

STATE OF NORTH CAROLINA
DIVISION OF HIGHWAYS

STATE PROJECT REFERENCE NO.	SHEET NO.
U-5009	TCP-1

**PLAN FOR PROPOSED
TRAFFIC CONTROL, MARKING & DELINEATION**

ANSON

U-5009

ROADWAY STANDARD DRAWINGS

THE FOLLOWING ROADWAY STANDARDS AS APPEAR IN "ROADWAY STANDARD DRAWINGS" - PROJECT SERVICES UNIT-N.C. DEPARTMENT OF TRANSPORTATION-RALEIGH, N.C., DATED JULY 2006 ARE APPLICABLE TO THIS PROJECT AND BY REFERENCE HEREBY ARE CONSIDERED A PART OF THESE PLANS:

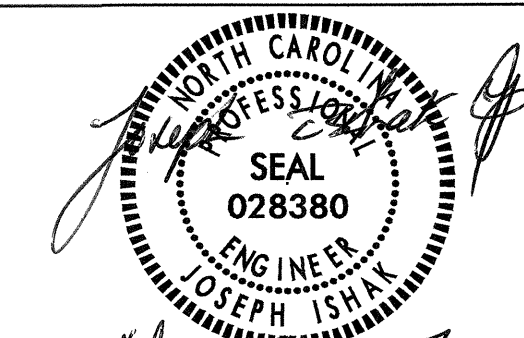
STD. NO.	TITLE
1101.02	TEMPORARY LANE CLOSURES
1101.03	TEMPORARY ROAD CLOSURES
1101.04	TEMPORARY SHOULDER CLOSURES
1101.05	WORK ZONE VEHICLE ACCESSES
1101.11	TRAFFIC CONTROL DESIGN TABLES
1110.01	STATIONARY WORK ZONE SIGNS
1110.02	PORTABLE WORK ZONE SIGNS
1115.01	FLASHING ARROW PANELS
1130.01	DRUM
1135.01	CONES
1145.01	BARRICADES
1150.01	FLAGGING DEVICES
1165.01	TRUCK MOUNTED IMPACT ATTENUATOR
1180.01	SKINNY-DRUM
1205.01	PAVEMENT MARKINGS - LINE TYPES & OFFSETS
1205.02	PAVEMENT MARKINGS - 2 LANE & MULTILANE ROADWAYS
1205.04	PAVEMENT MARKINGS - INTERSECTIONS
1205.05	PAVEMENT MARKINGS - TURN LANES
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1205.07	PAVEMENT MARKINGS - PEDESTRIAN CROSSWALKS
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1251.01	RAISED PAVEMENT MARKERS (TEMPORARY & PERMANENT)

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PM-1	FINAL PAVEMENT MARKING PLANS

LEGEND

- GENERAL**
- DIRECTION OF TRAFFIC FLOW
 - NORTH ARROW
 - PROPOSED PVMT. EXIST. PVMT.
 - WORK AREA
 - REMOVAL OF EXISTING PAVEMENT
- TRAFFIC CONTROL DEVICES**
- TYPE I BARRICADE
 - TYPE II BARRICADE
 - TYPE III BARRICADE
 - CONE
 - DRUM SKINNY DRUM
 - FLASHING ARROW PANEL (TYPE C)
 - STATIONARY SIGN
 - PORTABLE SIGN
 - STATIONARY OR PORTABLE SIGN
 - WARNING FLAGS
 - CRASH CUSHION
 - CHANGEABLE MESSAGE SIGN
 - TRUCK MOUNTED IMPACT ATTENUATOR (TMIA)
 - POLICE
 - FLAGGER
- PAVEMENT MARKINGS**
- CRYSTAL/CRYSTAL PAVEMENT MARKER
 - YELLOW/YELLOW PAVEMENT MARKER
 - CRYSTAL/RED PAVEMENT MARKER
 - PAVEMENT MARKING SYMBOLS

APPROVED: _____ DATE: _____	PLAN PREPARED BY: N.C.D.O.T. WORK ZONE TRAFFIC CONTROL UNIT
 May 22, 2007	STUART BOURNE, P.E. TRAFFIC CONTROL ENGINEER
	JOSEPH ISHAK, P.E. TRAFFIC CONTROL PROJECT ENGINEER
	HABIB LAWANDOS TRAFFIC CONTROL PROJECT DESIGN ENGINEER _____ TRAFFIC CONTROL DESIGN ENGINEER / TECHNICIAN

GENERAL NOTES

PROJECT NOTES

PROJ. REFERENCE NO.	SHEET NO.
U-5009	TCP-2

ADAPT THE TRAFFIC CONTROL PLANS, WHEN DIRECTED BY THE ENGINEER, TO MEET FIELD CONDITIONS TO PROVIDE SAFE AND EFFICIENT TRAFFIC MOVEMENT. CHANGES MAY BE REQUIRED WHEN PHYSICAL DIMENSIONS IN THE DETAIL DRAWINGS, STANDARD DETAILS AND ROADWAY DETAILS ARE NOT ATTAINABLE, OR RESULT IN DUPLICATE, OR UNDESIRED OVERLAPPING OF DEVICES. MODIFICATION MAY INCLUDE: MOVING, SUPPLEMENTING, COVERING OR REMOVAL OF DEVICES.

THE FOLLOWING GENERAL NOTES APPLY AT ALL TIMES FOR THE DURATION OF THE CONSTRUCTION PROJECT, EXCEPT WHEN OTHERWISE NOTED IN THE PLAN, OR DIRECTED BY THE ENGINEER.

TIME RESTRICTIONS

A) DO NOT CLOSE OR NARROW TRAVEL LANES DURING HOLIDAYS AND SPECIAL EVENTS AS FOLLOWS:

ROAD NAME	DAY AND TIME RESTRICTIONS
US 74	MONDAY THRU SUNDAY FROM 6:00 AM TO 10:00 PM

HOLIDAY

- FOR ANY EVENT THAT CREATES UNUSUALLY HIGH TRAFFIC VOLUMES, AS DIRECTED BY THE ENGINEER.
- FOR NEW YEAR'S, BETWEEN THE HOURS OF 6:00 A.M. DECEMBER 31st TO 10:00 P.M. JANUARY 2ND. IF NEW YEAR'S DAY IS ON A SATURDAY OR A SUNDAY, THEN UNTIL THE FOLLOWING TUESDAY.
- FOR EASTER, BETWEEN THE HOURS OF 6:00 A.M. THURSDAY AND 10:00 P.M. THE FOLLOWING MONDAY.
- FOR MEMORIAL DAY, BETWEEN THE HOURS OF 6:00 A.M. FRIDAY TO 10:00 P.M. THE FOLLOWING TUESDAY.
- FOR INDEPENDENCE DAY, BETWEEN THE HOURS OF 6:00 A.M. THE DAY BEFORE INDEPENDENCE DAY AND 10:00 P.M. THE DAY AFTER INDEPENDENCE DAY.

IF INDEPENDENCE DAY IS ON A FRIDAY, SATURDAY, SUNDAY OR MONDAY THEN BETWEEN THE HOURS OF 6:00 A.M. THE THURSDAY BEFORE INDEPENDENCE DAY AND 10:00 P.M. THE TUESDAY AFTER INDEPENDENCE DAY.

- FOR LABOR DAY, BETWEEN THE HOURS OF 6:00 A.M. FRIDAY AND 10:00 P.M. THE FOLLOWING TUESDAY.
- FOR THANKSGIVING DAY, BETWEEN THE HOURS OF 6:00 A.M. TUESDAY TO 10:00 P.M. THE FOLLOWING MONDAY.
- FOR CHRISTMAS, BETWEEN THE HOURS OF 6:00 A.M. THE FRIDAY BEFORE THE WEEK OF CHRISTMAS DAY AND 10:00 P.M. THE FOLLOWING MONDAY AFTER THE WEEK OF CHRISTMAS.

B) DO NOT STOP TRAFFIC OR CLOSE ROADS AS FOLLOWS:

ROAD NAME	DAY AND TIME RESTRICTIONS
US 74	MONDAY THRU SUNDAY FROM 6:00 AM TO 10:00 PM

C) DO NOT STOP TRAFFIC FOR MORE THAN 15 MINUTES AS FOLLOWS:

ROAD NAME	OPERATIONS
US 74	TRAFFIC SHIFT

D) DO NOT CONDUCT ANY HAULING OPERATIONS AGAINST THE FLOW OF TRAFFIC OF AN OPEN TRAVELWAY UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL OR OTHERWISE DIRECTED BY THE ENGINEER.

LANE AND SHOULDER CLOSURE REQUIREMENTS

- E) REMOVE LANE CLOSURE DEVICES FROM THE LANE WHEN WORK IS NOT BEING PERFORMED BEHIND THE LANE CLOSURE OR WHEN A LANE CLOSURE IS NO LONGER NEEDED, OR AS DIRECTED BY THE ENGINEER.
- F) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN 40 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN SHOULDER USING ROADWAY STANDARD DRAWING NO. 1101.04 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.
- G) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING ON THE SHOULDER ADJACENT TO AN UNDIVIDED FACILITY AND WITHIN 5 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN TRAVEL LANE USING ROADWAY STANDARD DRAWING NO. 1101.02 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.

WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING ON THE SHOULDER ADJACENT TO A DIVIDED FACILITY AND WITHIN 10 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN TRAVEL LANE USING ROADWAY STANDARD DRAWING NO. 1101.02 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.

