B), MAY BE GALVANIZED STEEL, OR MAY BE PAINTED GREEN BY THE POST MANUFACTURER. SQUARE STEEL TUBING POSTS HAVING EQUIVALENT STRENGTH OF THE 3 LB STEEL U-CHANNEL POST ARE ALSO ACCEPTABLE FOR USE. ERECT SIGNS PER ROADWAY STANDARD DRAWING 1110.01. PAYMENT FOR WOOD POSTS, 3LB STEEL U-CHANNEL AND SQUARE STEEL TUBING POSTS WITH SIGNS WILL BE MADE ACCORDING TO STANDARD SPECIFICATION "WORK ZONE SIGNS" SECTION 1110.
- WHEN NECESSARY, USE SPLICING IN ACCORDANCE WITH ROADWAY STANDARD DRAWING NO. 1110.01. REMOVE ENTIRE POST WHEN REMOVING SIGNS WITH SPLICED POSTS.
- DO NOT BACK BRACE SIGN SUPPORTS.

APPROVED: _____ DATE: _____	ADVANCED WORK ZONE WARNING SIGNS FOR FREEWAYS (4 LANES OR GREATER)		
	SCALE: NONE		
	DATE: 8/03		REVISIONS
	DWG. BY: JI		03/04
	DESIGN BY: JI		
REVIEWED BY: JWG			

25-JUN-2007 08:02 \\dot\dfsroot\0\Proj\IP\Projects-1\2810\Traffic\TrafficControl\Top\I2810.TC.TCP-18.Freeways\_4lanes\_or\_greater.dgn idonaldson AT WZTC212231



## "WORK ZONE" SPEED LIMIT REDUCTIONS, 10 MPH OR LESS



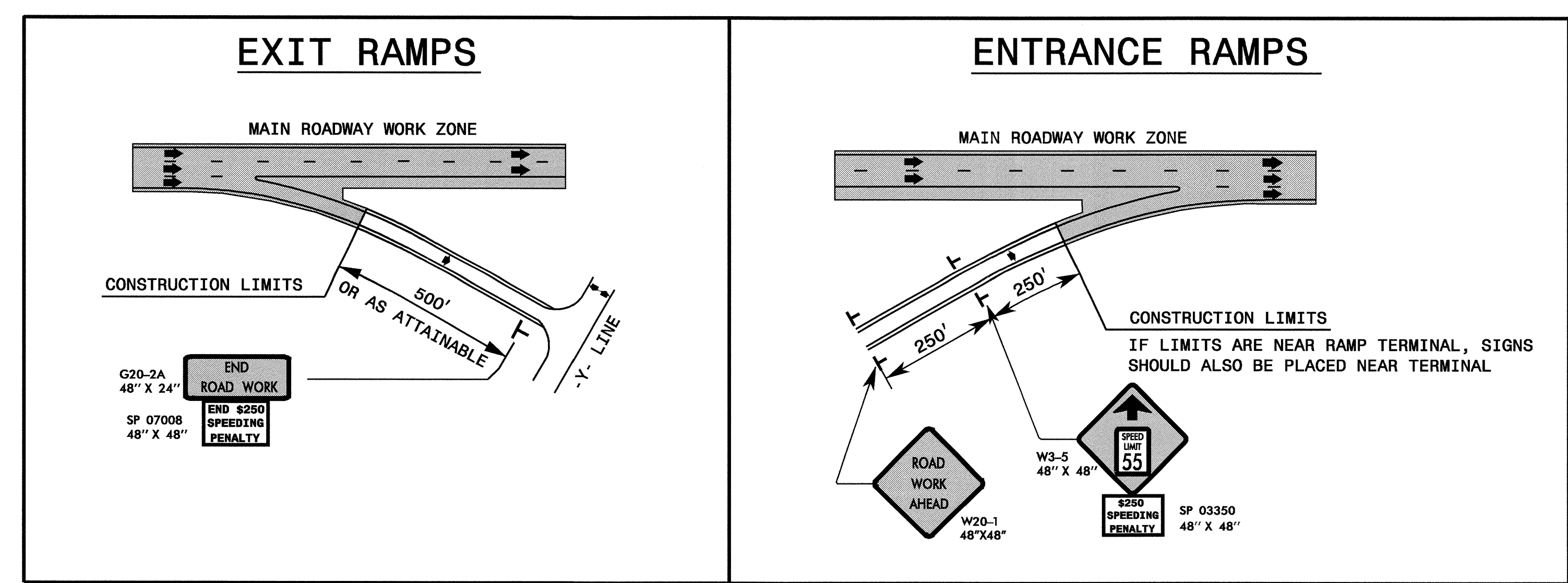
\* THESE SIGNS MAY BE INSTALLED INSIDE THE PROJECT LIMITS IF THE RESTRICTIVE FEATURES NECESSITATING THE SPEED LIMIT REDUCTION ARE NOT LOCATED AT THE PROJECT LIMITS. IF THIS OCCURS, SIGN W3-5 IS TO BE INSTALLED AT THE DISTANCE SHOWN IN ADVANCE OF WHERE THE SPEED LIMIT IS REDUCED

