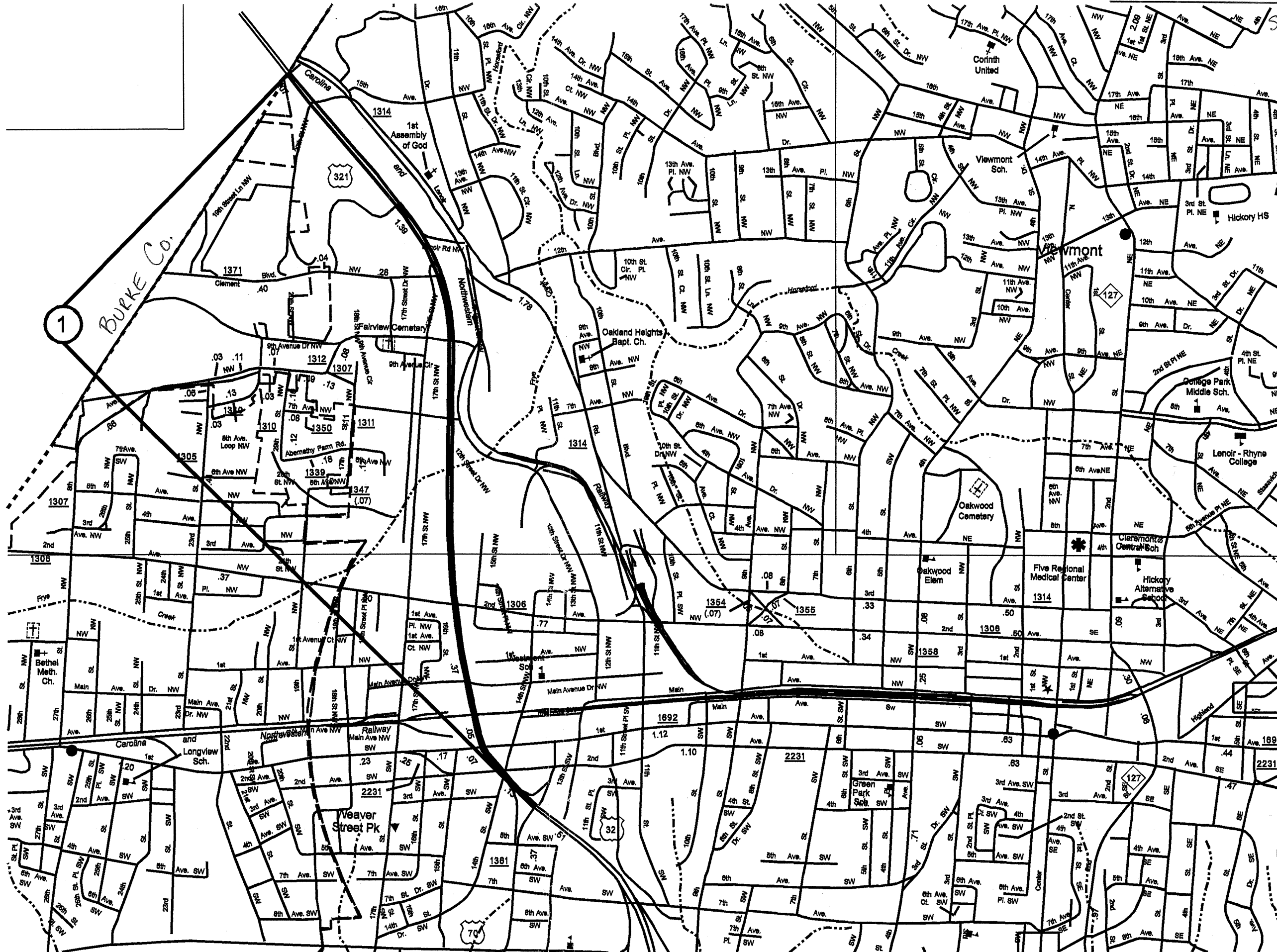


Catawba  
County  
N.C.