- H) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN A LANE OF TRAVEL OF AN UNDIVIDED OR DIVIDED FACILITY, CLOSE THE LANE ACCORDING TO THE TRAFFIC CONTROL PLANS, ROADWAY STANDARD DRAWINGS OR AS DIRECTED BY THE ENGINEER. CONDUCT THE WORK SO THAT ALL PERSONNEL AND/OR EQUIPMENT REMAIN WITHIN THE CLOSED TRAVEL LANE.
- I) DO NOT WORK SIMULTANEOUSLY, ON BOTH SIDES OF AN OPEN TRAVELWAY, WITHIN THE SAME LOCATION, ON A TWO-LANE, TWO-WAY ROAD.
- J) DO NOT PERFORM WORK INVOLVING HEAVY EQUIPMENT WITHIN 15 FT OF THE EDGE OF TRAVELWAY WHEN WORK IS BEING PERFORMED BEHIND A LANE CLOSURE ON THE OPPOSITE SIDE OF THE TRAVELWAY.
- K) DO NOT INSTALL MORE THAN ONE LANE CLOSURE, IN ANY ONE DIRECTION, ON US 74.
- L) PROVIDE TRAFFIC CONTROL FOR APPROPRIATE LANE CLOSURES FOR SURVEYING DONE BY THE DEPARTMENT.

PAVEMENT EDGE DROP OFF REQUIREMENTS

- M) BACKFILL AT A 6:1 SLOPE UP TO THE EDGE AND ELEVATION OF EXISTING PAVEMENT IN AREAS ADJACENT TO AN OPENED TRAVEL LANE THAT HAS A DROP-OFF AS FOLLOWS:
BACKFILL DROP-OFFS THAT EXCEED 2 INCHES ON ROADWAYS WITH POSTED SPEED LIMITS OF 45 MPH OR GREATER.
BACKFILL DROP-OFFS THAT EXCEED 3 INCHES ON ROADWAYS WITH POSTED SPEED LIMITS LESS THAN 45 MPH.
BACKFILL WITH SUITABLE COMPACTED MATERIAL, AS APPROVED BY THE ENGINEER, AT NO EXPENSE TO THE DEPARTMENT.
- N) DO NOT EXCEED A DIFFERENCE OF 2 INCHES IN ELEVATION BETWEEN OPEN LANES OF TRAFFIC FOR NOMINAL LIFTS OF 1.5 INCHES. INSTALL ADVANCE WARNING *UNEVEN LANES* SIGNS (W8-11) 500 FT IN ADVANCE AND A MINIMUM OF ONCE EVERY MILE THROUGHOUT THE UNEVEN AREA.

TRAFFIC PATTERN ALTERATIONS

- O) NOTIFY THE ENGINEER TWENTY ONE (21) CALENDAR DAYS PRIOR TO ANY TRAFFIC PATTERN ALTERATION.

SIGNING

- P) INSTALL ADVANCE WORK ZONE WARNING SIGNS WHEN WORK IS WITHIN 100 FT FROM THE EDGE OF TRAVEL LANE AND NO MORE THAN THREE (3) DAYS PRIOR TO THE BEGINNING OF CONSTRUCTION.
WHEN NO WORK IS BEING CONDUCTED FOR A PERIOD LONGER THAN ONE WEEK, REMOVE OR COVER ALL ADVANCE WORK ZONE WARNING SIGNS, AS DIRECTED BY THE ENGINEER, AT NO COST TO THE DEPARTMENT.
- Q) STATE FORCES WILL BE RESPONSIBLE FOR DETOUR SIGNING WITHIN AND OFF THE PROJECT LIMITS.
- R) STATE FORCES WILL COVER OR REMOVE ALL DETOUR SIGNS WITHIN AND OFF THE PROJECT LIMITS WHEN A DETOUR IS NOT IN OPERATION.
- S) ENSURE ALL NECESSARY SIGNING IS IN PLACE PRIOR TO ALTERING ANY TRAFFIC PATTERN.
- T) INSTALL BLACK ON ORANGE "DIP" SIGNS (W8-2) 500 FT IN ADVANCE OF THE UNEVEN AREA.
- U) INSTALL BLACK ON ORANGE "BUMP" SIGNS (W8-1) 500 FT IN ADVANCE OF THE UNEVEN AREA.

TRAFFIC BARRIER

- V) INSTALL TEMPORARY BARRIER ACCORDING TO THE TRAFFIC CONTROL PLANS A MAXIMUM OF TWO (2) WEEKS PRIOR TO BEGINNING WORK IN ANY LOCATION. ONCE TEMPORARY BARRIER IS INSTALLED AT ANY LOCATION, PROCEED IN A CONTINUOUS MANNER TO COMPLETE THE PROPOSED WORK IN THAT LOCATION UNLESS OTHERWISE STATED IN THE TRAFFIC CONTROL PLANS OR AS DIRECTED BY THE ENGINEER.
ONCE TEMPORARY BARRIER IS INSTALLED AT ANY LOCATION AND NO WORK IS PERFORMED BEHIND THE TEMPORARY BARRIER FOR A PERIOD LONGER THAN TWO (2) MONTHS, REMOVE/RESET TEMPORARY BARRIER AT NO COST TO THE DEPARTMENT UNLESS OTHERWISE STATED IN THE TRAFFIC CONTROL PLANS, BARRIER IS PROTECTING A HAZARD, OR AS DIRECTED BY THE ENGINEER.
INSTALL TEMPORARY BARRIER WITH THE TRAFFIC FLOW, BEGINNING WITH THE UPSTREAM SIDE OF TRAFFIC. REMOVE TEMPORARY BARRIER AGAINST THE TRAFFIC FLOW, BEGINNING WITH THE DOWNSTREAM SIDE OF TRAFFIC.

INSTALL AND SPACE DRUMS NO GREATER THAN TWICE THE POSTED SPEED LIMIT (MPH) TO CLOSE OR KEEP CLOSED THE SECTION OF THE ROADWAY UNTIL THE BARRIER CAN BE PLACED OR AFTER BARRIER IS REMOVED

TRAFFIC CONTROL DEVICES

- U) WHEN USING ROADWAY STANDARD NO. 1101.02, DRUMS MAY BE USED IN LIEU OF CONES ON US 70.
- V) SPACE CHANNELIZING DEVICES IN WORK AREAS NO GREATER THAN TWICE THE POSTED SPEED LIMIT (MPH), EXCEPT 10 FT ON-CENTER IN RADII, AND 3 FT OFF THE EDGE OF AN OPEN TRAVELWAY, WHEN LANE CLOSURES ARE NOT IN EFFECT.
- W) PLACE TYPE III BARRICADES, WITH "ROAD CLOSED" SIGN R11-2 ATTACHED, OF SUFFICIENT LENGTH TO CLOSE ENTIRE ROADWAY. STAGGER OR OVERLAP BARRICADES TO ALLOW FOR INGRESS OR EGRESS.
- X) PLACE SETS OF THREE DRUMS PERPENDICULAR TO THE EDGE OF THE TRAVELWAY ON 500 FT CENTERS WHEN UNOPENED LANES ARE CLOSED TO TRAFFIC. THESE DRUMS SHALL BE IN ADDITION TO CHANNELIZING DEVICES.

PAVEMENT MARKINGS AND MARKERS

- Y) INSTALL PAVEMENT MARKINGS AND PAVEMENT MARKERS ON THE FINAL SURFACE AS FOLLOWS:

ROAD NAME	MARKING	MARKER
ALL ROADS	THERMOPLASTIC	PERMANENT RAISED

- Z) INSTALL TEMPORARY PAVEMENT MARKINGS AND TEMPORARY PAVEMENT MARKERS ON INTERIM LAYERS OF PAVEMENT AS FOLLOWS:

ROAD NAME	MARKING	MARKER
ALL ROADS	PAINT	TEMPORARY RAISED

- AA) PLACE AT LEAST TWO APPLICATIONS OF PAINT PAVEMENT MARKINGS ON THE FINAL WEARING SURFACE ON NEW ASPHALT PAVEMENT. PLACE ADDITIONAL APPLICATIONS OF PAINT UPON SUFFICIENT DRYING TIME, AS DETERMINED BY THE ENGINEER.
- BB) TIE PROPOSED PAVEMENT MARKING LINES TO EXISTING PAVEMENT MARKING LINES.
- CC) REPLACE ANY PAVEMENT MARKINGS THAT HAVE BEEN DAMAGED BY THE END OF EACH DAY'S OPERATION.
- DD) TRACE THE EDGE OF PROPOSED MONOLITHIC ISLANDS WITH THE PROPER COLOR PAVEMENT MARKING PRIOR TO INSTALLATION OF A PROPOSED MONOLITHIC ISLAND.
- EE) PLACE AT LEAST TWO APPLICATIONS OF PAINT ON NEW ASPHALT WITH TEMPORARY TRAFFIC PATTERNS WHICH WILL REMAIN IN PLACE OVER THREE (3) MONTHS. PLACE ADDITIONAL APPLICATIONS OF PAINT UPON SUFFICIENT DRYING TIME, AS DETERMINED BY THE ENGINEER.

TEMPORARY / FINAL SIGNALS

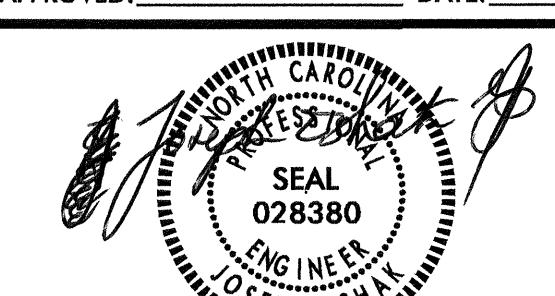
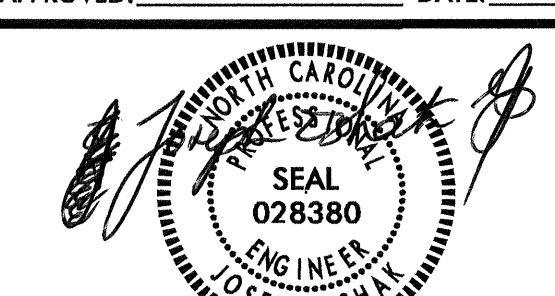
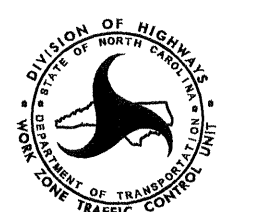
- FF) NOTIFY THE ENGINEER TWO (2) MONTHS BEFORE A TRAFFIC SIGNAL INSTALLATION BY STATE FORCES IS REQUIRED.
- GG) STATE FORCES WILL SHIFT AND REVISE ALL SIGNAL HEADS.