**"WORK ZONE" SPEED LIMIT**  
 NOTE: THE NEED AND LOCATION OF ADDITIONAL POSTED "WORK ZONE" SPEED LIMIT SIGNS WITHIN THE PROJECT LIMITS IS TO BE DETERMINED BY THE REGIONAL TRAFFIC ENGINEER

**EXISTING POSTED SPEED LIMIT**  
 NOTE: IF THE "WORK ZONE SPEED LIMIT" ONLY APPLIES TO A SPECIFIC PORTION OF THE PROJECT AND NOT THE ENTIRE PROJECT, THE EXISTING SPEED LIMIT IS TO BE REESTABLISHED INSIDE THE PROJECT LIMITS. THE EXISTING SPEED LIMIT SIGNS AND THE "END \$250 SPEEDING PENALTY" SIGNS ARE TO BE INSTALLED AT THE LOCATION WHERE THE EXISTING SPEED LIMIT IS TO RESUME. (SEE NOTE 3 BELOW)

### GUIDELINES

1. THIS DRAWING IS INTENDED TO SHOW THE SIGNING REQUIRED FOR A "WORK ZONE" SPEED LIMIT REDUCTION ON A FREEWAY WHICH IS 10 MPH OR LESS THAN THE EXISTING SPEED LIMIT.
2. EACH DIRECTION OF THE PROJECT IS TO BE EVALUATED FOR THE "WORK ZONE" SPEED LIMIT REDUCTION. THIS DRAWING INTENTIONALLY HAS 1 DIRECTION SIGNED AS A REMINDER TO CAREFULLY CONSIDER WHETHER BOTH DIRECTIONS OF THE PROJECT NEED TO HAVE THE SPEED LIMIT REDUCED.
3. IN ADDITION, FOR PROJECTS THAT EXCEED 2 MILES IN LENGTH, AN EVALUATION IS TO BE MADE TO DETERMINE IF THE "WORK ZONE" SPEED LIMIT REDUCTION APPLIES TO THE ENTIRE PROJECT LENGTH OR ONLY A PORTION OF THE PROJECT LENGTH. THE "WORK ZONE" SPEED LIMIT REDUCTION MAY TERMINATE BEFORE THE END OF THE PROJECT LIMITS. THE DRAWING IS TO BE MODIFIED AS NEEDED TO REFLECT THESE CONDITIONS.
4. THE \$250 SPEEDING PENALTY APPLIES FOR ALL PROJECTS THAT QUALIFY FOR A "WORK ZONE" SPEED LIMIT REDUCTION.
5. ALL "WORK ZONE" SPEED LIMIT REDUCTION SIGNAGE SHALL BE REMOVED WHEN THE CONDITION THAT WARRANTED THE REDUCTION AND PENALTY IS REMOVED. THE REGIONAL TRAFFIC ENGINEER SHALL BE NOTIFIED BY THE RESIDENT ENGINEER AT THIS TIME TO RESCIND THE ORDINANCE AND RETURN TO THE EXISTING POSTED SPEED LIMIT. THIS SHOULD TAKE PLACE BEFORE THE PROJECT IS 100% COMPLETE AND ACCEPTED FOR MAINTENANCE.



APPROVED: _____	DATE: _____	<b>"WORK ZONE" SPEED LIMIT REDUCTIONS, 10 MPH OR LESS</b>					
		SCALE: NONE					
		DATE: APRIL 07					
		DESIGN BY: LKD		REVISIONS			
		REVIEWED BY: JWG		<table border="1" style="width: 100%; height: 40px;"> <tr><td> </td><td> </td></tr> <tr><td> </td><td> </td></tr> <tr><td> </td><td> </td></tr> </table>			

25-JUN-2007 08:01 \\dot\dfsroot\1\proj\TIP\Projects-12810\trafficcontrol\top\12810.TC.TCP-19.10 MPH or Below.dgn  
 donaldson AT WZTC21231



