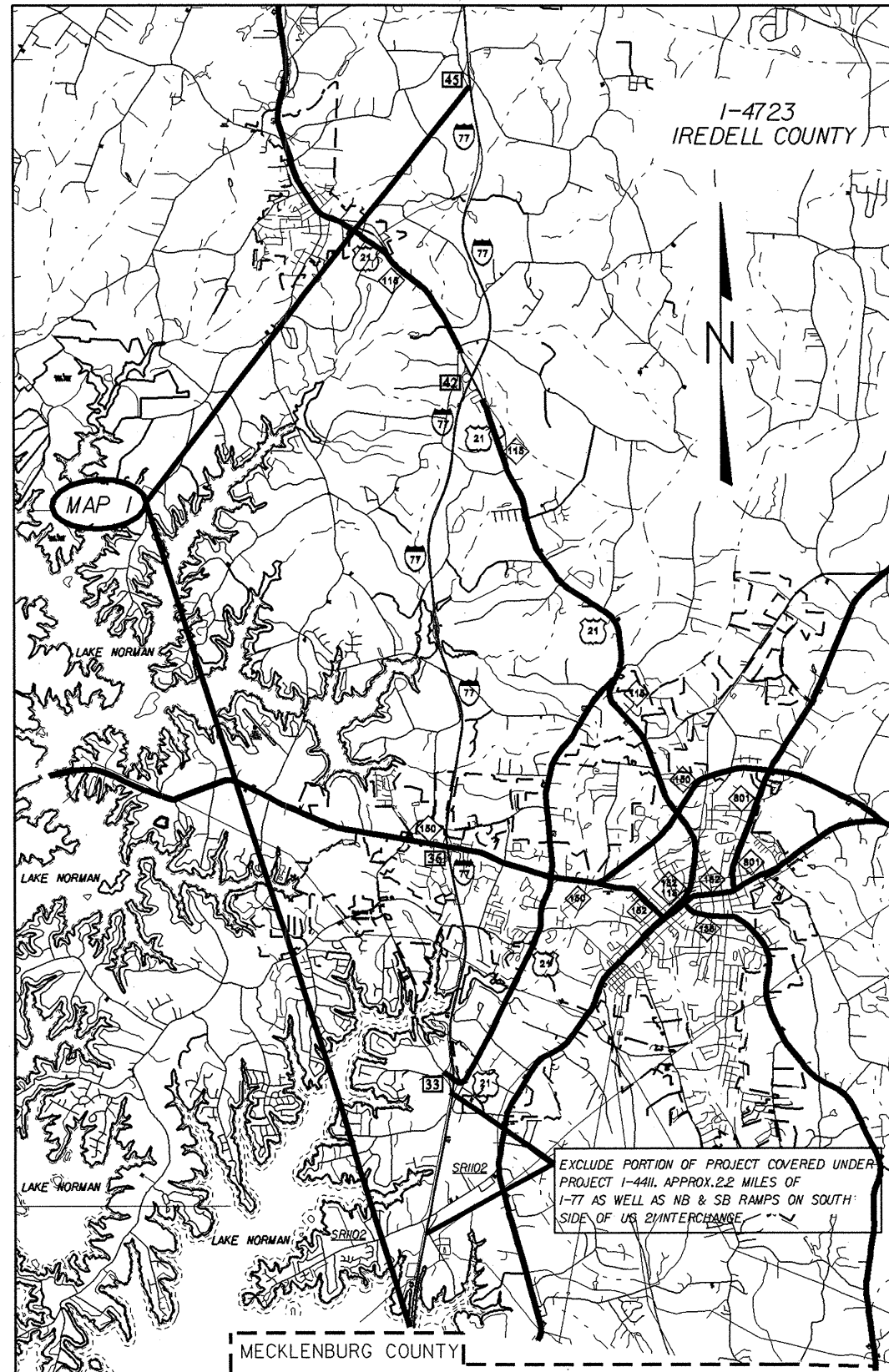


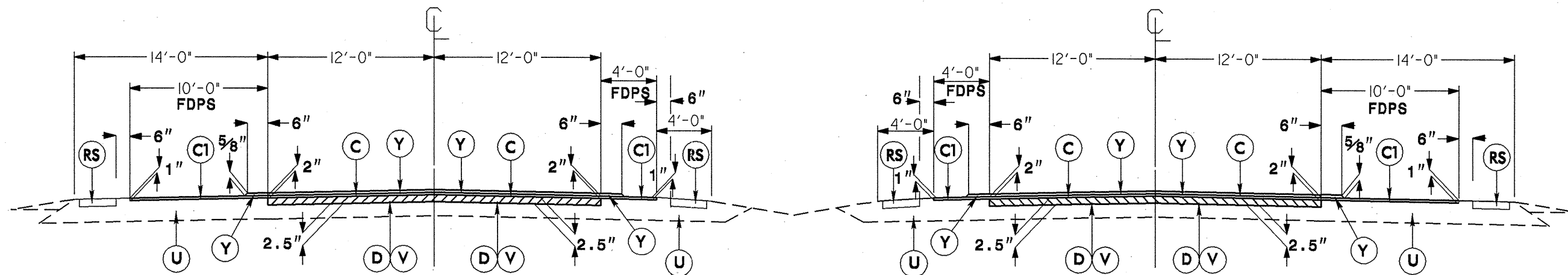
C201591

PROJECT NO.	SHEET NO.	TOTAL SHEET
38735.3.GV1	1	5