MISCELLANEOUS

- HH) POLICE MAY BE USED TO MAINTAIN TRAFFIC THROUGH INTERSECTIONS.
- II) PLACE SKINNY DRUMS TO DELINEATE ISLANDS

LOCAL NOTES

- STATE FORCES WILL BE RESPONSIBLE FOR PEDESTRIAN DETOUR SIGNING OFF THE PROJECT LIMITS.
- STATE FORCES WILL BE RESPONSIBLE FOR REMOVING PEDESTRIAN DETOUR SIGNING OFF THE PROJECT LIMITS.
- REFER TO SIGNALS PLAN FOR STOP BAR LOCATIONS ON EACH PHASE.

APPROVED: 	DATE: <u>Aug 10, 2007</u>	<h2>PROJECT NOTES</h2>		<table border="1"> <tr> <th colspan="2">REVISIONS</th> </tr> <tr> <td> </td> <td> </td> </tr> </table>		REVISIONS			
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PHASING

PROJ. REFERENCE NO.	SHEET NO.
U-5009	TCP-3

USING RSD 1101.02 SHEETS 1 OF 9 & 3 OF 9, AND TCP-8
INSTALL ADVANCE WARNING SIGNS ON ALL ROADS

PHASE I

STEP 1:

USING RSD 1101.02 SHEET 3 OF 9, MODIFY THE PAVEMENT MARKING ON -L- AS SHOWN ON SHEET TCP-4

NOTIFY THE ENGINEER TO HAVE STATE FORCES INSTALL DETOUR SIGNS FOR W. MARTIN ST., N. RUTHERFORD ST. (NORTH AND SOUTH OF US 74), INCLUDING DETOUR SIGNING FOR PEDESTRIAN

USING RSD 1101.03 SHEET 2 OF 9, PERFORM THE FOLLOWING:
SEE SHEET TCP-4 AND DETAIL "A" ON SHEET TCP-7

- CLOSE TO TRAFFIC N. RUTHERFORD ST. NORTH OF THE INTERSECTION WITH US 74
- CLOSE TO TRAFFIC N. RUTHERFORD ST. SOUTH OF THE INTERSECTION WITH US 74
- CLOSE TO TRAFFIC W. MARTIN ST. AT THE INTERSECTIONS WITH US 74
- CLOSE TO THRU TRAFFIC N. RUTHERFORD ST. SOUTH OF THE INTERSECTION WITH WHEELER ST.
- CLOSE TO THRU TRAFFIC W. MARTIN ST. WEST OF THE INTERSECTION WITH N. RUTHERFORD ST.
- CLOSE SIDEWALKS AS SHOWN ON TCP-3.

USING RSD 1101.02 SHEET 3 OF 9, PERFORM THE FOLLOWING:

- NOTIFY THE ENGINEER TO HAVE STATE FORCES INSTALL TEMPORARY SIGNALS FOR THE PHASE I TEMPORARY TRAFFIC PATTERN AT US 74/NC 109 AND US 74/N. WASHINGTON ST. INTERSECTIONS.
- SHIFT US 74 TRAFFIC ONTO TWO LANE-TWO WAY TEMPORARY TRAFFIC PATTERN (RIGHT SIDE OF -L-) AND INSTALL WATER-FILLED BARRIER AS SHOWN ON TCP-4

STEP 2:

BEHIND BARRIER, CONSTRUCT THE PROPOSED CONSTRUCTION ON -L- & -Y- LINES UP TO BUT NOT INCLUDING THE FINAL LAYER OF SURFACE COURSE AS SHOWN ON SHEET TCP-4

PHASE II

STEP 1:

BEHIND WATER-FILLED BARRIER INSTALLED IN PHASE I, PLACE TEMPORARY PAVEMENT MARKING (PAINT) AND TEMPORARY MARKERS FROM -L- STA. 10+00 +/- TO -L- STA. 21+00 +/- AS SHOWN ON SHEET TCP-5

USING RSD 1101.02 SHEET 3 OF 9 MODIFY THE PAVEMENT MARKING ON -L- (US 74) FROM THE INTERSECTION WITH NC 109 TO THE INTERSECTION WITH WASHINGTON ST. AS SHOWN ON TCP-5, INCLUDING INSERT 1.

NOTIFY THE ENGINEER TO HAVE STATE FORCES INSTALL DETOUR SIGNS FOR PEDESTRIAN AND FOR GRAHAM ST CLOSURE

USING RSD 1101.03 SHEET 2 OF 9, PERFORM THE FOLLOWING:

- CLOSE GRAHAM ST. AT THE INTERSECTION WITH US 74 AND CLOSE SIDEWALKS AS SHOWN ON SHEET TCP-5
- CLOSE GRAHAM ST. TO THRU TRAFFIC AS SHOWN ON SHEET TCP-7, DETAIL B.
- NOTIFY THE ENGINEER TO HAVE STATE FORCES INSTALL PHASE II TEMPORARY SIGNALS.
- SHIFT US 74 TRAFFIC ONTO TWO LANE-TWO WAY TEMPORARY TRAFFIC PATTERN (LEFT SIDE OF -L-), OPEN N. RUTHERFORD ST. (NORTH OF US 74) AND RESET THE WATER-FILLED BARRIER AS SHOWN ON TCP-5. NOTIFY THE ENGINEER TO HAVE STATE FORCES REMOVE THE DETOUR SIGNS FOR N. RUTHERFORD ST. (NORTH OF US 74)

STEP 2:

BEHIND BARRIER, CONSTRUCT THE PROPOSED CONSTRUCTION ON -L- & -Y- LINES UP TO BUT NOT INCLUDING THE FINAL LAYER OF SURFACE COURSE AS SHOWN ON SHEET TCP-5.

PHASE III

STEP 1:

USING RSD 1101.02 SHEET 3 OF 9, PERFORM THE FOLLOWING:

- REMOVE THE WATER-FILLED BARRIER INSTALLED IN PHASE II
- MODIFY THE PAVEMENT MARKING ON -L- AS SHOWN ON TCP-6
- NOTIFY THE ENGINEER TO HAVE STATE FORCES REMOVE OFFSITE DETOUR SIGNS FOR PEDESTRIAN AND FOR GRAHAM ST.
- REMOVE BARRICADES AND OPEN SIDEWALKS, GRAHAM ST., N. RUTHERFORD ST. (SOUTH OF US 74), AND W. MARTIN ST. TO TRAFFIC
- NOTIFY THE ENGINEER TO HAVE STATE FORCES INSTALL PHASE III TEMPORARY SIGNALS.
- SHIFT US 74 TRAFFIC ONTO A TEMPORARY TRAFFIC PATTERN AS SHOWN ON TCP-6

STEP 2:


BEHIND DRUMS, CONSTRUCT THE PROPOSED MEDIAN CONCRETE ISLAND AND CURB & GUTTERS

USING RSD 1101.02 SHEET 1 AND 3 OF 9, PERFORM THE FOLLOWING IN A CONTINUOUS MANNER:

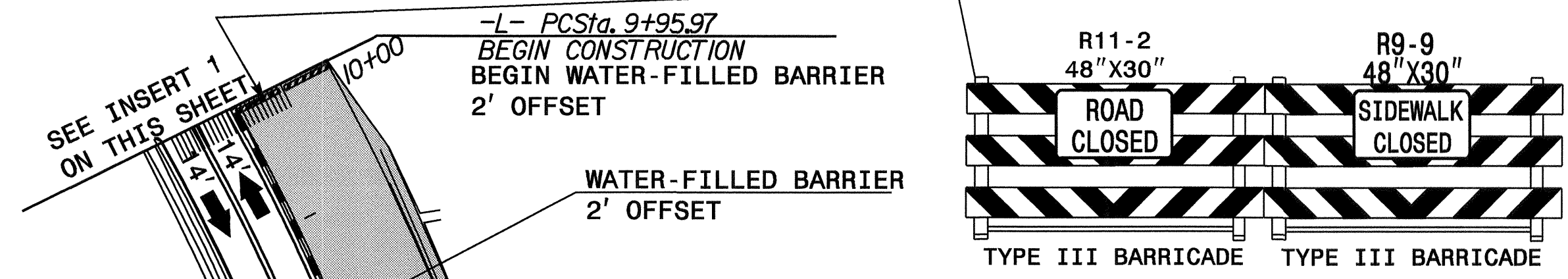
- PRIOR TO PLACEMENT OF FINAL SURFACE COURSE, MILL AND FILL EXISTING -L- AS SHOWN ON ROADWAY TYPICAL SECTIONS 7 & 8
- NOTIFY THE ENGINEER TO HAVE STATE FORCES INSTALL FINAL TRAFFIC SIGNALS.
- PLACE FINAL LAYER OF SURFACE COURSE ON ALL ROADS WITHIN THE PROJECT LIMITS
- USING PM-1, PLACE FINAL PAVEMENT MARKINGS (THERMOPLASTIC) AND PERMANENT RAISED MARKERS ON ALL ROADS WITHIN THE PROJECT LIMITS AND INCLUDING THE AREA WHICH HAS BEEN MILLED AND FILLED OUT.

