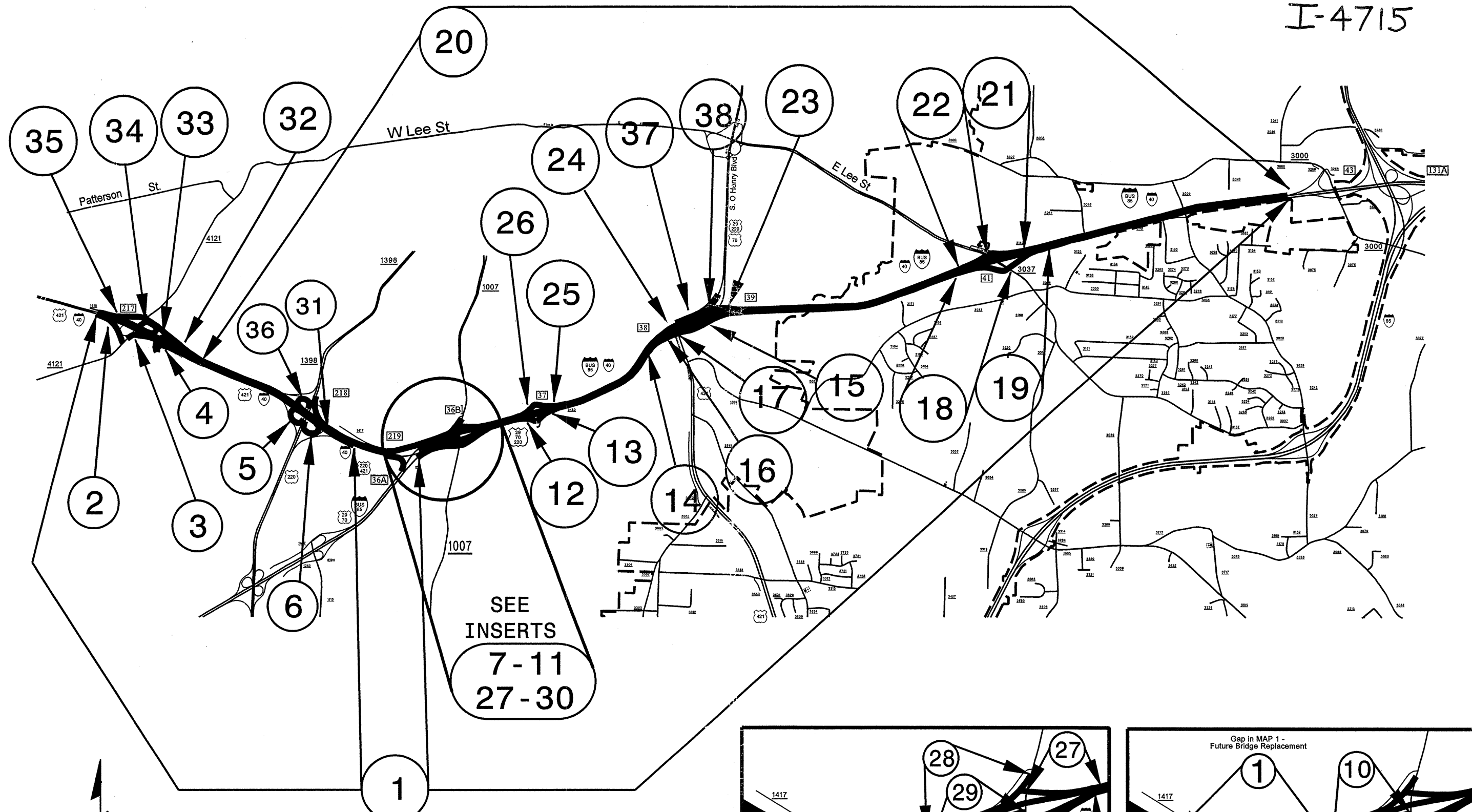


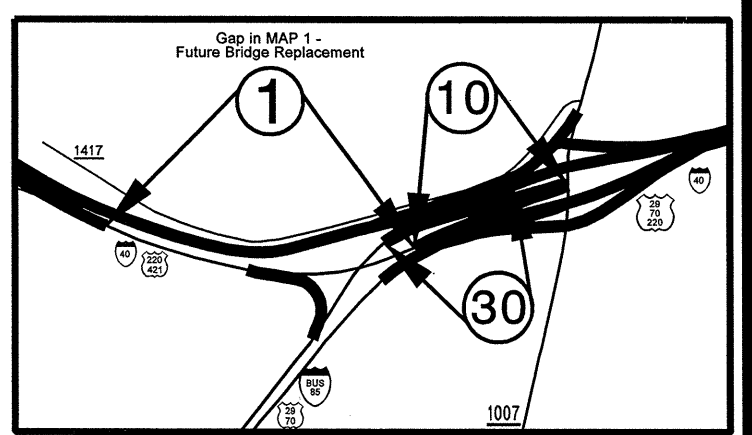
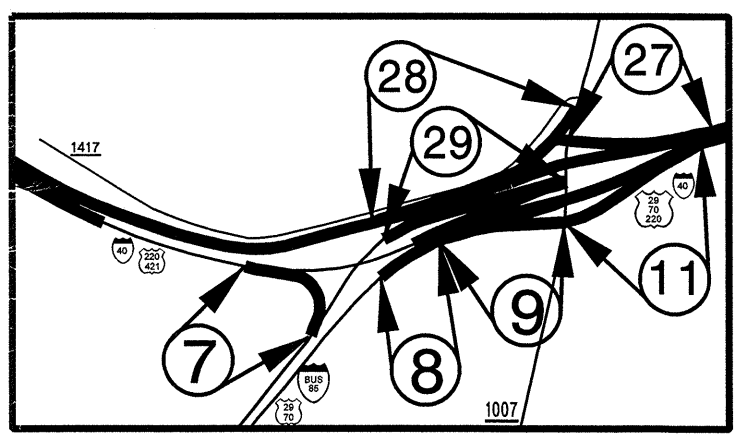
I-4715

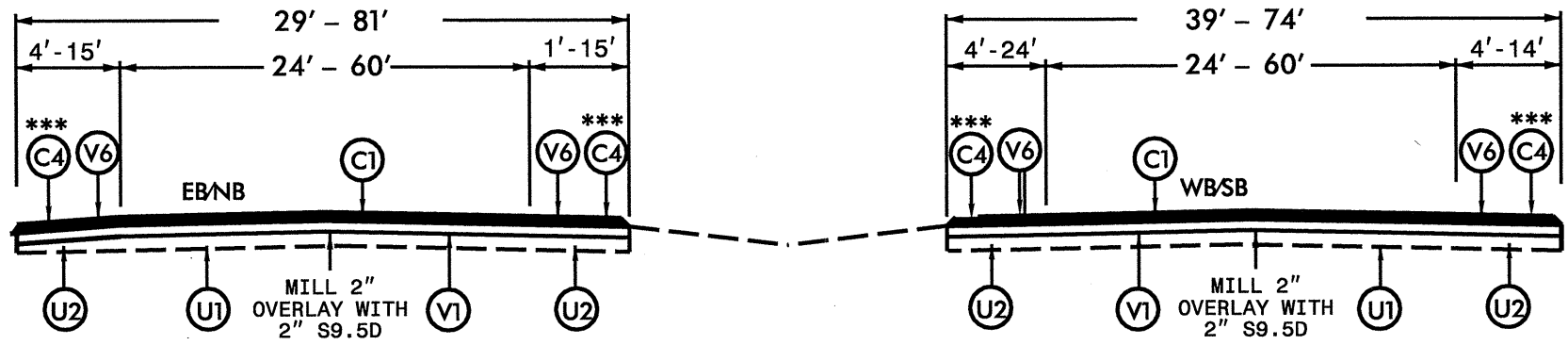


SEE
INSERTS
7-11
27-30

* SEE INSERT

GUILFORD COUNTY

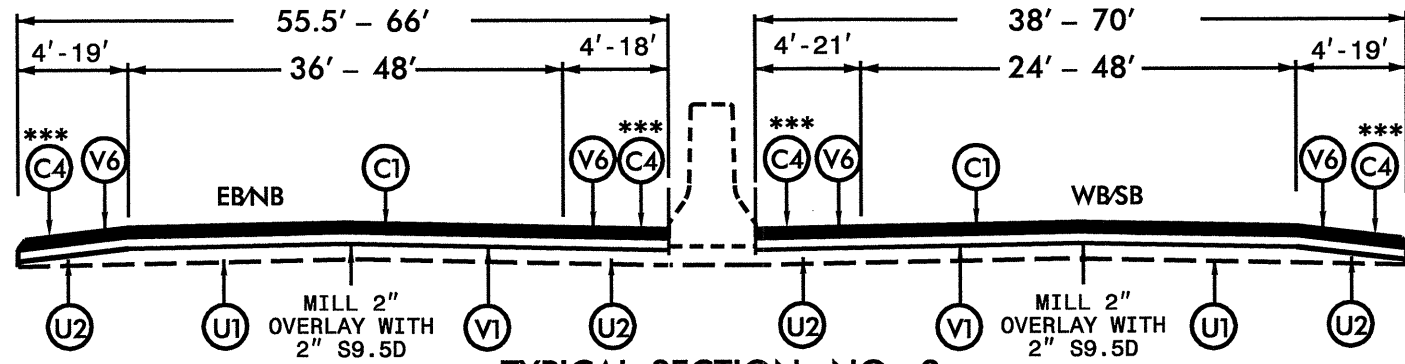




TYPICAL SECTION NO. 1

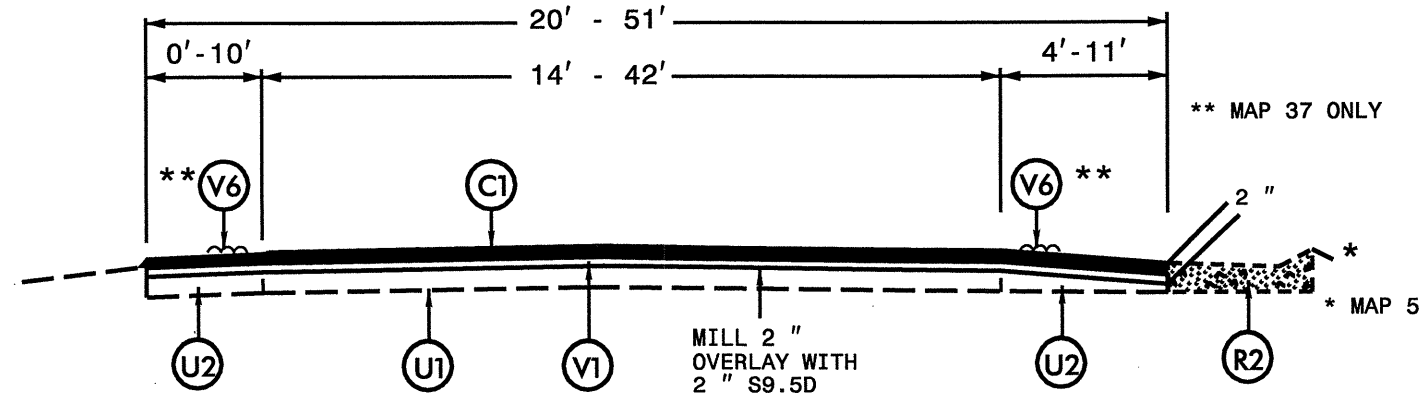
Use Typical Section No. 1 on Maps 1 and 20

***NOTE:
WHERE EXISTING PAVED SHOULDER IS 4' OR LESS ON TYPICALS 1 AND 2, THE CONTRACTOR SHALL CONTINUE TO USE C1 (2" S9.5D) UNLESS OTHERWISE DIRECTED BY THE ENGINEER.



TYPICAL SECTION NO. 2

Use Typical Section No. 1 on Maps 1 and 20



TYPICAL SECTION NO. 3 (RAMPS)

MAPS 3,5,6,8,15-17,30,37

NOTE: STA 0+00 EB - STARTS AT NEW PAVEMENT JOINT EAST OF EB EXIT RAMP TO HIGH POINT ROAD

STA 0+00 WBSB - STARTS AT NEW PAVEMENT JOINT EAST OF McCONNELL ROAD

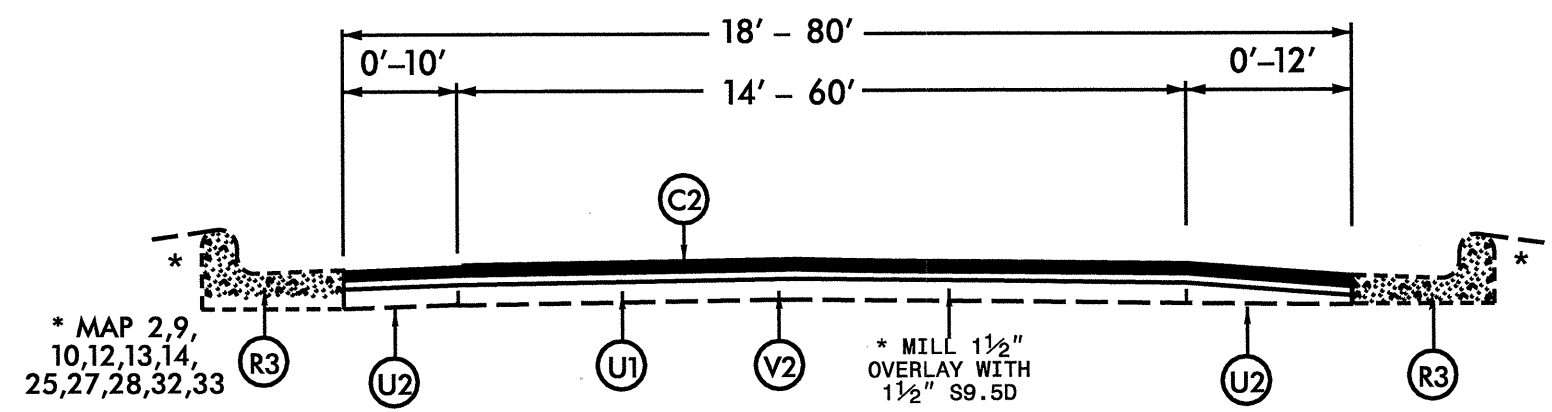
DO NOT PAVE BRIDGES AT THE FOLLOWING LOCATIONS:
WBSB STA 99+67 TO STA 102+45
WBSB STA 170+30 TO STA 172+53
WB STA 350+67 TO STA 352+22
RAMP TO FOUR SEASON STA 3+52 TO STA 5+90
EB/NB STA 243+61 TO STA 246+75
EB/NB STA TO STA **BRIDGE OVER U29NB **
EB/NB STA 279+57 TO STA 281+78
EB/NB STA 351+31 TO STA 354+13

PAVEMENT SCHEDULE

C1	PROP. APPROX. 2" ASPHALT. CONC. SURFACE COURSE, TYPE S9.5D, AT AN AVERAGE RATE OF 224 LBS. PER SQ. YD.		
C2	PROP. APPROX. 1½" ASPHALT. CONC. SURFACE COURSE, TYPE S9.5D, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.		
C3	PROP. APPROX. 3" ASPHALT. CONC. SURFACE COURSE, TYPE S9.5D, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD. IN EACH OF TWO LAYERS		
C4	PROP. APPROX. 2" ASPHALT. CONC. SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 224 LBS. PER SQ. YD.		
D1	PROP. APPROX. 2½" ASPHALT. CONC. INTERMEDIATE COURSE, TYPE I19.0D, AT AN AVERAGE RATE OF 285 LBS. PER SQ. YD.		
D2	PROP. APPROX. 3" ASPHALT. CONC. INTERMEDIATE COURSE, TYPE I19.0D, AT AN AVERAGE RATE OF 342 LBS. PER SQ. YD.		
R1	EXISTING CONCRETE MEDIAN BARRIER WALL		
R2	EXISTING SHOULDER BERM GUTTER	R3	EXISTING 2'-6" CURB & GUTTER
U1	EXISTING TRAVELWAY	U2	EXISTING PAVED SHOULDER

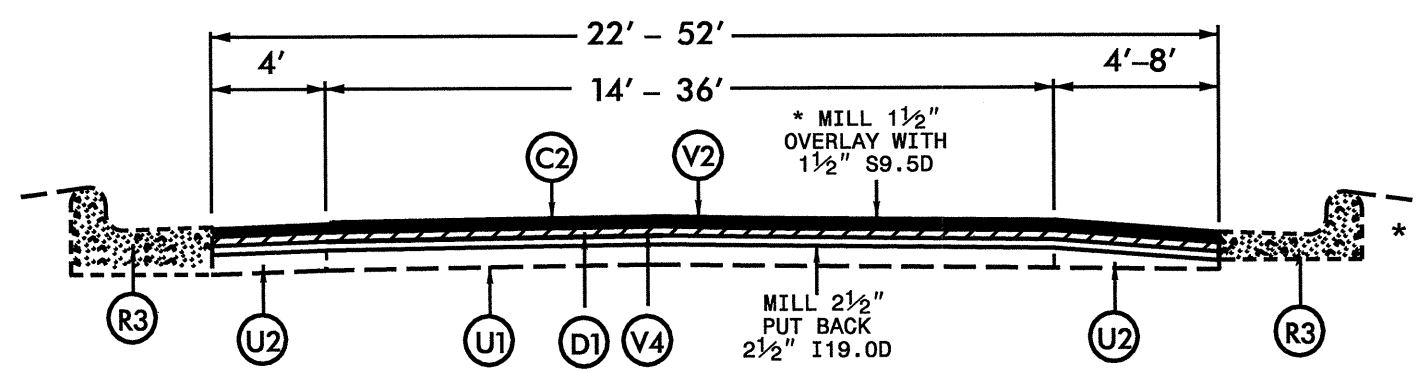
MILLING SCHEDULE

V1	MILLING BITUMINOUS PAVEMENT 2" DEPTH
V2	MILLING BITUMINOUS PAVEMENT 1½" DEPTH
V3	MILLING BITUMINOUS PAVEMENT 3" DEPTH
V4	MILLING BITUMINOUS PAVEMENT 2½" DEPTH
V5	MILLING BITUMINOUS PAVEMENT 4½" DEPTH
V6	MILLED RUMBLE STRIPS, USE IN CONJUNCTION WITH STANDARD DRAWING NO. 665.01



TYPICAL SECTION NO. 4 (RAMPS)

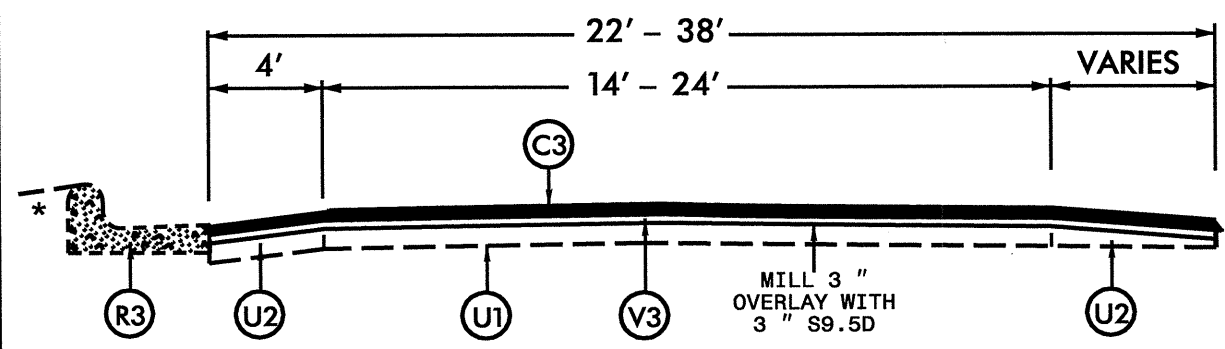
MAPS 2,4,9-14,18, 23-29,31-33,35,36,38



TYPICAL SECTION NO. 5 (RAMPS)

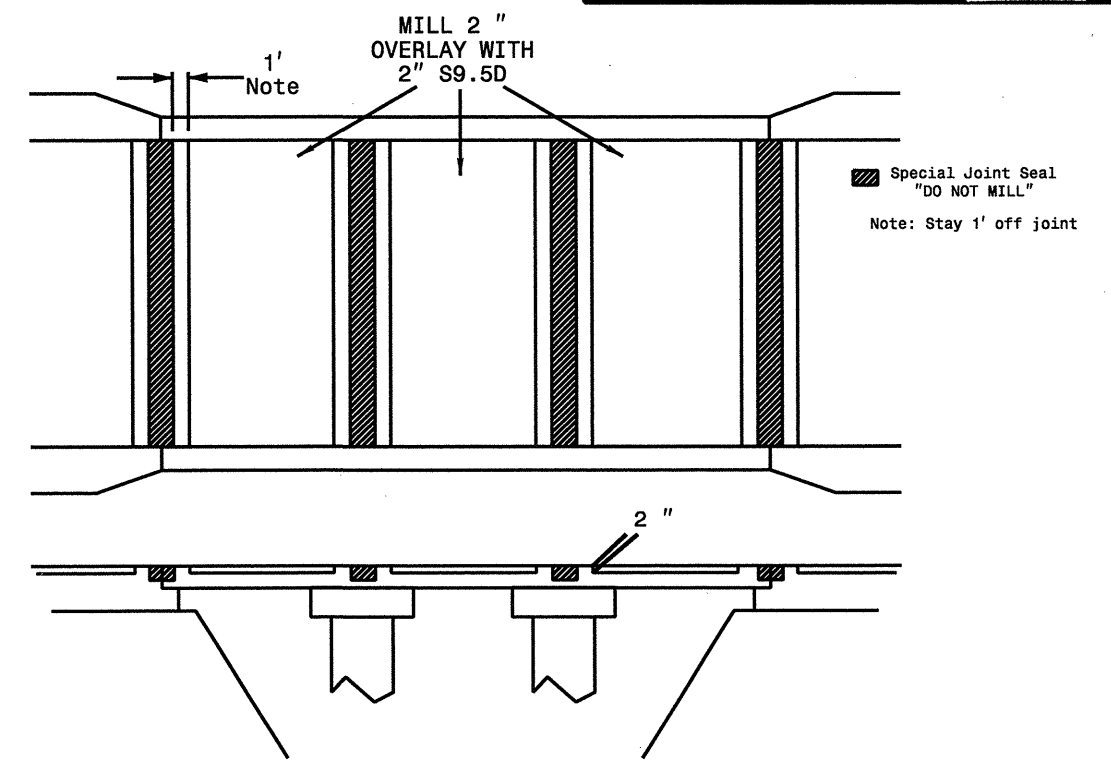
MAPS 14, 21, 22

* NOTE: MAP 14 area with no C&G, mill 2 1/2". Area with C&G, mill 1 1/2".