I-4723

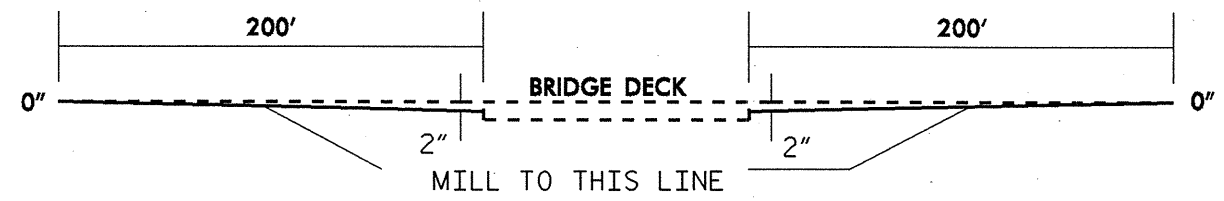


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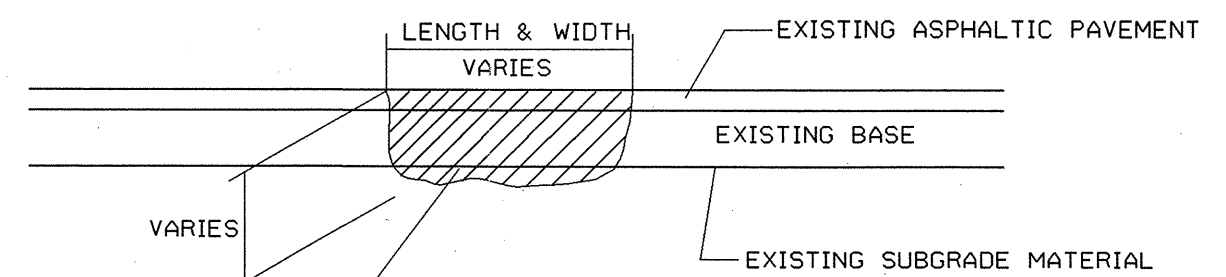