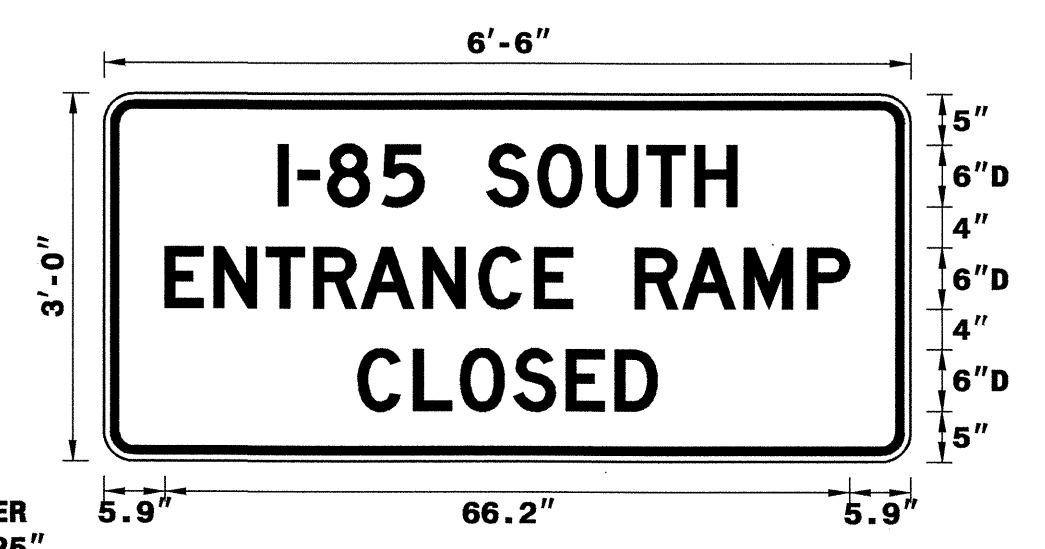




N. C. DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS  
TRAFFIC ENGINEERING BRANCH  
SIGNING DEPARTMENT

SIGN NUMBER: SP07070  
TYPE: D  
QUANTITY: 6  
SIGN WIDTH: 78"  
HEIGHT: 36"  
TOTAL AREA: 19.5 Sq.Ft.  
BORDER TYPE: FLUSH  
RECESS: 0.63"  
WIDTH: 0.88"  
RADII: 2.25"  
NO. Z BARS: LENGTH:  
BACKG COLOR: Fluorescent Orange  
COPY COLOR: Black  
SYMBOL X Y WID HT  
MAT'L: 0.125" (3.2 mm) ALUMINUM  
0.079" COMPOSITE

DESIGN BY: R. HENNEIN  
PROJECT ID: I-2810  
CHECKED BY: DIV: 5  
STD #: N/A  
DATE: Mar 16, 2007



USE NOTES:  
1. Legend and border shall be direct applied non-reflective sheeting.  
2. Background shall be Type VII, VIII, or IX (prismatic) retroreflective sheeting.

LETTER POSITIONS

Letter spacings are to start of next letter													Series/Size			
	I	-	S	S	O	U	T	H					Text Length			
	16.8	1.9	2.9	5.3	4.1	6	5	5.6	5	4.6	4.1	16.8	D 2000 44.3			
	E	N	T	R	A	N	C	E	R	A	M	P	D 2000 66.2			
	5.9	4.7	5	4.6	4.6	6	5.4	5.4	3.7	6	4.6	6	6.1	4.1	5.9	
	C	L	O	S	E	D										D 2000 29
	24.5	5.4	4.5	5.2	5.1	4.7	4.1	24.5								

Spacing Factor is 1 unless specified otherwise  
FILENAME: SP07070\_I-85southentrancerclosed