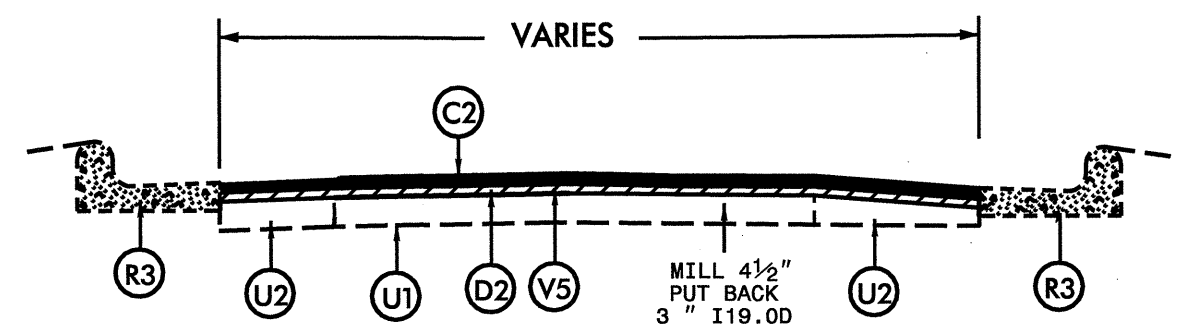


TYPICAL SECTION NO. 6 (RAMPS)

MAPS 7,19,34



BRIDGE MILLING TYPICAL NO. 1



MAPS 11, 13 & 28 (Location of 4 1/2" milling is at the intersection of the Ramp & the -Y-Line) MAP 35 (Location of 4 1/2" milling is on the shoulder near the intersection of the Ramp & the Y-Line) as determined by the Engineer

USE IN CONJUNCTION WITH TYPICAL 4

MILLING TYPICAL

PAVEMENT SCHEDULE

C1	PROP. APPROX. 2" ASPHALT. CONC. SURFACE COURSE, TYPE S9.5D, AT AN AVERAGE RATE OF 224 LBS. PER SQ. YD.	U1	EXISTING TRAVELWAY	U2	EXISTING PAVED SHOULDER
C2	PROP. APPROX. 1 1/2" ASPHALT. CONC. SURFACE COURSE, TYPE S9.5D, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.	MILLING SCHEDULE			
C3	PROP. APPROX. 3" ASPHALT. CONC. SURFACE COURSE, TYPE S9.5D, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD. IN EACH OF TWO LAYERS	V1	MILLING BITUMINOUS PAVEMENT 2" DEPTH		
D1	PROP. APPROX. 2 1/2" ASPHALT. CONC. INTERMEDIATE COURSE, TYPE I19.0D, AT AN AVERAGE RATE OF 285 LBS. PER SQ. YD.	V2	MILLING BITUMINOUS PAVEMENT 1 1/2" DEPTH		
D2	PROP. APPROX. 3" ASPHALT. CONC. INTERMEDIATE COURSE, TYPE I19.0D, AT AN AVERAGE RATE OF 342 LBS. PER SQ. YD.	V3	MILLING BITUMINOUS PAVEMENT 3" DEPTH		
R1	EXISTING CONCRETE MEDIAN BARRIER WALL	V4	MILLING BITUMINOUS PAVEMENT 2 1/2" DEPTH		
R2	EXISTING SHOULDER BERM GUTTER	V5	MILLING BITUMINOUS PAVEMENT 4 1/2" DEPTH		
R3	EXISTING 2'-6" CURB & GUTTER	V6	MILLED RUMBLE STRIPS, USE IN CONJUNCTION WITH STANDARD DRAWING NO. 665.01		

PROJECT NO.	SHEET NO.	TOTAL NO.
38727.3.GV1	4	

SUMMARY OF QUANTITIES

PROJECT NO.	COUNTY	MAP NO.	ROUTE	DESCRIPTION	TYP	LENGTH MI	WIDTH FT	1 1/2" MILLING SY	3" MILLING SY	4.5" MILLING SY	2.5" MILLING SY	2" MILLING SY	MILLED RUMBLE STRIPS LF	INTERMEDIATE COURSE, 119.0D TONS	SURFACE COURSE, \$9.5B TONS	SURFACE COURSE, \$9.5D TONS	PG 64-22 PLANT MIX TONS	PG 70-22 PLANT MIX TONS	PG 76-22 PLANT MIX TONS	ADJ. OF GRATES AND/OR FRAMES ON DROP INLETS EA	REPAIR OF DROP INLETS EA	2'-6" CURB & GUTTER LF	ADJ. OF MANHOLES EA	PORTABLE LIGHTING LS	TRENCHING (UNPAVED) (1 2") LF	PULL BOX (STANDARD) EA	INDUCTIVE LOOP SAW CUT LF	LEAD-IN CABLE (18-2) LF	LEAD-IN CABLE (18-4) LF					
																														NO	MI	FT	SY	SY
38727.3.GV1	Guilford	1	I-40 EB	I-40/85 Business from I-85 Bypass to SR 4121 (High Point Rd.)	1,2	8.85	29-81						295,301	85,704		9,967	23,197	598		1,276	50	20												
		2	RAMP FROM I-40 EB TO HIGH POINT RD.1 (EXIT 217)	Off Ramp	4	0.19	46 - 80	6,694									564			32												1,150		
		3	RAMP FROM HIGH POINT RD. TO I-40 EB EXIT 217	On Ramp	3	0.11	20 - 31							2,083			235			13														
		4	RAMP FROM KOURY BVLD. TO I-40 EB.1 (EXIT 217)	On Ramp	4	0.1	18 - 24	1,338									113			7														
		5	RAMP FROM US 220 SB TO I-40 EB (EXIT 218)	On Ramp	3	0.17	22							2,194			248			14														
		6	RAMP FROM I-40 EB TO US 220 NB (EXIT 218B)	Off Ramp	3	0.17	23							2,294			259			15														
		7	RAMP FROM I-40 EB TO I-85 BUS. (EXIT 219)	Off Ramp	6	0.06	24 - 28			898							152			9														
		8	I-85 BUS. NB TO I-40 EB	I-85 Bus. NB / I-40EB Merge	3	0.18	36 - 50							5,210	950		118	468	7	26														
		9	RAMP FROM I-85 BUS. TO RANDLEMAN RD. (EXIT 36)	Off Ramp	4	0.08	26 - 33	1,323									112			7												290		
		10	RAMP FROM I-40 EB TO RANDLEMAN RD. (EXIT 36)	Off Ramp	4	0.14	22 - 25	2,001									169			10												290		
		11	RAMP FROM RANDLEMAN RD. TO I-40 EB (EXIT 36)	On Ramp	4	0.18	18 - 29	2,162			848					146	254			7	14											875		
		12	RAMP FROM I-40 EB TO SOUTH ELM-EUGENE ST. (EXIT 37)	Off Ramp	4	0.08	26 - 34	1,643									139			8												1,000		
		13	RAMP FROM SOUTH ELM-EUGENE ST. TO I-40 EB (EXIT 37)	On Ramp	4	0.1	25	1,320			323					55	139			3	8													
		14	RAMP FROM I-40 EB TO US 421 (EXIT 38)	Off Ramp	4,5	0.15	24 - 52	1,608					2230			319	323			15	18											500		
		15	RAMP FROM US 421 TO I-40 EB/ US 29 NB (EXIT 38)	On Ramp	3	0.37	32 - 39				358			7,436			897			50				50										
		16	RAMP US 421 RAMP TO US 29 NB (EXIT 38)	On Ramp	3	0.05	24							704			80			5				50										
		17	RAMP FROM I-40 EB TO US 29 NB (EXIT 39)	Off Ramp	3	0.23	39 - 44							6,453			725			40														
		18	RAMP FROM I-40 EB TO LEE ST. (EXIT 41)	Off Ramp	4	0.23	25 - 34	2,787									236			13														
		19	RAMP FORM LEE ST. TO I-40 EB (EXIT 41)	On Ramp	6	0.16	32 - 38				3127						529			30						25	1	213	25	25				
		20	I-40 WB	I-40/85 Business from SR 4121 (High Point Rd.) to I-85 Bypass	1,2	8.05	39 - 74							274,383	83,634		9,168	21,650	550		1,191													
		21	RAMP FROM I-40 WB TO LEE ST. (EXIT 41)	Off Ramp	5	0.1	25 - 31						1,737				227			10	8					25	1	213	25	25				
		22	RAMP FROM LEE ST. TO I-40 WB (EXIT 41)	On Ramp	5	0.07	22 - 28						921				132			6	5													
		23	RAMP FROM I-40 WB TO US 421 (EXIT 38)	Off Ramp (Mill Between Joints & Pave Bridge)	4	0.43	19 - 39	7,653									646			36												500		
		24	RAMP FROM US 421 TO I-40 WB (EXIT 38)	On Ramp	4	0.11	18 - 22	1,596									135			8														
		25	RAMP FROM I-40 WB TO SOUTH ELM-EUGENE ST. (EXIT 37)	Off Ramp	4	0.13	24 - 28	2,127									180			10												1,000		
		26	RAMP FROM SOUTH ELM-EUGENE ST. TO I-40 WB (EXIT 37)	On Ramp	4	0.12	21 - 24	1,860									157			9														
		27	RAMP FROM I-40 WB TO RANDLEMAN RD. (EXIT 36)	Off Ramp	4	0.11	23.5 - 24.5	1,654									140			8												295		
		28	RAMP FROM RANDLEMAN RD. TO I-40 WB (EXIT 36B)	On Ramp	4	0.27	19 - 25	3,227				663				113	328			5	19													
		29	RAMP FROM RANDLEMAN RD. TO I-85 BUS. SB (EXIT 36B)	On Ramp	4	0.24	19 - 24	3,186									269			15														
		30	I-85 BUS. SB (EXIT 36A)	From Gore @ I-40 WB to Paving Joint on I-85 SB	3	0.07	51 - 58							2,001			47	178	3	10														
		31	RAMP FROM I-40 WB TO FREEMAN MILL RD. (EXIT 218B)	Off Ramp	4	0.12	24 - 25	1,701									144			8														
		32	RAMP FROM I-40 WB TO HIGH POINT RD. (EXIT 217)	Off Ramp	4	0.36	38 - 60	12,877									1,084			60													875	
		33	RAMP FROM HIGH POINT RD. I-40 WB RAMP TO KOURY BVLD. (EXIT 217)	Off Ramp	4	0.18	35 - 47	3,259									275			16												875		
		34	RAMP FROM HIGH POINT RD. NB TO I-40 WB (EXIT 217)	On Ramp	6	0.15	22				1,936						329			19														
		35	RAMP FROM HIGH POINT RD. SB TO I-40 WB EXIT 217)	On Ramp	4	0.19	30	3,344			10					2	282			16														
		36	RAMP FROM I-40 WB TO US 220 SB (EXIT 218A)	Off Ramp	4	0.13	23 - 34	2,053									173			10														
		37	US 29 SB RAMP TO I-40 WB (EXIT 38)	On Ramp	3	0.74	36 - 40							18,785	7,813		2,112			117														
		38	RAMP FROM US 29 TO US 421 (EXIT 38)	Off Ramp	4	0.12	26	1,830									155			9														
TOTAL FOR PROJECT NUMBER 38727.3.GV1						8.85		67,243	6,319	1,844	4,888	616,844	178,101	994	19,300	57,287	1,158	46	3,169	50	20	50	1	1	50	2	8,076	50	50					

PROJECT NO.	SHEET NO.	TOTAL NO.
38727.3.GV1	5	

THERMOPLASTIC AND PAINT QUANTITIES

PROJECT NO.	COUNTY	MAP	ROUTE	DESCRIPTION	STATIONARY SIGNS	PORTABLE SIGNS	BARRICADE SIGNS	FLASHING ARROW PANELS, TYPE C	FLASHING ARROW PANELS, TYPE C (SHORT TERM, 18 EA)	CHANGEABLE MESSAGE SIGN	CHANGEABLE MESSAGE SIGNS (SHORT TERM, 22 EA)	DRUMS	BARRICADES (TYPE III)	TMA	8" X 120 M WHITE THERMO	6" X 90 M WHITE THERMO	6" X 90 M YELLOW THERMO	6" X 120 M WHITE THERMO	6" X 120 M YELLOW THERMO	12" X 90 M WHITE THERMO	12" X 90 M YELLOW THERMO	12" X 120 M WHITE THERMO	24" X 120 M WHITE THERMO	THERMO MSG ONLY 120 M
NO		NO			SF	SF	SF	EA	DAY	EA	DAY	EA	LF	EA	LF	LF	LF	LF	LF	LF	LF	LF	LF	EA
38727.3.GV1	Guilford	1	I-40 EB	I-40/85 Business from I-85 Bypass to SR 4121 (High Point Rd.)	4696	1716	302	6	216	4	264	940	600	4		44,816	53,000	25,364	1,003	10,231				20
"	Guilford	2	RAMP FROM I-40 EB TO HIGH POINT RD.1 (EXIT 217)	OFF RAMP											170	1,003	1,003	585					72	12
"	Guilford	3	RAMP FROM HIGH POINT RD. TO I-40 EB (EXIT 217)	ON RAMP												581	581			82				
"	Guilford	4	RAMP FROM KOURY BVLD. TO I-40 EB (EXIT 217)	ON RAMP												528	528							
"	Guilford	5	RAMP FROM US 220 SB TO I-40 EB (EXIT 218)	ON RAMP													910							
"	Guilford	6	RAMP FROM I-40 EB TO US 220 NB (EXIT 218B)	OFF RAMP																				
"	Guilford	7	RAMP FROM I-40 EB TO I-85 BUS (EXIT 219)	OFF RAMP												317	317				52			
"	Guilford	8	I-85 BUS NB TO I-40 EB	I-85 BUS. NB / I-40 EB MERGE												950	950	356		770				
"	Guilford	9	RAMP FROM I-85 BUS TO RANDLEMAN RD. (EXIT 36)	OFF RAMP												422	422	175						
"	Guilford	10	RAMP FROM I-40 EB TO RANDLEMAN RD. (EXIT 36)	OFF RAMP												739	739	159						
"	Guilford	11	RAMP FROM RANDLEMAN RD. TO I-40 EB (EXIT 36)	ON RAMP												1,162	950	103						
"	Guilford	12	RAMP FROM I-40 EB TO SOUTH ELM-EUGENE ST. (EXIT 37)	OFF RAMP												370	370	320						
"	Guilford	13	RAMP FROM SOUTH ELM-EUGENE ST TO I-40 EB (EXIT 37)	ON RAMP												528	528							
"	Guilford	14	RAMP FROM I-40 EB TO US 421 (EXIT 38)	OFF RAMP												739	528	468					60	
"	Guilford	15	RAMP FROM US 421 TO I-40 EB/US 29 NB (EXIT 38)	ON RAMP												1,954	1,954			463				
"	Guilford	16	RAMP FROM RAMP FROM US 421 TO US 29 NB (EXIT 38)	ON RAMP												264	264							
"	Guilford	17	RAMP FROM I-40 EB TO US 29 NB (EXIT 39)	OFF RAMP												1,214	1,214	172		1,630				
"	Guilford	18	RAMP FROM I-40 EB TO LEE ST. (EXIT 41)	OFF RAMP												1,214	1,214	12						
"	Guilford	19	FROM FROM LEE ST. TO I-40 EB (EXIT 41)	ON RAMP												845	845	168						
"	Guilford	20	I-40 WB	I-40/85 Business from SR 4121 (High Point Rd.) to I-85 Bypass												42,504	41,606	22,455	898	8,627	974			8
"	Guilford	21	RAMP FROM I-40 WB TO LEE ST. (EXIT 41)	OFF RAMP												528	528	22						
"	Guilford	22	RAMP FROM LEE ST. TO I-40 WB (EXIT 41)	ON RAMP												370	370							
"	Guilford	23	RAMP FROM I-40 WB TO US 421 (EXIT 38)	OFF RAMP												2,270	2,270	208		66			37	
"	Guilford	24	RAMP FROM US 421 TO I-40 WB (EXIT 38)	ON RAMP												581	581							
"	Guilford	25	RAMP FROM I-40 WB TO SOUTH ELM-EUGENE ST. (EXIT 37)	OFF RAMP												686	686	54		149				
"	Guilford	26	RAMP FROM SOUTH ELM-EUGENE ST. TO I-40 WB (EXIT 37)	ON RAMP												634	634							
"	Guilford	27	RAMP FROM I-40 WB TO RANDLEMAN RD. (EXIT 36)	OFF RAMP														200						
"	Guilford	28	RAMP FROM RANDLEMAN RD. TO I-40 WB (EXIT 36B)	ON RAMP												600	450							
"	Guilford	29	RAMP FROM RANDLEMAN RD. TO I-85 BUS SB (EXIT 36B)	ON RAMP												1,280	1,040							
"	Guilford	30	I-85 BUS SB @ EXIT 36A	FROM GORE @ I-40 WB TO PAVING JOINT ON I-85 SB												370	370			450				
"	Guilford	31	RAMP FROM I-40 TO FREEMAN MILL RD.(EXIT 218B)	OFF RAMP												634	634							
"	Guilford	32	RAMP FROM I-40 WB TO HIGH POINT RD. (EXIT 217)	OFF RAMP												1,901	1,901	1,084		1,642			51	12
"	Guilford	33	RAMP FROM HIGH POINT RD. I-40 WB RAMP TO KOURY BVLD. (EXIT 217)	OFF RAMP												950	950	527					27	12
"	Guilford	34	RAMP FROM HIGH POINT RD. NB TO I-40 WB (EXIT 217)	ON RAMP														385				68		
"	Guilford	35	RAMP FROM HIGH POINT RD. SB TO I-40 WB (EXIT 217)	ON RAMP												1,003	950							
"	Guilford	36	RAMP FROM I-40 WB TO US 220 NB (EXIT 218 A)	OFF RAMP												686	686							
"	Guilford	37	US 29 SB RAMP TO I-40 WB (EXIT38)	ON RAMP												3,907	3,907	1,021		778				
"	Guilford	38	RAMP FROM US 29 TO US 421 (EXIT 38)	OFF RAMP												634	634							
GRAND TOTAL					4,696	1,716	302	6	216	4	264	940	600	4	170	117,184	125,779	53,453	1,901	24,940	974	68	247	64
																242,963	55,354	25,914						

PROJECT NO.	SHEET NO.	TOTAL NO.
38727.3.GV1	6	

THERMOPLASTIC AND PAINT QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	THERMO MERGE LEFT ARROW 90 M	THERMO RT ARROW 90 M	THERMO MERGE RIGHT ARROW 90 M	THERMO LT ARROW 90 M	THERMO STR ARROW 90 M	THERMO STR & RT ARROW 90 M	THERMO STR & LT ARROW 90 M	THERMO LT STR RT ARROW 90 M	6" WHITE PAINT	6" YELLOW PAINT	12" WHITE PAINT	24" WHITE PAINT	PAINT MSG ONLY	PAINT STR ARROW	PAINT RT ARROW	PAINT LT ARROW	PAINT STR & LT ARROW	PAINT LT STR RT ARROW	SNOW PLOWABLE MARKERS	REPLACEMENT BARRIER DELINEATORS
NO		NO			EA	EA	EA	EA	EA	EA	EA	EA	LF	LF	LF	LF	EA	EA	EA	EA	EA	EA	EA	EA
38727.3.GV1	Guilford	1	I-40 EB	I-40/85 Business from I-85 Bypass to SR 4121 (High Point Rd.)	13	10	3						150,476	95,750	15,152		40	32	20				2,013	131
"	Guilford	2	RAMP FROM I-40 EB TO HIGH POINT RD.1 (EXIT 217)	OFF RAMP		1		3	5				3,178	12,251	340	144	24	10	2	6			40	
"	Guilford	3	RAMP FROM HIGH POINT RD. TO I-40 EB (EXIT 217)	ON RAMP									1,268	1,162	164								4	
"	Guilford	4	RAMP FROM KOURY BVLD. TO I-40 EB (EXIT 217)	ON RAMP									1,056	1,056										
"	Guilford	5	RAMP FROM US 220 SB TO I-40 EB (EXIT 218)	ON RAMP										1,820										
"	Guilford	6	RAMP FROM I-40 EB TO US 220 NB (EXIT 218B)	OFF RAMP										1,760										
"	Guilford	7	RAMP FROM I-40 EB TO I-85 BUS (EXIT 219)	OFF RAMP									632	632	104								3	
"	Guilford	8	I-85 BUS NB TO I-40 EB	I-85 BUS. NB / I-40 EB MERGE									2,614	1,901	1,540								58	
"	Guilford	9	RAMP FROM I-85 BUS TO RANDLEMAN RD. (EXIT 36)	OFF RAMP				2		2			1,194	844									9	
"	Guilford	10	RAMP FROM I-40 EB TO RANDLEMAN RD. (EXIT 36)	OFF RAMP		2					2		1,796	1,478					4		4		8	
"	Guilford	11	RAMP FROM RANDLEMAN RD. TO I-40 EB (EXIT 36)	ON RAMP		2		2					2,530	1,900					4	4			5	
"	Guilford	12	RAMP FROM I-40 EB TO SOUTH ELM-EUGENE ST. (EXIT 37)	OFF RAMP		3		3			3		1,378	738					6	6	6		16	
"	Guilford	13	RAMP FROM SOUTH ELM-EUGENE ST TO I-40 EB (EXIT 37)	ON RAMP									1,056	1,056										
"	Guilford	14	RAMP FROM I-40 EB TO US 421 (EXIT 38)	OFF RAMP		6					3		2,414	1,056		120			12		6		24	
"	Guilford	15	RAMP FROM US 421 TO I-40 EB/US 29 NB (EXIT 38)	ON RAMP									3,908	3,908	926								25	
"	Guilford	16	RAMP FROM RAMP FROM US 421 TO US 29 NB (EXIT 38)	ON RAMP									528	528										
"	Guilford	17	RAMP FROM I-40 EB TO US 29 NB (EXIT 39)	OFF RAMP									2,772	2,429	3,260								92	
"	Guilford	18	RAMP FROM I-40 EB TO LEE ST. (EXIT 41)	OFF RAMP									2,429	2,429										
"	Guilford	19	FROM FROM LEE ST. TO I-40 EB (EXIT 41)	ON RAMP			4						2,025	1,690					8				18	
"	Guilford	20	I-40 WB	I-40/85 Business from SR 4121 (High Point Rd.) to I-85 Bypass	12	4							127,422	111,091	19,237		16	24	8				1,615	131
"	Guilford	21	RAMP FROM I-40 WB TO LEE ST. (EXIT 41)	OFF RAMP																				
"	Guilford	22	RAMP FROM LEE ST. TO I-40 WB (EXIT 41)	ON RAMP																				
"	Guilford	23	RAMP FROM I-40 WB TO US 421 (EXIT 38)	OFF RAMP				2				2	2,740	2,218	416	74				2		4	33	
"	Guilford	24	RAMP FROM US 421 TO I-40 WB (EXIT 38)	ON RAMP									1,162	1,162										
"	Guilford	25	RAMP FROM I-40 WB TO SOUTH ELM-EUGENE ST. (EXIT 37)	OFF RAMP		5					4		1,462	1,373	298				10		8		8	
"	Guilford	26	RAMP FROM SOUTH ELM-EUGENE ST. TO I-40 WB (EXIT 37)	ON RAMP									1,267	1,267										
"	Guilford	27	RAMP FROM I-40 WB TO RANDLEMAN RD. (EXIT 36)	OFF RAMP		2					2		400						4		4		10	
"	Guilford	28	RAMP FROM RANDLEMAN RD. TO I-40 WB (EXIT 36B)	ON RAMP									1,200	900										
"	Guilford	29	RAMP FROM RANDLEMAN RD. TO I-85 BUS SB (EXIT 36B)	ON RAMP									2,560	2,080										
"	Guilford	30	I-85 BUS SB @ EXIT 36A	FROM GORE @ I-40 WB TO PAVING JOINT ON I-85 SB									739	739	900								30	
"	Guilford	31	RAMP FROM I-40 TO FREEMAN MILL RD. (EXIT 218B)	OFF RAMP									1,267	1,267										
"	Guilford	32	RAMP FROM I-40 WB TO HIGH POINT RD. (EXIT 217)	OFF RAMP		6		4					5,969	3,802	3,284	102	24		12	8			119	
"	Guilford	33	RAMP FROM HIGH POINT RD. I-40 WB RAMP TO KOURY BVLD. (EXIT 217)	OFF RAMP		6		5	6				2,956	1,902		54	24	12	12	10			19	
"	Guilford	34	RAMP FROM HIGH POINT RD. NB TO I-40 WB (EXIT 217)	ON RAMP										770	136									
"	Guilford	35	RAMP FROM HIGH POINT RD. SB TO I-40 WB (EXIT 217)	ON RAMP									2,006	1,900										
"	Guilford	36	RAMP FROM I-40 WB TO US 220 NB (EXIT 218 A)	OFF RAMP									1,373	1,373										
"	Guilford	37	US 29 SB RAMP TO I-40 WB (EXIT 38)	ON RAMP	10								9,856	7,814	1,556			20					90	
"	Guilford	38	RAMP FROM US 29 TO US 421 (EXIT 38)	OFF RAMP									1,267	1,267										
GRAND TOTAL					35	47	7	21	11	2	14	2	344,920	275,313	47,313	494	128	106	94	36	28	4	4,237	262
					139								620,233				268							

STATE OF NORTH CAROLINA
DIVISION OF HIGHWAYS

STATE PROJECT REFERENCE NO.	SHEET NO.
I-4715	TCP-1

**PLAN FOR PROPOSED
TRAFFIC CONTROL, MARKING & DELINEATION
GUILFORD**

TIP PROJECT: I-4715 (C201588: 38727.3.GVI)

ROADWAY STANDARD DRAWINGS

THE FOLLOWING ROADWAY STANDARDS AS APPEAR IN "ROADWAY STANDARD DRAWINGS"- PROJECT SERVICES UNIT-N.C. DEPARTMENT OF TRANSPORTATION-RALEIGH, N.C., DATED JULY 2006 ARE APPLICABLE TO THIS PROJECT AND BY REFERENCE HEREBY ARE CONSIDERED A PART OF THESE PLANS:

STD. NO.	TITLE
1101.02	TEMPORARY LANE CLOSURES
1101.03	TEMPORARY ROAD CLOSURES
1101.04	TEMPORARY SHOULDER CLOSURES
1101.05	WORK ZONE VEHICLE ACCESSES
1101.11	TRAFFIC CONTROL DESIGN TABLES
1110.01	STATIONARY WORK ZONE SIGNS
1110.02	PORTABLE WORK ZONE SIGNS
1115.01	FLASHING ARROW PANELS
1130.01	DRUM
1135.01	CONES
1145.01	BARRICADES
1150.01	FLAGGING DEVICES
1165.01	TRUCK MOUNTED IMPACT ATTENUATOR
1180.01	SKINNY-DRUM
1205.01	PAVEMENT MARKINGS - LINE TYPES & OFFSETS
1205.02	PAVEMENT MARKINGS - 2 LANE & MULTILANE ROADWAYS
1205.03	PAVEMENT MARKINGS - INTERCHANGES
1205.04	PAVEMENT MARKINGS - INTERSECTIONS
1205.05	PAVEMENT MARKINGS - TURN LANES
1205.06	PAVEMENT MARKINGS - THRU LANE DROPS
1205.07	PAVEMENT MARKINGS - PEDESTRIAN CROSSWALKS
1205.08	PAVEMENT MARKINGS - SYMBOLS & WORD MESSAGES
1205.12	PAVEMENT MARKINGS - BRIDGES
1250.01	PAVEMENT MARKER SPACING
1251.01	RAISED PAVEMENT MARKERS (TEMPORARY & PERMANENT)
1253.01	SNOWPLOWABLE RAISED PAVEMENT MARKERS
1261.01	GUARDRAIL & BARRIER DELINEATOR SPACING
1261.02	GUARDRAIL & BARRIER DELINEATOR TYPES
1262.01	GUARDRAIL END DELINEATION

- ** NOTES:**
- COORDINATE MESSAGES AND EXACT LOCATIONS FOR BARRICADES, SIGNS, CHANGEABLE MESSAGE SIGNS (CMSs) AND OVERHEAD DYNAMIC MESSAGE SIGNS (DMSs) WITH THE RESIDENT ENGINEER.
 - CHANGEABLE MESSAGE SIGNS (CMSs) SHOWN ON OVERVIEWS ARE SUPPLEMENTAL TO THE STATIONARY DETOUR SIGNS AND CMSs REQUIRED FOR LANE CLOSURES.
 - ALL DISTANCES ARE APPROXIMATE AND SHOULD BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.
 - FOR LANE CLOSURES, USE ROADWAY STANDARD DRAWINGS (RSD) 1101.02 FOR TEMPORARY LANE CLOSURE REQUIREMENTS AND GENERAL NOTES AND DETAILS PROVIDED IN PLANS.
 - FOR ROAD CLOSURES, USE ROADWAY STANDARD DRAWINGS (RSD) 1101.03 FOR TEMPORARY ROAD CLOSURE REQUIREMENTS AND GENERAL NOTES AND DETAILS PROVIDED IN PLANS.
 - USE GREEN BACKGROUND FOR INTERSTATE BUSINESS SIGN SHIELDS AND REPLACE 'INTERSTATE' WITH 'BUSINESS'.
 - SIGN G21-065 ('ALL TRAFFIC USE NEXT EXIT') AND SIGN SP-03355 ('ALL TRAFFIC' WITH 45 DEGREE ARROW) MAY BE PORTABLE.