**TYPICAL SECTION NO. 1**

USE TYPICAL SECTION NO. 1  
USE TYPICAL NO. 1 IF USING OPTION NO. J

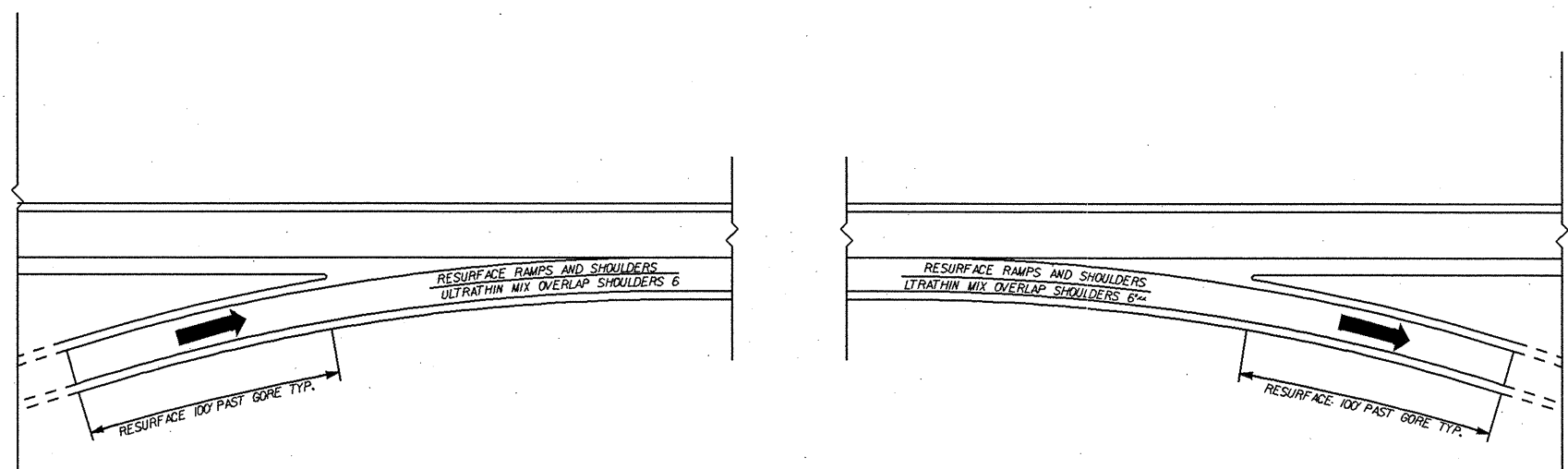


**BRIDGE PROFILE**



MILL EXISTING ASPHALT PAVEMENT AND REMOVE EXISTING LOOSE BASE AND/OR SUBGRADE MATERIAL AND REPLACE WITH ACBC OR ACSC AS DIRECTED BY THE ENGINEER

**PATCHING EXISTING PAVEMENT**

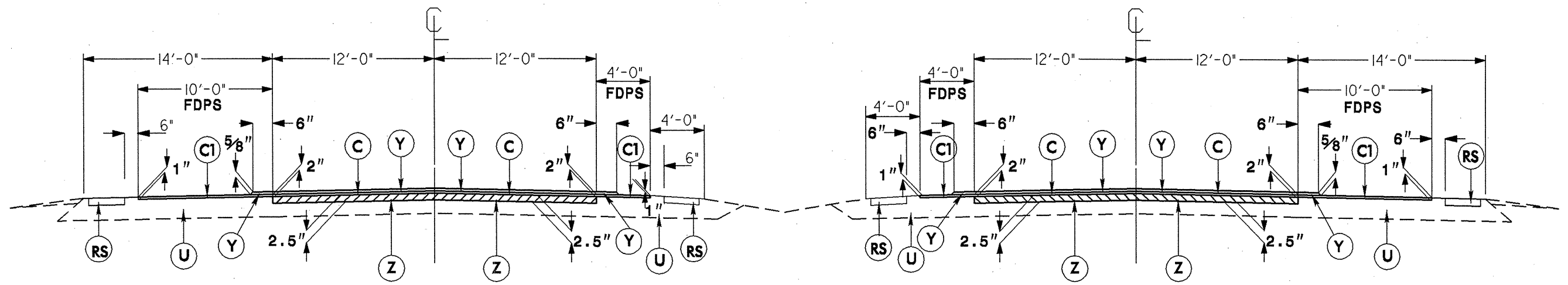


**DETAIL OF GORE AND RAMP RESURFACING**

C	PROP. APPROX. 2" ASPHALT CONCRETE SURFACE COURSE, TYPE 89.6D, AT AN AVERAGE RATE OF 224 LBS. PER SQ. YD.
C1	PROP. VAR. DEPTH ASPHALT CONCRETE SURFACE COURSE, TYPE 89.6D AT AN AVERAGE RATE OF 112 LBS. PER SQ. YD. PER 1" DEPTH. TO BE PLACED IN LAYERS NOT TO EXCEED 2" IN DEPTH.
D	PROP. APPROX. 2 1/2" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 119.0C, AT AN AVERAGE RATE OF 288 LBS. PER SQ. YD.
RS	PROP. MILLED RUMBLE STRIPS
U	EXISTING PAVEMENT.
V	MILLING BITUMINOUS PAVEMENT. 2 1/2" DEPTH.
Y	PROP. APPROX. 5/8" ULTRA-THIN BONDED WEARING COURSE, AT AN AVERAGE RATE OF 70 LBS. PER SQ. YD. PER 5/8" DEPTH