12CR.10181.4

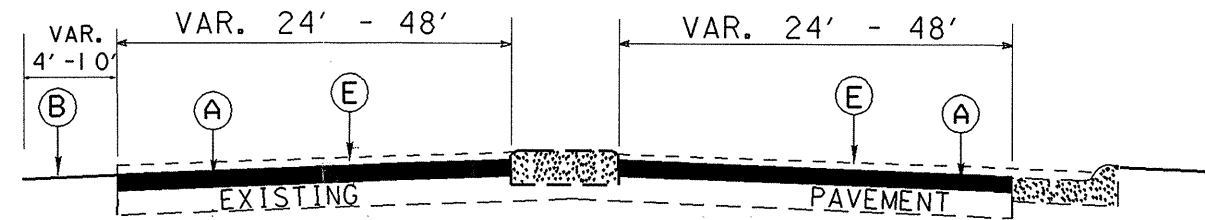
Primary  
Resurfacing  
Map

Sheet  
No. 1



PAVEMENT SCHEDULE	
A	PROP. APPROX. 1½" ASPHALT CONCRETE SURFACE COURSE, TYPE 99.5C, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
B	SHOULDER RECONSTRUCTION
C	PROP. APPROX. 8.0" ASPHALT INTERMEDIATE COURSE, I19.0C, AT AN AVERAGE RATE OF 448 LBS. PER SQ. YD. IN EACH OF TWO 4" LIFTS
E	MILL ASPHALT PAVEMENT APPROX. 0 - 3 "
F	MILL ASPHALT PAVEMENT APPROX. 8" TOTAL DEPTH IN TWO 4" PASSES.

NOTE: PAVEMENT EDGE SLOPES ARE 1:1 UNLESS SHOWN OTHERWISE.  
MILL BRIDGE APPROACHES 100' TO PROVIDE A SMOOTH TRANSITION AS DIRECTED.

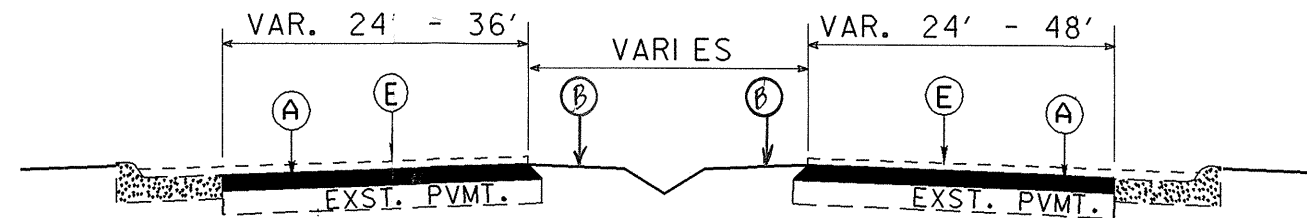


TYPICAL SECTION NO. 1

STA. 0+00 to 6+60, APPROX. 0.13 MILE

NOTE: LT. CURB BEGINS AT STA 4+00 +/-

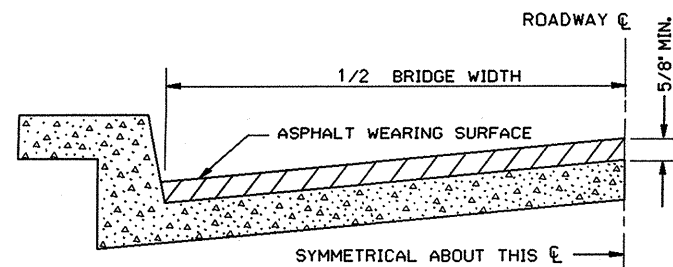
\*\*NOTE: MILL ADDITIONAL 8" DEPTH WHERE EXST. ASPHALT IS DEEPLY RUTTED AND REPLACE WITH ASPHALT INTERMEDIATE COURSE, TYPE I19.0C, AT AN AVG. RATE OF 448 LBS. PER SQ. YD. FOR EACH OF 2 LIFTS.