INDEX OF SHEETS

SHEET NO.	TITLE
TCP-1	LIST OF APPLICABLE ROADWAY STANDARD DRAWINGS, LEGEND AND INDEX OF SHEETS
TCP-2	I-40 BUSINESS EAST CLOSURE DETOUR OVERVIEW
TCP-3	DETAIL 'A-E': DETOUR FOR I-40 BUS E / US 421 S (EXIT 212)
TCP-4	DETAIL 'B-E': DETOUR FOR ALL ROUTES (EXITS 119, 120 & 122)
TCP-5	DETAIL 'C-E': DETOUR FOR US 220 N/US 29 N/US 70 E/US 421 S (EXIT 126)
TCP-6	DETAIL 'D-E': DETOUR FOR I-40 BUS E / I-85 BUS N (EXIT 131)
TCP-7	DETAIL 'E-E': ROAD CLOSURE OF I-85 BUS N AT US 220 N & REHOBETH RD (EXIT 35AB&C)
TCP-8	DETAIL 'F-E': ROAD CLOSURE OF I-40 BUS E / I-85 BUS N (EXITS 36-39)
TCP-9	DETAIL 'G-E': ROAD CLOSURE OF I-40 BUS E AT US 220 N (OLD EXIT 218)
TCP-10	LANE CLOSURES OF I-40 E AT I-40 BUS E (EXIT 212)
TCP-11	LANE CLOSURES OF I-40 BUS E AT US 220 N (OLD EXIT 218) [1 OF 3]
TCP-12	LANE CLOSURES OF I-40 BUS E AT US 220 N (OLD EXIT 218) [2 OF 3]
TCP-13	LANE CLOSURES OF I-40 BUS E AT US 220 N (OLD EXIT 218) [3 OF 3]
TCP-14	LANE CLOSURES OF I-85 N AT I-85 BUS N (EXIT 120)
TCP-15	LANE CLOSURES OF I-85 BUS NB AT US 220 N (EXIT 35A & 35B)
TCP-16	I-40 BUSINESS WEST CLOSURE DETOUR OVERVIEW
TCP-17	DETAIL 'A-W': DETOUR FOR I-40 BUS W / US 421 N AT I-40 W (EXIT 212)
TCP-18	DETAIL 'B-W': DETOUR FOR ALL ROUTES (EXITS 119, 120 & 122)
TCP-19	DETAIL 'C-W': DETOUR FOR US 421 N (EXIT 126)
TCP-20	DETAIL 'D-W': DETOUR FOR US 220 S / US 29 S / US 70 W (EXIT 131)
TCP-21	DETAIL 'F-W'/'H-W': ROAD CLOSURE OF I-40 BUS W / I-85 BUS S (EXITS 36-39)
TCP-22	LANE CLOSURES OF I-40 W / I-85 S AT I-40 BUS W (EXITS 44 & 131)
TCP-23	LANE CLOSURES OF I-40 W / I-85 S AT I-40 BUS W (EXITS 131 & 132)
TCP-24	ADVANCE WORK ZONE WARNING SIGNS FOR FREEWAYS (4 LANES OR GREATER)
TCP-25	SIGN DESIGN FOR ADVANCE WORK ZONE WARNING SIGNS

LEGEND

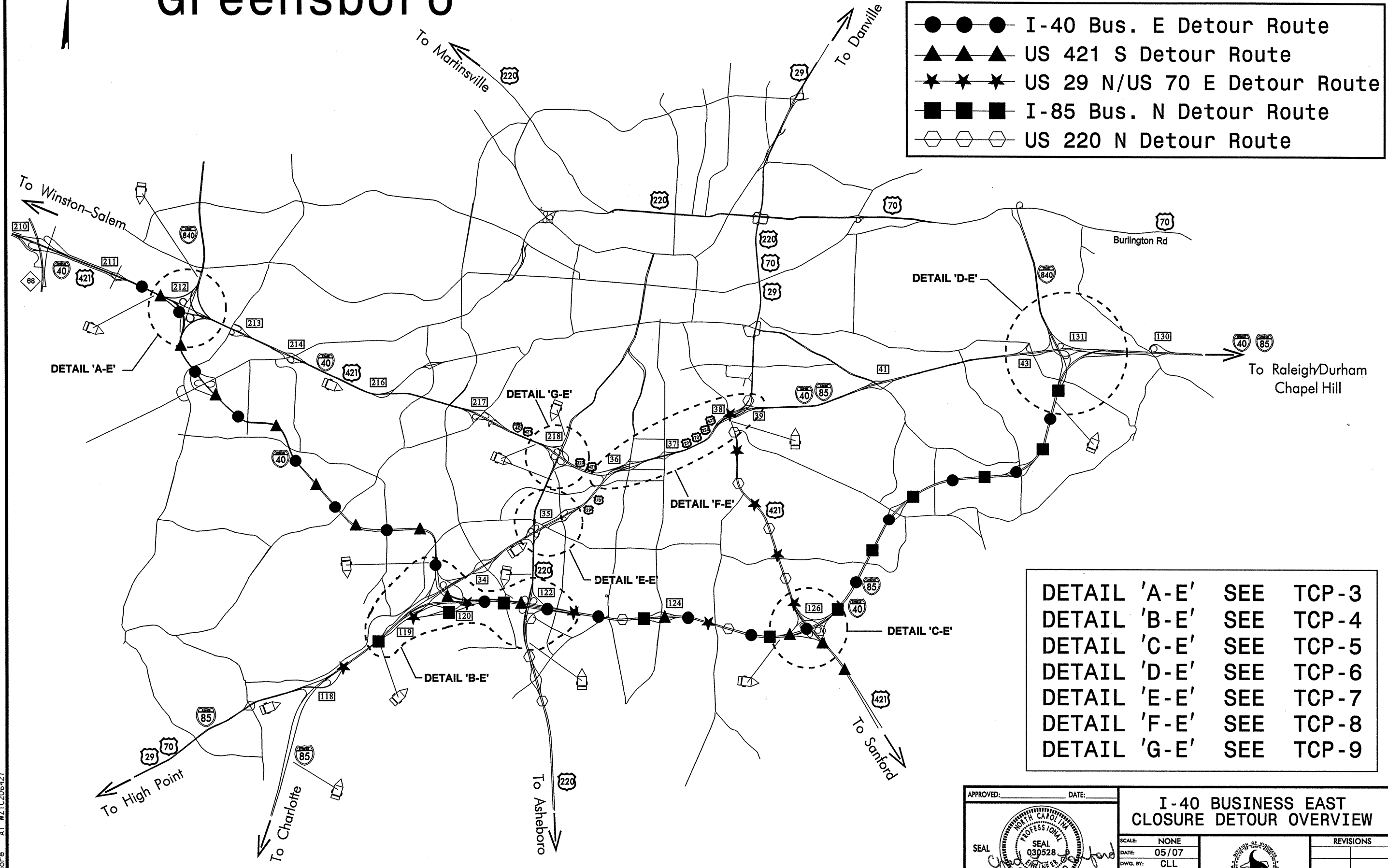
- GENERAL**
- DIRECTION OF TRAFFIC FLOW
 - NORTH ARROW
 - PROPOSED PVMT. EXIST. PVMT.
 - WORK AREA
 - REMOVAL OF EXISTING PAVEMENT
- TRAFFIC CONTROL DEVICES**
- TYPE I BARRICADE
 - TYPE II BARRICADE
 - TYPE III BARRICADE
 - CONE
 - DRUM SKINNY DRUM
 - FLASHING ARROW PANEL (TYPE C)
 - STATIONARY SIGN
 - PORTABLE SIGN
 - STATIONARY OR PORTABLE SIGN
 - CRASH CUSHION
 - CHANGEABLE MESSAGE SIGN
 - TRUCK MOUNTED IMPACT ATTENUATOR (TMIA)
 - POLICE
 - FLAGGER
- PAVEMENT MARKINGS**
- CRYSTAL/CRYSTAL PAVEMENT MARKER
 - YELLOW/YELLOW PAVEMENT MARKER
 - CRYSTAL/RED PAVEMENT MARKER
 - PAVEMENT MARKING SYMBOLS

APPROVED: DATE: 8/15/07	PLAN PREPARED BY: N.C.D.O.T. WORK ZONE TRAFFIC CONTROL UNIT
SEAL 	J.S. BOURNE, PE TRAFFIC CONTROL ENGINEER
	M. MCDIARMID, PE TRAFFIC CONTROL PROJECT ENGINEER
	C.LANFORD, PE TRAFFIC CONTROL PROJECT DESIGN ENGINEER
	P.SEYMORE TRAFFIC CONTROL DESIGN ENGINEER / TECHNICIAN

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Greensboro

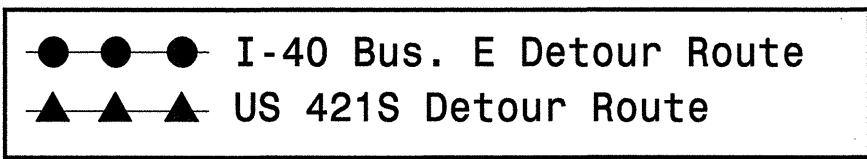
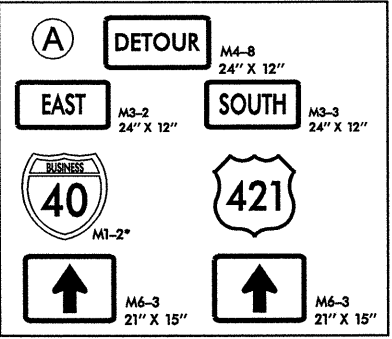
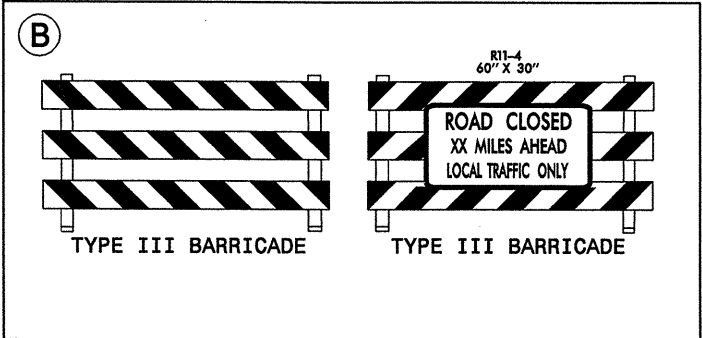
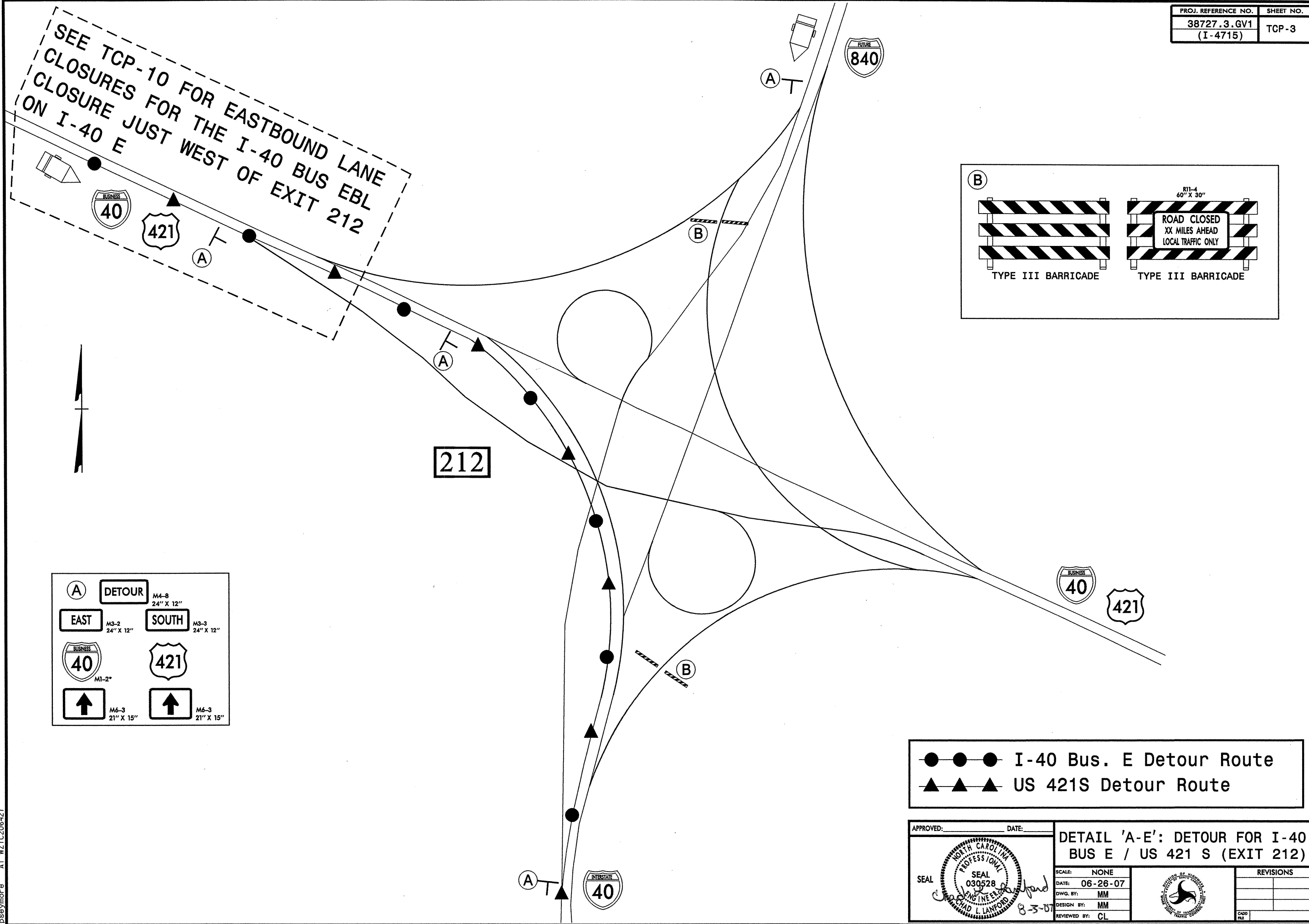
- I-40 Bus. E Detour Route
- ▲▲▲ US 421 S Detour Route
- ★★★ US 29 N/US 70 E Detour Route
- ■ ■ I-85 Bus. N Detour Route
- ◇◇◇ US 220 N Detour Route



DETAIL 'A-E'	SEE	TCP-3
DETAIL 'B-E'	SEE	TCP-4
DETAIL 'C-E'	SEE	TCP-5
DETAIL 'D-E'	SEE	TCP-6
DETAIL 'E-E'	SEE	TCP-7
DETAIL 'F-E'	SEE	TCP-8
DETAIL 'G-E'	SEE	TCP-9

APPROVED: _____	DATE: _____	I-40 BUSINESS EAST CLOSURE DETOUR OVERVIEW										
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		REVISIONS										
DWG. BY: CLL	DESIGN BY: CLL	REVIEWED BY: CLL										

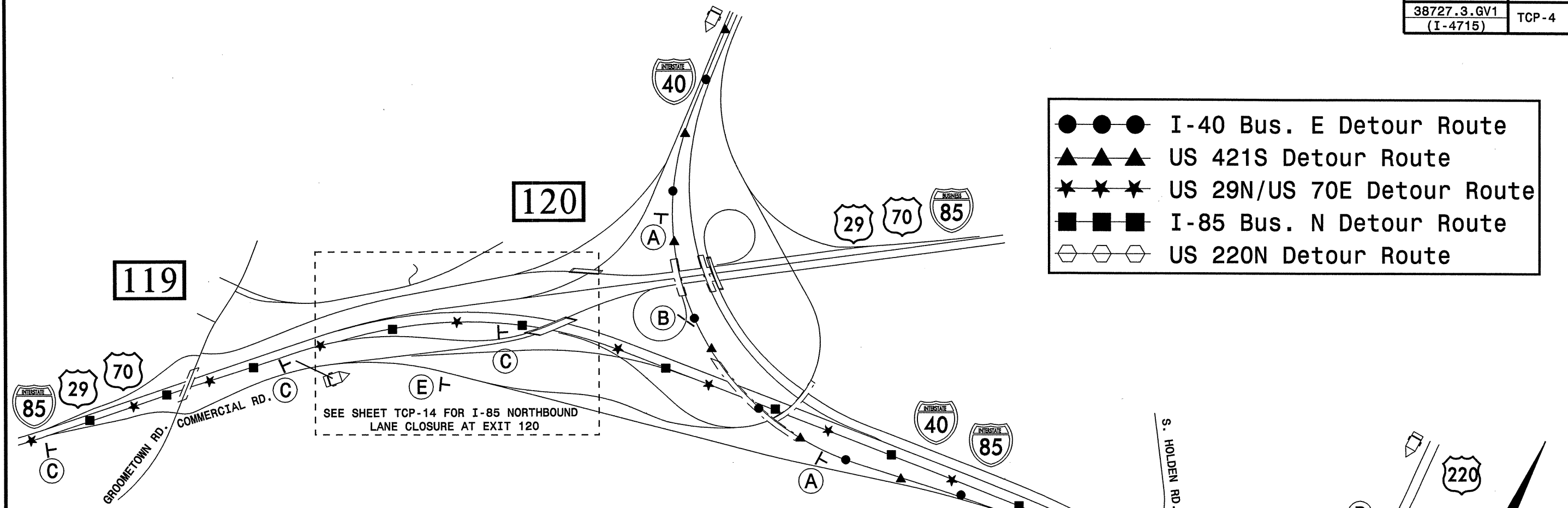
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 pseymore AT WZTCCC



APPROVED:	DATE:	DETAIL 'A-E': DETOUR FOR I-40 BUS E / US 421 S (EXIT 212)	<table border="1"> <tr> <td>SCALE:</td> <td>NONE</td> </tr> <tr> <td>DATE:</td> <td>06-26-07</td> </tr> <tr> <td>DWG. BY:</td> <td>MM</td> </tr> <tr> <td>DESIGN BY:</td> <td>MM</td> </tr> <tr> <td>REVIEWED BY:</td> <td>CL</td> </tr> </table>	SCALE:	NONE	DATE:	06-26-07	DWG. BY:	MM	DESIGN BY:	MM	REVIEWED BY:	CL
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DATE:	06-26-07												
DWG. BY:	MM												
DESIGN BY:	MM												
REVIEWED BY:	CL												
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 psey@more AT WZTCC206427

- I-40 Bus. E Detour Route
- ▲▲▲ US 421S Detour Route
- ★★★ US 29N/US 70E Detour Route
- ■ ■ I-85 Bus. N Detour Route
- ○ ○ US 220N Detour Route



(A) DETOUR M4-8 24" X 12"

EAST M3-2 24" X 12"	NORTH M3-1 24" X 12"	SOUTH M3-3 24" X 12"
M1-2*	M1-2*	M1-2*
M6-3 21" X 15"	M6-3 21" X 15"	M6-3 21" X 15"

(B) R11-4 60" X 30"

TYPE III BARRICADE

(C) DETOUR M4-8 24" X 12"

NORTH M3-1 24" X 12"	NORTH M3-1 24" X 12"
M1-2*	M1-2*
M6-3 21" X 15"	M6-3 21" X 15"
EAST M3-2 24" X 12"	NORTH M3-3 24" X 12"
M1-2*	M1-2*
M6-3 21" X 15"	M6-3 21" X 15"

(D) DETOUR M4-8 24" X 12"

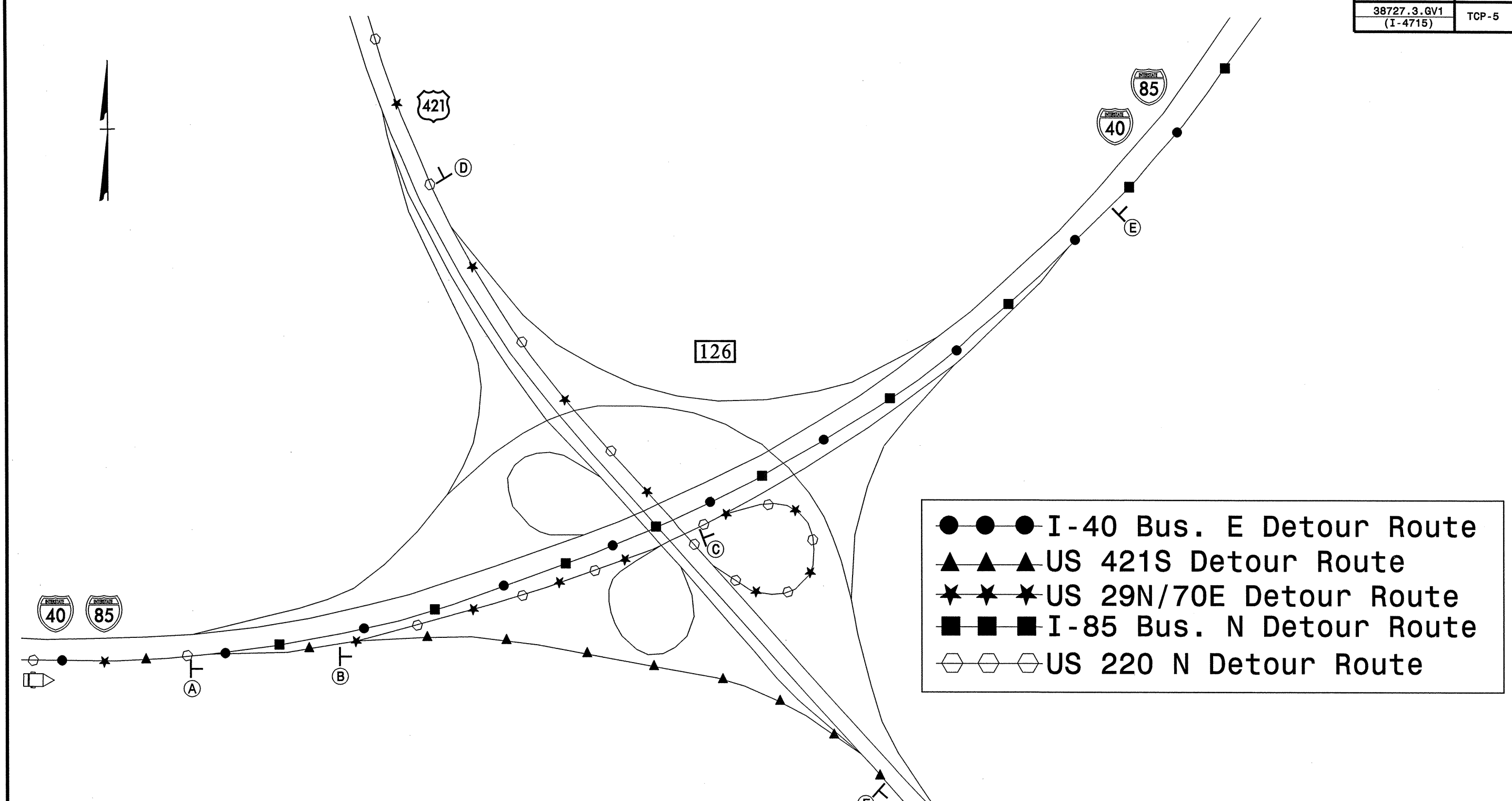
EAST M3-2 24" X 12"	NORTH M3-1 24" X 12"	NORTH M3-1 24" X 12"
M1-2*	M1-2*	M1-2*
M6-3 21" X 15"	M6-3 21" X 15"	M6-3 21" X 15"
EAST M3-2 24" X 12"	NORTH M3-3 24" X 12"	SOUTH M3-3 24" X 12"
M1-2*	M1-2*	M1-2*
M6-3 21" X 15"	M6-3 21" X 15"	M6-3 21" X 15"

(E) DETOUR M4-8 24" X 12"

EAST M3-2 24" X 12"	NORTH M3-1 24" X 12"	NORTH M3-1 24" X 12"
M1-2*	M1-2*	M1-2*
M6-2 21" X 15"	M6-2 21" X 15"	M6-2 21" X 15"
EAST M3-2 24" X 12"	NORTH M3-1 24" X 12"	SOUTH M3-3 24" X 12"
M1-2*	M1-2*	M1-2*
M6-2 21" X 15"	M6-2 21" X 15"	M6-2 21" X 15"

APPROVED: _____	DATE: _____	DETAIL 'B-E': DETOUR FOR ALL ROUTES (EXITS 119, 120 & 122)		
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	REVIEWED BY: CI			

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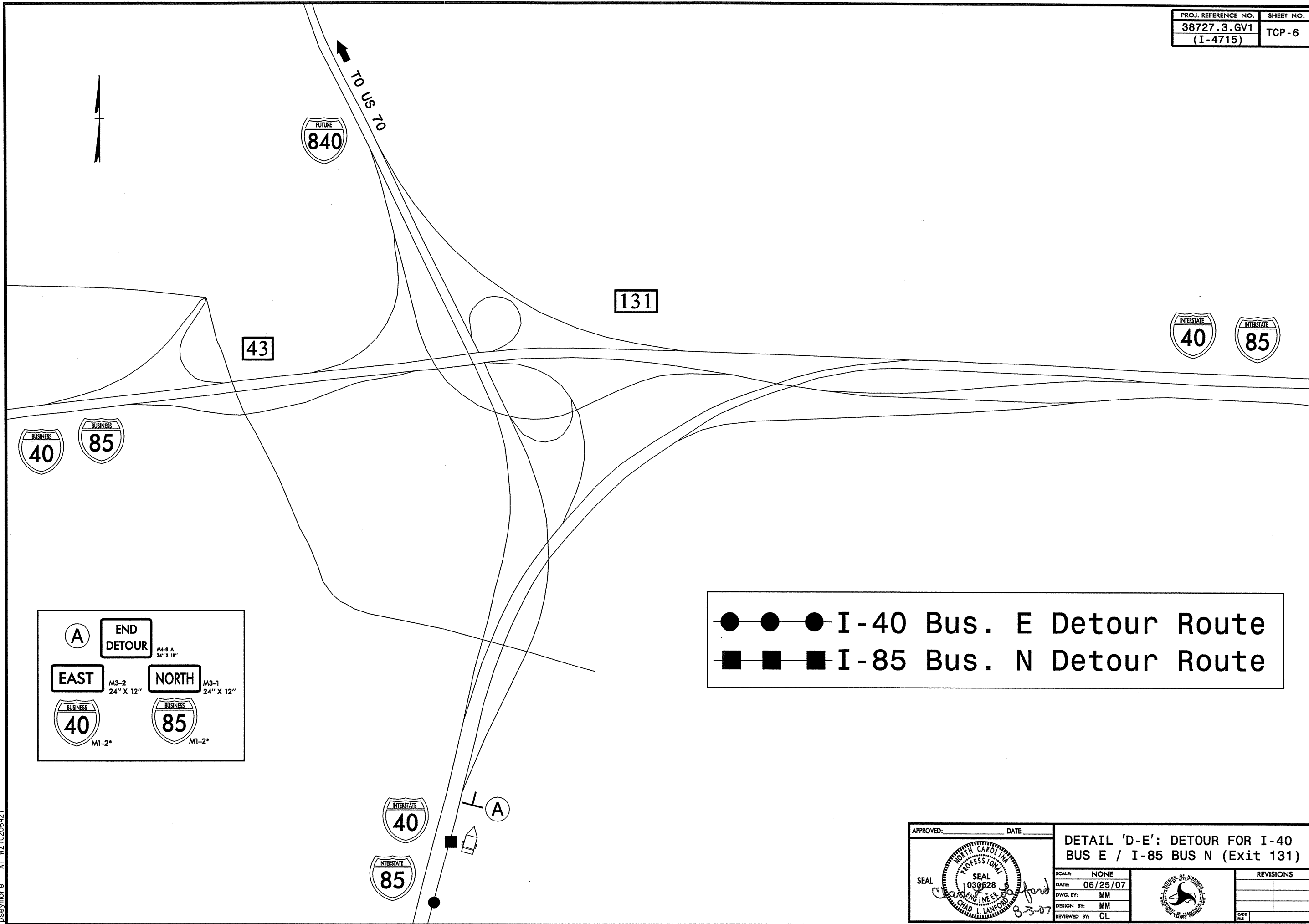