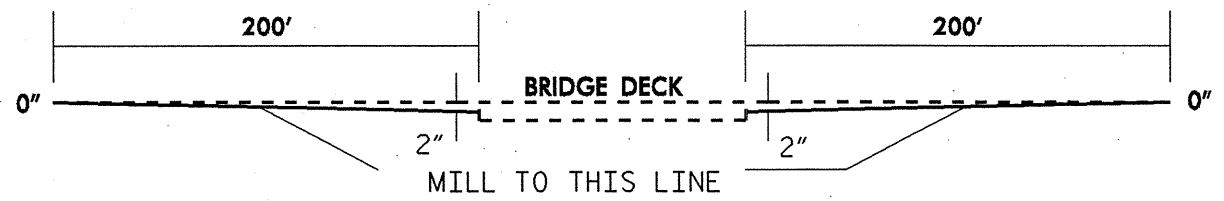
NOTE: PAVEMENT EDGE SLOPES 1:1 UNLESS OTHERWISE SHOWN

5/14/99

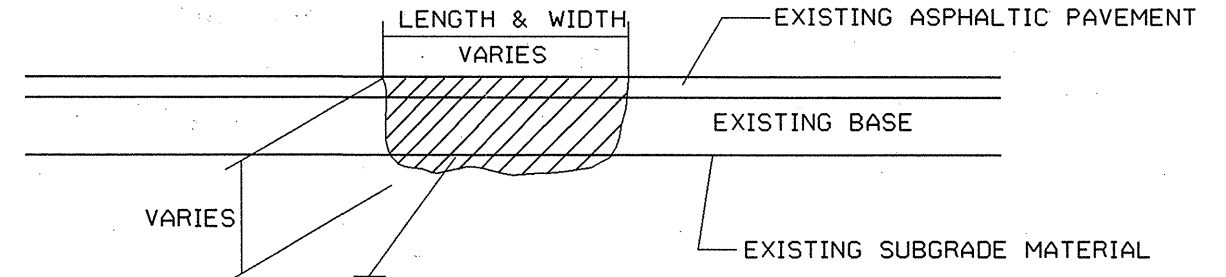


**TYPICAL SECTION NO. 2**

USE TYPICAL SECTION NO. 2  
 USE TYPICAL NO.2 IF USING OPTION NO.2

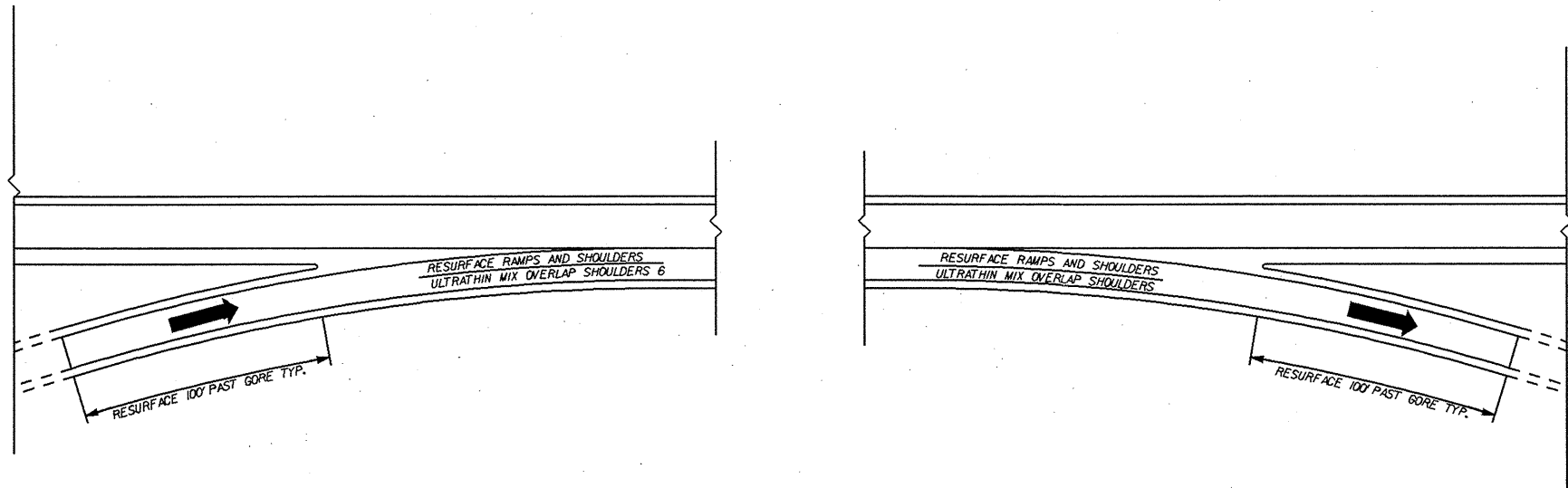


**BRIDGE PROFILE**



MILL EXISTING ASPHALT PAVEMENT AND REMOVE EXISTING LOOSE BASE AND/OR SUBGRADE MATERIAL AND REPLACE WITH ACBC OR ACSC AS DIRECTED BY THE ENGINEER

**PATCHING EXISTING PAVEMENT**



*DETAIL OF GORE AND RAMP RESURFACING*

C	PROP. APPROX. 2" ASPHALT CONCRETE SURFACE COURSE, TYPE 89.5D, AT AN AVERAGE RATE OF 224 LBS. PER SQ. YD.
C1	PROP. VAR. DEPTH ASPHALT CONCRETE SURFACE COURSE, TYPE 89.5D AT AN AVERAGE RATE OF 112 LBS. PER SQ. YD. PER 1" DEPTH. TO BE PLACED IN LAYERS NOT TO EXCEED 2" IN DEPTH.
RS	PROP. MILLED RUMBLE STRIPS
U	EXISTING PAVEMENT.
Y	PROP. APPROX. 5/8" ULTRA-THIN BONDED WEARING COURSE, AT AN AVERAGE RATE OF 70 LBS. PER SQ. YD. PER 5/8" DEPTH
Z	HOT IN PLACE RECYCLING