REMOVE TRAFFIC CONTROL DEVICES AND OPEN ALL ROADS TO FINAL TRAFFIC PATTERN

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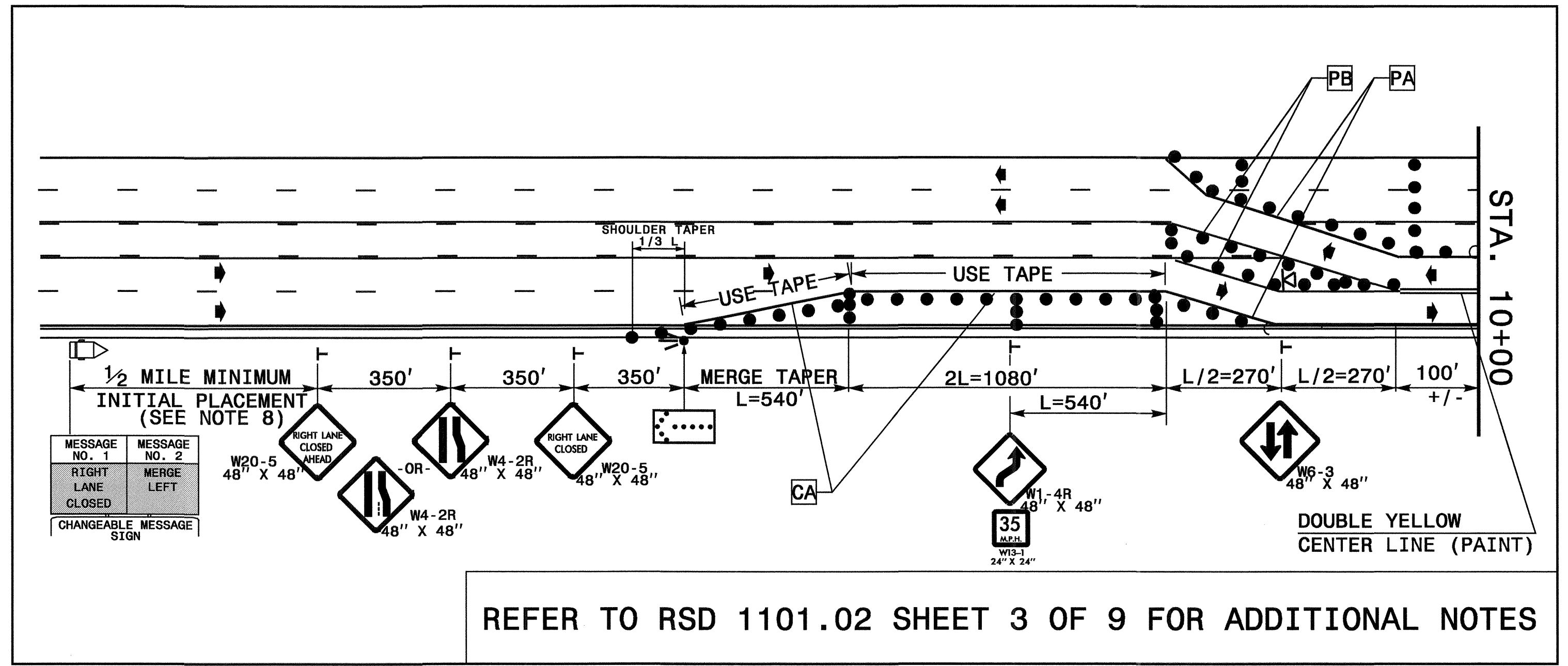
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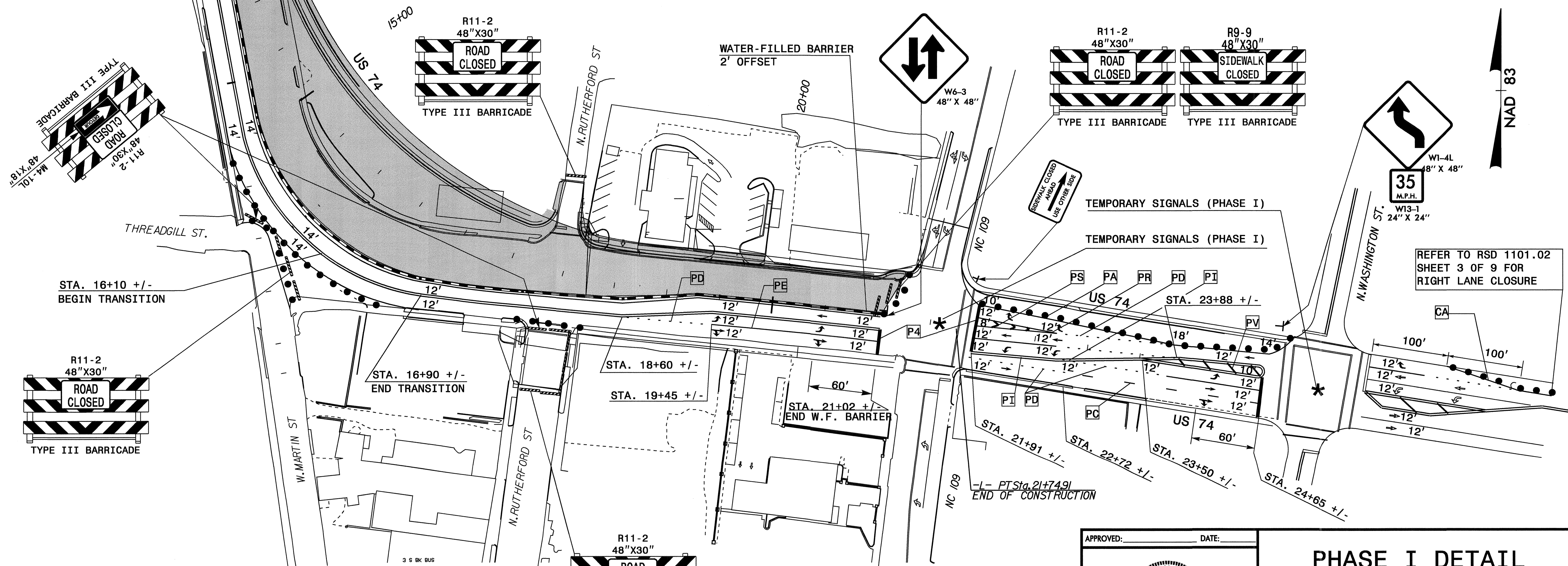


TEMPORARY PAVEMENT MARKINGS SCHEDULE

CA	WHITE EDGELINE (TAPE, Type 1, (4"))
PA	WHITE EDGELINE (PAINT(4"))
PB	YELLOW EDGELINE (PAINT(4"))
PC	10 FT. WHITE SKIP (PAINT(4"))
PD	2 FT. WHITE MINISKIP (PAINT(4"))
PE	WHITE SOLID LANE LINE (PAINT(4"))
PI	YELLOW DOUBLE CENTER (PAINT(4"))
PR	WHITE GORLINE (PAINT(8"))
PS	WHITE DIAGONAL (PAINT(8"))
PV	YELLOW DIAGONAL (PAINT(8"))
P4	WHITE STOPBAR (PAINT(24"))



REFER TO RSD 1101.02 SHEET 3 OF 9 FOR ADDITIONAL NOTES



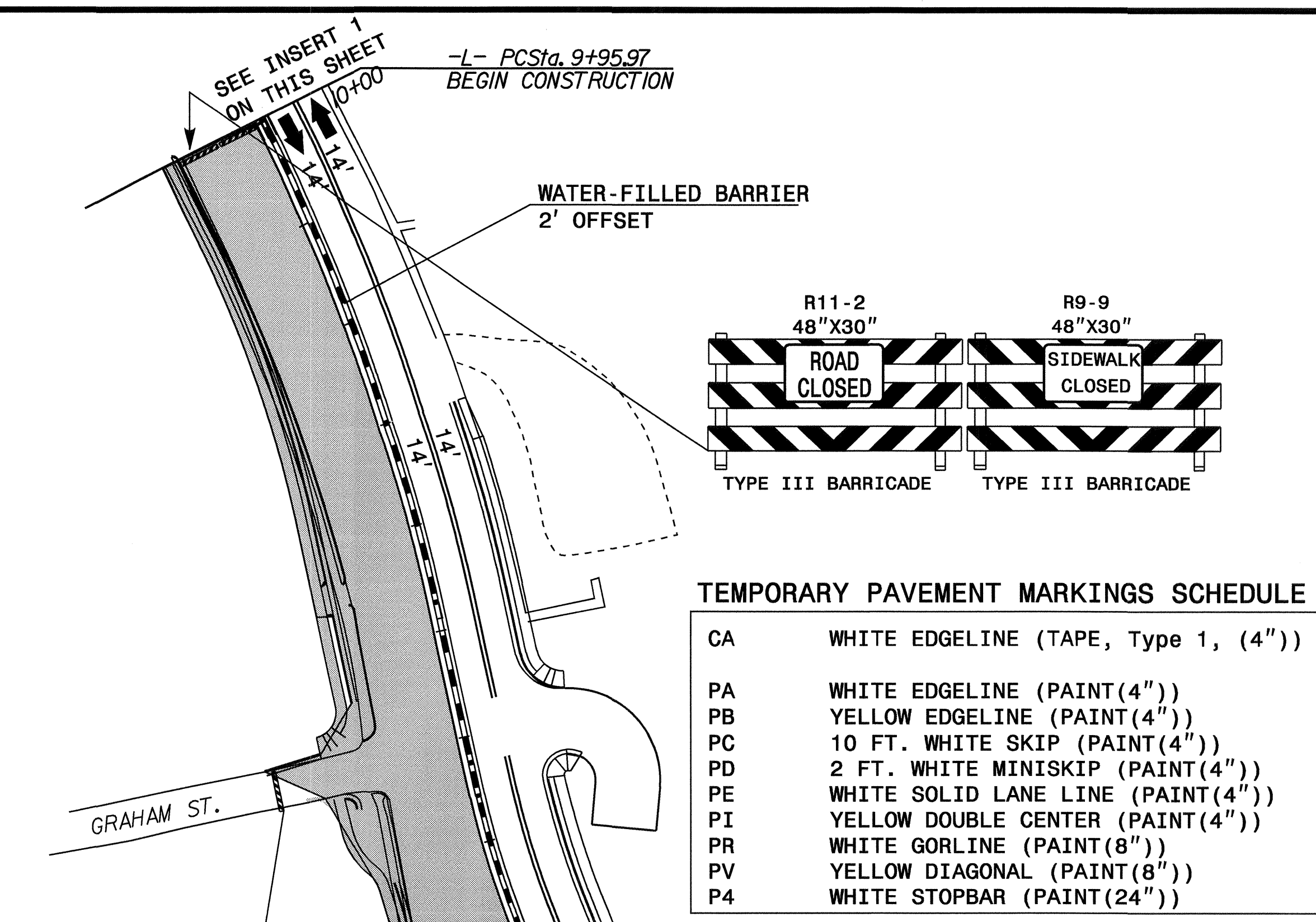
STATE FORCES WILL BE RESPONSIBLE FOR INSTALLING AND REMOVING DETOUR SIGNING FOR PEDESTRIAN AND TRAFFIC.