●—●—●	I-40 Bus. E Detour Route
▲—▲—▲	US 421S Detour Route
★—★—★	US 29N/70E Detour Route
■—■—■	I-85 Bus. N Detour Route
⬡—⬡—⬡	US 220 N Detour Route

<p>(A) DETOUR M4-8 24" X 12"</p> <table border="1"> <tr> <td>EAST M3-2 24" X 12"</td> <td>NORTH M3-1 24" X 12"</td> <td>NORTH M3-1 24" X 12"</td> </tr> <tr> <td></td> <td></td> <td></td> </tr> <tr> <td></td> <td></td> <td></td> </tr> <tr> <td>EAST M3-2 24" X 12"</td> <td>NORTH M3-1 24" X 12"</td> <td>SOUTH M3-3 24" X 12"</td> </tr> <tr> <td></td> <td></td> <td></td> </tr> <tr> <td></td> <td></td> <td></td> </tr> </table>	EAST M3-2 24" X 12"	NORTH M3-1 24" X 12"	NORTH M3-1 24" X 12"							EAST M3-2 24" X 12"	NORTH M3-1 24" X 12"	SOUTH M3-3 24" X 12"							<p>(B) DETOUR M4-8 24" X 12"</p> <table border="1"> <tr> <td>NORTH M3-1 24" X 12"</td> <td>EAST M3-2 24" X 12"</td> </tr> <tr> <td></td> <td></td> </tr> <tr> <td></td> <td></td> </tr> <tr> <td>NORTH M3-1 24" X 12"</td> <td>SOUTH M3-3 24" X 12"</td> </tr> <tr> <td></td> <td></td> </tr> <tr> <td></td> <td></td> </tr> </table>	NORTH M3-1 24" X 12"	EAST M3-2 24" X 12"					NORTH M3-1 24" X 12"	SOUTH M3-3 24" X 12"					<p>(C) DETOUR M4-8 24" X 12"</p> <table border="1"> <tr> <td>NORTH M3-1 24" X 12"</td> </tr> <tr> <td></td> </tr> <tr> <td></td> </tr> <tr> <td>EAST M3-2 24" X 12"</td> <td>NORTH M3-1 24" X 12"</td> </tr> <tr> <td></td> <td></td> </tr> <tr> <td></td> <td></td> </tr> </table>	NORTH M3-1 24" X 12"			EAST M3-2 24" X 12"	NORTH M3-1 24" X 12"					<p>(D) DETOUR M4-8 24" X 12"</p> <table border="1"> <tr> <td>NORTH M3-1 24" X 12"</td> </tr> <tr> <td></td> </tr> <tr> <td></td> </tr> <tr> <td>EAST M3-2 24" X 12"</td> <td>NORTH M3-1 24" X 12"</td> </tr> <tr> <td></td> <td></td> </tr> <tr> <td></td> <td></td> </tr> </table>	NORTH M3-1 24" X 12"			EAST M3-2 24" X 12"	NORTH M3-1 24" X 12"					<p>(E) DETOUR M4-8 24" X 12"</p> <table border="1"> <tr> <td>EAST M3-2 24" X 12"</td> <td>NORTH M3-1 24" X 12"</td> </tr> <tr> <td></td> <td></td> </tr> <tr> <td></td> <td></td> </tr> </table>	EAST M3-2 24" X 12"	NORTH M3-1 24" X 12"					<p>(F) END DETOUR M4-8 A 24" X 18"</p> <table border="1"> <tr> <td>SOUTH M3-3 24" X 12"</td> </tr> <tr> <td></td> </tr> </table>	SOUTH M3-3 24" X 12"	
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SOUTH M3-3 24" X 12"																																																													

APPROVED: _____ DATE: _____	DETAIL 'C-E': DETOUR FOR US 220 N / US 29 N/US 70 E/US 421 S (Exit 126)					
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	DATE: 06/25/07					
	DESIGN BY: MM					
	REVIEWED BY: CL					
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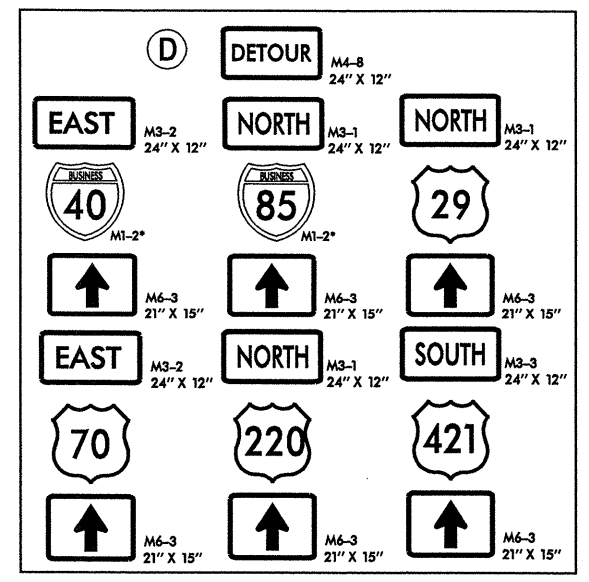
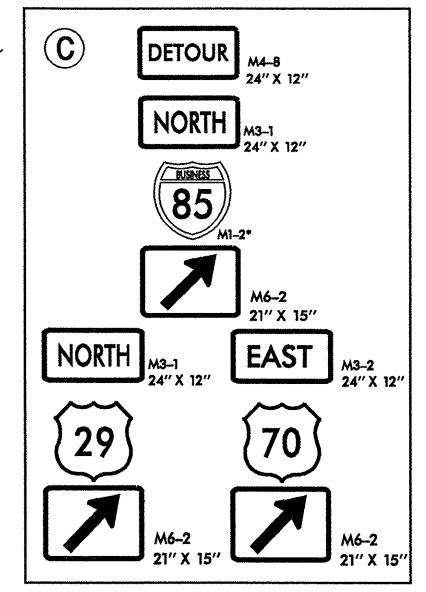
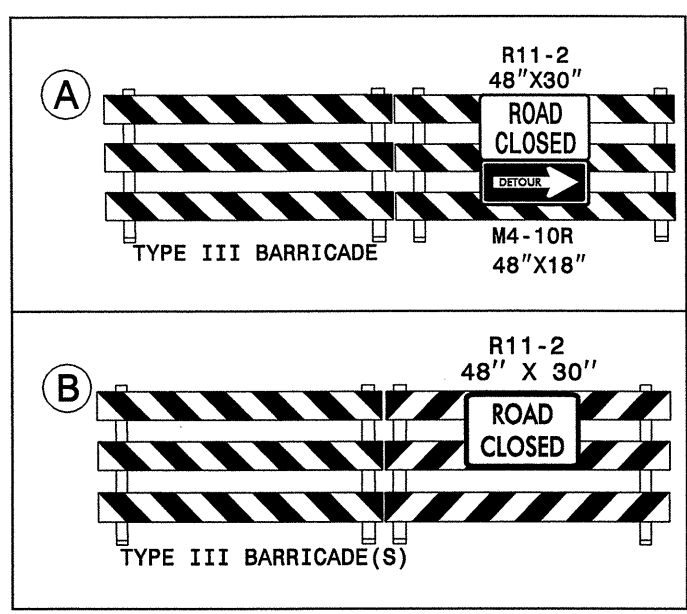


(A)	END DETOUR	M4-8 A 24" X 18"
EAST	NORTH	M3-1 24" X 12"
M3-2 24" X 12"		
BUSINESS 40 M1-2*	BUSINESS 85 M1-2*	

● ● ●	I-40 Bus. E Detour Route
■ ■ ■	I-85 Bus. N Detour Route

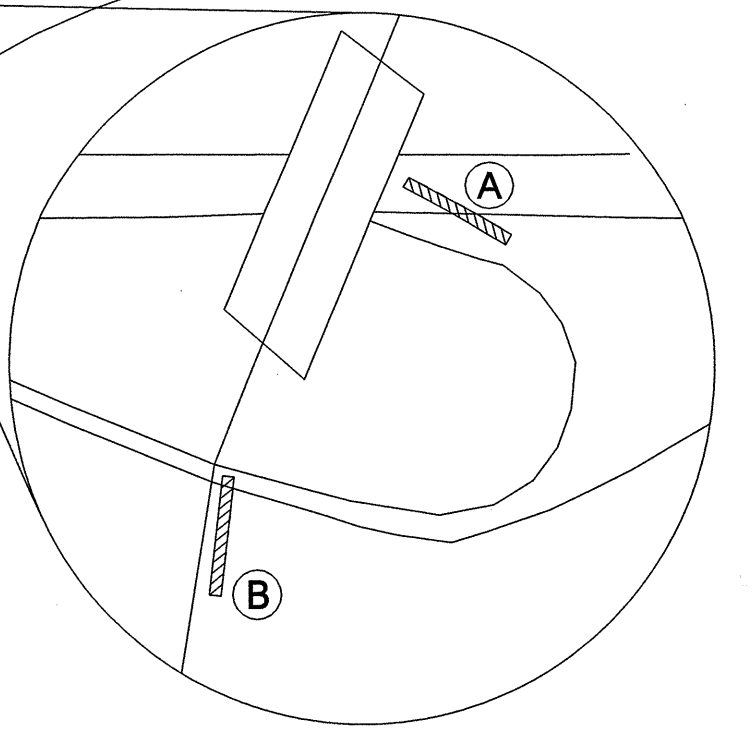
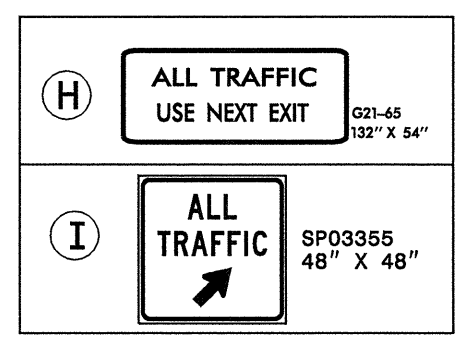
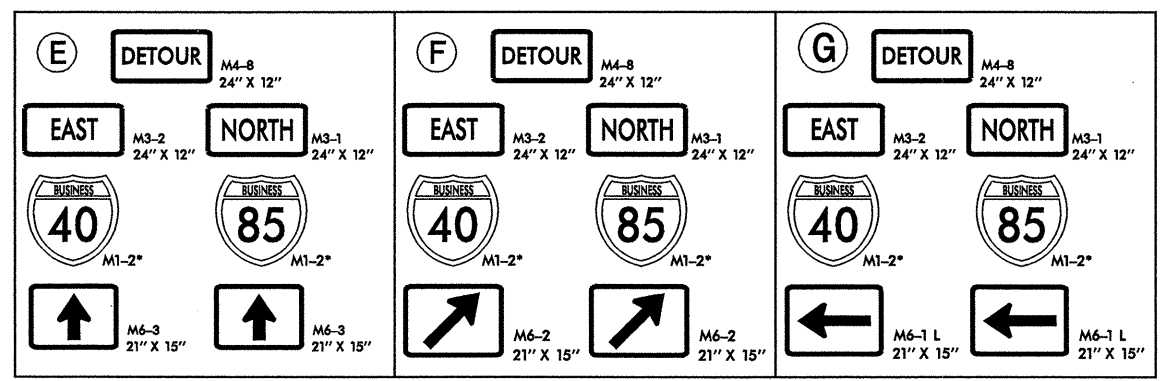
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 pseymore AT WZTC206427

APPROVED: _____	DATE: _____	DETAIL 'D-E': DETOUR FOR I-40 BUS E / I-85 BUS N (Exit 131)	
	SCALE: NONE		REVISIONS
	DATE: 06/25/07		
	DWG. BY: MM		
	DESIGN BY: MM		
	REVIEWED BY: CL		



SEE TCP-15 FOR EASTBOUND LANE CLOSURE OF I-85 BUS N JUST WEST OF EXIT 35A & B

- I-40 Bus. E/I-85 Bus. N Detour Route From Exits 36 & 37
- ★-★-★ US 29N/US 70E Detour Route
- I-85 Bus. N Detour Route
- ◇-◇-◇ US 220N Detour Route



APPROVED: _____ DATE: _____

DETAIL 'E-E': ROAD CLOSURE OF I-85 BUS N AT US 220 N & REHOBETH RD (EXIT 35A&B)

SCALE: NONE DATE: 06/25/07

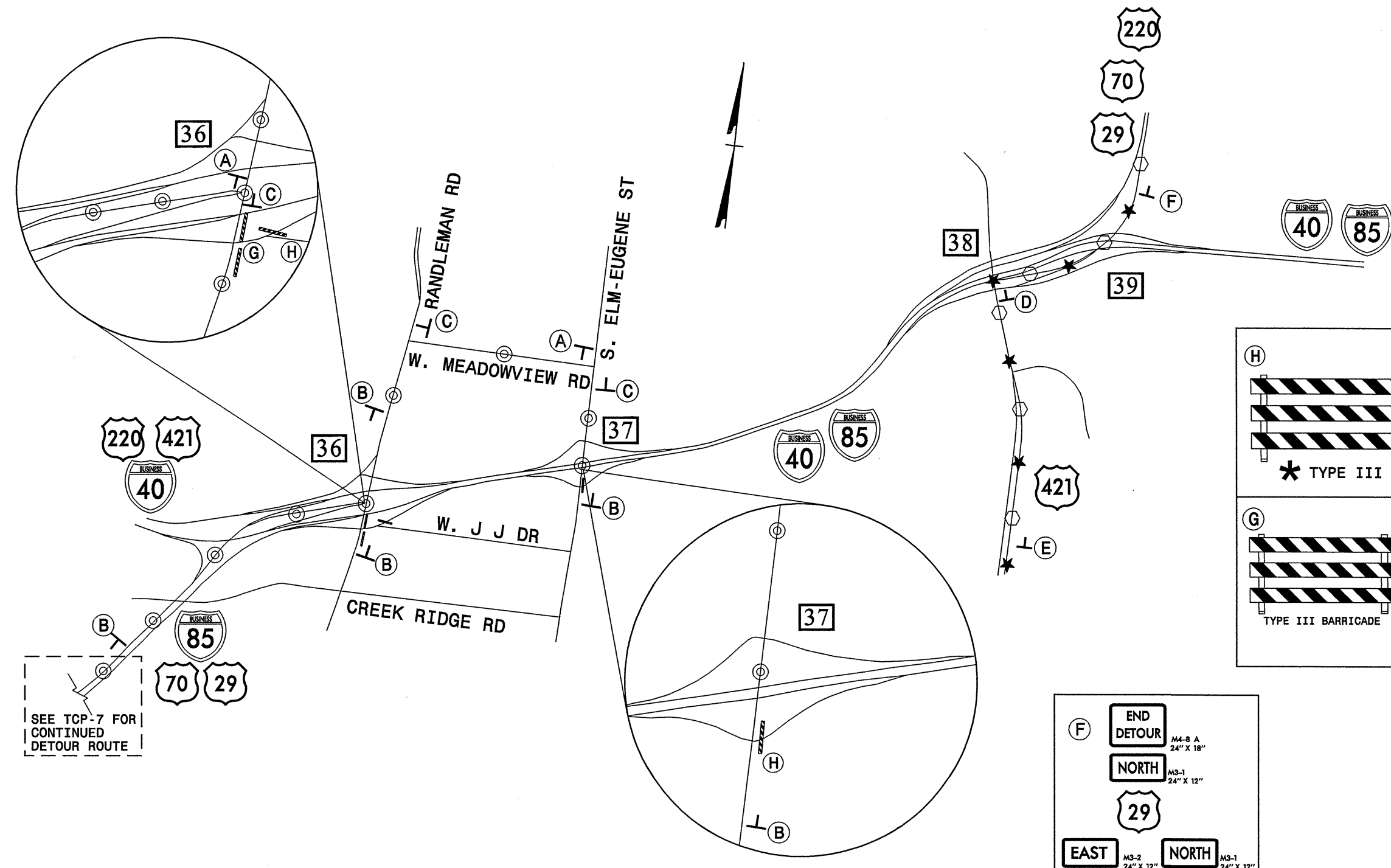
DESIGN BY: MM

REVIEWED BY: CL

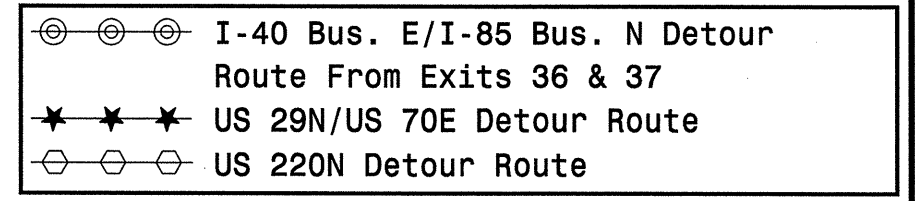
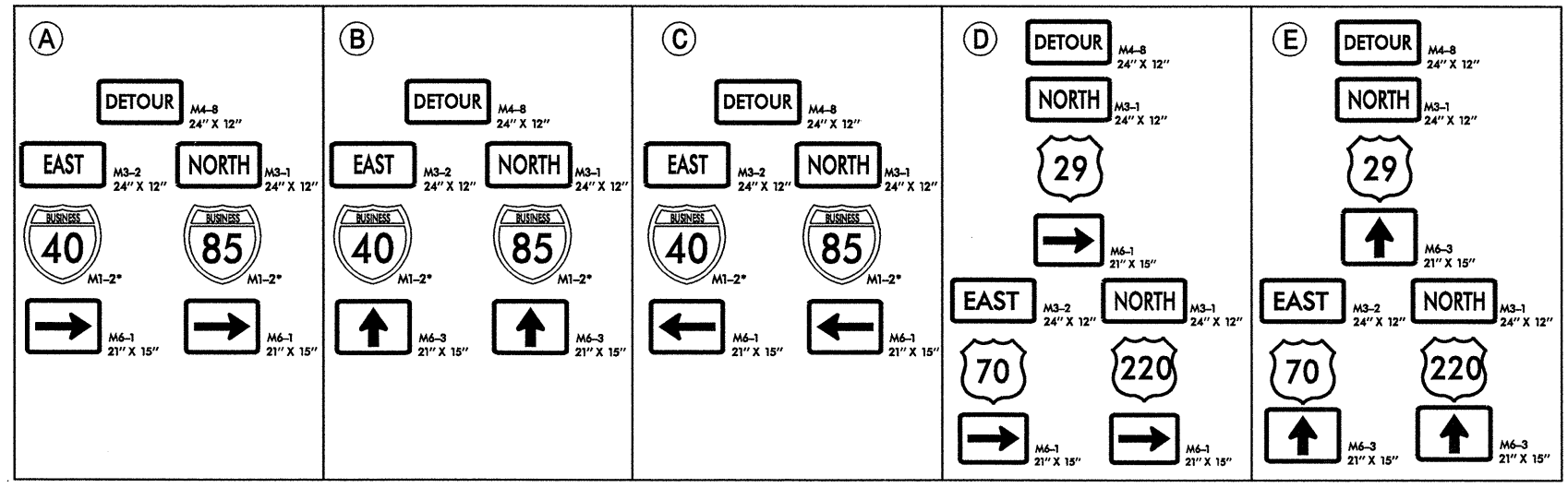
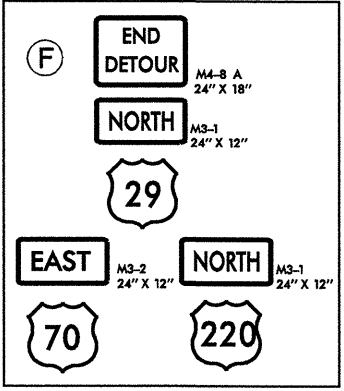
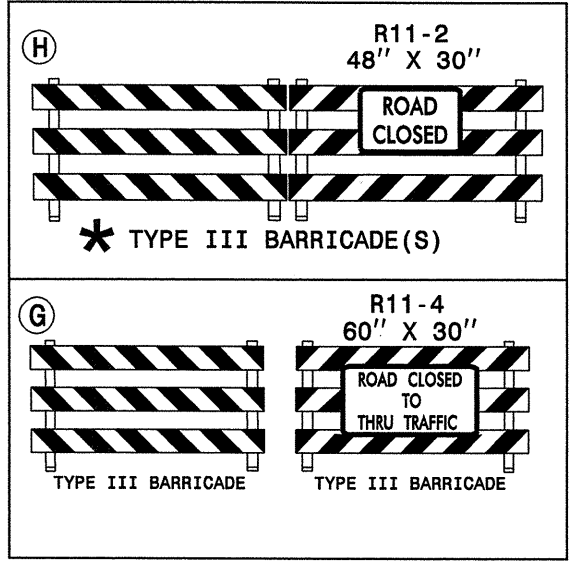
SEAL: CHAD L. LANFORD ENGINEER

REVISIONS

03-AUG-2007 13:45 \\DOT\DFSR00701\GROUPS-WZTCC\design\group4\resur\facings\resur\4715\4715_eastbound\wgs\1-4715_tcp07_Detail_E-E.dgn
 pse\more AT WZTC206427



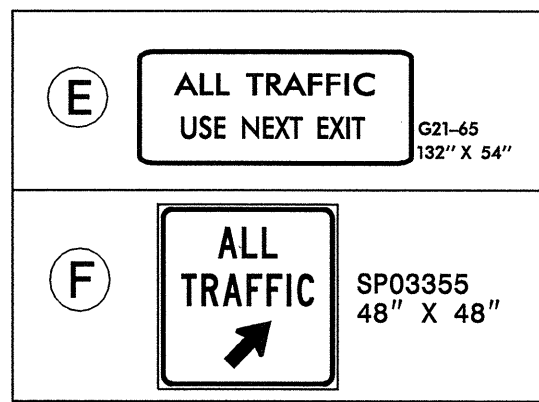
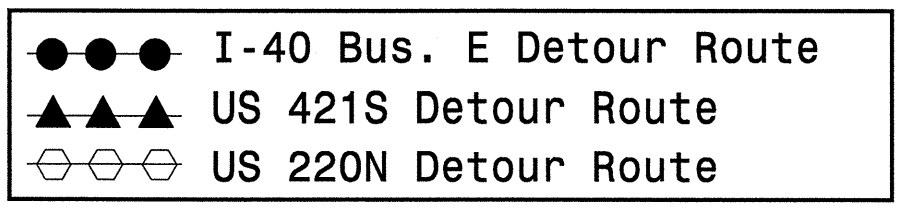
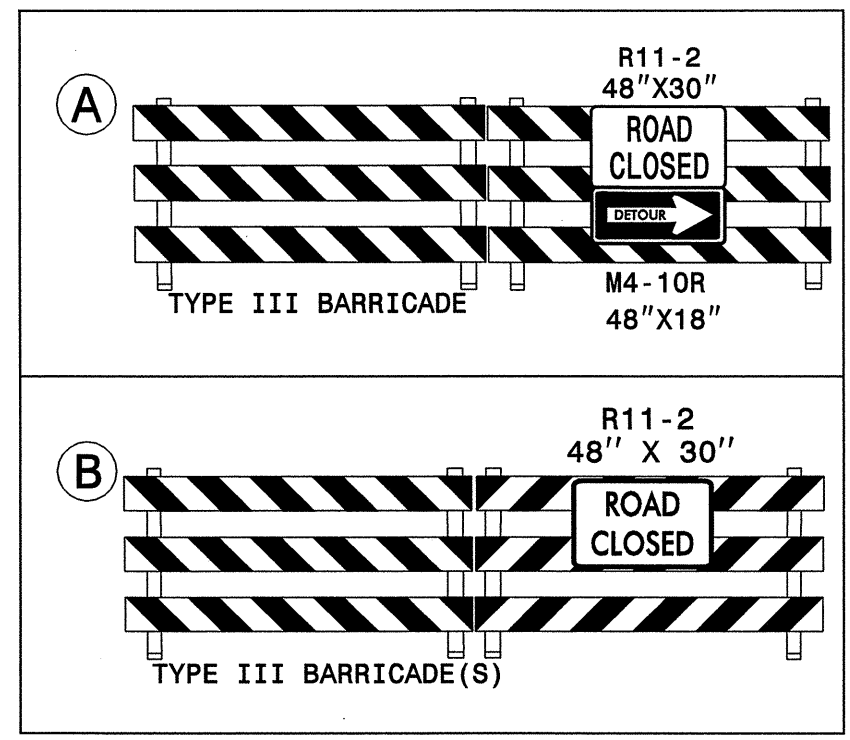
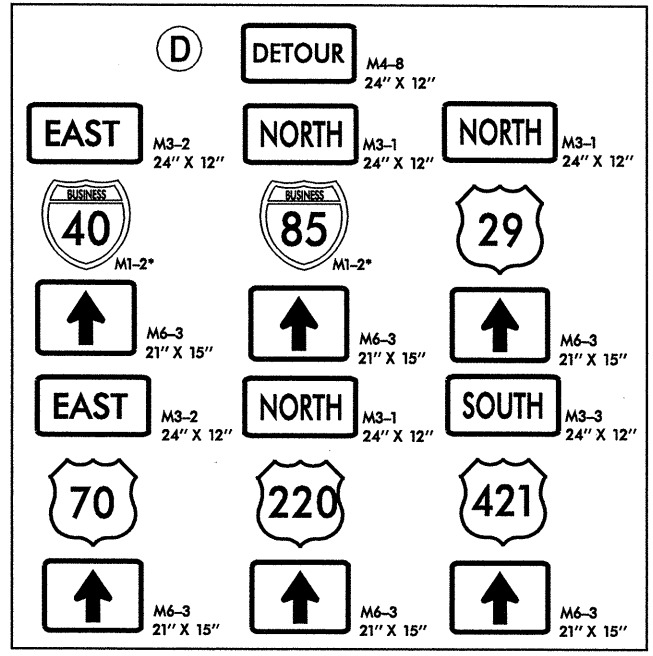
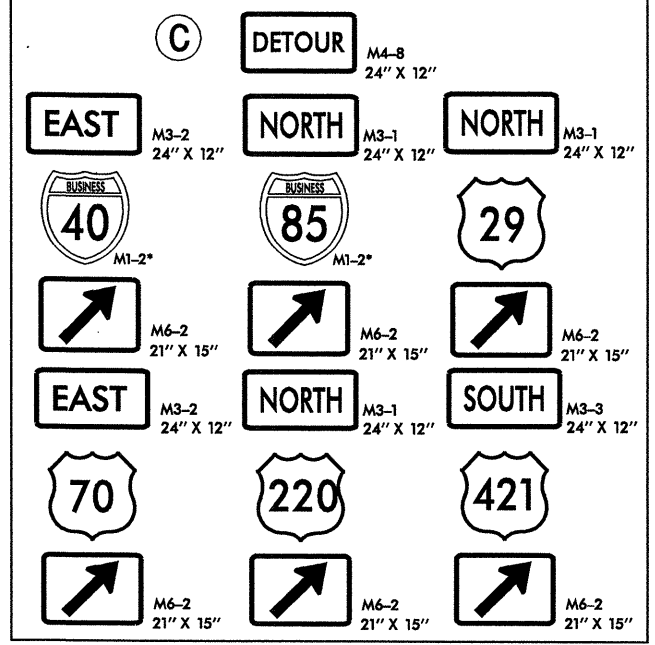
SEE TCP-7 FOR CONTINUED DETOUR ROUTE




APPROVED: _____ DATE: _____	DETAIL 'F-E': ROAD CLOSURE OF I-40 BUS E / I-85 BUS N (EXITS 36-39)									
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	REVISIONS									
DATE: 06/26/07	DESIGN BY: MM									
REVIEWED BY: CL										

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SEE TCP-11, TCP-12 & TCP-13 FOR EASTBOUND
LANE CLOSURES FOR THE ROAD CLOSURE OF I-40
BUSINESS E JUST WEST OF EXIT 218 ON I-40 BUS E.