TYPICAL SECTION NO. 2

STA 6+60 TO STA 17+25, APPROXIMATELY 0.20 MILE

\*\*NOTE: MILL ADDITIONAL 8" DEPTH WHERE EXST. ASPHALT IS DEEPLY RUTTED AND REPLACE WITH ASPHALT INTERMEDIATE COURSE, TYPE I19.0C, AT AN AVG. RATE OF 448 LBS. PER SQ. YD. FOR EACH OF 2 LIFTS.



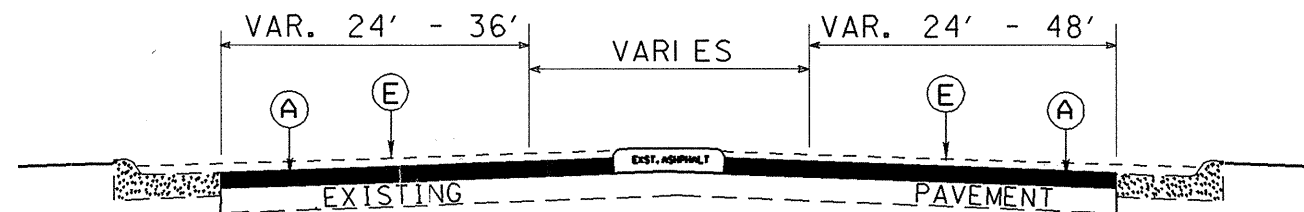
BRIDGE HALF TYPICAL SECTION

FOR BRIDGES WITH FLOOR DRAINS, CARE SHALL BE EXERCISED IN PLACING THE WEARING SURFACE AROUND FLOOR DRAINS SO AS NOT TO HINDER EFFECTIVE DRAINAGE. ALL DRAINS SHALL BE LEFT OPEN.

THE PROPOSED WEARING SURFACE SHALL VARY IN THICKNESS AS NECESSARY TO PROVIDE A SMOOTH RIDING SURFACE. A THICKNESS OF NOT LESS THAN 5/8" SHALL BE PROVIDED. THE MAXIMUM THICKNESS SHALL PREFERABLY BE 1-1/2" UNLESS IT IS IMPRACTICAL TO PROVIDE A SMOOTH RIDING SURFACE OTHERWISE.

NOTES

ALL UNPAVED S.R. ROADS TO BE SURFACED 50' FROM EDGE OF PAVEMENT OF MAIN PROJECT.  
ALL PAVED S.R. ROADS TO BE RESURFACED TO THE ENDS OF THE RADII, OR AS DIRECTED BY THE ENGINEER.  
EDGES, PAVEMENT WIDENING, INTERSECTIONS AND BRIDGE FLARES ARE INCLUDED IN THE TABLE OF QUANTITIES.  
SHOULDERS AND DITCHES ARE TO BE CONSTRUCTED BY OTHERS UNLESS OTHERWISE NOTED.  
BRIDGES TO BE RESURFACED AT LOCATIONS AND TO DEPTH AS DIRECTED BY THE ENGINEER.



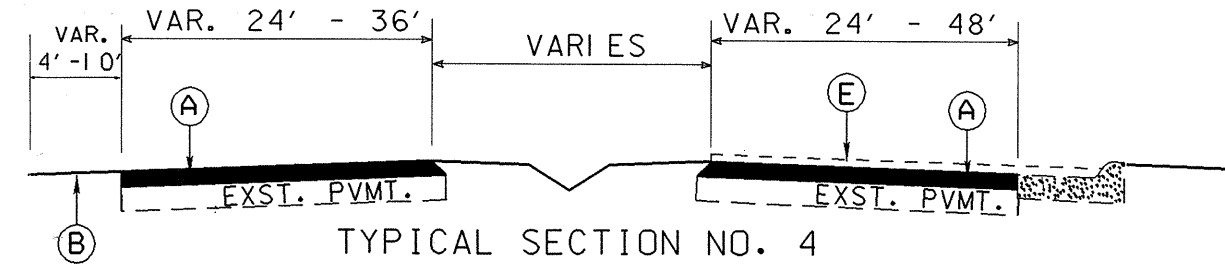
TYPICAL SECTION NO. 3

STA 17+25 TO STA 21+00, APPROXIMATELY 0.07 MILE)