NOTE: PAVEMENT EDGE SLOPES 1:1 UNLESS OTHERWISE SHOWN

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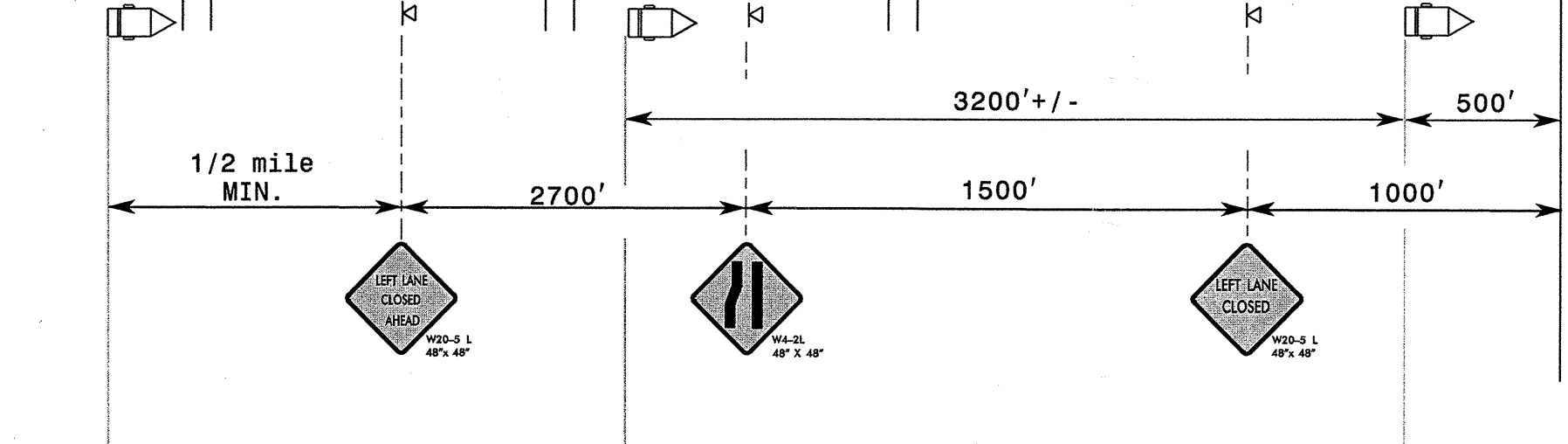
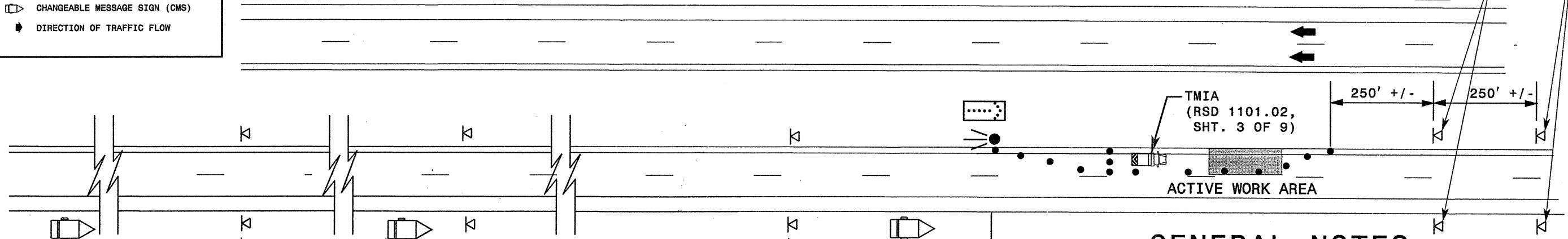
																	PROJECT NO.	SHEET NO.	TOTAL SHEETS								
																	38735.3.GV1	4	5								
SUMMARY OF QUANTITIES																											
								BASE QUANTITIES									OPTION 1				OPTION 2						
PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LENGTH MI	WIDTH FT	SURFACE COURSE, S9.5D TONS	MILLED RUMBLE STRIPS LF	INCIDENTAL MILLING SY	ULTRATHIN ASPHALT TYPE B TONS	APPLICATION OF ULTRATHIN SY	PATCHING EXISTING PAVEMENT TONS	PG 76-22 PLANT MIX TONS	PG 70-28 PLANT MIX TONS	STEEL BM GUARDRAIL LF	ADDITIONAL GUARDRAIL POSTS EA	GUARDRAIL ANCHOR UNITS, TYPE 350 EA	PORTABLE LIGHTING LS	2.5" MILLING SY	INTERMEDIATE COURSE, I19.0C TONS	PG 64-22 PLANT MIX TONS	HIP REC ASP CONC TYP I19.0C SY	EMULS ASP REJUV AGENT GAL	HOT MIX ASPHALT ADMIX TON	PG 64-22 PLANT MIX TONS	
I-4723 OPTION 1	Iredell	1	I-77	FROM MECKLENBURG COUNTY LINE TO EXIT 45 EXCLUDING WORK ALREADY BEING DONE UNDER PROJECT I-4111	1	12.6	38	61,301	266,152	1,300	15,234.00	426,720	1,500	3,372	793	500	5	5	1.00	354,816	51,573	2,424	0	0	0		
<b>TOTAL FOR PROJ NO. I-4723 OPTION 1</b>						<b>12.6</b>	<b>38</b>	<b>61,301</b>	<b>266,152</b>	<b>1,300</b>	<b>15,234.00</b>	<b>426,720</b>	<b>1,500</b>	<b>3,372</b>	<b>793</b>	<b>500</b>	<b>5</b>	<b>5</b>	<b>1.00</b>	<b>354,816</b>	<b>51,573</b>	<b>2,424</b>	<b>0</b>	<b>0</b>	<b>0</b>		
I-4723 OPTION 2		1	I-77	FROM MECKLENBURG COUNTY LINE TO EXIT 45 EXCLUDING WORK ALREADY BEING DONE UNDER PROJECT I-4111	2	12.6	38	61,301	266,152	1,300	15,234.00	426,720	1,500	3,372	793	500	5	5	1.00	0	0	0	354,816	35,482	8,871	417	
<b>TOTAL FOR PROJ NO. I-4723 OPTION 2</b>						<b>12.6</b>	<b>38</b>	<b>61,301</b>	<b>266,152</b>	<b>1,300</b>	<b>15,234.00</b>	<b>426,720</b>	<b>1,500</b>	<b>3,372</b>	<b>793</b>	<b>500</b>	<b>5</b>	<b>5</b>	<b>1.00</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>354,816</b>	<b>35,482</b>	<b>8,871</b>	<b>417</b>	