APPROVED: _____ DATE: _____

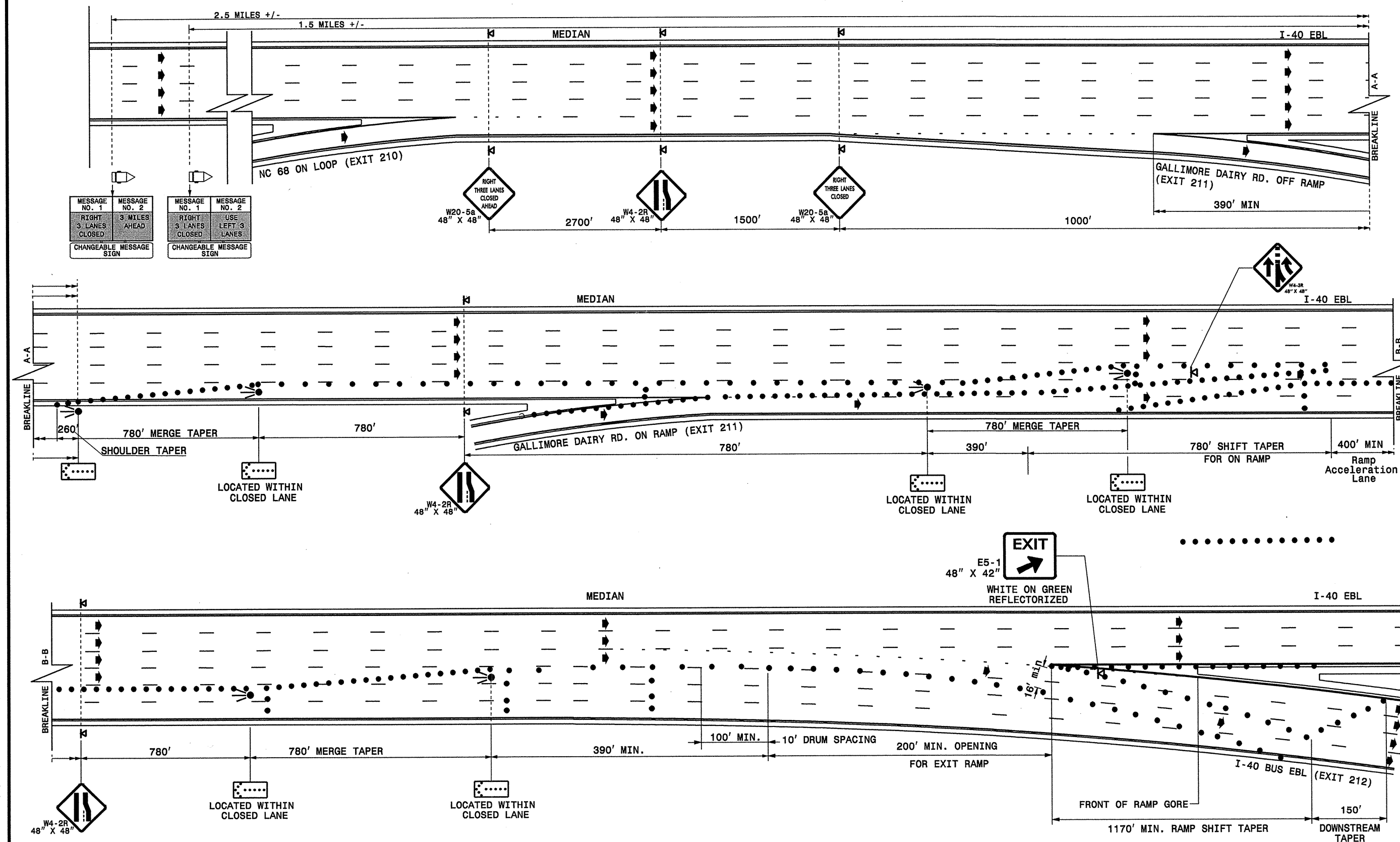
SEAL: 

**DETAIL 'G-E': ROAD CLOSURE
OF I-40 BUS E
AT US 220 N (OLD EXIT 218)**

SCALE: NONE		REVISIONS
DATE: 06/25/07		
DWG. BY: MM		
DESIGN BY: MM		
REVIEWED BY: CL		

CAD FILE

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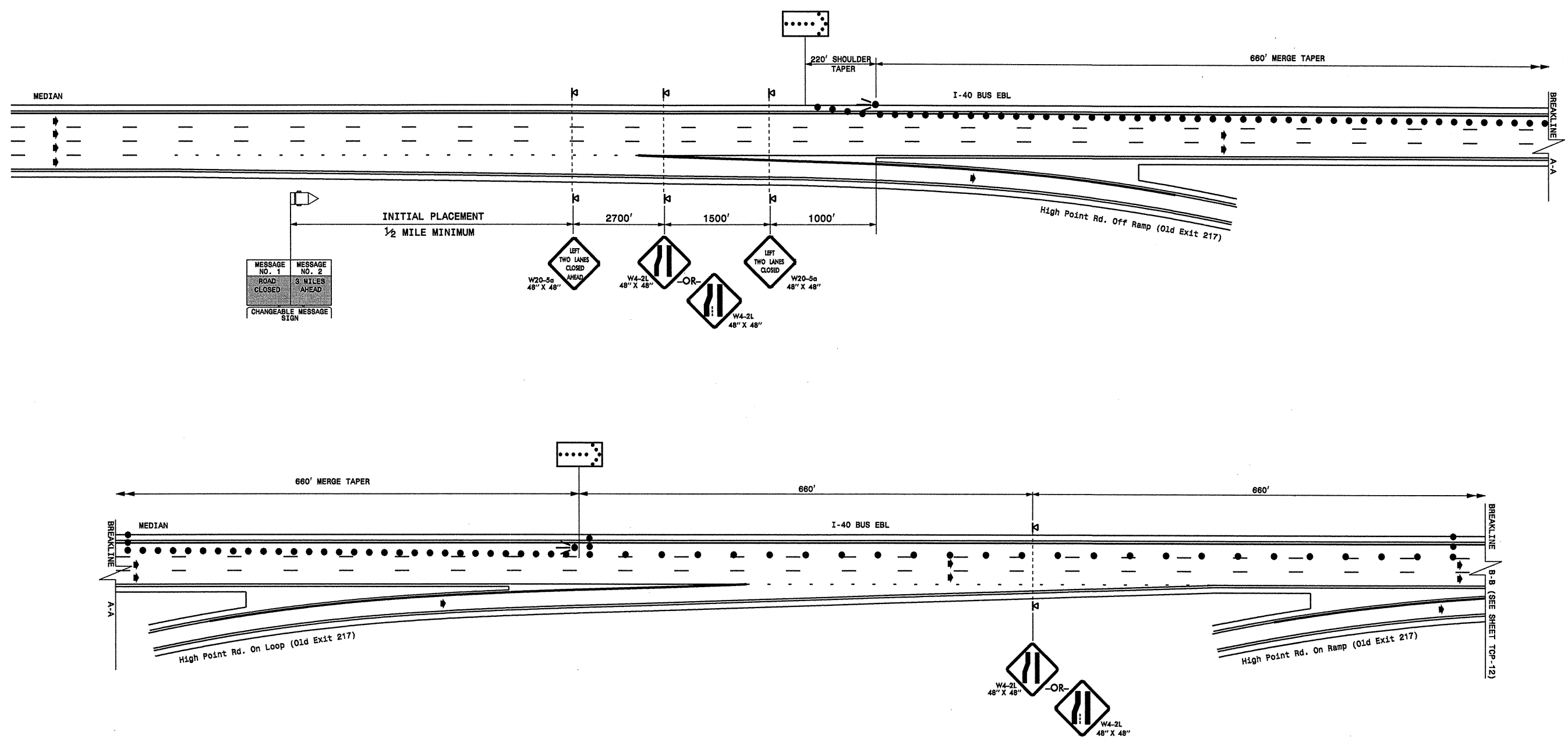


MESSAGE NO. 1 RIGHT 3 LANES CLOSED	MESSAGE NO. 2 3 MILES AHEAD	MESSAGE NO. 1 RIGHT 3 LANES CLOSED	MESSAGE NO. 2 USE LEFT 3 LANES
CHANGEABLE MESSAGE SIGN		CHANGEABLE MESSAGE SIGN	

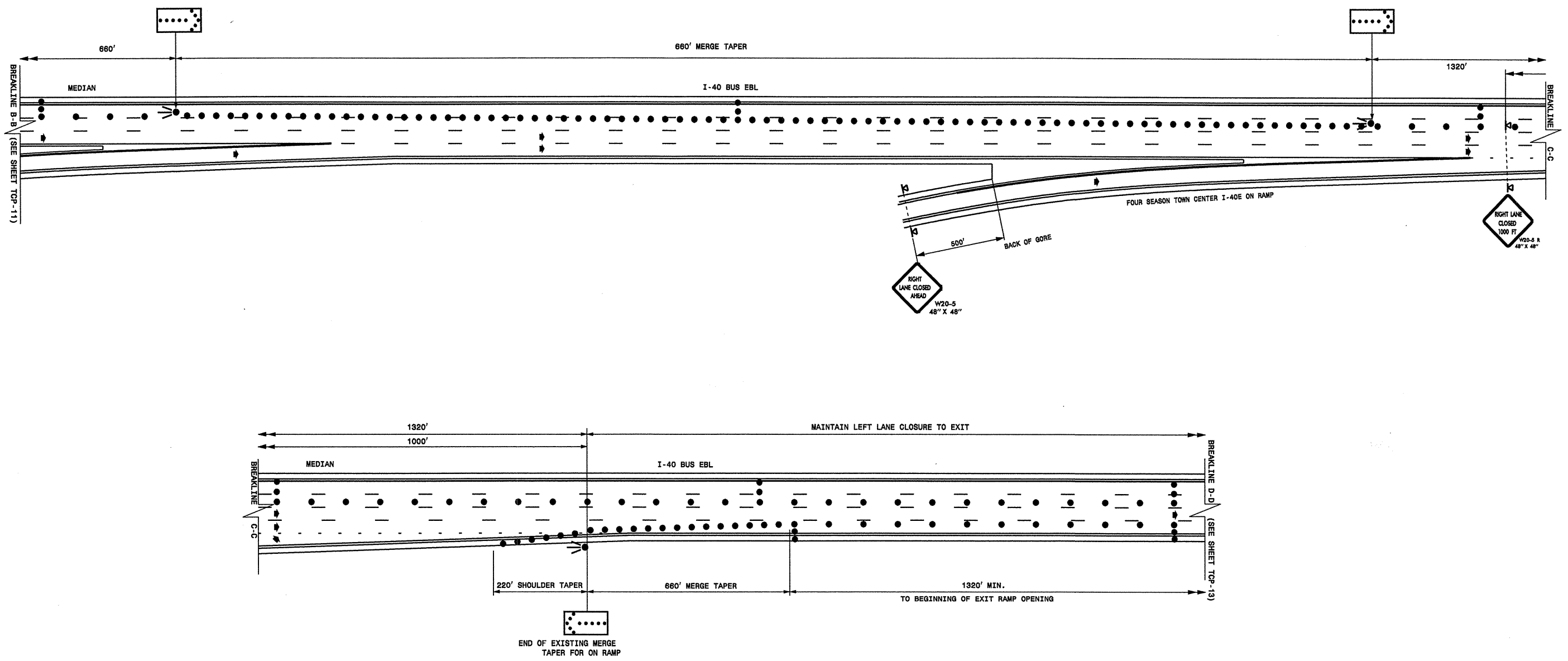
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 AT WZT C224247

APPROVED: _____	DATE: _____	LANE CLOSURES OF I-40 E AT I-40 BUS E (EXIT 212)			SCALE: NONE	<table border="1"> <tr><th colspan="2">REVISIONS</th></tr> <tr><td> </td><td> </td></tr> <tr><td> </td><td> </td></tr> <tr><td> </td><td> </td></tr> </table>	REVISIONS							
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 AT WZTC206427
 pssymore

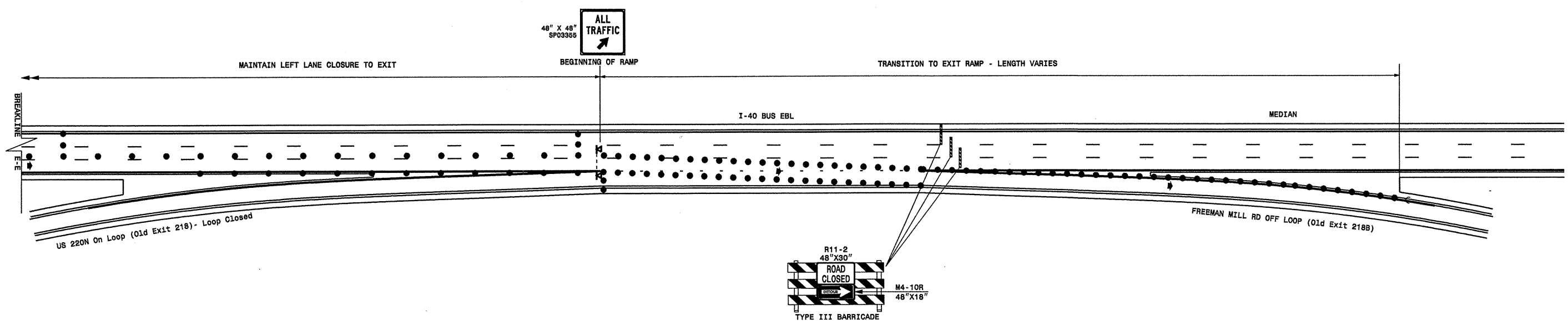
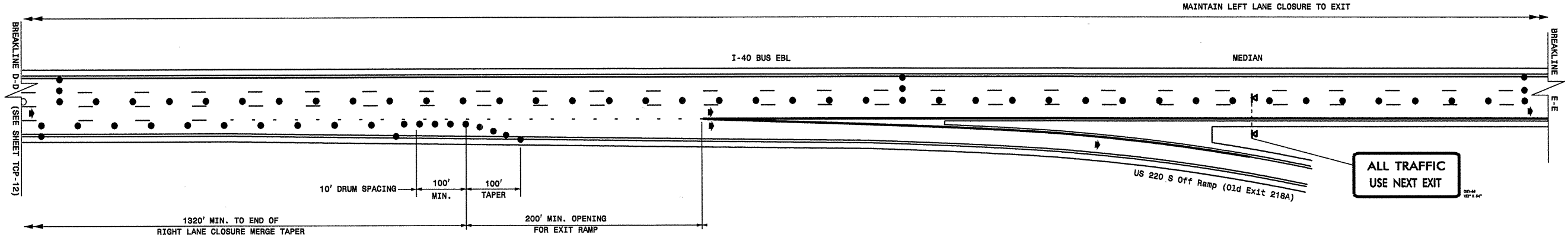


APPROVED:	DATE:	LANE CLOSURES OF I-40 BUS E AT US 220 N (OLD EXIT 218) [1 OF 3]	
SCALE:	NONE		REVISIONS
DATE:	06/07		
DWG. BY:	CLL		
DESIGN BY:	CLL		
REVIEWED BY:	CLL		



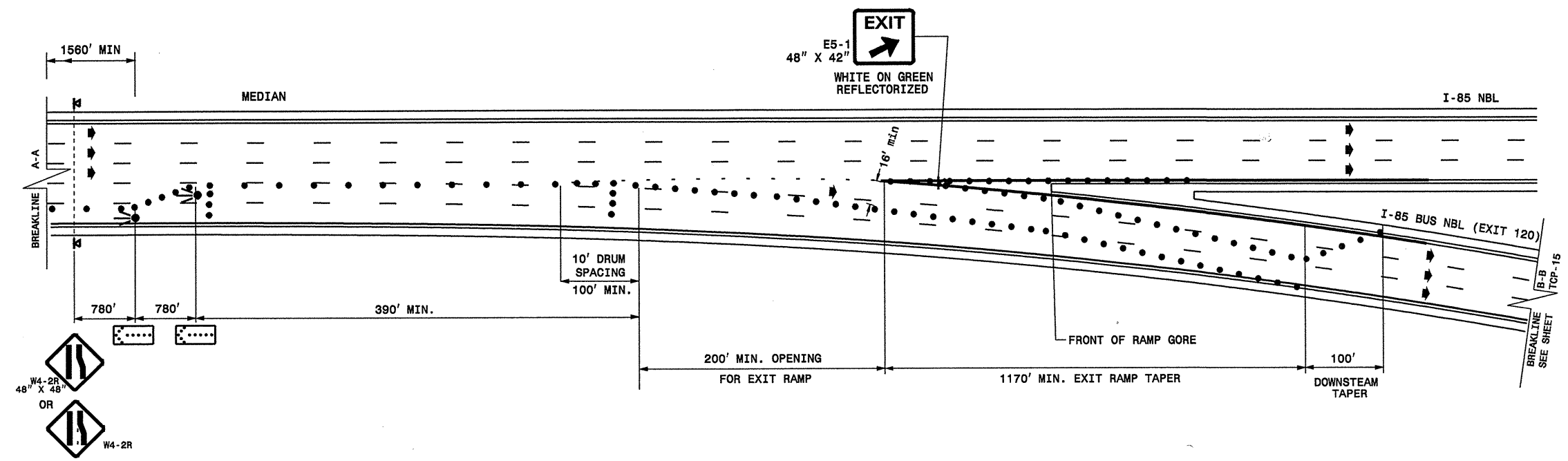
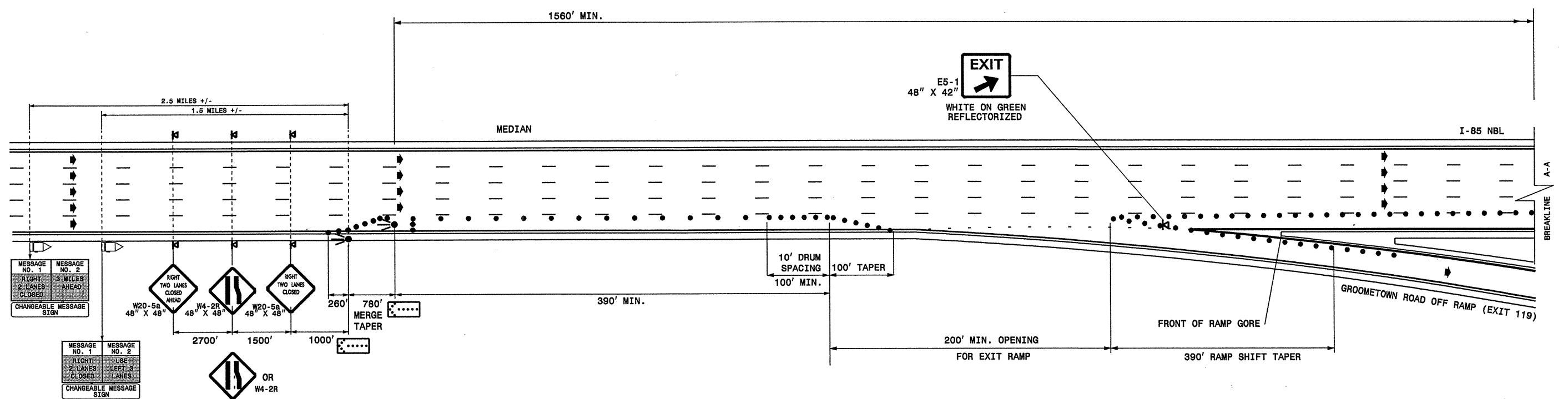
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 psey@more AT WZTCC206427

APPROVED:	DATE:	LANE CLOSURES OF I-40 BUS E AT US 220 N (EXIT 218) [2 OF 3]	SCALE: NONE		REVISIONS
			DATE: 06/07		
				DWG. BY: CLL	
		DESIGN BY: CLL			
		REVIEWED BY: CLL			



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 AT WZTCC24247
 clantford

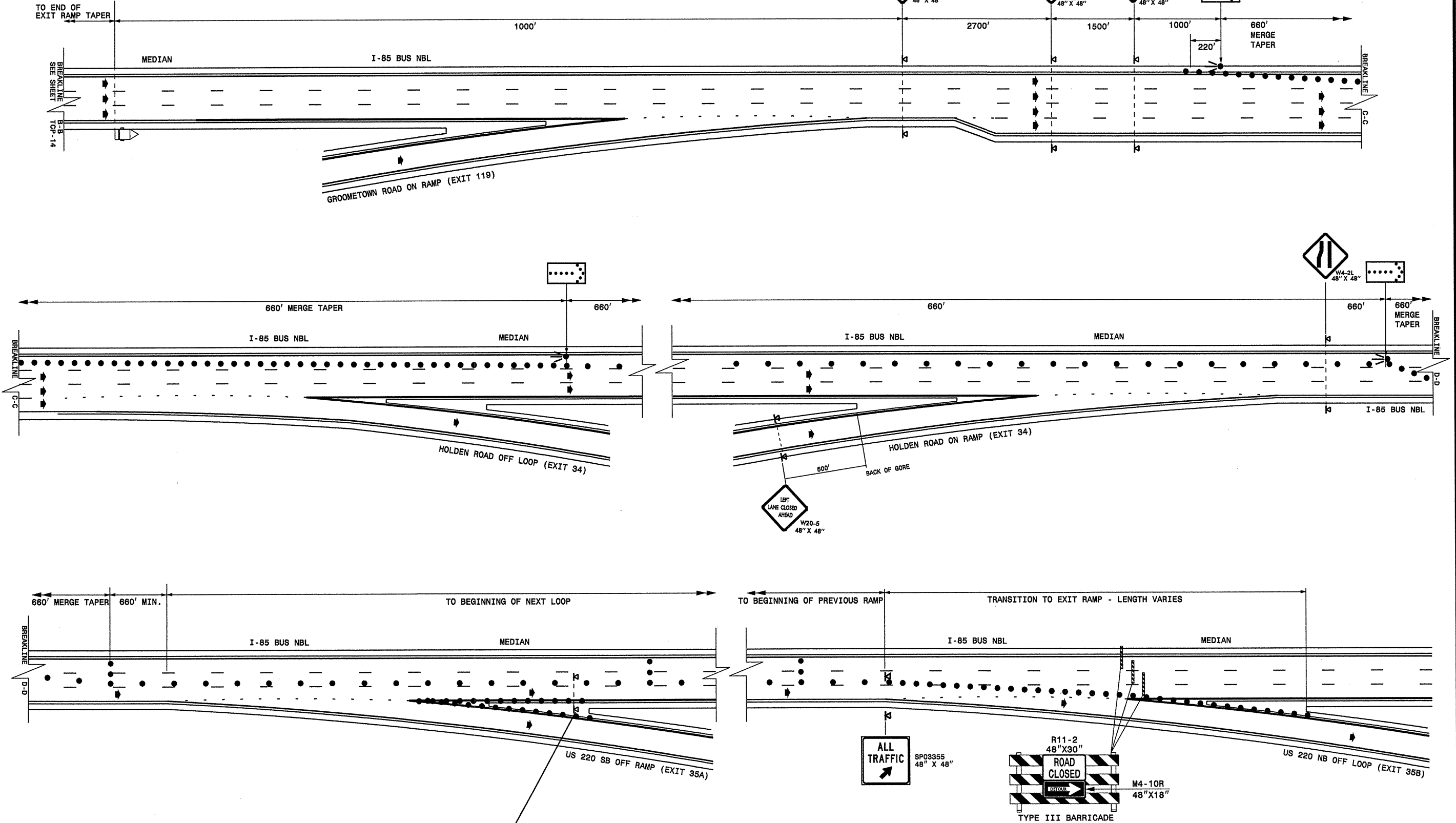
APPROVED: _____	DATE: _____	LANE CLOSURES OF I-40 BUS E AT US 220 N (OLD EXIT 218) [3 OF 3]	
		SCALE: NONE	REVISIONS
		DATE: 06/07	
		DWG. BY: CLL	
		DESIGN BY: CLL	
		REVIEWED BY: CLL	



03-AUG-2007 19:06
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 clartf.dwg AT WZTC224247

APPROVED:	DATE:	LANE CLOSURES OF I-85 N AT I-85 BUS N (EXIT 120)	
SCALE: NONE	DATE: 06/07		REVISIONS
DWG. BY: JS	DESIGN BY: JS		
REVIEWED BY: JS			

MESSAGE NO. 1 ROAD CLOSED	MESSAGE NO. 2 2 MILES AHEAD
CHANGEABLE MESSAGE SIGN	



03-AUG-2007 19:16
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 clarnford AT WZTC224247