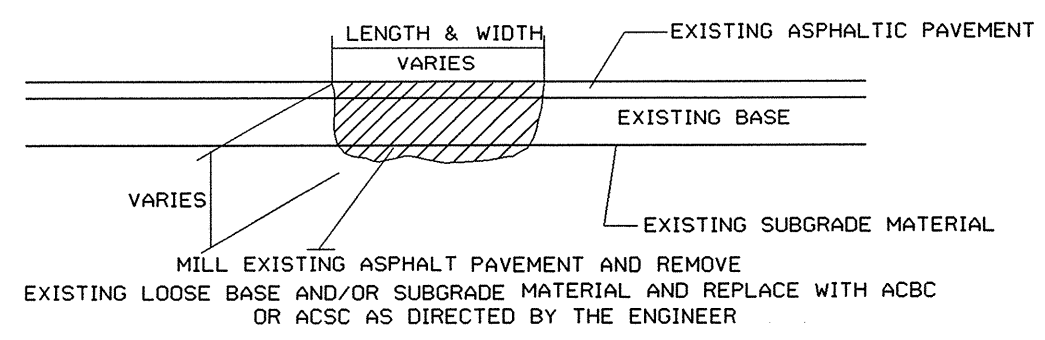
NOTE: LEFT CURB ENDS AT STA 19+25 +/-

PAVEMENT SCHEDULE	
A	PROP. APPROX. 1½" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 188 LBS. PER SQ. YD.
B	SHOULDER RECONSTRUCTION
C	PROP. APPROX. 8.0" ASPHALT INTERMEDIATE COURSE, I19.0C, AT AN AVERAGE RATE OF 448 LBS. PER SQ. YD. IN EACH OF TWO 4" LIFTS
E	MILL ASPHALT PAVEMENT APPROX. 0 - 3 "
F	MILL ASPHALT PAVEMENT APPROX. 8 " TOTAL DEPTH IN TWO 4" PASSES.

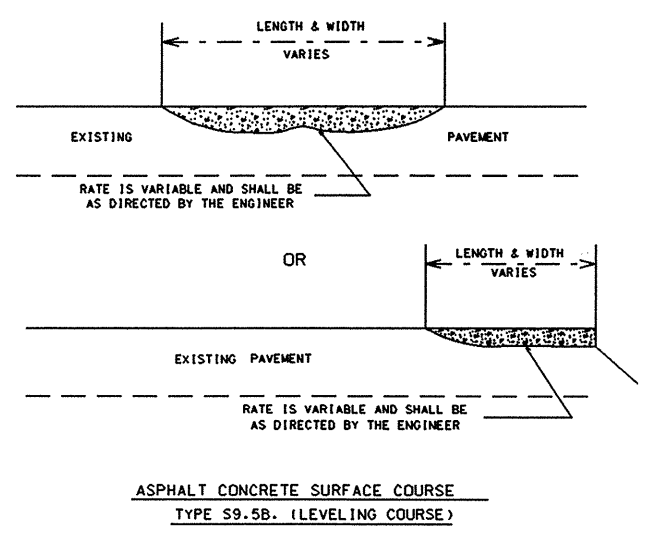
NOTE: PAVEMENT EDGE SLOPES ARE 1:1 UNLESS SHOWN OTHERWISE.  
MILL BRIDGE APPROACHES 100' TO PROVIDE A SMOOTH TRANSITION AS DIRECTED.