																	PROJECT NO.	SHEET NO.	TOTAL NO.
																	38735.3.GV1	5	5
<b>THERMOPLASTIC AND PAINT QUANTITIES</b>																			
PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	4400000000-E STATIONARY WORK ZONE SIGN SF	4405000000-E PORTABLE WORK ZONE SIGN SF	4415000000-E FLASHING ARROW PNL TY C EA	4420000000-N CHANGEABLE MESSAGE SIGNS	4430000000-E DRUMS EA	4480000000-N TRUCK MTD IMP. ATTN. 60 MPH EA	4688000000-E 6" X 90 M WHITE THERMO LF	4688000000-E 6" X 90 M YELLOW THERMO LF	4690000000-E 6" X 120 M WHITE THERMO LF	4700000000-E 12" X 90 M WHITE THERMO LF	4810000000-E 4" YELLOW PAINT LF	4810000000-E 4" WHITE PAINT LF	4820000000-E 8" WHITE PAINT LF	4905000000-N SNOW PLOWABLE MARKERS EA	
I-4723	Iredell	1	I-77	FROM MECKLENBURG COUNTY LINE TO EXIT 45, EXCLUDING WORK ALREADY BEING DONE UNDER PROJECT I-4111	384	128	2.00	3.00	125	1.00	133,056	133,056	33,264	8,000	399,168	498,960	24,000	2,100	
<b>TOTAL FOR PROJ NO. I-4723</b>					<b>384</b>	<b>128</b>	<b>2</b>	<b>3</b>	<b>125</b>	<b>1</b>	<b>133,056</b>	<b>133,056</b>	<b>33,264</b>	<b>8,000</b>	<b>399,168</b>	<b>498,960</b>	<b>24,000</b>	<b>2,100</b>	
<b>GRAND TOTAL</b>					<b>384</b>	<b>128</b>	<b>2</b>	<b>3</b>	<b>125</b>	<b>1</b>	<b>266,112</b>		<b>33,264</b>	<b>8,000</b>	<b>898,128</b>		<b>24,000</b>	<b>2,100</b>	