APPROVED: _____ DATE: _____

PHASE I DETAIL			REVISIONS		
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DESIGN BY: HAL					
REVIEWED BY: JI					

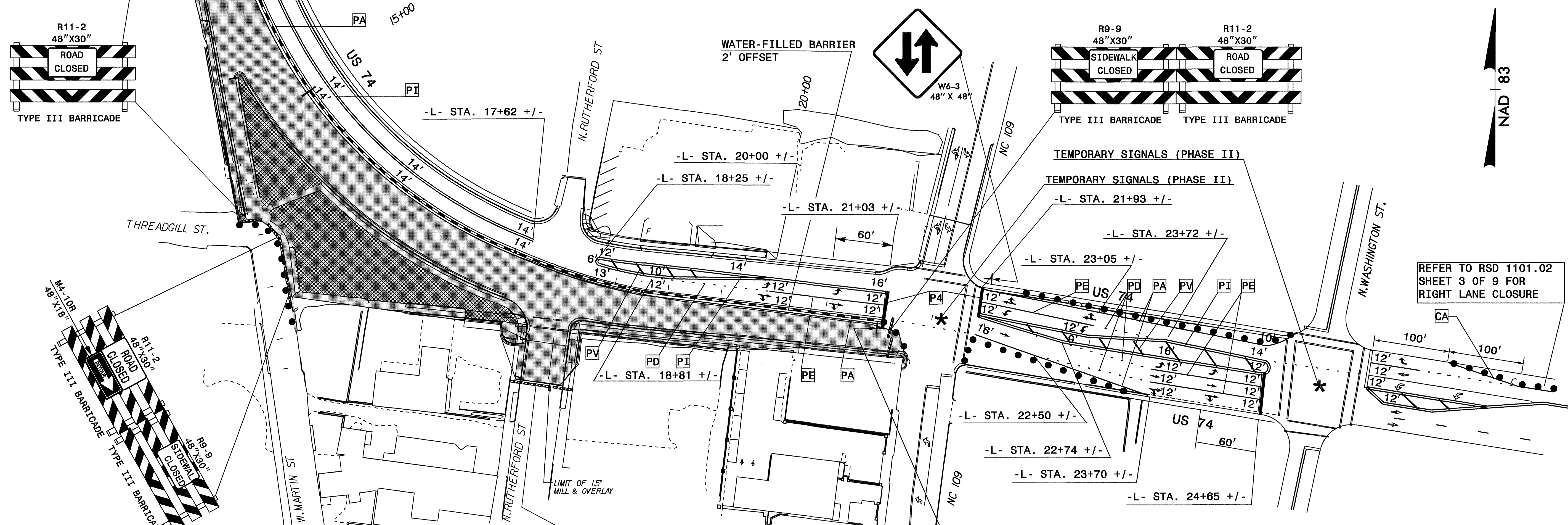
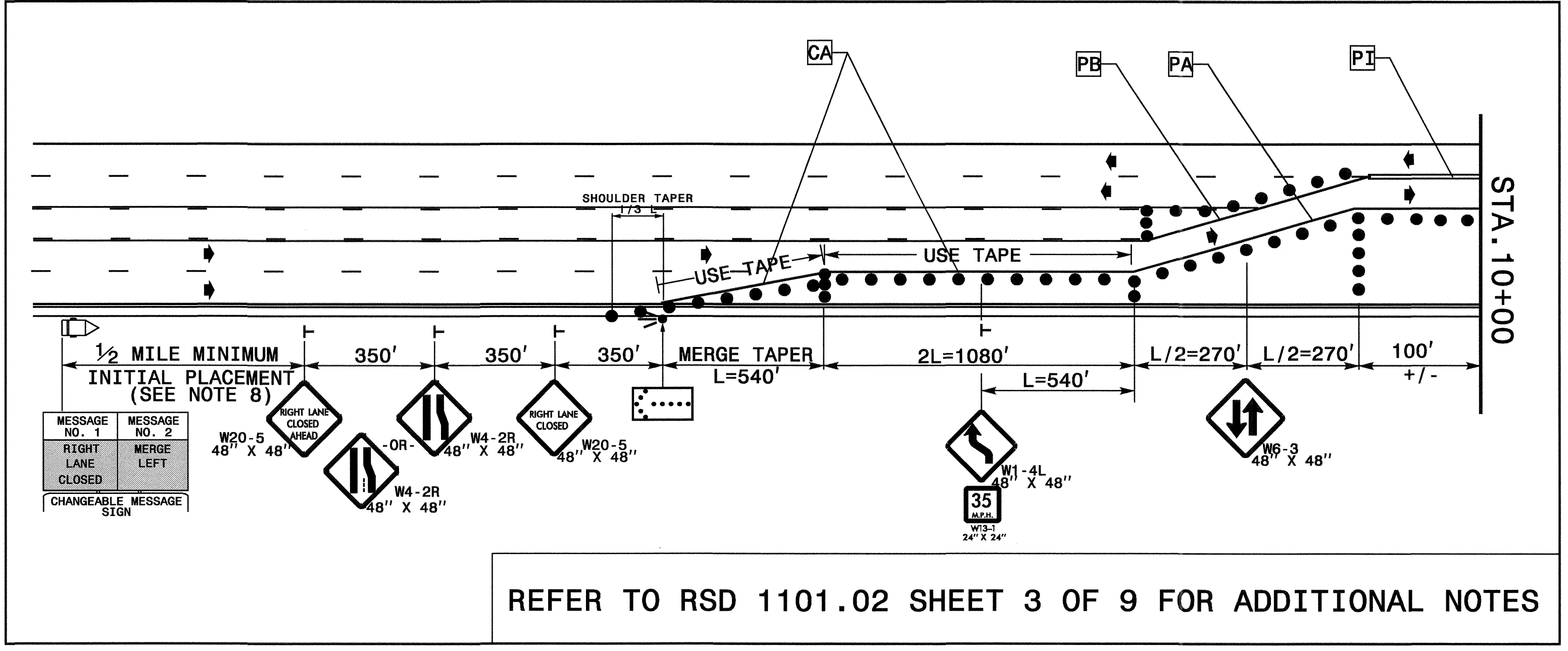
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INSERT 1

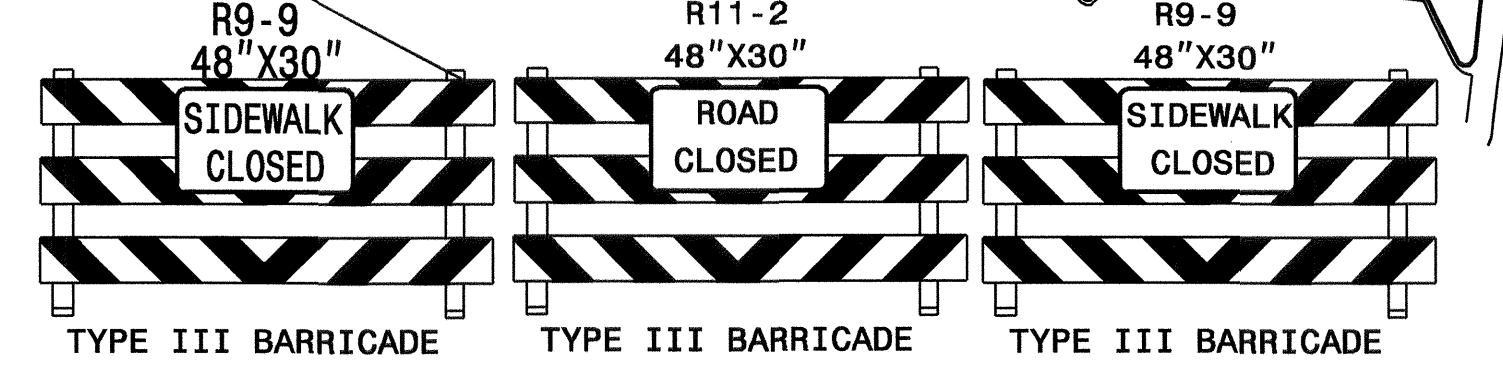


TEMPORARY PAVEMENT MARKINGS SCHEDULE

CA	WHITE EDGELINE (TAPE, Type 1, (4"))
PA	WHITE EDGELINE (PAINT(4"))
PB	YELLOW EDGELINE (PAINT(4"))
PC	10 FT. WHITE SKIP (PAINT(4"))
PD	2 FT. WHITE MINISKIP (PAINT(4"))
PE	WHITE SOLID LANE LINE (PAINT(4"))
PI	YELLOW DOUBLE CENTER (PAINT(4"))
PR	WHITE GORLINE (PAINT(8"))
PV	YELLOW DIAGONAL (PAINT(8"))
P4	WHITE STOPBAR (PAINT(24"))



STATE FORCES WILL BE RESPONSIBLE FOR INSTALLING AND REMOVING DETOUR SIGNING FOR PEDESTRIAN AND TRAFFIC.