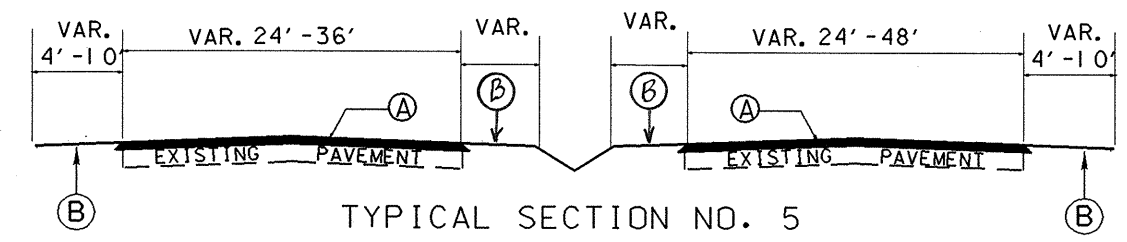
TYPICAL SECTION NO. 4  
STA 21+00 TO STA 32+00, APPROXIMATELY 0.21 MILE



PATCHING EXISTING PAVEMENT

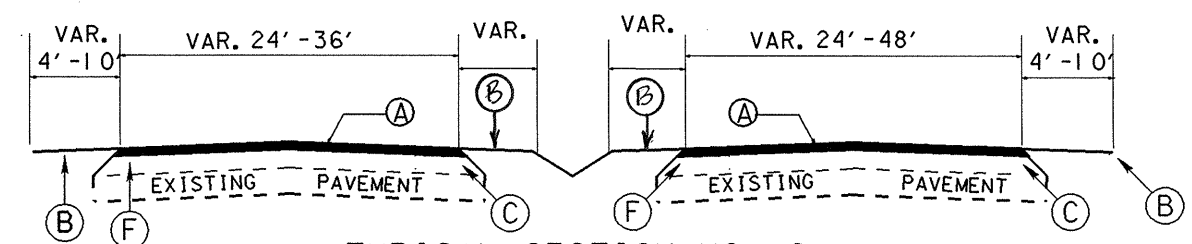


ASPHALT CONCRETE SURFACE COURSE  
TYPE S9.5B. (LEVELING COURSE)



TYPICAL SECTION NO. 5  
STA 32+00 TO STA 108+70 APPROXIMATELY 1.45 MILES

\*\* NOTE: FOR INTERSECTIONS SEE TYPICAL NO. 7



TYPICAL SECTION NO. 6  
(INTERSECTIONS WHERE EXISTING ASPHALT IS DEEPLY RUTTED)

APPROXIMATE STATIONS:  
52+75 to 56+75  
63+25 to 67+25  
72+75 to 76+75

\*\*NOTE: MILL 8" DEPTH WHERE EXISTING ASPHALT IS RUTTED OR AS DIRECTED BY THE ENGINEER.

PROJECT NO. 12CR10181.4	SHEET NO. 4	TOTAL NO.
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### SUMMARY OF QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP	LENGTH MI	WIDTH FT	INCIDENTAL STONE TONS	SHOULDER RECONST. SMI	4" MILLING SY	0" TO 3" MILLING SY	INTERMEDIATE COURSE, I19.0C TONS	SURFACE COURSE, S9.5C TONS	LEVELING COURSE, S9.5C TONS	PG 64-22 PLANT MIX TONS	PG 70-22 PLANT MIX TONS	PATCHING EXISTING PAVEMENT TONS	ADJUST. OF CATCH BASIN EA	ADJUST. OF DROP INLET EA	ADJUST. OF MANHOLES EA	ADJUST. OF METER OR VALVE BOX EA	PORTABLE LIGHTING LS	
12CR.10181.4	Catawba	1	US321B	FROM 13 ST. SW NORTH TO THE BURKE CO. LINE	1	0.13	vars. 48-84'																
					2	0.20	vars 48-84'																
					3	0.07	vars 48-84'																
					4	0.21	vars 48-84'																
					5	1.45	vars 48-84'	200	6	15000	22667	3420	6883	1500	161	511	1500	1	1	5	10	1.00	
<b>TOTAL FOR PROJ NO. 12CR.10181.4</b>						<b>2.06</b>		<b>200</b>	<b>6</b>	<b>15000</b>	<b>22667</b>	<b>3420</b>	<b>6883</b>	<b>1500</b>	<b>161</b>	<b>511</b>	<b>1500</b>	<b>1</b>	<b>1</b>	<b>5</b>	<b>10</b>	<b>1.00</b>	
<b>GRAND TOTAL</b>						<b>2.06</b>		<b>200</b>	<b>6</b>	<b>15000</b>	<b>22667</b>	<b>3420</b>	<b>6883</b>	<b>1500</b>	<b>161</b>	<b>511</b>	<b>1500</b>	<b>1</b>	<b>1</b>	<b>5</b>	<b>10</b>	<b>1.00</b>	