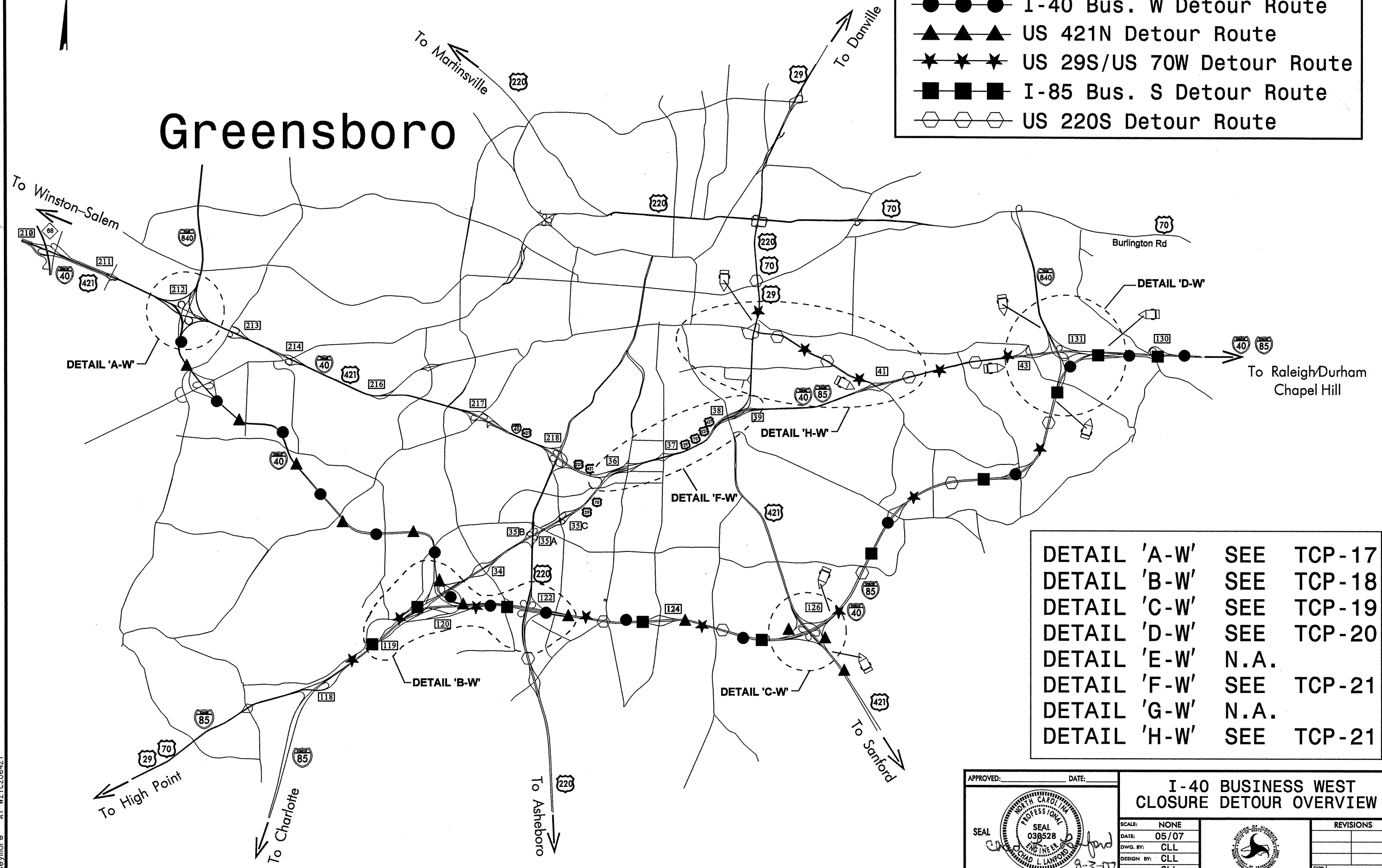
APPROVED: _____ DATE: _____

LANE CLOSURES OF I-85 BUS N AT US 220 N (EXIT 35A & 35B)

SCALE: NONE		REVISIONS
DATE: 06/07		
DWG. BY: CLL		
DESIGN BY: CLL		
REVIEWED BY: CLL		

Greensboro

- I-40 Bus. W Detour Route
- ▲▲▲ US 421N Detour Route
- ★★★ US 29S/US 70W Detour Route
- ■ ■ I-85 Bus. S Detour Route
- ○ ○ US 220S Detour Route

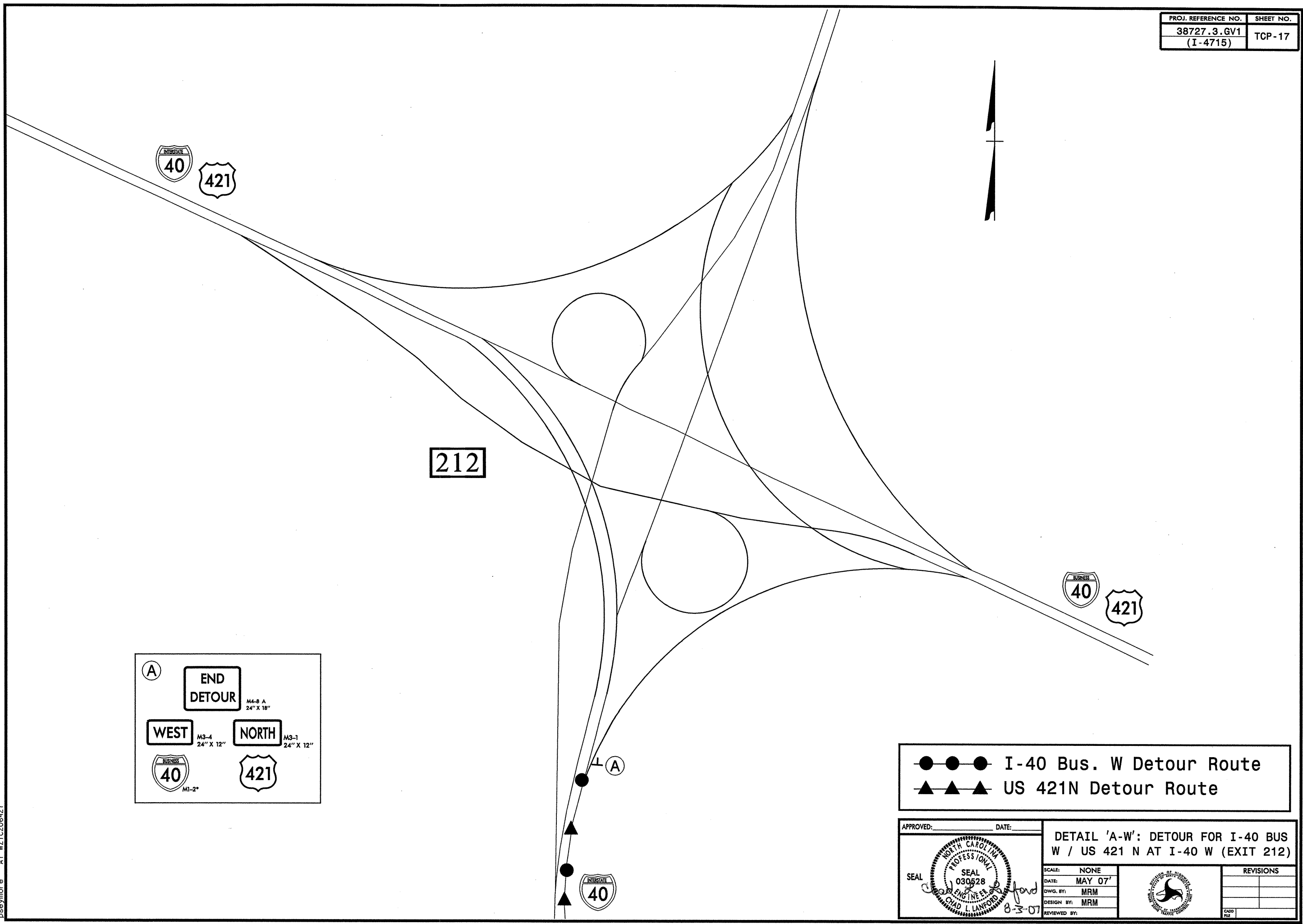


DETAIL 'A-W'	SEE	TCP-17
DETAIL 'B-W'	SEE	TCP-18
DETAIL 'C-W'	SEE	TCP-19
DETAIL 'D-W'	SEE	TCP-20
DETAIL 'E-W'	N.A.	
DETAIL 'F-W'	SEE	TCP-21
DETAIL 'G-W'	N.A.	
DETAIL 'H-W'	SEE	TCP-21



APPROVED:	DATE:	I-40 BUSINESS WEST CLOSURE DETOUR OVERVIEW									
SCALE:	NONE										
DATE:	05/07										
DESIGN BY:	CLL										
REVIEWED BY:	CLL										
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 pssymore AT WZTC206427



03-AUG-2007 15:38
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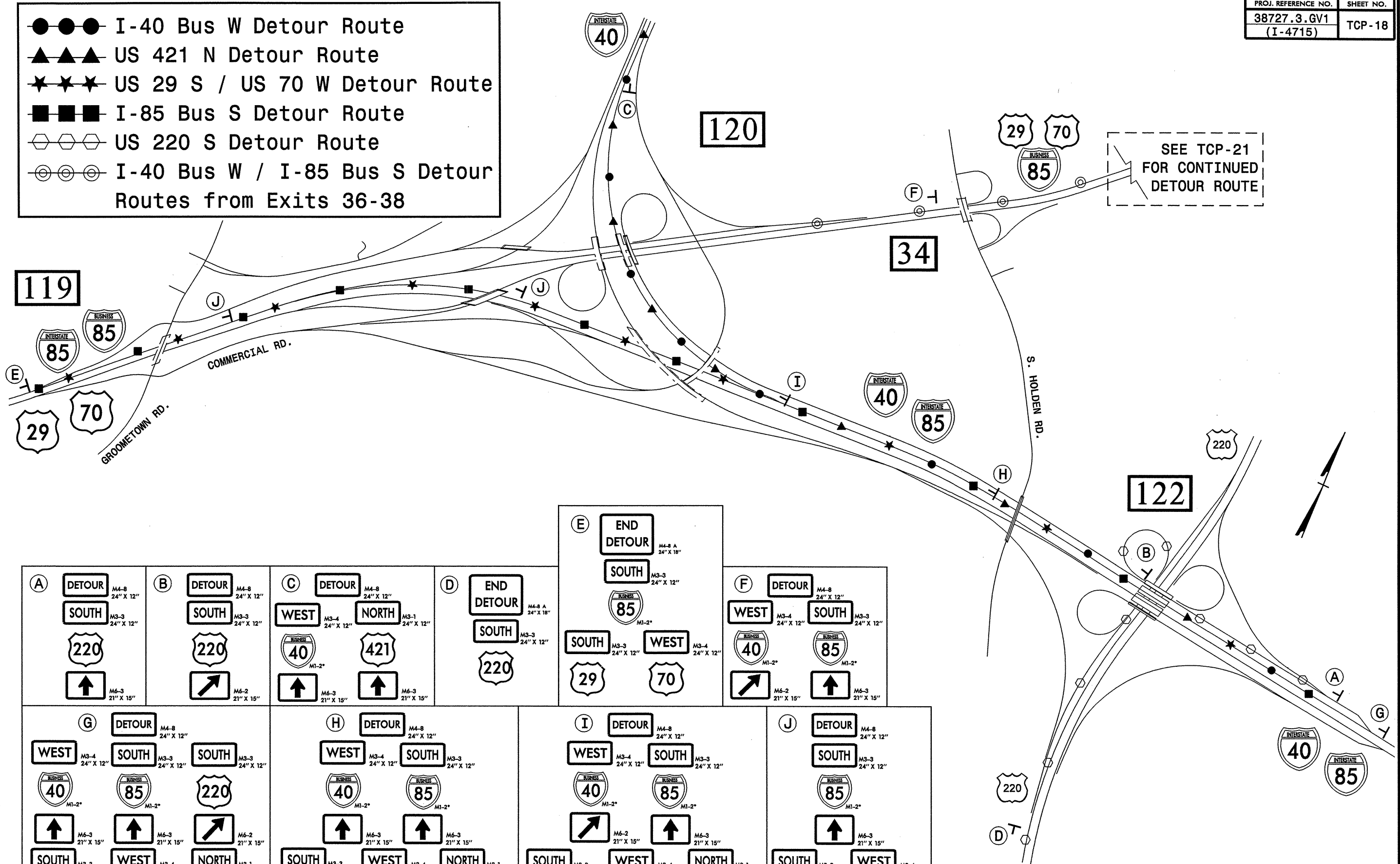
(A)

END DETOUR <small>M4-B A 24" X 18"</small>	
WEST <small>M3-4 24" X 12"</small>  <small>M1-2*</small>	NORTH <small>M3-1 24" X 12"</small> 

	I-40 Bus. W Detour Route
	US 421N Detour Route

APPROVED:	DATE:	DETAIL 'A-W': DETOUR FOR I-40 BUS W / US 421 N AT I-40 W (EXIT 212)								
										
SCALE:	NONE									
DATE:	MAY 07'									
DESIGN BY:	MRM									
REVIEWED BY:	MRM	<table border="1"> <thead> <tr> <th colspan="2">REVISIONS</th> </tr> <tr> <th>NO.</th> <th>DESCRIPTION</th> </tr> </thead> <tbody> <tr> <td> </td> <td> </td> </tr> <tr> <td> </td> <td> </td> </tr> </tbody> </table>	REVISIONS		NO.	DESCRIPTION				
REVISIONS										
NO.	DESCRIPTION									

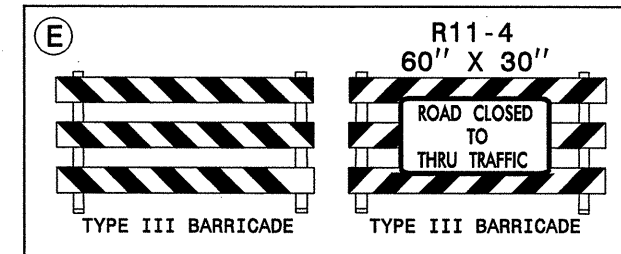
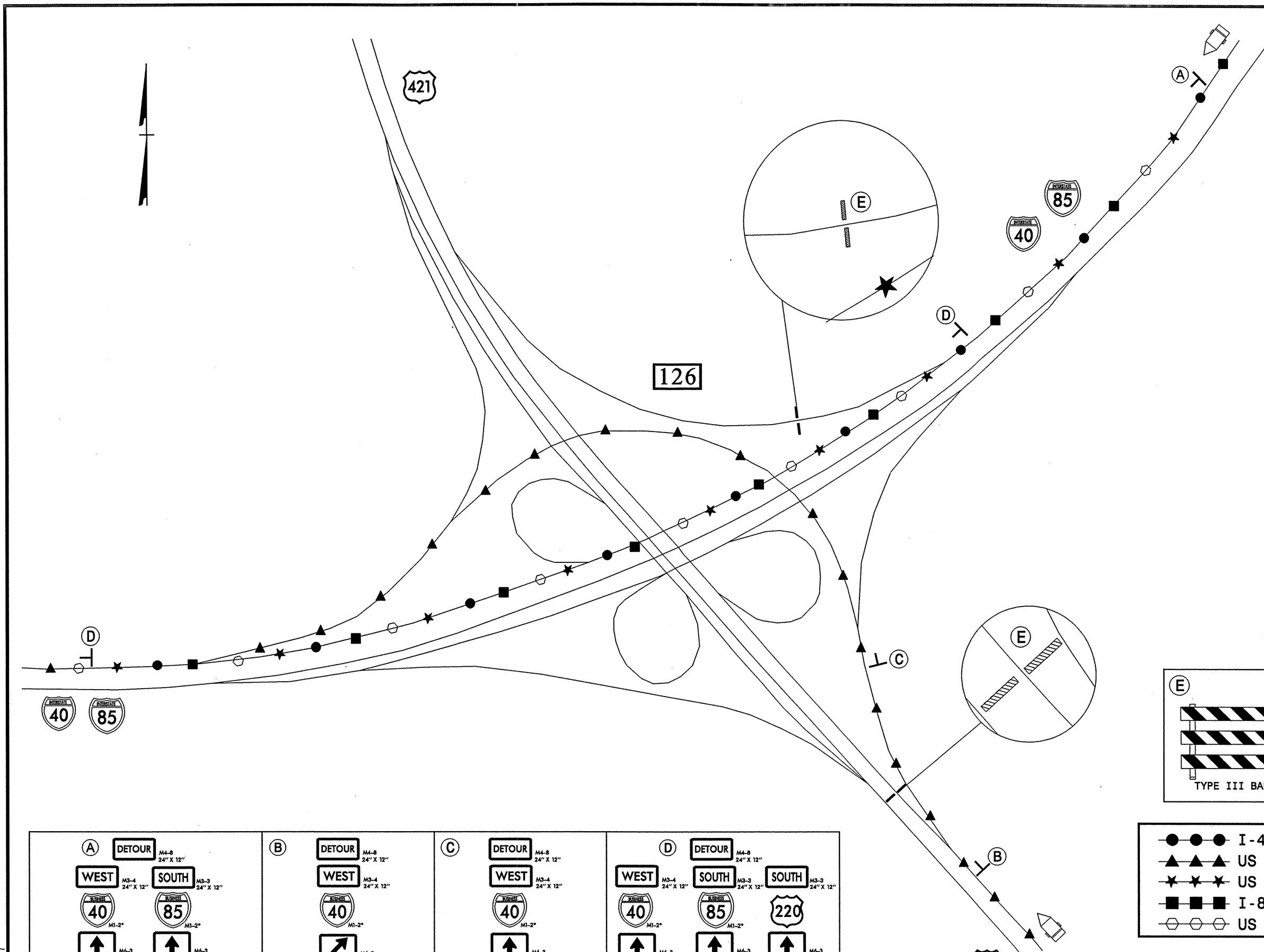
- I-40 Bus W Detour Route
- ▲▲▲ US 421 N Detour Route
- ★★★ US 29 S / US 70 W Detour Route
- ■ ■ I-85 Bus S Detour Route
- ○ ○ US 220 S Detour Route
- ⊙ ⊙ ⊙ I-40 Bus W / I-85 Bus S Detour Routes from Exits 36-38



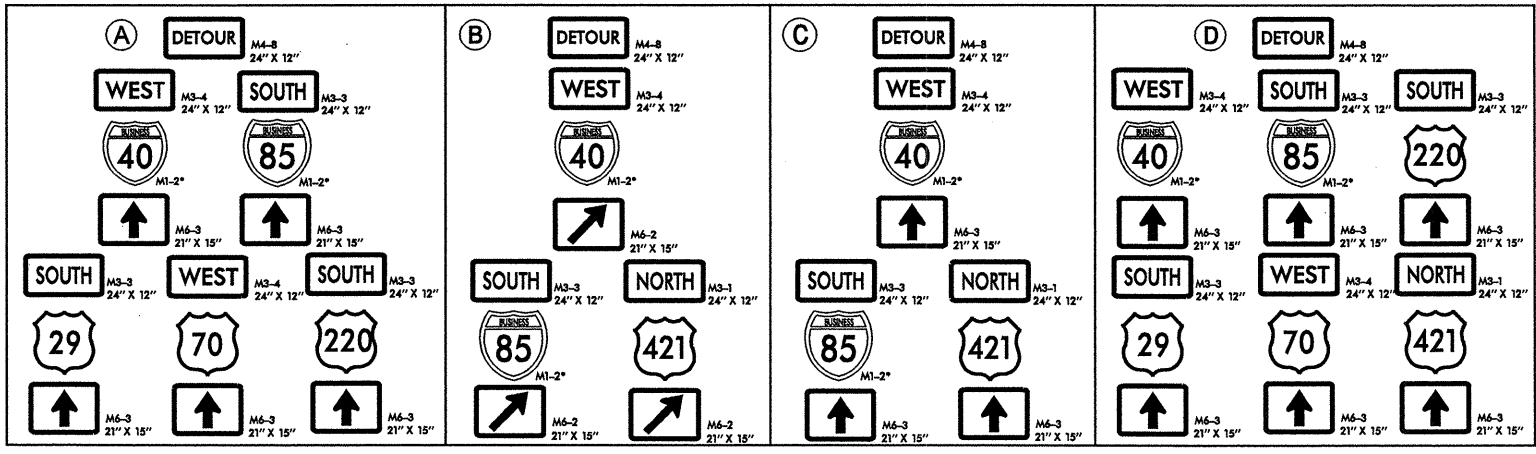
<p>(A) DETOUR SOUTH 220 ↑</p> <p>M4-8 24" X 12" M3-3 24" X 12" M6-3 21" X 15"</p>	<p>(B) DETOUR SOUTH 220 ↗</p> <p>M4-8 24" X 12" M3-3 24" X 12" M6-2 21" X 15"</p>	<p>(C) DETOUR WEST 40 ↑ NORTH 421 ↑</p> <p>M4-8 24" X 12" M3-4 24" X 12" M3-1 24" X 12" M1-2* 24" X 12" M6-3 21" X 15" M6-3 21" X 15"</p>	<p>(D) END DETOUR SOUTH 220 ↑</p> <p>M4-8 A 24" X 18" M3-3 24" X 12" M6-3 21" X 15"</p>	<p>(E) END DETOUR SOUTH 29 ↑ WEST 70 ↑</p> <p>M4-8 A 24" X 18" M3-3 24" X 12" M1-2* 24" X 12" M3-3 24" X 12" M3-4 24" X 12"</p>	<p>(F) DETOUR WEST 40 ↗ SOUTH 85 ↑</p> <p>M4-8 24" X 12" M3-4 24" X 12" M3-3 24" X 12" M1-2* 24" X 12" M6-2 21" X 15" M6-3 21" X 15"</p>
<p>(G) DETOUR WEST 40 ↑ SOUTH 29 ↑ SOUTH 70 ↑ SOUTH 421 ↑</p> <p>M4-8 24" X 12" M3-4 24" X 12" M3-3 24" X 12" M1-2* 24" X 12" M6-3 21" X 15" M6-3 21" X 15" M6-2 21" X 15" M3-3 24" X 12" M3-4 24" X 12" M3-1 24" X 12" M6-3 21" X 15" M6-3 21" X 15" M6-3 21" X 15"</p>	<p>(H) DETOUR WEST 40 ↑ SOUTH 70 ↑ NORTH 421 ↑</p> <p>M4-8 24" X 12" M3-4 24" X 12" M3-3 24" X 12" M1-2* 24" X 12" M6-3 21" X 15" M6-3 21" X 15" M3-3 24" X 12" M3-4 24" X 12" M3-1 24" X 12" M6-3 21" X 15" M6-3 21" X 15" M6-3 21" X 15"</p>	<p>(I) DETOUR WEST 40 ↗ SOUTH 70 ↑ NORTH 421 ↗</p> <p>M4-8 24" X 12" M3-4 24" X 12" M3-3 24" X 12" M1-2* 24" X 12" M6-2 21" X 15" M6-3 21" X 15" M3-3 24" X 12" M3-4 24" X 12" M3-1 24" X 12" M6-3 21" X 15" M6-3 21" X 15" M6-2 21" X 15"</p>	<p>(J) DETOUR SOUTH 29 ↑ WEST 70 ↑</p> <p>M4-8 24" X 12" M3-3 24" X 12" M1-2* 24" X 12" M6-3 21" X 15" M3-3 24" X 12" M3-4 24" X 12" M6-3 21" X 15" M6-3 21" X 15"</p>		

APPROVED: _____ DATE: _____	<p>DETAIL 'B-W': DETOUR FOR ALL ROUTES (EXITS 119, 120 & 122)</p> <p>SCALE: NONE DATE: 06/25/07 DWG. BY: PS DESIGN BY: PS REVIEWED BY: CL</p>	<p>REVISIONS</p> <table border="1"> <tr><td> </td><td> </td></tr> <tr><td> </td><td> </td></tr> <tr><td> </td><td> </td></tr> </table>						
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 psey@more AT WZTCCC05427



- I-40 Bus. W Detour Route
- ▲▲▲▲ US 421N Detour Route
- ★★★★ US 29S/US 70W Detour Route
- ■ ■ ■ I-85 Bus. S Detour Route
- ○ ○ ○ US 220S Detour Route