# ADVANCE WARNING, LANE CLOSURE AND SPEED REDUCTION SIGNS

**LEGEND**

- FLASHING ARROW PANEL (TYPE C)
- TRUCK MOUNTED IMPACT ATTENUATOR (TMIA)
- DRUM
- PORTABLE SIGN
- CHANGEABLE MESSAGE SIGN (CMS)
- DIRECTION OF TRAFFIC FLOW



MESSAGE NO. 1	MESSAGE NO. 2
LEFT LANE CLOSED	MERGE RIGHT

CHANGEABLE MESSAGE SIGN

MESSAGE NO. 1	MESSAGE NO. 2
REDUCE SPEED AHEAD	WORKERS IN ROADWAY

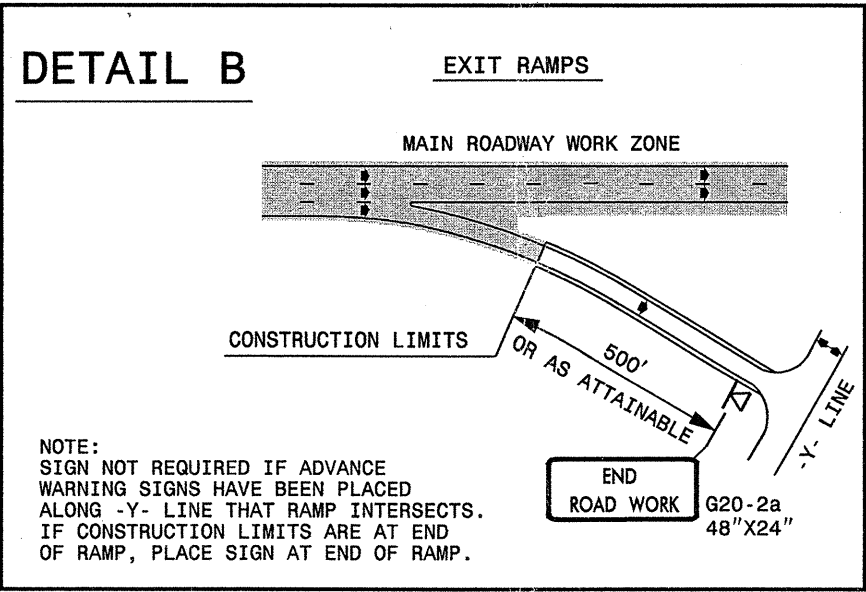
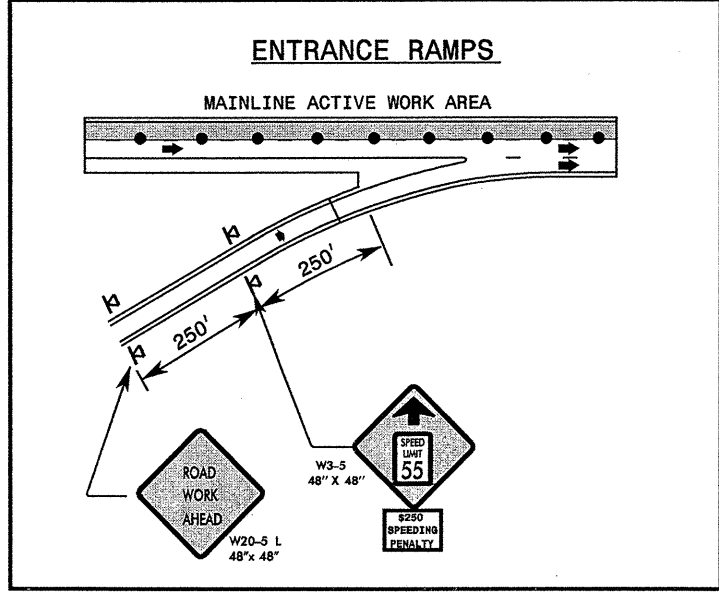
CHANGEABLE MESSAGE SIGN