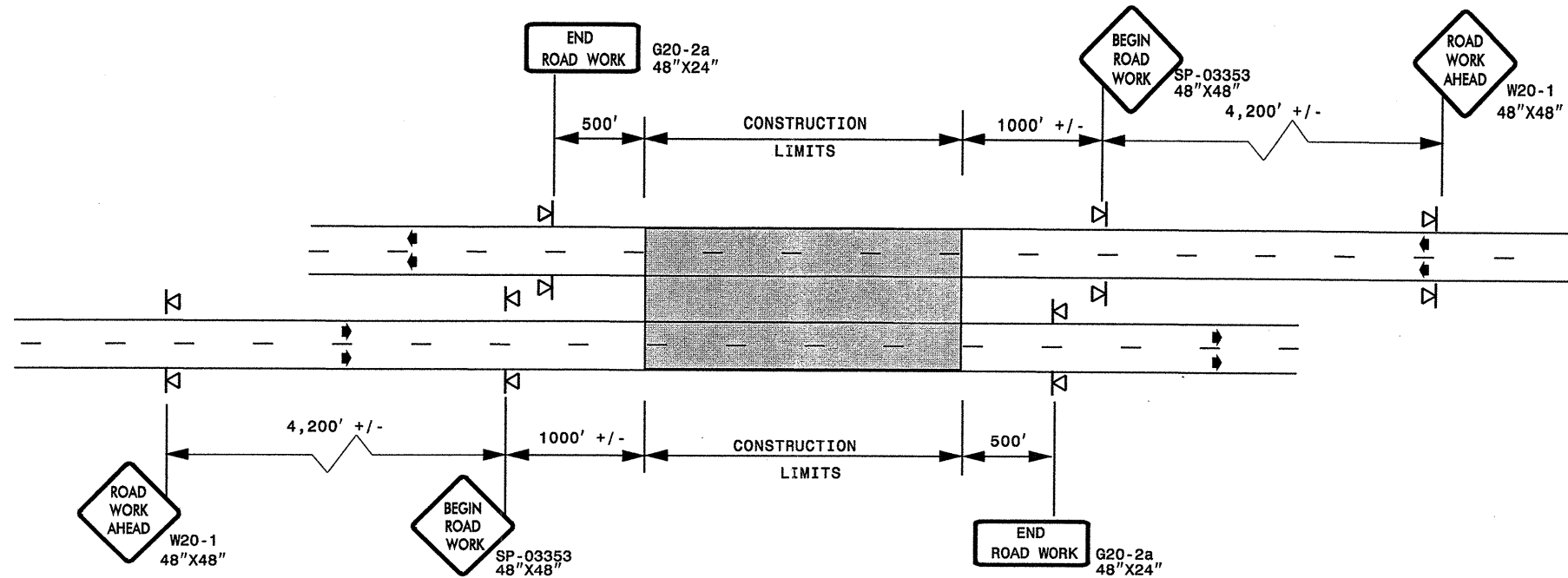
PROJECT NO.	SHEET NO.	TOTAL NO.
12CR.10181.4	5	