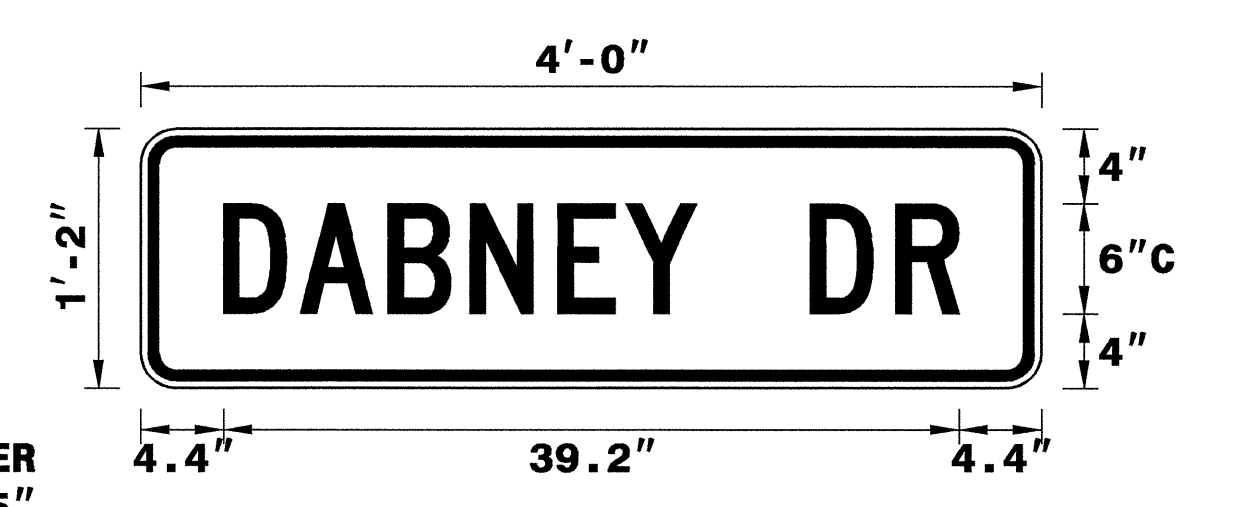
NORTH CAROLINA D.O.T. SIGN DETAIL



N. C. DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS  
TRAFFIC ENGINEERING BRANCH  
SIGNING DEPARTMENT

SIGN NUMBER: SP07064  
TYPE: D  
QUANTITY: 4  
SIGN WIDTH: 48"  
HEIGHT: 14"  
TOTAL AREA: 4.7 Sq.Ft.  
BORDER TYPE: FLUSH  
RECESS: 0.38"  
WIDTH: 0.63"  
RADII: 1.5"  
NO. Z BARS: LENGTH:  
BACKG COLOR: Fluorescent Orange  
COPY COLOR: Black  
SYMBOL X Y WID HT  
MAT'L: 0.080" (2.0 mm) ALUMINUM  
0.079" COMPOSITE

DESIGN BY: R. HENNEIN  
PROJECT ID: I-2810  
CHECKED BY: DIV: 5  
STD #: N/A  
DATE: Mar 15, 2007



USE NOTES:  
1. Legend and border shall be direct applied non-reflective sheeting.  
2. Background shall be Type VII, VIII, or IX (prismatic) retroreflective sheeting.

LETTER POSITIONS

Letter spacings are to start of next letter													Series/Size
	D	A	B	N	E	Y	D	R					Text Length
	4.4	4.1	4.7	4.4	4.7	3.6	3.8	6	4.6	3.4	4.4		C 2000 39.2

Spacing Factor is 1 unless specified otherwise  
FILENAME: SP07064\_dabneydr

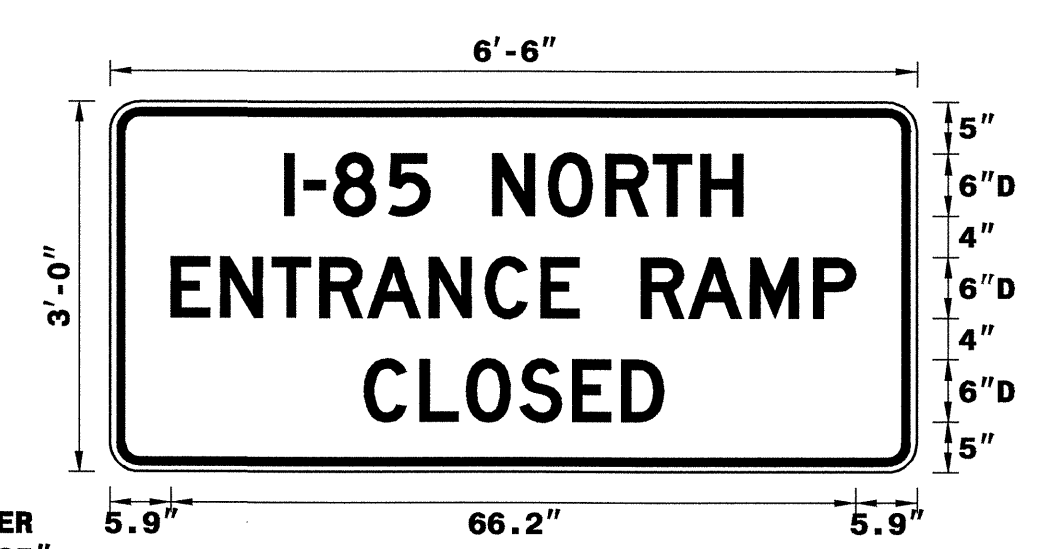
NORTH CAROLINA D.O.T. SIGN DETAIL



N. C. DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS  
TRAFFIC ENGINEERING BRANCH  
SIGNING DEPARTMENT

SIGN NUMBER: SP07071  
TYPE: D  
QUANTITY: 8  
SIGN WIDTH: 78"  
HEIGHT: 36"  
TOTAL AREA: 19.5 Sq.Ft.  
BORDER TYPE: FLUSH  
RECESS: 0.63"  
WIDTH: 0.88"  
RADII: 2.25"  
NO. Z BARS: LENGTH:  
BACKG COLOR: Fluorescent Orange  
COPY COLOR: Black  
SYMBOL X Y WID HT  
MAT'L: 0.125" (3.2 mm) ALUMINUM  
0.079" COMPOSITE