APPROVED: _____ DATE: _____

PHASE II

SCALE: NONE
 DATE: FEB. 2007
 DWG. BY: HAL
 DESIGN BY: HAL
 REVIEWED BY: JI

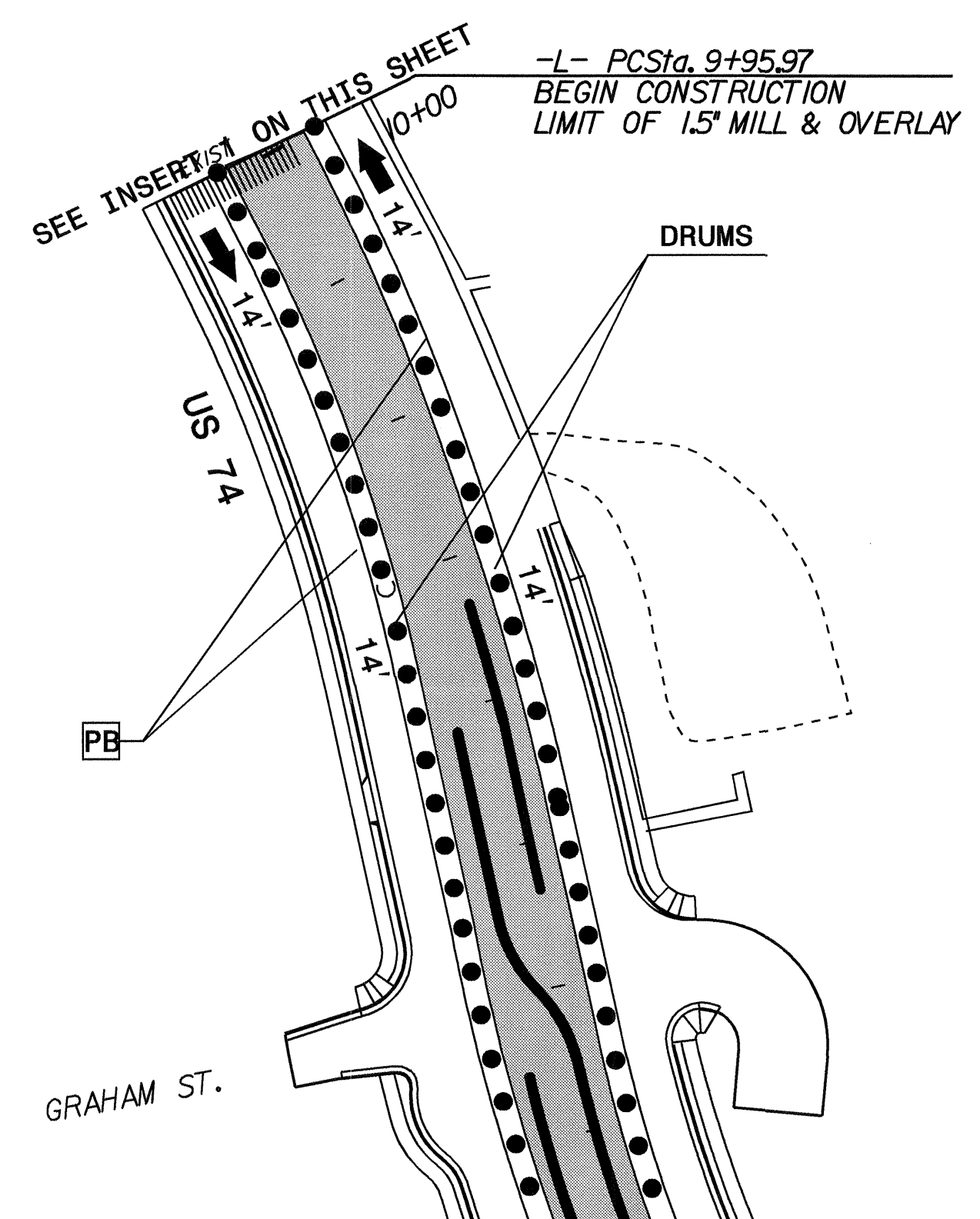
SEAL: PROFESSIONAL ENGINEER
 JOSEPH ISHAY
 028380
 NORTH CAROLINA
 MARCH 16 2007

REVISIONS

CADD FILE

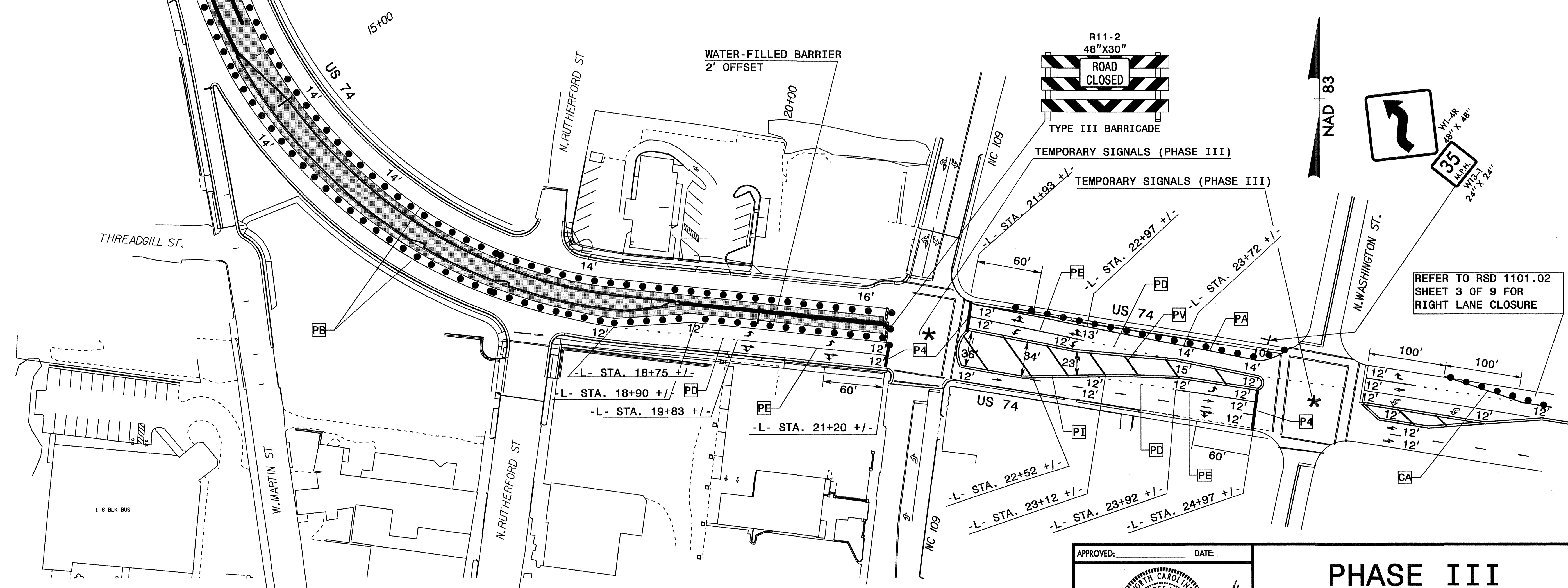
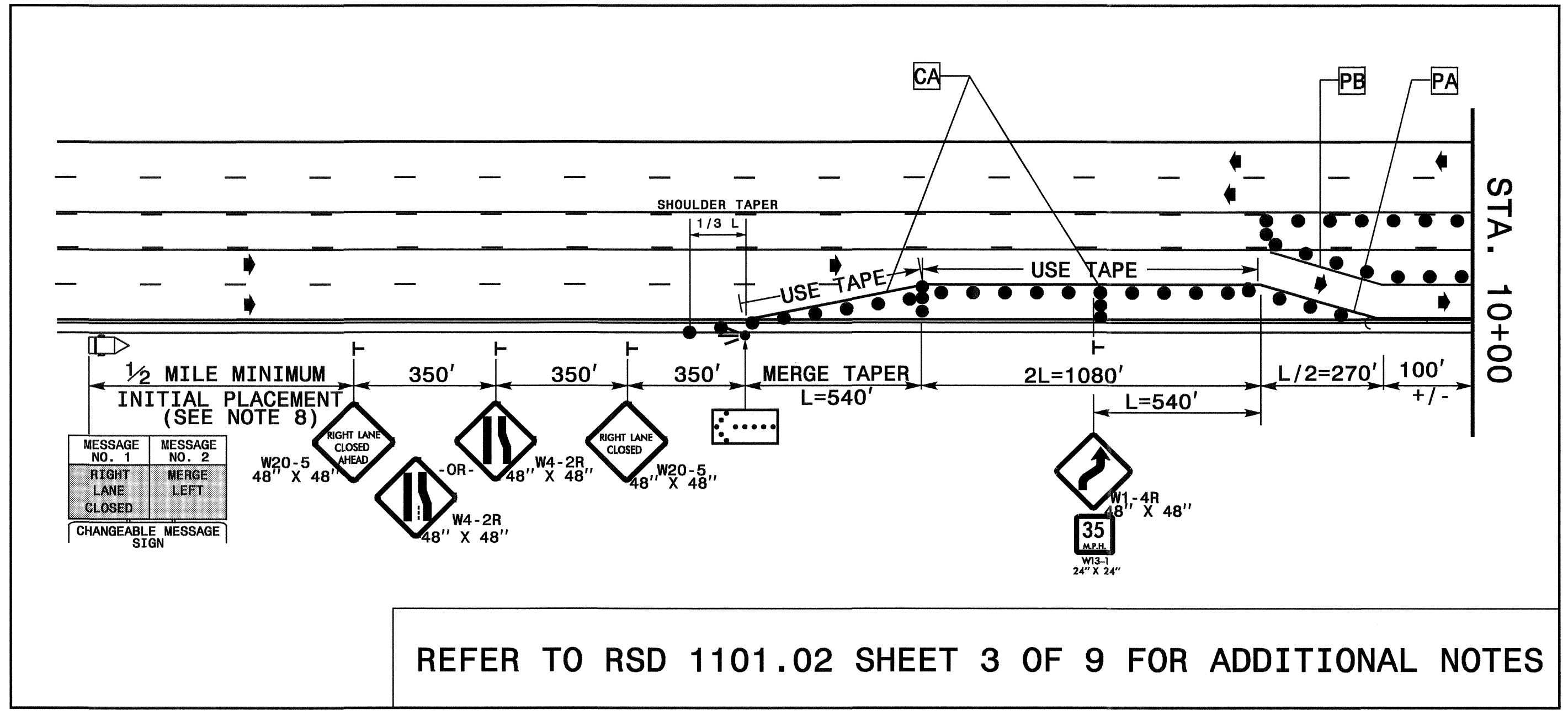
14-MAR-2007 14:10 d:\user_data\jhal\projects\us 74 readlig. in wadesboro\top\top\U5009.TCP-Ph2.dgn