### THERMOPLASTIC AND PAINT QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	4" X 90 M	4" X 90 M	4" X 120 M	4" X 120M	8" X 90 M	8" X 90 M WHITE	16" X 120M	24" X 120 M	THERMO	THERMO	THERMO LT	THERMO RT	THERMO STR	THERMO	4" WHITE	4" YELLOW	24" WHITE	SNOW
					WHITE	YELLOW	WHITE	YELLOW	YELLOW	THERMO	WHITE THERMO	WHITE	CHARS. RXR	ARROW 90 M	ARROW 90	& RT ARROW	STR.	WHITE	YELLOW	WHITE	PAINT	PAINT
					LF	LF	LF	LF	LF	LF	LF	LF	120 M	EA	EA	EA	EA	EA	LF	LF	LF	MARKERS
12CR.10181.4	Catawba	1	US321B	FROM 13 ST. SW NORTH TO THE BURKE CO. LINE	22,500	22,500	15,000	500	500	500	100	1,000	16	4	45	25	12	15	37,500	22,500	1,000	600
TOTAL FOR PROJ NO. 12CR.10181.4					45,000	45,000	15,500	500	500	500	100	1,000	20	4	45	25	12	15	37,500	22,500	1,000	600
GRAND TOTAL					45,000	45,000	15,500	500	500	500	100	1,000	20	4	45	25	12	15	37,500	22,500	1,000	600

**ADVANCED WORK ZONE WARNING SIGNING FOR FREEWAYS (4 LANES OR GREATER)**

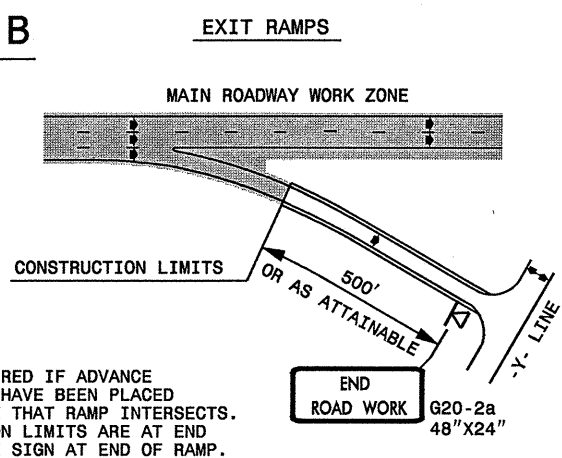
**DETAIL A**



STATE OF NORTH CAROLINA  
DEPT. OF TRANSPORTATION  
DIVISION OF HIGHWAYS  
RALEIGH, N.C.