DESIGN BY: R. HENNEIN  
PROJECT ID: I-2810  
CHECKED BY: DIV: 5  
STD #: N/A  
DATE: Mar 16, 2007



USE NOTES:  
1. Legend and border shall be direct applied non-reflective sheeting.  
2. Background shall be Type VII, VIII, or IX (prismatic) retroreflective sheeting.

LETTER POSITIONS

Letter spacings are to start of next letter													Series/Size			
	I	-	S	N	O	R	T	H					Text Length			
	16.8	1.9	2.9	5.3	4.1	6	5.4	5.6	4.6	4.6	4.1	16.8	D 2000 44.3			
	E	N	T	R	A	N	C	E	R	A	M	P	D 2000 66.2			
	5.9	4.7	5	4.6	4.6	6	5.4	5.4	3.7	6	4.6	6	6.1	4.1	5.9	
	C	L	O	S	E	D										D 2000 29
	24.5	5.4	4.5	5.2	5.1	4.7	4.1	24.5								

Spacing Factor is 1 unless specified otherwise  
FILENAME: SP07071\_I-85northentrancerclosed

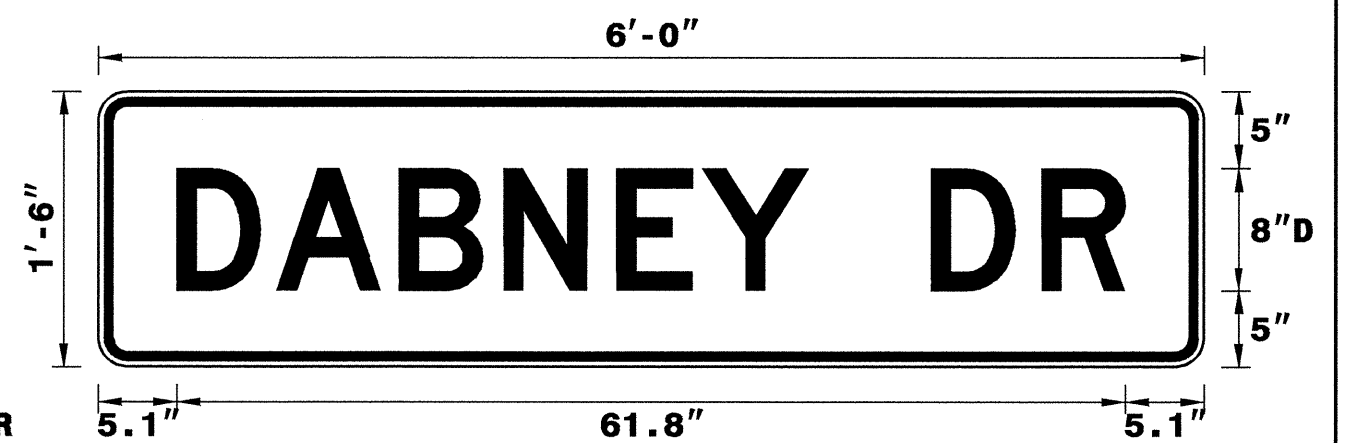
NORTH CAROLINA D.O.T. SIGN DETAIL



N. C. DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS  
TRAFFIC ENGINEERING BRANCH  
SIGNING DEPARTMENT

SIGN NUMBER: SP07067  
TYPE: D  
QUANTITY: 1  
SIGN WIDTH: 72"  
HEIGHT: 18"  
TOTAL AREA: 9.0 Sq.Ft.  
BORDER TYPE: FLUSH  
RECESS: 0.38"  
WIDTH: 0.63"  
RADII: 1.5"  
NO. Z BARS: LENGTH:  
BACKG COLOR: Fluorescent Orange  
COPY COLOR: Black  
SYMBOL X Y WID HT  
MAT'L: 0.125" (3.2 mm) ALUMINUM  
0.079" COMPOSITE

DESIGN BY: R. HENNEIN  
PROJECT ID: I-2810  
CHECKED BY: DIV: 5  
STD #: N/A  
DATE: Mar 16, 2007



USE NOTES:  
1. Legend and border shall be direct applied non-reflective sheeting.  
2. Background shall be Type VII, VIII, or IX (prismatic) retroreflective sheeting.