APPROVED: _____ DATE: _____

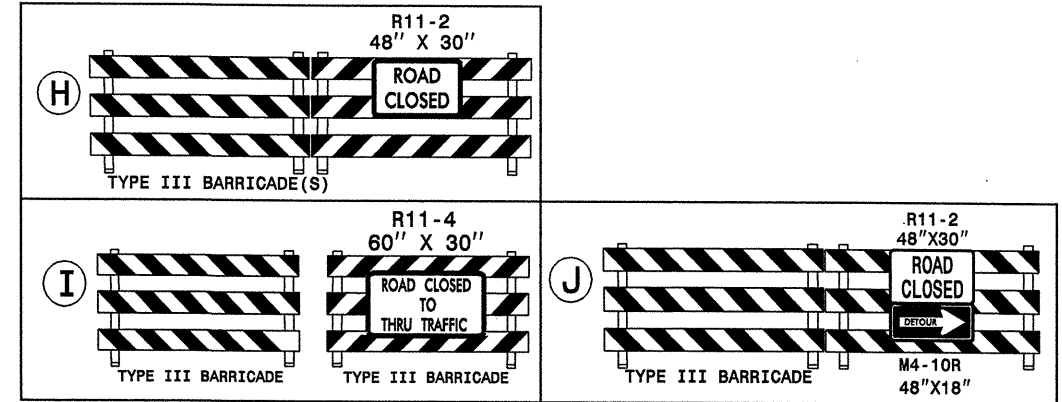
SEAL

 CHAD L. LANFORD
 8-3-07

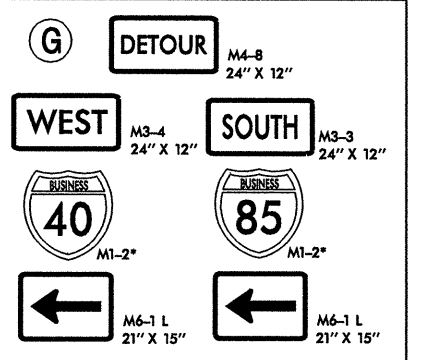
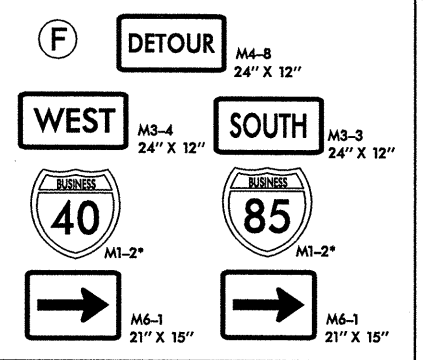
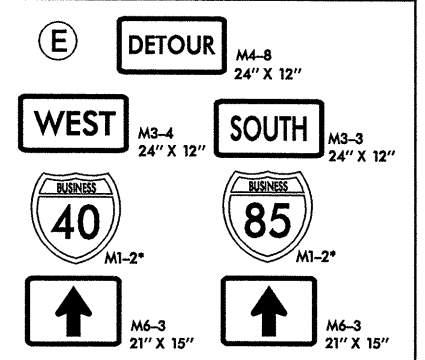
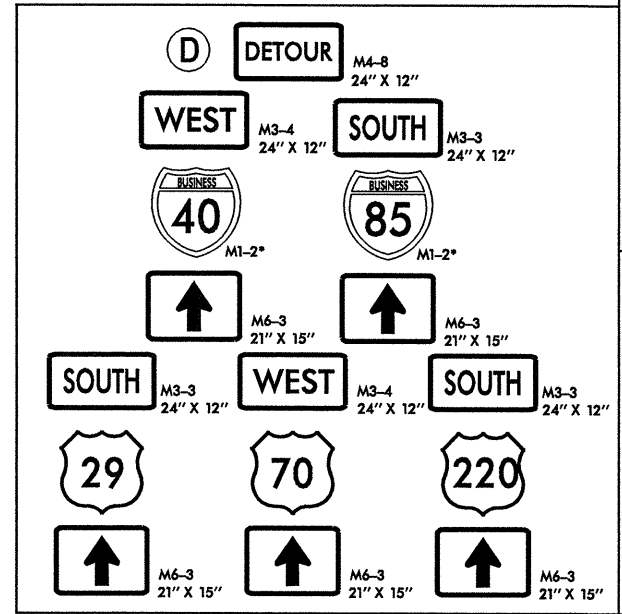
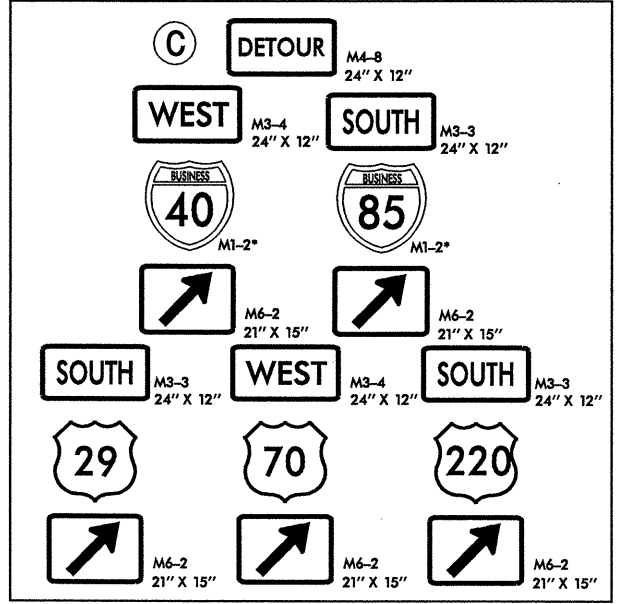
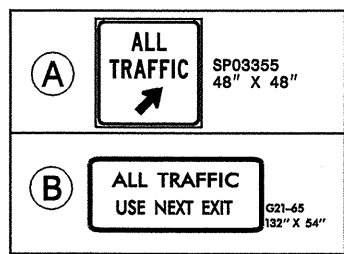
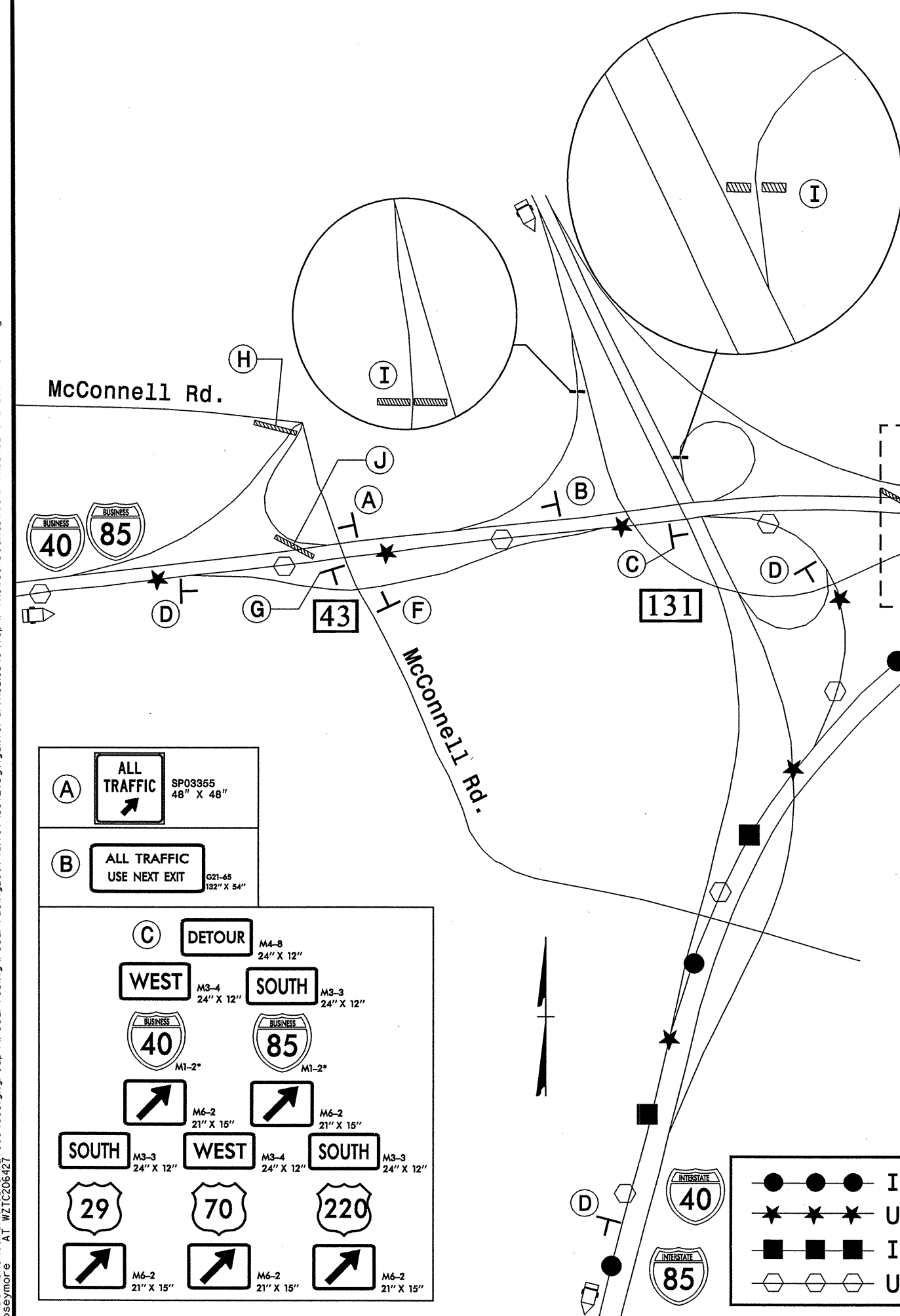
DETAIL 'C-W': DETOUR FOR
US 421 N (EXIT 126)

SCALE: NONE		REVISIONS
DATE: 06/22/07		
DWG. BY: PS		
DESIGN BY: PS		
REVIEWED BY: CL		

03-AUG-2007 14:52
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 pseymore AT WZTC206427



SEE TCP-22 AND TCP-23 FOR WESTBOUND LANE CLOSURES FOR THE I-40 BUS. W CLOSURE JUST EAST OF EXIT 131 ON I-40/I-85.



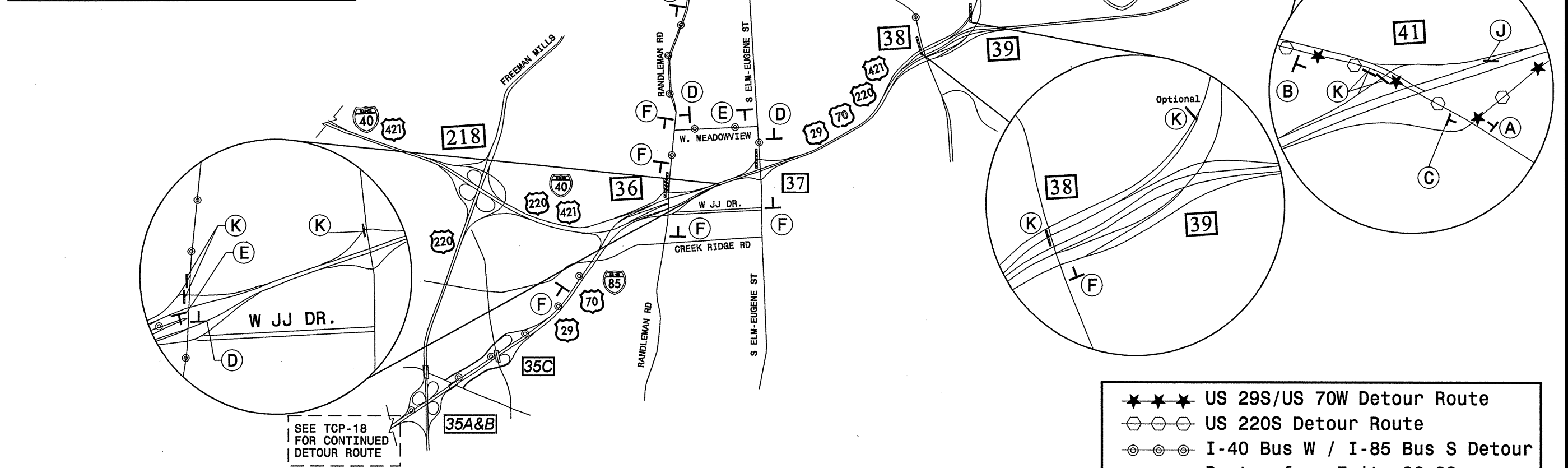
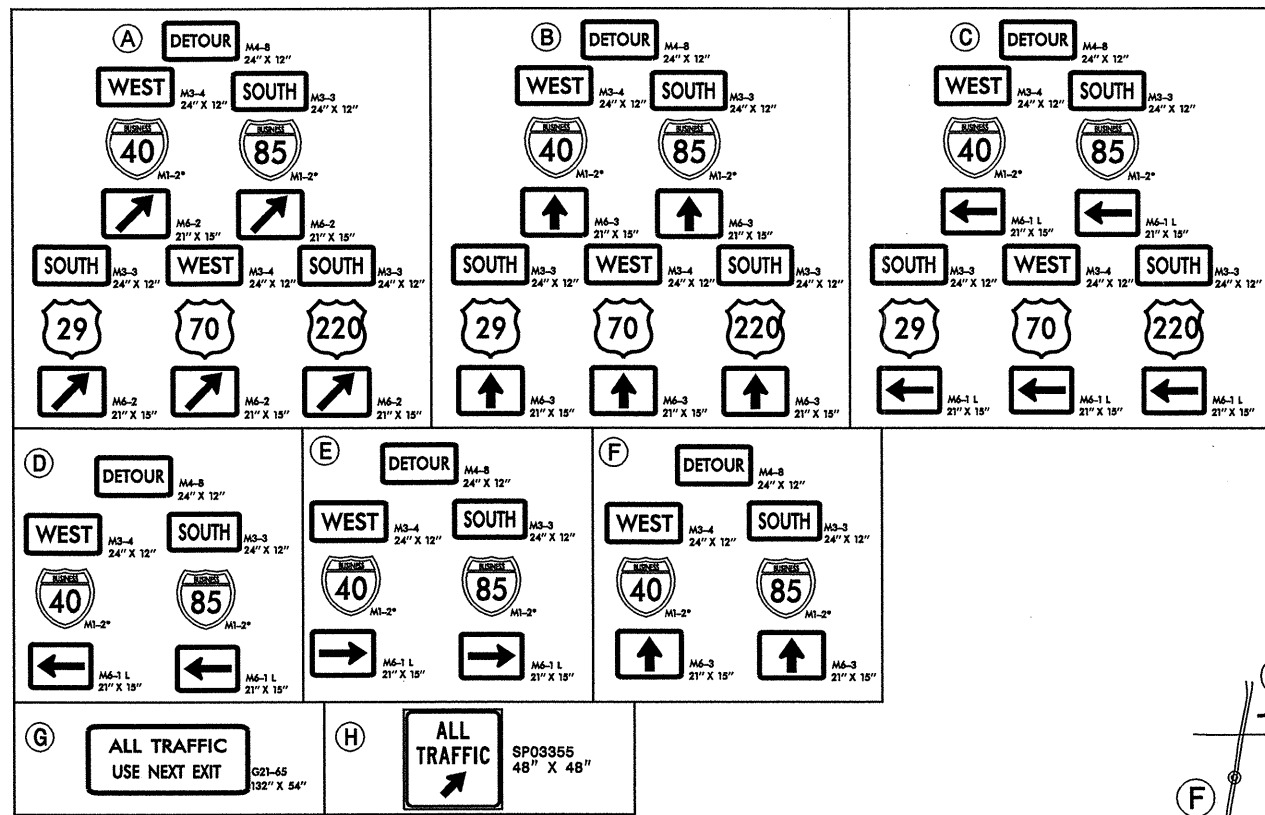
- ● ● I-40 Bus. W Detour Route
- ★ ★ ★ US 29S/US 70W Detour Route
- ■ ■ I-85 Bus. S Detour Route
- ○ ○ US 220S Detour Route

APPROVED: _____ DATE: _____
 SEAL: [Professional Engineer Seal for Charles L. Lanford, No. 039528, State of North Carolina]
 8-3-07

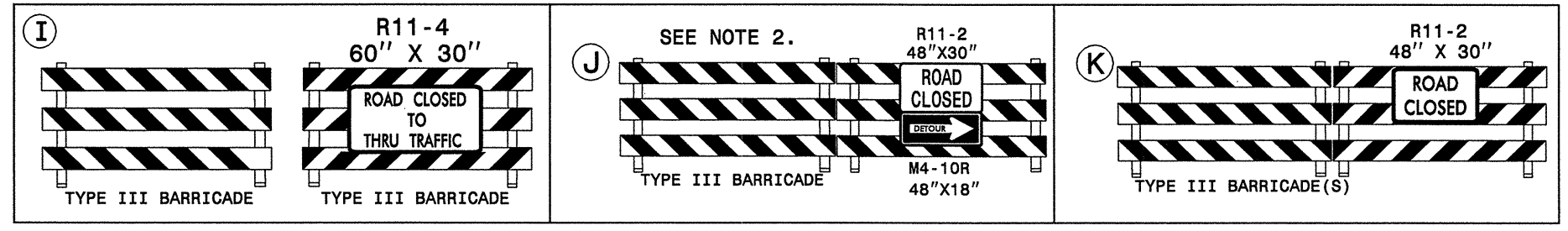
DETAIL 'D-W': DETOUR FOR US 220 S / US 29 S / US 70 W (EXIT 131)

SCALE: NONE	REVISIONS
DATE: 06/22/07	
DWG. BY: PS	
DESIGN BY: PS	
REVIEWED BY: CL	CADD FILE

03_AUG_2007 14:40 C:\WORK\GROUPS-WZ\TCCC\design\group\resurfacing\resurfacing2007\div07\387273\gvl\for\di4715_18540\tcp\i-4715westbound\DWG\I-4715_TCP20_Detail-D-W.dgn
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 D:\WORK\GROUPS-WZ\TCCC\design\group\resurfacing\resurfacing2007\div07\387273\gvl\for\di4715_18540\tcp\i-4715westbound\DWG\I-4715_TCP20_Detail-D-W.dgn



*** US 29S/US 70W Detour Route
 ○○ US 220S Detour Route
 ⊙ I-40 Bus W / I-85 Bus S Detour Routes from Exits 36-38



APPROVED: _____ DATE: _____

SEAL: CHAD L. LANFORD 030528 ENGINEER

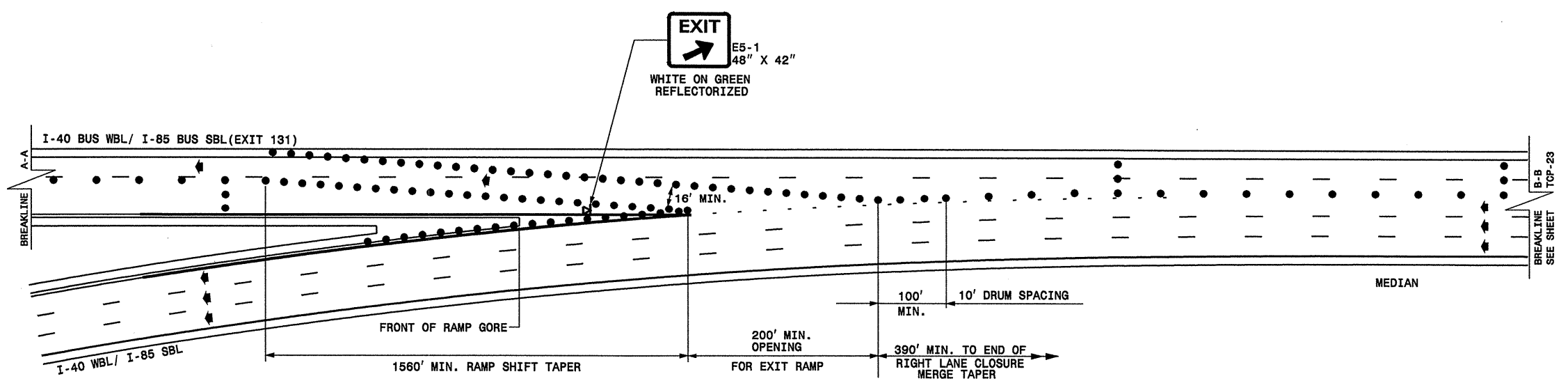
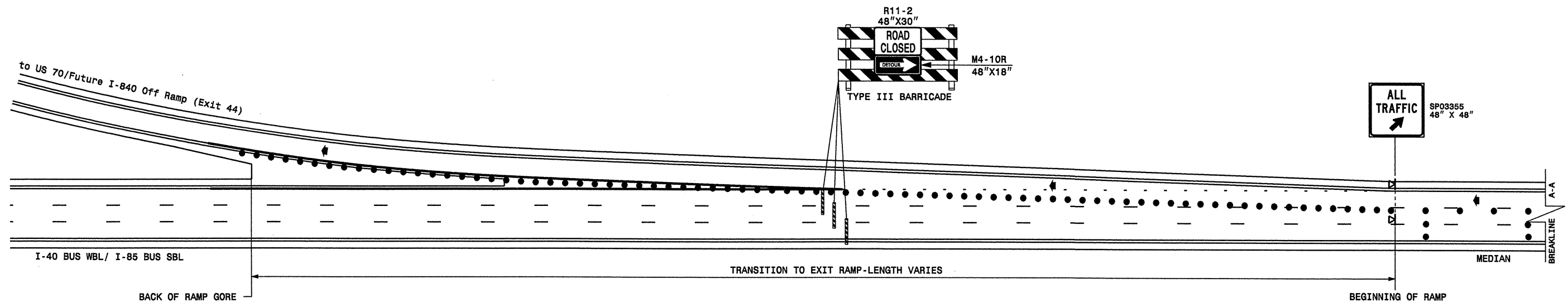
DETAIL 'F-W'/'H-W': ROAD CLOSURE OF I-40 BUS W / I-85 BUS S (EXITS 36-39)

SCALE: NONE
 DATE: 07/19/07
 DWG. BY: PS
 DESIGN BY: MM
 REVIEWED BY: CL

REVISIONS

CADD FILE

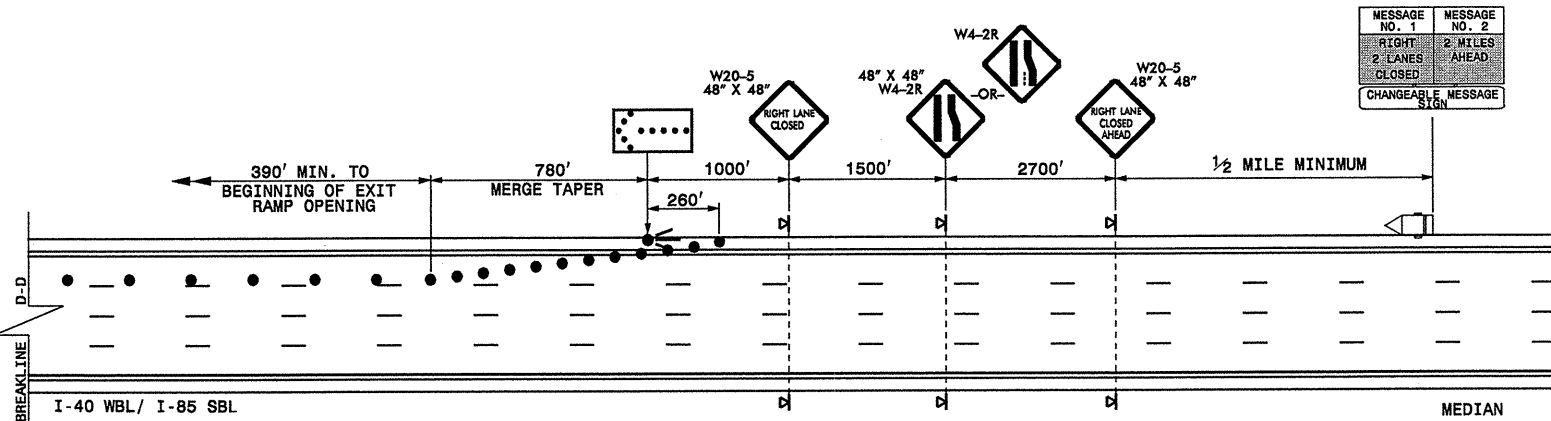
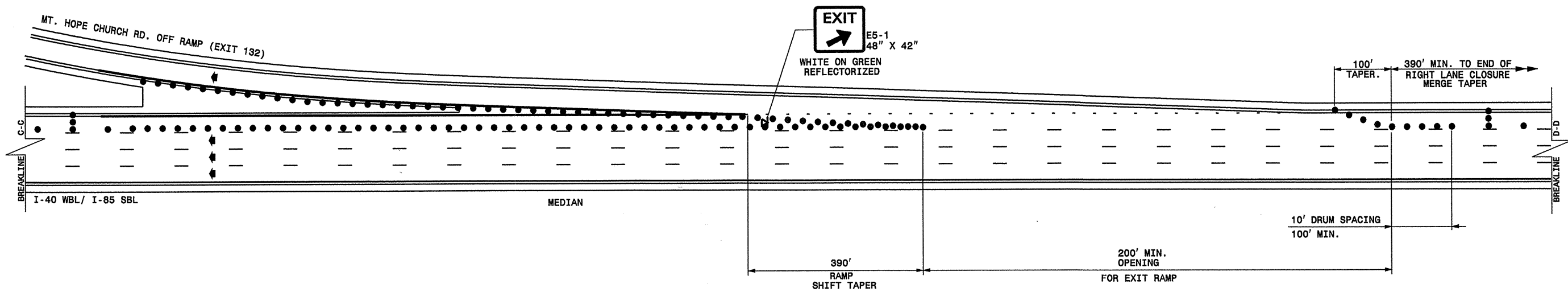
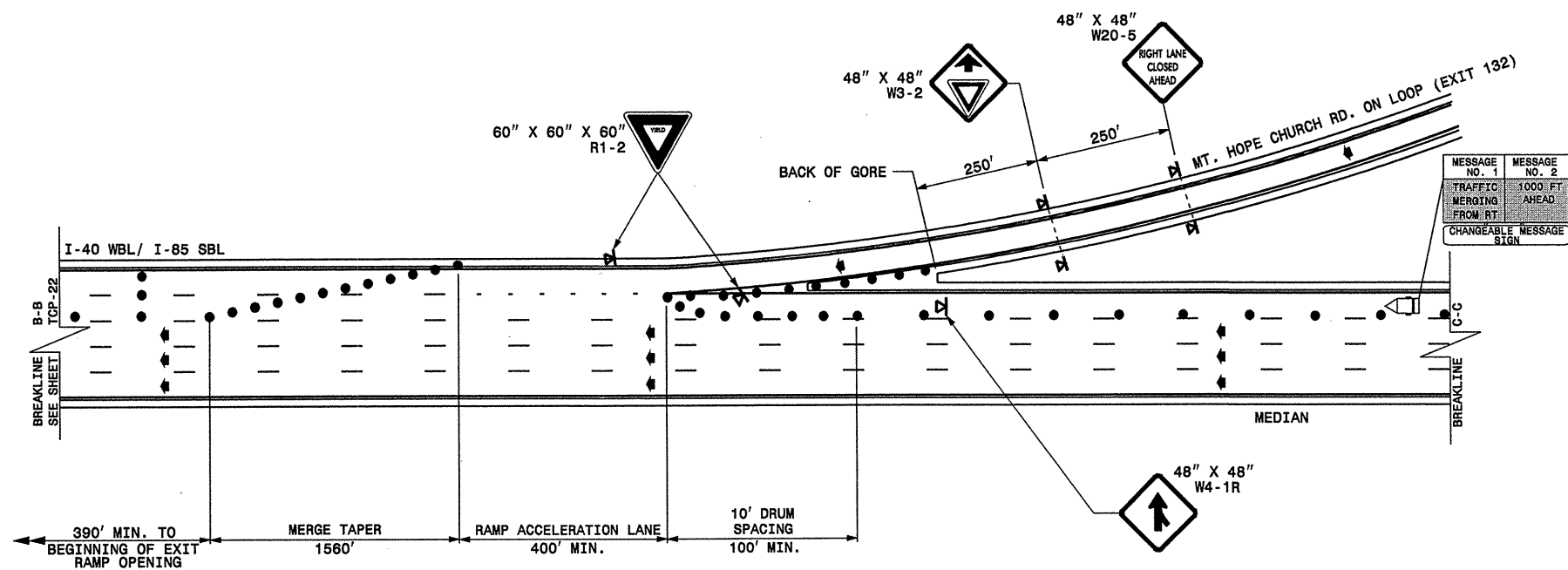
03-AUG-2007 14:43 \\dot\dfsroot\groups-wz\cc\design\group4\resur\fac\ing\1-4715.fc.tcp21.detail_fh-w_ramp_36_37_west40rev.dgn
 psymore AT WZTC206427