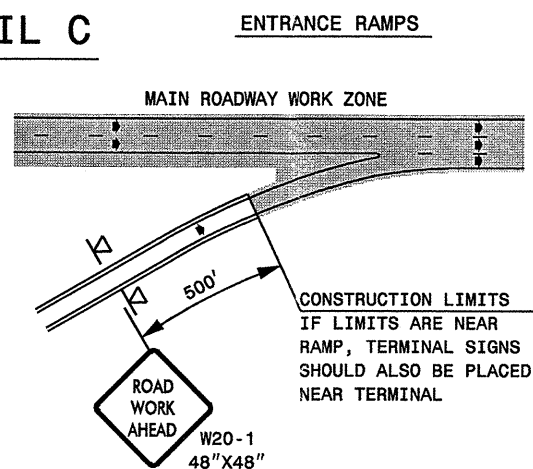
**ROADWAYS INTERSECTING ALONG FREEWAY WORK ZONE (Y-LINES)**

**DETAIL B**



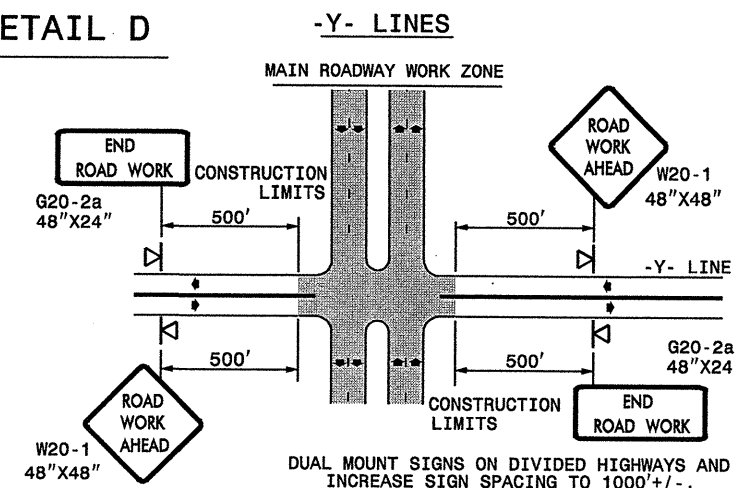
NOTE:  
SIGN NOT REQUIRED IF ADVANCE  
WARNING SIGNS HAVE BEEN PLACED  
ALONG -Y- LINE THAT RAMP INTERSECTS.  
IF CONSTRUCTION LIMITS ARE AT END  
OF RAMP, PLACE SIGN AT END OF RAMP.

**DETAIL C**



CONSTRUCTION LIMITS  
IF LIMITS ARE NEAR  
RAMP, TERMINAL SIGNS  
SHOULD ALSO BE PLACED  
NEAR TERMINAL

**DETAIL D**



DUAL MOUNT SIGNS ON DIVIDED HIGHWAYS AND  
INCREASE SIGN SPACING TO 1000' +/-.

**DETAIL DRAWING  
FOR FREEWAYS  
WORK ZONE WARNING SIGNS  
(SHORT-DURATION LANE CLOSURES)**

**GENERAL NOTES**

- USE FLUORESCENT ORANGE SHEETING (TYPE VII OR HIGHER) ON ALL ADVANCED WORK ZONE SIGNS.
- DO NOT INSTALL ADVANCE WARNING SIGNS MORE THAN 3 DAYS PRIOR TO BEGINNING OF WORK.
- ALL SIGN SPACING DIMENSIONS ARE APPROXIMATE, FIELD ADJUST AS NECESSARY OR AS DIRECTED.
- USE PORTABLE WORK ZONE SIGNS ONLY WITH PORTABLE WORK ZONE SIGN STANDS SPECIFICALLY DESIGNED FOR ONE ANOTHER. PORTABLE WORK ZONE SIGNS MAY BE ROLL UP OR APPROVED COMPOSITE.
- PROVIDE PORTABLE WORK ZONE SIGN STANDS, PORTABLE SIGNS AND SIGN SHEETING WHICH ARE LISTED ON THE NORTH CAROLINA DEPARTMENT OF TRANSPORTATION'S APPROVED PRODUCT LIST OR ACCEPTED AS TRAFFIC QUALIFIED BY THE TRAFFIC CONTROL UNIT.
- \*\* TWO-WAY UNDIVIDED ADVANCE WARNING SIGN CONFIGURATION MAY BE USED ON MULTI-LANE FACILITIES WHERE CONDITIONS LIMIT THE USE OF DUAL MOUNTED SIGNS AS DETERMINED BY THE ENGINEER.

**LEGEND**

◁ PORTABLE SIGN

➡ DIRECTION OF TRAFFIC FLOW

APPROVED: _____	DATE: _____		<b>DETAIL DRAWING FOR FREEWAYS WORK ZONE WARNING SIGNS</b>									
SEAL			SCALE: NONE DATE: _____ DWG. BY: _____ DESIGN BY: _____ REVIEWED BY: _____	<table border="1"> <tr> <th colspan="2">REVISIONS</th> </tr> <tr> <td>7-98</td> <td>10/01</td> </tr> <tr> <td>10-98</td> <td>03/04</td> </tr> <tr> <td>01/01</td> <td>11/04</td> </tr> </table>		REVISIONS		7-98	10/01	10-98	03/04	01/01
REVISIONS												
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