LETTER POSITIONS

Letter spacings are to start of next letter													Series/Size
	D	A	B	N	E	Y	D	R					Text Length
	5.1	6.5	8	6.8	7.4	5.6	6.9	8	7.2	5.4	5.1		D 2000 61.8

Spacing Factor is 1 unless specified otherwise  
FILENAME: SP07067\_dabneydr

NORTH CAROLINA D.O.T. SIGN DETAIL

25-JUN-2007 09:34 \\dot\dfsroot\01\Pro\TIP\Projects\I2810\trafficcontrol\tcp\2810\_tcp\_21\_tcp-22\_signdes.dgn idonaldson AT WZTC2231

APPROVED: _____ DATE: _____	<b>SIGN DESIGNS</b>					
	SCALE: NONE					
	DATE: APRIL 07					
	DESIGN BY: LKD					
	REVIEWED BY: JWJ					
<table border="1"> <thead> <tr> <th>REVISIONS</th> </tr> </thead> <tbody> <tr><td> </td></tr> <tr><td> </td></tr> <tr><td> </td></tr> </tbody> </table>		REVISIONS				CADD FILE
REVISIONS						

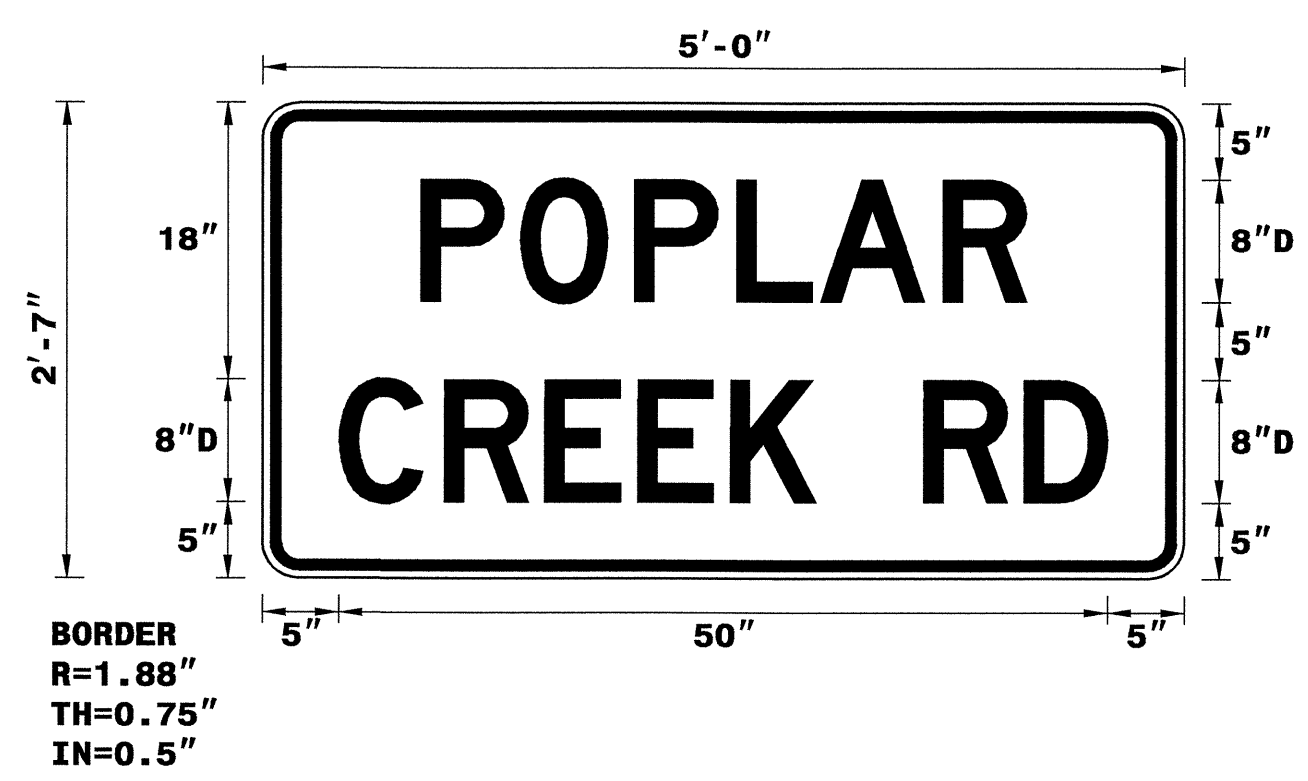


N. C. DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS  
TRAFFIC ENGINEERING BRANCH  
SIGNING DEPARTMENT

SIGN NUMBER: SP07069  
TYPE: D  
QUANTITY: 2  
SIGN WIDTH: 60"  
HEIGHT: 31"  
TOTAL AREA: 12.9 Sq.Ft.  
BORDER TYPE: FLUSH  
RECESS: 0.5"  
WIDTH: 0.75"  
RADII: 1.88"  
NO. Z BARS:  
LENGTH:

SYMBOL	X	Y	WID	HT

DESIGN BY: R. HENNEIN  
PROJECT ID: I-2810  
CHECKED BY:  
DIV: 5  
STD #: N/A  
DATE: Mar 16, 2007



USE NOTES:  
1. Legend and border shall be direct applied non-reflective sheeting.  
2. Background shall be Type VII, VIII, or IX (prismatic) retroreflective sheeting.

LETTER POSITIONS

Letter spacings are to start of next letter													Series/Size
													Text Length
	P	O	P	L	A	R							D 2000
10.3	6.5	7.4	6.6	5.4	8	5.4	10.3						39.4
	C	R	E	E	K	R	D						D 2000
5	6.8	6.5	6	6	5.6	7	6.5	5.4	5				50

Spacing Factor is 1 unless specified otherwise

FILENAME: SP07069\_poplarcreekrd NORTH CAROLINA D.O.T. SIGN DETAIL

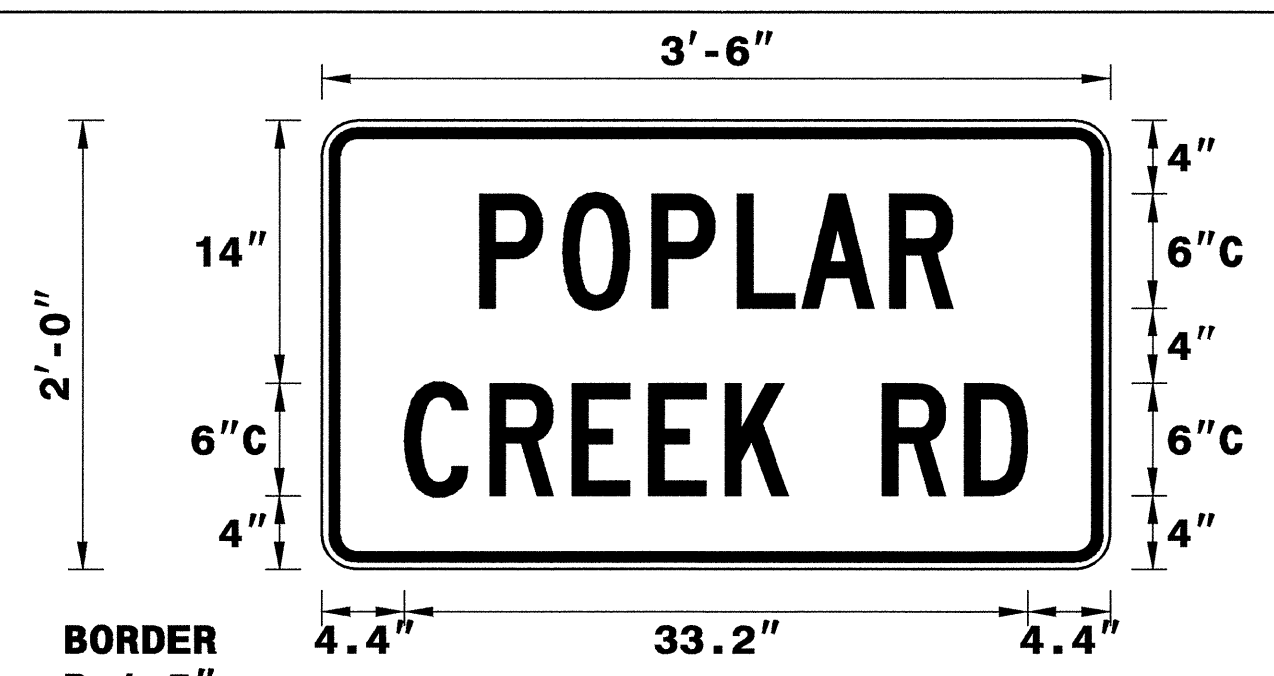


N. C. DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS  
TRAFFIC ENGINEERING BRANCH  
SIGNING DEPARTMENT

SIGN NUMBER: SP07066  
TYPE: D  
QUANTITY: 4  
SIGN WIDTH: 42"  
HEIGHT: 24"  
TOTAL AREA: 7.0 Sq.Ft.  
BORDER TYPE: FLUSH  
RECESS: 0.38"  
WIDTH: 0.63"  
RADII: 1.5"  
NO. Z BARS:  
LENGTH:

SYMBOL	X	Y	WID	HT

DESIGN BY: R. HENNEIN  
PROJECT ID: I-2810  
CHECKED BY:  
DIV: 5  
STD #: N/A  
DATE: Mar 15, 2007



USE NOTES:  
1. Legend and border shall be direct applied non-reflective sheeting.  
2. Background shall be Type VII, VIII, or IX (prismatic) retroreflective sheeting.

LETTER POSITIONS

Letter spacings are to start of next letter													Series/Size
													Text Length
	P	O	P	L	A	R							C 2000
8.4	4.4	4.7	4.6	3.4	4.7	3.4	8.4						25.2
	C	R	E	E	K	R	D						C 2000
4.4	4.6	4.4	4.1	4.1	3.4	5	4.4	3.4	4.4				33.2

Spacing Factor is 1 unless specified otherwise

FILENAME: SP07066\_poplarcreekrd NORTH CAROLINA D.O.T. SIGN DETAIL



N. C. DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS  
TRAFFIC ENGINEERING BRANCH  
SIGNING DEPARTMENT

SIGN NUMBER: SP07068  
TYPE: D  
QUANTITY: 2  
SIGN WIDTH: 60"  
HEIGHT: 31"  
TOTAL AREA: 12.9 Sq.Ft.  
BORDER TYPE: FLUSH  
RECESS: 0.5"  
WIDTH: 0.75"  
RADII: 1.88"  
NO. Z BARS:  
LENGTH:

SYMBOL	X	Y	WID	HT

DESIGN BY: R. HENNEIN  
PROJECT ID: I-2810  
CHECKED BY:  
DIV: 5  
STD #: N/A  
DATE: Mar 16, 2007



USE NOTES:  
1. Legend and border shall be direct applied non-reflective sheeting.  
2. Background shall be Type VII, VIII, or IX (prismatic) retroreflective sheeting.

LETTER POSITIONS

Letter spacings are to start of next letter													Series/Size
													Text Length
	R	U	I	N									D 2000
18.6	6.8	7.4	3.2	5.4	18.6								22.8
	C	R	E	E	K	R	D						D 2000
5	6.8	6.5	6	6	5.6	7	6.5	5.4	5				50

Spacing Factor is 1 unless specified otherwise

FILENAME: SP07068\_ruinccreekrd NORTH CAROLINA D.O.T. SIGN DETAIL



N. C. DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS  
TRAFFIC ENGINEERING BRANCH  
SIGNING DEPARTMENT

SIGN NUMBER: SP07065  
TYPE: D  
QUANTITY: 4  
SIGN WIDTH: 42"  
HEIGHT: 24"  
TOTAL AREA: 7.0 Sq.Ft.  
BORDER TYPE: FLUSH  
RECESS: 0.38"  
WIDTH: 0.63"  
RADII: 1.5"  
NO. Z BARS:  
LENGTH:

SYMBOL	X	Y	WID	HT

DESIGN BY: R. HENNEIN  
PROJECT ID: I-2810  
CHECKED BY:  
DIV: 5  
STD #: N/A  
DATE: Mar 15, 2007



USE NOTES:  
1. Legend and border shall be direct applied non-reflective sheeting.  
2. Background shall be Type VII, VIII, or IX (prismatic) retroreflective sheeting.

LETTER POSITIONS

Letter spacings are to start of next letter													Series/Size
													Text Length
	R	U	I	N									C 2000
13.7	4.4	4.7	2.2	3.4	13.7								14.6
	C	R	E	E	K	R	D						C 2000
4.4	4.6	4.4	4.1	4.1	3.4	5	4.4	3.4	4.4				33.2

Spacing Factor is 1 unless specified otherwise

FILENAME: SP07065\_ruinccreekrd NORTH CAROLINA D.O.T. SIGN DETAIL

25-JUN-2007 09:35 \\dot\feroo\01\Projects\N2810\Traffic\trfficcontrol\top\N2810\_TC\_TCP-22\TCP-22\_signdes.dgn

APPROVED:	DATE:	SIGN DESIGNS	
	6/26/07		
SCALE: NONE	DATE: APRIL 07		REVISIONS
DWG. BY: LKD	DESIGN BY: LKD		
REVIEWED BY: JWC			