INSERT 1



TEMPORARY PAVEMENT MARKINGS SCHEDULE

CA	WHITE EDGELINE (TAPE, Type 1, (4"))
PA	WHITE EDGELINE (PAINT(4"))
PB	YELLOW EDGELINE (PAINT(4"))
PD	2 FT. WHITE MINISKIP (PAINT(4"))
PE	WHITE SOLID LANE LINE (PAINT(4"))
PI	YELLOW DOUBLE CENTER (PAINT(4"))
PV	YELLOW DIAGONAL (PAINT(8"))
P4	WHITE STOPBAR (PAINT(24"))



APPROVED: _____ DATE: _____

JOSEPH A. SHAW
 ENGINEER
 NORTH CAROLINA
 028380

March 16, 2007

PHASE III

SCALE:	NONE
DATE:	FEB. 2007
DWG. BY:	HAL
DESIGN BY:	HAL
REVIEWED BY:	JI

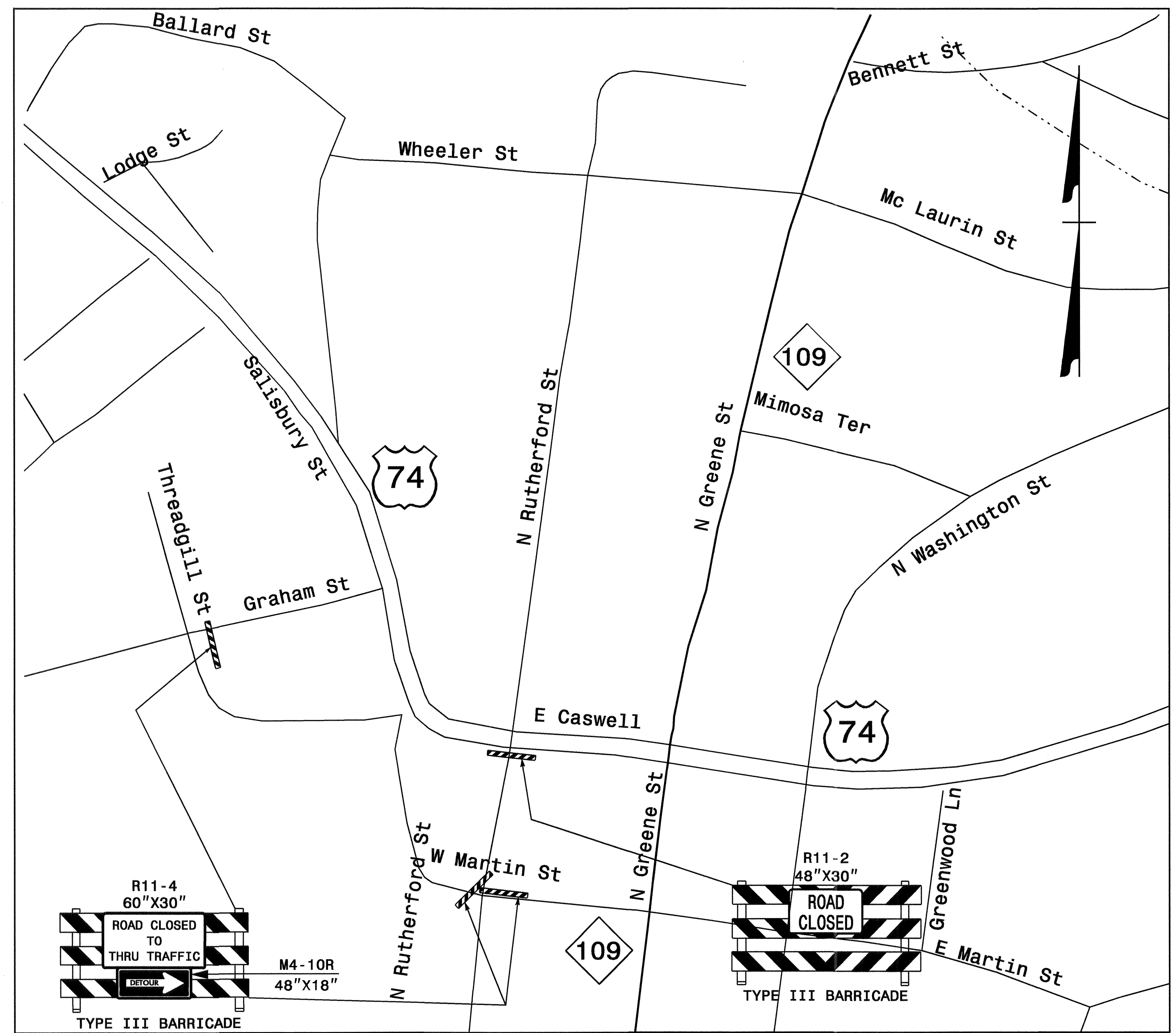
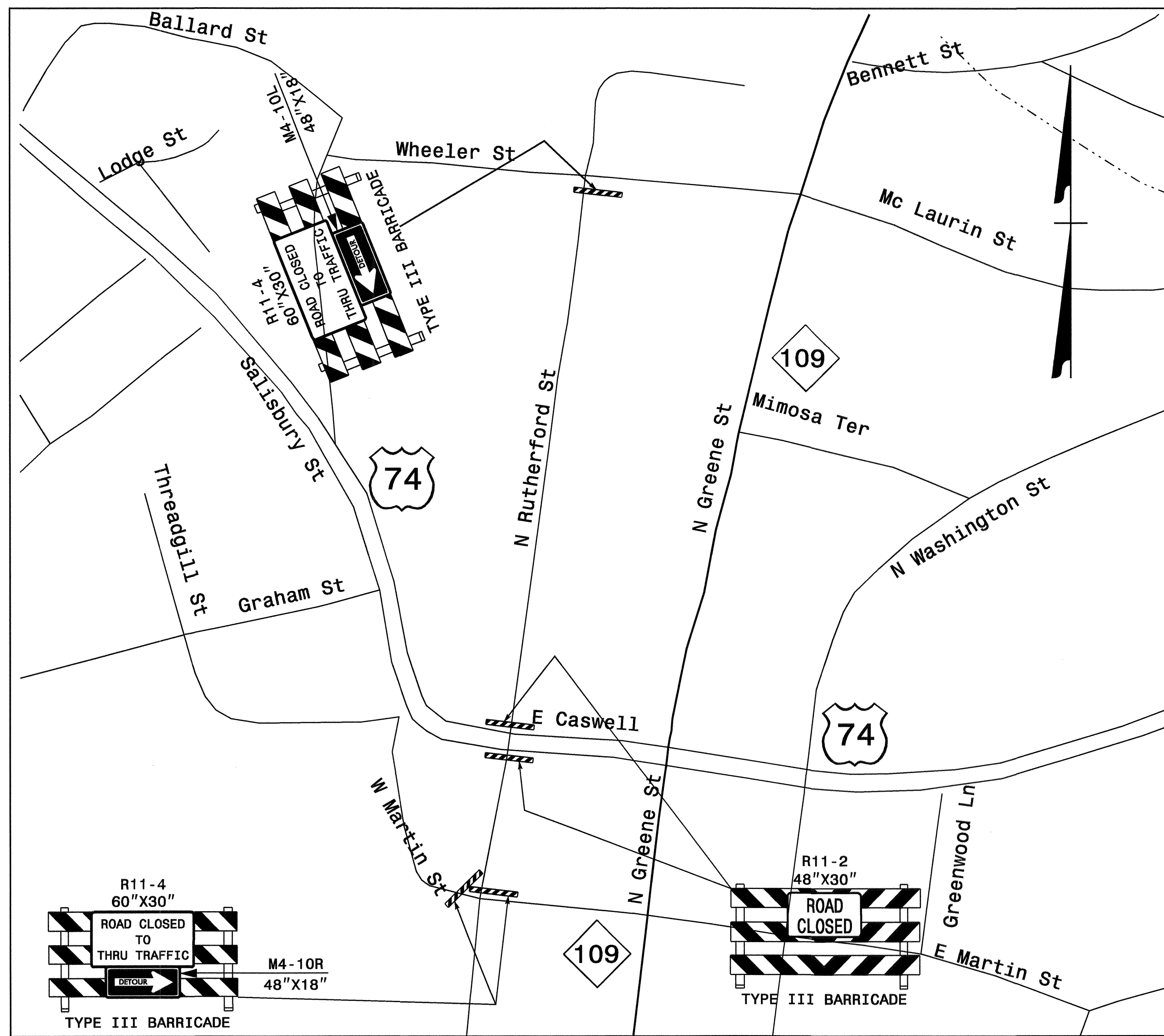
REVISIONS

CADD FILE

14-MAR-2007 14:09
 c:\user\cdrt\dwg\projects\us 74 realig. in wadesboro\top\us5009_tcp-ph3.dgn
 newandos AT WZ12224230

DETAIL "A" (PHASE I)

DETAIL "B" (PHASE II)