03-AUG-2007 15:43
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 pseyvmore AT WZT206427

APPROVED: _____	DATE: _____	LANE CLOSURES OF I-40 W / I-85 S AT I-40 BUS W (EXITS 44 & 131)	<table border="1"> <tr> <th colspan="2">REVISIONS</th> </tr> <tr> <td> </td> <td> </td> </tr> <tr> <td> </td> <td> </td> </tr> </table>	REVISIONS									
REVISIONS													
		<table border="1"> <tr> <td>SCALE:</td> <td>NONE</td> </tr> <tr> <td>DATE:</td> <td>06/07</td> </tr> <tr> <td>DWG. BY:</td> <td>CLL</td> </tr> <tr> <td>DESIGN BY:</td> <td>CLL</td> </tr> <tr> <td>REVIEWED BY:</td> <td>CLL</td> </tr> </table>	SCALE:	NONE	DATE:	06/07	DWG. BY:	CLL	DESIGN BY:	CLL	REVIEWED BY:	CLL	
SCALE:	NONE												
DATE:	06/07												
DWG. BY:	CLL												
DESIGN BY:	CLL												
REVIEWED BY:	CLL												

03-AUG-2007 15:25
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 AT WZTCCC24247



APPROVED: _____ DATE: _____	LANE CLOSURES OF I-40 W / I-85 S AT I-40 BUS W (EXITS 131 & 132)	
SCALE: NONE	DATE: 06/07	REVISIONS
DWG. BY: CLL	DESIGN BY: CLL	
REVIEWED BY: CLL	8-3-07	

SIGN NUMBER: SP-03350	BACKG COLOR: White	DESIGN BY: CL DOWNEY	CHECKED BY:	STD #: REGULATORY
TYPE: D	COPY COLOR: Black	PROJECT ID:	DIV: DIV	DATE: Aug 18,2003
QUANTITY: 1				
SIGN WIDTH: 4'-0"				
HEIGHT: 2'-6"				
TOTAL AREA: 10.0 Sq.Ft.				
BORDER TYPE: FLUSH				
RECESS: 0.4"				
WIDTH: 0.6"				
RADII: 1.5"				
NO. Z BARS: N/A	MAT'L:			
LENGTH: N/A				

BORDER
R=1.5"
TH=0.5"
IN=0.4"

SP 03350

Letter spacings are to start of next letter

Letter	S	P	E	E	D	I	N	G	Text Length	Series/Size
\$	2	5	0							C6
\$	5.3	4.6	4.2	3.5	15.4				17.6	C6
S	4.8	4.2	4	4.6	2.1	4.4	3.4	8.2		C6
S	8.1	4.8	4.2	4	4.6	2.1	4.4	3.4	8.2	31.8
P	4.6	4	4.3	4.7	3.4	3.3	3.7	8.2		C6
P	11.9	4.6	4	4.3	4.7	3.4	3.3	3.7	8.2	28

Spacing Factor is 1 unless specified otherwise

SIGN NUMBER: SP07008	BACKG COLOR: White	DESIGN BY: R. HENNEIN	CHECKED BY:	STD #: N/A
TYPE: D	COPY COLOR: Black	PROJECT ID: N/A	DIV: N/A	DATE: Jan 12,2007
QUANTITY: 1				
SIGN WIDTH: 48"				
HEIGHT: 30"				
TOTAL AREA: 10.0 Sq.Ft.				
BORDER TYPE: FLUSH				
RECESS: 0.5"				
WIDTH: 0.75"				
RADII: 1.88"				
NO. Z BARS:	MAT'L: 0.125" (3.2 mm) ALUMINUM			
LENGTH:	0.079" COMPOSITE			

BORDER
R=1.88"
TH=0.75"
IN=0.5"

SP 07008

Letter spacings are to start of next letter

Letter	E	N	D	S	2	5	0	Text Length	Series/Size	
E	4.1	4.7	3.4	8	4.3	4.1	4.3	3.5	6.9	C 2000
E	6.9	4.1	4.7	3.4	8	4.3	4.1	4.3	3.5	34.3
S	4.4	4.6	4.1	4.1	4.6	2.2	4.6	3.4	8.1	C 2000
S	8.1	4.4	4.6	4.1	4.1	4.6	2.2	4.6	3.4	31.7
P	4.6	4.1	4.2	4.7	3.4	3.4	3.8	9.9		C 2000
P	9.9	4.6	4.1	4.2	4.7	3.4	3.4	3.8	9.9	28.2

Spacing Factor is 1 unless specified otherwise

SP 03353

SIGN NUMBER: SP-03353	BACKG COLOR: Fluorescent Orange	DESIGN BY: CL DOWNEY	CHECKED BY: CHECKED	STD #: W20-1
TYPE: A	COPY COLOR: Black	PROJECT ID: ALL PROJECTS	DIV: DIV	DATE: Aug 20,2003
QUANTITY: 1				
SIGN WIDTH: 4'-0"				
HEIGHT: 4'-0"				
TOTAL AREA: 16.0 Sq.Ft.				
BORDER TYPE: FLUSH				
RECESS: 0.59"				
WIDTH: 0.75"				
RADII: 1.38"				
NO. Z BARS: N/A	MAT'L:			
LENGTH: N/A				

BORDER
R=1.38"
TH=0.75"
IN=0.59"

Letter spacings are to start of next letter

Letter	B	E	G	I	N	Text Length	Series/Size
B	5.3	4.6	5.4	2.5	3.6	22.4	C7
B	22.4	5.3	4.6	5.4	2.5	3.6	21.6
R	5.2	5.8	3.8				C7
R	23.4	5.2	5.8	3.8			19.6
W	5.4	5.6	5.2	4			C7
W	22.6	5.4	5.6	5.2	4		21.2

Spacing Factor is 1 unless specified otherwise

GENERAL NOTES FOR THE "BEGIN ROAD WORK" SIGN

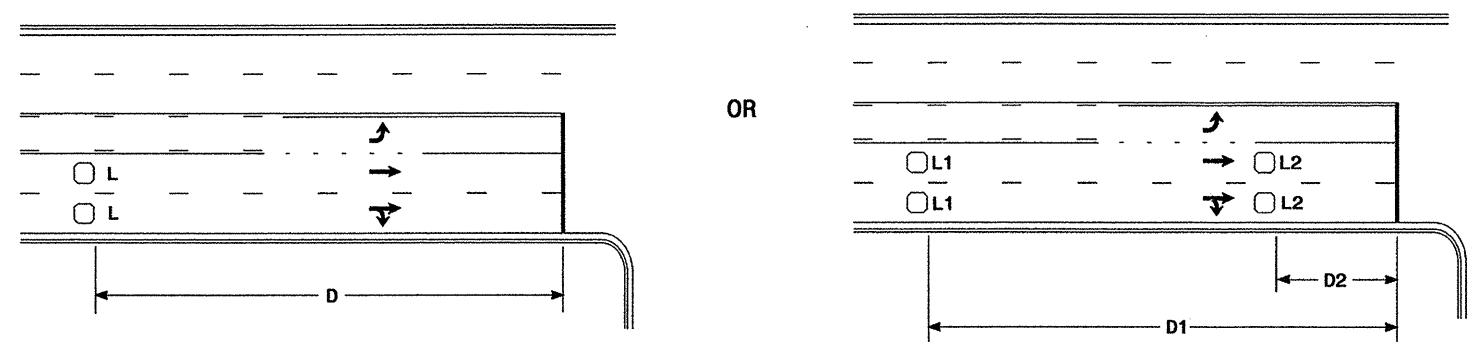
-SIGN SP-03353 "BEGIN ROAD WORK" ONLY APPLIES TO FULL CONTROL AND PARTIAL CONTROL OF ACCESS ROADWAYS

-WHEN USED, INSTALL SIGN SP-03353, "BEGIN ROAD WORK", ACCORDING TO DETAIL A ON SHEET TCP-22.

APPROVED: _____	DATE: _____	SIGN DESIGNS FOR ADVANCE WORK ZONE WARNING SIGNS	
	SCALE: NONE		REVISIONS
	DATE:		
	DESIGN BY:		
	REVIEWED BY:		

03-AUG-2007 15:03
 \\DOT\DFSR00701\GROUPS-WZTCCC\designgroup4\resur-facing\div07\387273\gvlifor-d14715_18540\tcp\1-4715_TC_TCP25_sfgn_designs.dgn
 AT WZTCC206427
 pseymore

High Speed Detection [≥40 mph (64 km/hr)]



Speed Limit mph (km/hr)	D ft (m)
40 (64)	250 (75)
45 (72)	300 (90)
50 (80)	355 (110)
55 (88)	420 (130)

L = 6ft X 6ft (1.8m X 1.8m)
Wired in series for TS1
Controllers
Wired separately for TS2,
170, and 2070L Controllers

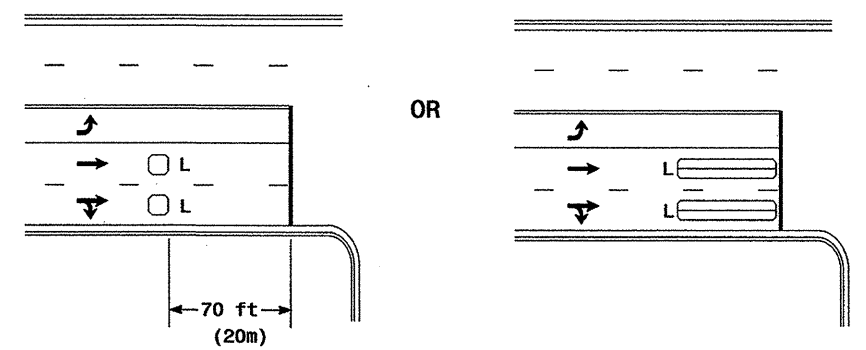
Speed Limit mph (km/hr)	D1 ft (m)	D2 ft (m)
40 (64)	250 (75)	80 (25)
45 (72)	300 (90)	90 (27)
50 (80)	355 (110)	100 (30)
55 (88)	420 (130)	110 (35)

L1 = 6ft X 6ft
(1.8m X 1.8m)
Wired in series
L2 = 6ft X 6ft
(1.8m X 1.8m)
Wired in series

Volume Density Operation

"Stretch" Operation

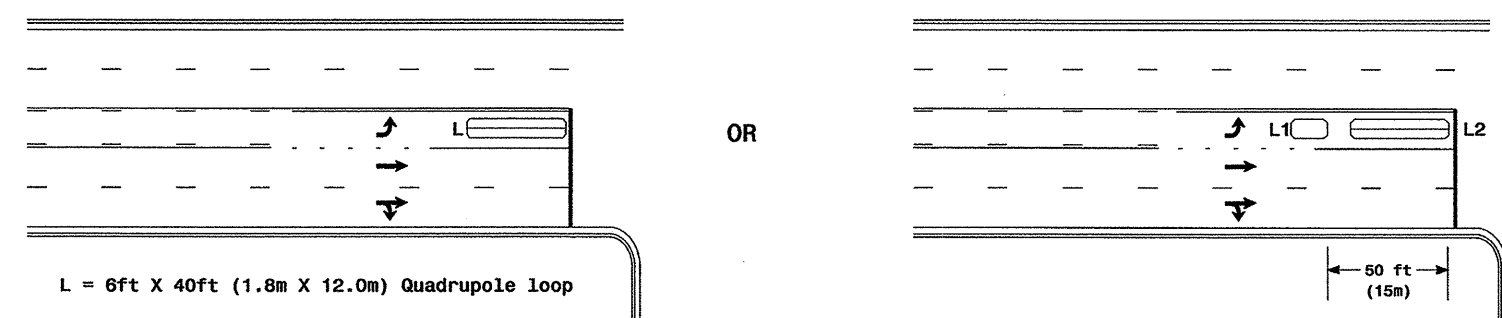
Low Speed Detection [≤35 mph (56 km/hr)]



L = 6ft X 6ft (1.8m X 1.8m)
Wired in series

L = 6ft X 40ft (1.8m X 12.0m)
Quadrupole loop, wired separately

Left Turn Lane Detection



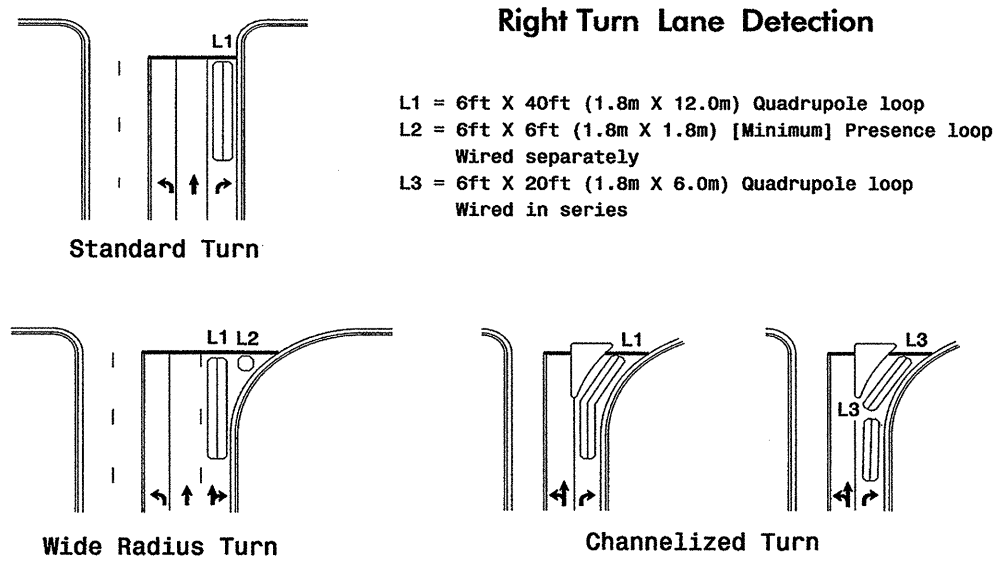
L = 6ft X 40ft (1.8m X 12.0m) Quadrupole loop

L1 = 6ft X 15ft (1.8m X 4.6m) Queue detector
L2 = 6ft X 40ft (1.8m X 12.0m) Quadrupole loop

Presence Loop Detection

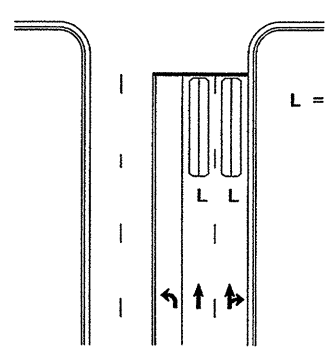
Queue Loop Detection

Right Turn Lane Detection



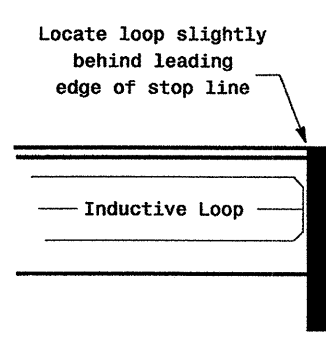
L1 = 6ft X 40ft (1.8m X 12.0m) Quadrupole loop
L2 = 6ft X 6ft (1.8m X 1.8m) [Minimum] Presence loop
Wired separately
L3 = 6ft X 20ft (1.8m X 6.0m) Quadrupole loop
Wired in series

Side Street Detection



L = 6ft X 40ft (1.8m X 12.0m)
Quadrupole loop
Wired to separate
detectors/channels

Presence Loop Placement at Stop Lines



Locate loop slightly
behind leading
edge of stop line

Note:
Loop may be located in advance
of stop line when stop line is
greater than 15' (4.5m) from edge
of intersecting roadway; or, when
loop detects a permissive or
protected/permissive left turn.

Recommended Number of Turns

Single 6' X 6' (1.8m X 1.8m)
loop (wired separately):

Length of Lead-in ft (m)	Number of Turns
< 250 (75)	3
250-375 (75-115)	4
375-525 (115-160)	5
> 525 (160)	6

Quadrupole loops: Use 2-4-2 turns
6' X 15' (1.8m X 4.6m) Loops:
Lead-in < 150' (45 m), use 2 turns
Lead-in > 150' (45 m), use 3 turns

Typical Loop Locations

PLAN DATE: June 2006	REVIEWED BY:
PREPARED BY: P. L. Alexander	REVIEWED BY:
SCALE: N/A	REVISIONS:
	INIT. DATE
	12/1/06
	6/6/06

SIG. INVENTORY NO.

STATE OF NORTH CAROLINA
 DEPT. OF TRANSPORTATION
 DIVISION OF HIGHWAYS
 RALEIGH, N.C.

ENGLISH STANDARD DRAWING FOR
DEEP-CUT INDUCTIVE DETECTION LOOPS
 (FOR INSTALLATION PRIOR TO MILLING)

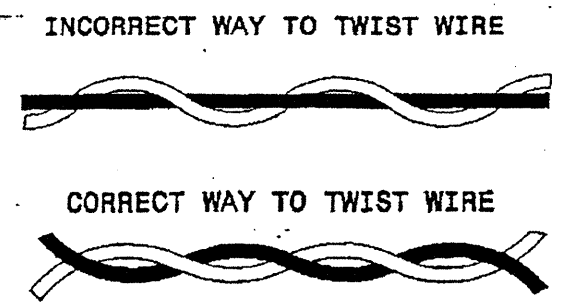
NOTES

- OVERLAP SAW CUTS AT CORNERS AND INTERSECTION POINTS TO ENSURE UNIFORM SAW SLOT DEPTH.
- PROVIDE 5/8" MINIMUM SLOT FROM EDGE OF LOOP TO EDGE OF PAVEMENT FOR TWISTED LOOP WIRE TAIL SECTIONS.
- MAINTAIN 12" SPACING BETWEEN TWISTED LOOP WIRE TAIL SECTIONS.
- WIRE LOOPS CONNECTED TO THE SAME DETECTOR IN SERIES.
- LOCATE LOOPS IN CENTER OF LANES UNLESS OTHERWISE SHOWN ON PLANS.
- USE A SERIES OF ONE INCH PIECES OF BACKER ROD SPACED ONE FOOT APART ALONG THE ENTIRE LENGTH OF THE FEEDER SLOT AND LOOP SAW SLOT.
- CONSULT LOOP SEALANT MANUFACTURER TO DETERMINE CURING TIME REQUIRED PRIOR TO MILLING.

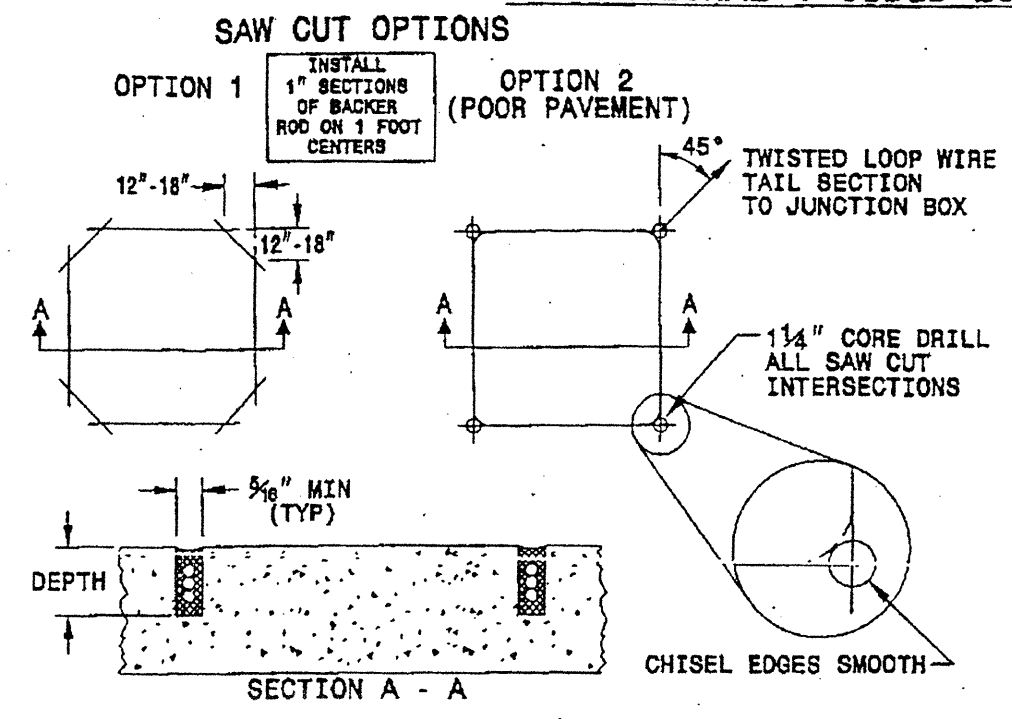
SAW SLOT DEPTH CHART
 ASSUMING 2" MILLING DEPTH

DEPTH (IN)	NO. OF WIRE LAYERS				
	2	3	4	5	6
SAW SLOT DEPTH	4.0	4.5	5.0	5.0	5.0
MINIMUM TOTAL ASPHALT DEPTH REQUIRED	5.0	5.5	6.0	6.0	6.0

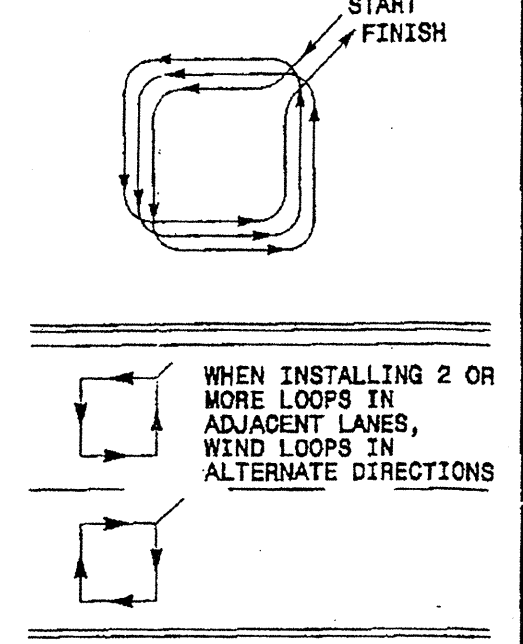
LOOP WIRE TWISTING METHOD



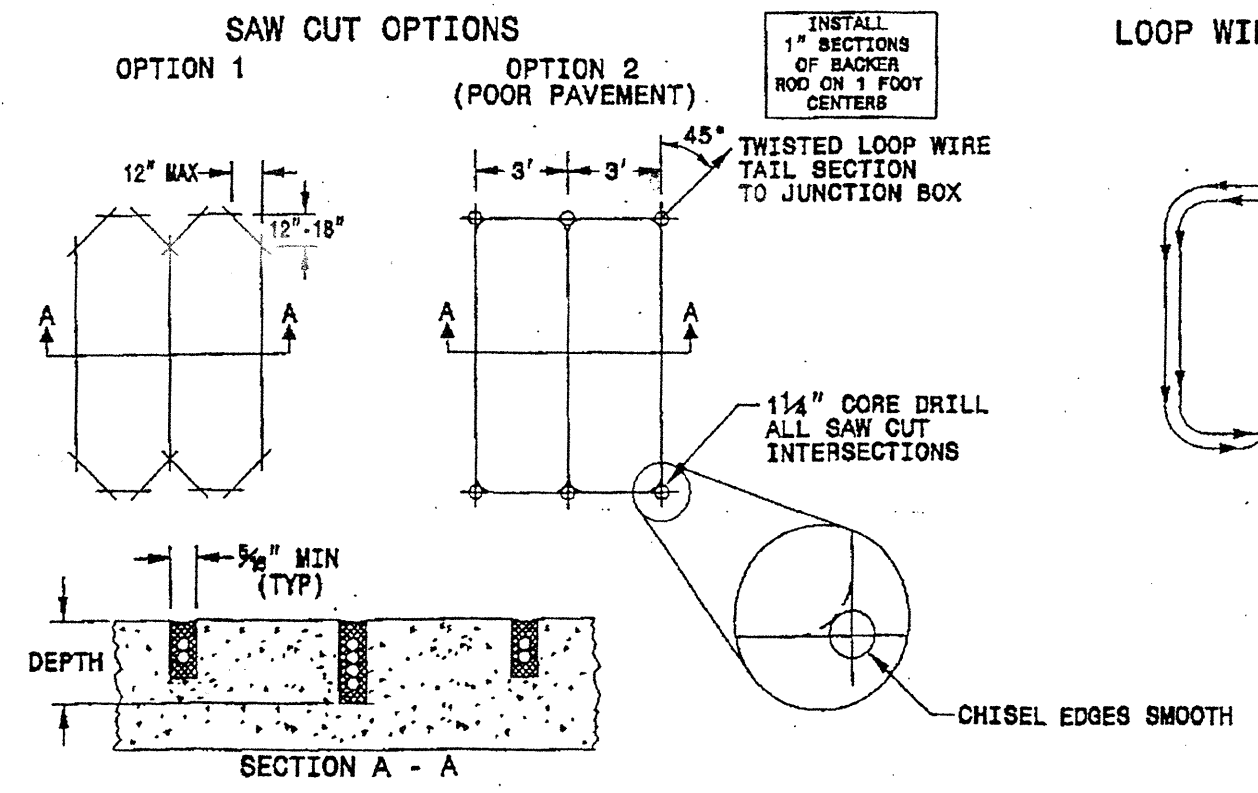
CONVENTIONAL 4-SIDED LOOP



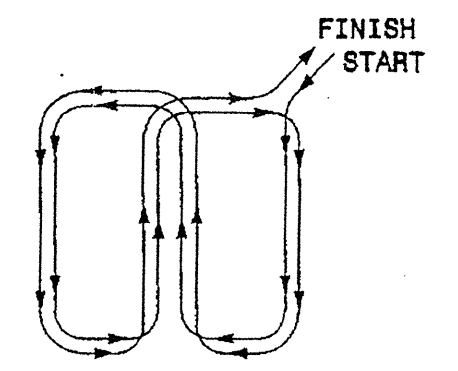
LOOP WINDING METHOD



QUADRUPOLE LOOP



LOOP WINDING METHOD



STATE OF NORTH CAROLINA
 DEPT. OF TRANSPORTATION
 DIVISION OF HIGHWAYS
 RALEIGH, N.C.

ENGLISH STANDARD DRAWING FOR
DEEP-CUT INDUCTIVE DETECTION LOOPS
 (FOR INSTALLATION PRIOR TO MILLING)