MESSAGE NO. 1	MESSAGE NO. 2
SPEED LIMIT 55	\$250 SPEEDING PENALTY

CHANGEABLE MESSAGE SIGN

## GENERAL NOTES

- THIS DRAWING IS INTENDED TO SHOW THE CMS LOCATION AND MESSAGING REQUIRED FOR A "TEMPORARY SPEED LIMIT" REDUCTION ON A FREEWAY WHICH IS TO BE REDUCED FROM 65 MPH TO 55 MPH. REFER TO THE ROADWAY STANDARD DRAWING 1101.02 SHEET 3 OF 9 FOR ADDITIONAL LANE CLOSURE REQUIREMENTS AND GENERAL NOTES.
- EACH DIRECTION OF THE PROJECT IS TO BE EVALUATED FOR THE "TEMPORARY SPEED LIMIT" REDUCTION. THIS DRAWING INTENTIONALLY HAS 1 DIRECTION SIGNED AS A REMINDER TO CAREFULLY CONSIDER WHETHER BOTH DIRECTIONS OF THE PROJECT NEED TO HAVE THE SPEED LIMIT REDUCED.
- IN ADDITION, FOR ACTIVE WORK AREAS THAT EXCEED 1 MILE IN LENGTH, AN EVALUATION IS TO BE MADE TO DETERMINE IF ADDITIONAL CMS'S ARE NEEDED TO SUPPLEMENT THE INITIAL ONES. PORTABLE MOUNTED W3-5 SIGNS WITH SPEED PENALTY SIGNS ARE TO BE PLACED ALONG ENTRANCE RAMPS LOCATED WITHIN THE ACTIVE WORK AREA.
- THE \$250 SPEEDING PENALTY APPLIES FOR ALL PROJECTS THAT QUALIFY FOR A "TEMPORARY SPEED LIMIT" REDUCTION.
- THE "TEMPORARY SPEED LIMIT" REDUCTION IS ONLY IN EFFECT WHEN WORKERS ARE PRESENT. WHEN WORKERS ARE NO LONGER PRESENT, THE SPEED LIMIT AND SPEED PENALTY MESSAGES SHALL BE REMOVED AND THE SIGNS SHALL BE TURNED OFF UNLESS OTHER PERTINENT MESSAGING NEED TO BE DISPLAYED AS DIRECTED BY THE ENGINEER IN COORDINATION WITH THE WORK ZONE TRAFFIC CONTROL UNIT (919-250-4159). AT THE COMPLETION OF THE ACTIVITY, THE ENGINEER SHALL NOTIFY THE REGIONAL TRAFFIC ENGINEER TO REScind THE ORDINANCE.
- WHEN "TEMPORARY SPEED LIMIT" REDUCTIONS ARE IN EFFECT, THE CONTRACTOR IS TO COVER ANY EXISTING SPEED LIMIT SIGNS LOCATED WITHIN THE ACTIVE WORK AREA THAT CONFLICT WITH THE "TEMPORARY SPEED LIMIT" REDUCTION.
- USE FLUORESCENT ORANGE SHEETING (TYPE VII OR HIGHER) ON ALL ADVANCED WORK ZONE SIGNS.
- DO NOT INSTALL ADVANCE WARNING SIGNS MORE THAN 3 DAYS PRIOR TO BEGINNING OF WORK.
- ALL SIGN SPACING DIMENSIONS ARE APPROXIMATE, FIELD ADJUST AS NECESSARY OR AS DIRECTED.
- USE PORTABLE WORK ZONE SIGNS ONLY WITH PORTABLE WORK ZONE SIGN STANDS SPECIFICALLY DESIGNED FOR ONE ANOTHER. PORTABLE WORK ZONE SIGNS MAY BE ROLL UP OR APPROVED COMPOSITE.
- PROVIDE PORTABLE WORK ZONE SIGN STANDS, PORTABLE SIGNS AND SIGN SHEETING WHICH ARE LISTED ON THE NORTH CAROLINA DEPARTMENT OF TRANSPORTATION'S APPROVED PRODUCT LIST OR ACCEPTED AS TRAFFIC QUALIFIED BY THE TRAFFIC CONTROL UNIT.
- \*\* TWO-WAY UNDIVIDED ADVANCE WARNING SIGN CONFIGURATION MAY BE USED ON MULTI-LANE FACILITIES WHERE CONDITIONS LIMIT THE USE OF DUAL MOUNTED SIGNS AS DETERMINED BY THE ENGINEER.
- COORDINATE USE OF OVERHEAD DMSS WITH THE DIVISION.
- SYMBOLS ARE SHOWN FOR RIGHT LANE CLOSURE. USE APPROPRIATE SIGNS, MESSAGES AND ARROWS FOR LEFT LANE CLOSURE. CHANGEABLE MESSAGE SIGNS (CMS) ARE TO REMAIN POSITIONED AS SHOWN.



NOTE:  
SIGN NOT REQUIRED IF ADVANCE WARNING SIGNS HAVE BEEN PLACED ALONG -Y- LINE THAT RAMP INTERSECTS. IF CONSTRUCTION LIMITS ARE AT END OF RAMP, PLACE SIGN AT END OF RAMP.

APPROVED: *Chao Lanford* DATE: 5/1/07

**ADVANCE WARNING, LANE CLOSURE AND SPEED REDUCTION SIGNS FOR 10 MPH**

SCALE: NONE	REVISIONS
DATE: 02/15/07	04/07
DWG. BY: PS	05/07
DESIGN BY: CLL	
REVIEWED BY: JSK	

01-MAY-2007 14:49  
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