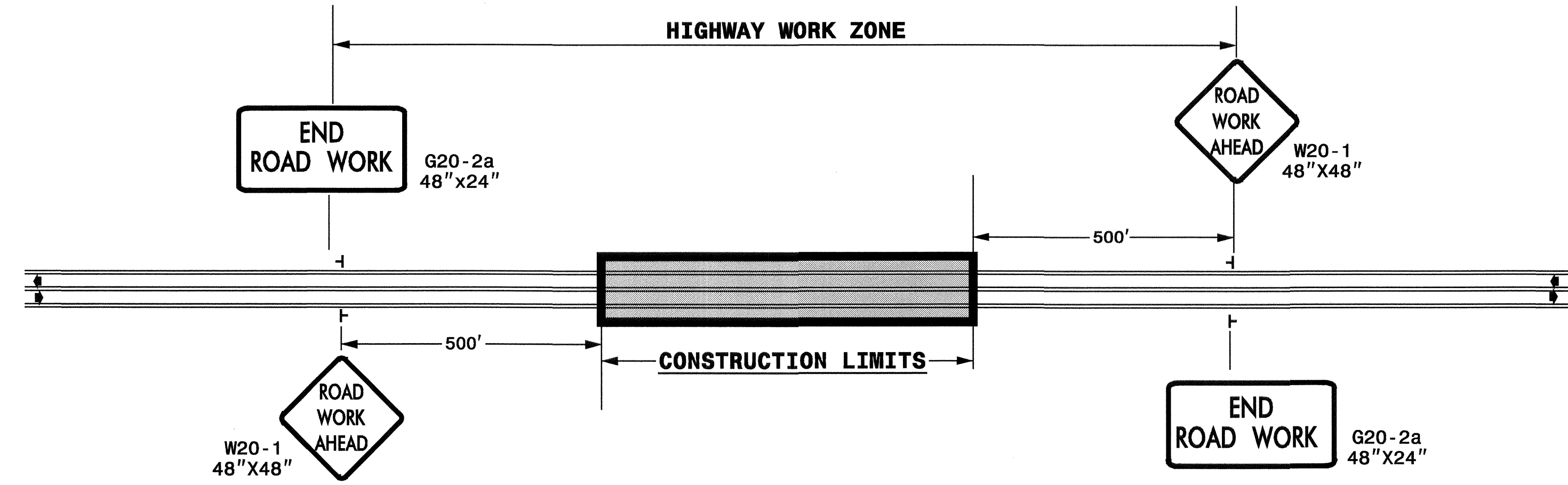


NOTE: THE BARRICADES SHOWN ON THIS SHEET TO BE INSTALLED BY THE CONTRACTOR

14-MAR-2007 14:07
 I:\wadesboro\projects\us 74 redlig.in wadesboro\tcp\U5009-TCP-7.dgn
 Halwando AT W:\TC224250

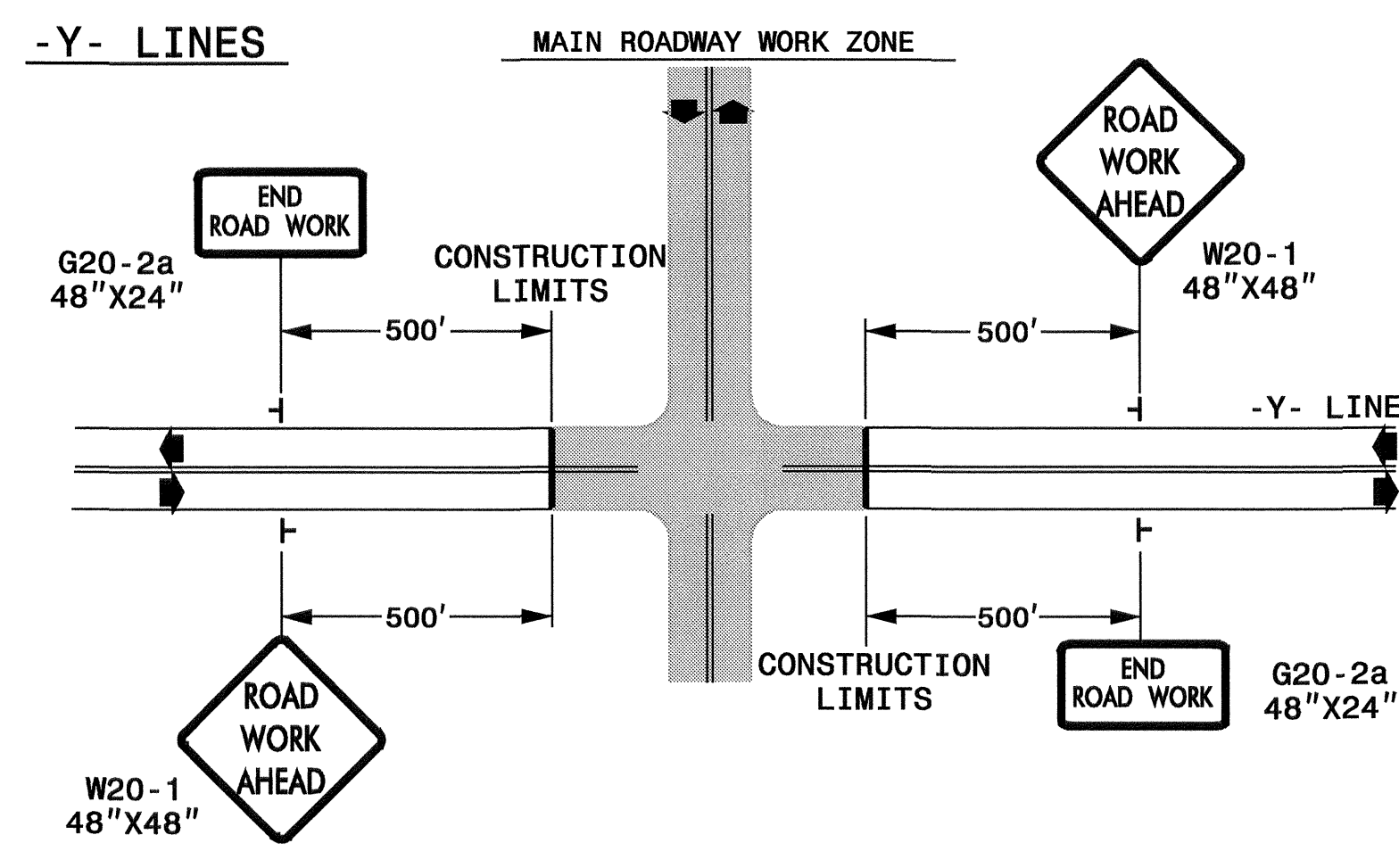
APPROVED:	DATE:	BARRICADE LOCATIONS OFF THE PROJECT LIMITS		REVISIONS
SCALE: NONE				
DATE: FEB. 2007				
DWG. BY: HAL				
DESIGN BY: HAL				
REVIEWED BY: JI				

TWO-WAY UNDIVIDED ** (L-LINES)



STATE OF NORTH CAROLINA
 DEPT. OF TRANSPORTATION
 DIVISION OF HIGHWAYS
 RALEIGH, N.C.

ROADWAYS INTERSECTING ALONG 2 WAY UNDIVIDED WORK ZONE (Y-LINES)



DETAIL DRAWING FOR
 TWO-WAY UNDIVIDED
 WORK ZONE WARNING SIGNS

GENERAL NOTES

- USE FLUORESCENT ORANGE SHEETING (TYPE VII OR HIGHER) ON ALL ADVANCED WORK ZONE SIGNS.
- DO NOT INSTALL ADVANCE WARNING SIGNS MORE THAN 3 DAYS PRIOR TO BEGINNING OF WORK.
- SIGNS SHOWN ARE REQUIRED FOR WORK ZONES THAT WILL REMAIN IN EFFECT OVERNIGHT. FOR SHORT-TERM DAILY MAINTENANCE TYPE OPERATIONS, THIS SIGNING APPLICATION IS OPTIONAL; MAY USE ONLY APPLICABLE ROADWAY STANDARD DRAWINGS INSTEAD. HOWEVER, IF THIS SIGNING APPLICATION IS USED, SIGNS MAY BE PORTABLE MOUNTED.
- ALL SIGN SPACING DIMENSIONS ARE APPROXIMATE, FIELD ADJUST AS NECESSARY OR AS DIRECTED.
- USE 3LB STEEL U-CHANNEL POST OR 4" X 4" WOOD POST FOR ALL WORK ZONE SIGNS. 3LB STEEL U-CHANNEL POSTS MUST MEET THE REQUIREMENTS OF STANDARD SPECIFICATION SECTION 1094-1(B), MAY BE GALVANIZED STEEL, OR MAY BE PAINTED GREEN BY THE POST MANUFACTURER. SQUARE STEEL TUBING POSTS HAVING EQUIVALENT STRENGTH OF THE 3 LB STEEL U-CHANNEL POST ARE ALSO ACCEPTABLE FOR USE. ERECT SIGNS PER ROADWAY STANDARD DRAWING 1110.01. PAYMENT FOR WOOD POSTS, 3LB STEEL U-CHANNEL AND SQUARE STEEL TUBING POSTS WITH SIGNS WILL BE MADE ACCORDING TO STANDARD SPECIFICATION "WORK ZONE SIGNS" SECTION 1110.
- WHEN NECESSARY, USE SPlicing IN ACCORDANCE WITH ROADWAY STANDARD DRAWING NO. 1110.01. REMOVE ENTIRE POST WHEN REMOVING SIGNS WITH SPLICED POSTS.
- DO NOT BACK BRACE SIGN SUPPORTS.
- ** TWO-WAY UNDIVIDED ADVANCE WARNING SIGN CONFIGURATION MAY BE USED ON URBAN MULTI-LANE FACILITIES WHERE CONDITIONS LIMIT THE USE OF DUAL MOUNTED SIGNS AS DETERMINED BY THE ENGINEER.

LEGEND

⊥	STATIONARY SIGN
⇐	DIRECTION OF TRAFFIC FLOW

APPROVED: _____	DATE: _____
DETAIL DRAWING FOR TWO-WAY UNDIVIDED AND URBAN FREEWAYS ADVANCED WORK ZONE WARNING SIGNS	
SCALE: NONE	REVISIONS
DATE: _____	7-98 10/01
DWG. BY: _____	10-98 03/04
DESIGN BY: _____	01/01 11/04
REVIEWED BY: _____	

14-MAR-2007 14:08
 I:\Users\jgarrido\Documents\Projects\us 74 realig. in wadesboro\top\2way_undiv. &_urban_frways.dgn
 jgarrido AT 11/12/2007