



STATE OF NORTH CAROLINA
DEPARTMENT OF TRANSPORTATION

MICHAEL F. EASLEY
GOVERNOR

LYNDO TIPPETT
SECRETARY

June 13, 2007

Addendum No. 1

RE: Contract ID: C201749
WBS# 40248.3.GV1
F. A. # IMS-40-3 (119) 148
Orange, Alamance Counties (I-4918)
I-40 / I-85 From NC-54 (MP 148) In Alamance County to
West of SR-1114 (Buckhorn Rd.) in Orange County

June 19, 2007 Letting

To Whom It May Concern:

Reference is made to the Sketch Maps and Proposal Form recently furnished to you on this project.

The following revisions have been made to the Proposal Form:

On Page No. 24, Table 1, a change has been made in the row entitled "Rutting". Please void Page No. 24 in your proposal and staple the revised Page No. 24 thereto.

On Page No. 25, a sentence was added under "Measurement and Payment". Please void Page No. 25 in your proposal and staple the revised Page No. 25 thereto.

On Page No. 1 of the Item Sheets, the quantity for Line Item "15-1524200000-E-610 Asphalt Concrete Surface Course, Type S9.5D" has been revised. By copy of this addendum the quantity is hereby reduced from 112,596 Tons to 97,123 Tons. The Contractor's bid price must be based on this revised pay item quantity.

On Page No. 2 of the Item Sheets, by copy of this addendum the quantity for Line Item "16-156000000-E-620 Asphalt Binder for Plant Mix, Grade PG 64-22" is hereby increased from 256 Tons to 1,184 Tons. Also on Page No. 2 of the Item Sheets, by copy of this addendum the quantity for Line Item "18-157000000-E-620 Asphalt Binder for Plant Mix, Grade PG 76-22" is hereby decreased from 6,198 Tons to 5,348 Tons. The Contractor's bid price must be based on these revised pay item quantities. The contract will be prepared accordingly.

MAILING ADDRESS:
NC DEPARTMENT OF TRANSPORTATION
DESIGN SERVICES UNIT
1591 MAIL SERVICE CENTER
RALEIGH NC 27699-1591

TELEPHONE: 919-250-4128
FAX: 919-250-4119

WEBSITE: WWW.DOH.DOT.STATE.NC.US

LOCATION:
CENTURY CENTER COMPLEX
ENTRANCE B2
1020 BIRCH RIDGE DRIVE
RALEIGH NC

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On Page No. 4 of the Item Sheets, by copy of this addendum the following new Pay Item is hereby added: "51-1519000000-E-610 Asphalt Concrete Surface Course, Type S9.5B" (Quantity = 15, 464 Tons). The Contractor's bid price must include this new pay item. The contract will be prepared accordingly.

The Expedite File has been updated to reflect this revision. Please download the Expedite Addendum File and follow the instructions for applying the addendum. Bid Express will not accept your bid unless the addendum has been applied.

The following revisions have been made to the Sketch Maps:

On Sheet No. 2, the Summary of Quantities has been revised to reflect the above mentioned quantity changes. Please void Sheet No. 2 in your Sketch Maps and staple the revised Sheet No. 2 thereto.

On Sheet No. 4, the Typical Sections have been revised to change the Shoulder Pavement Design from ACSC Type S9.5D to ACSC Type S9.5B. Please void Sheet No. 4 in your Sketch Maps and staple the revised Sheet No. 4 thereto.

As a matter of information to prospective bidders, to assist you in preparation of your bids, the Department is providing data showing surface rutting measurements on several sections of I-40 and /or I-85 in Alamance, Orange and Durham Counties. The data was obtained utilizing the Department's high speed profilometer. This information is available under the link "Plans, Proposals and Addendums" for this project on our web site at:
<http://www.ncdot.org/doh/preconstruct/ps/contracts/letting.html>

The data set labeled "I-40 Rutting and IRI Readings" on the web site contains data showing rutting and IRI readings on I-85/I-40 in Alamance and Orange Counties over a period of several years. It should be noted that the readings run from county line to county line.

The data set labeled " I-40/I-85 Active Project" on the web site contains data from an active construction project. This project is I-40/I-85 from milepost 145.66 to milepost 148.37 in Alamance County. The data shows Lane 1, 2 etc. Lane 1 is the outside lane and the lane numbers proceed toward the median.

The data set labeled " Durham County Rutting and IRI Readings" on the web site contains Data showing Rutting and IRI readings on I-40 EB and I-85 NB in Durham county over a period of several years.

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Orange, Alamance Counties (I-4918)

We hope you find this information beneficial in the preparation of your bid.

Sincerely,



R. A. Garris, PE
Contract Officer

RAG/jag/blr
Attachment

cc: Mr. W. F. Rosser, PE
Mr. W. S. Varnedoe, PE
Mr. E. C. Powell, PE
Mr. J. M. Mills, PE
Ms. D. M. Barbour, PE
Mr. Art McMillan, PE
Mr. J. V. Barbour, PE
Mr. Mark Staley (2)
Mr. Robert Memory
Mr. R. E. Davenport, Jr., PE
Ms. Norma Smith
Ms. Marsha Byrd
Mr. Ronnie Higgins
Ms. Taylor Mishoe
Project File (2)

**Table 1
Required corrective action for levels of distress for Asphalt Concrete Pavement**

Distress Type	Allowable Extent of Severity	Corrective Action
Rutting	<p style="text-align: center;">1 mile sections.</p> <p>A section will be considered to fail if 3 consecutive 0.1 mile test results (0.3 mile total) exceeds 0.25 inch in either wheelpath as measured by high-speed electronic profilometer</p>	Mill and fill the affected travel lane with Asphalt Concrete Pavement
Bleeding	Loss of surface texture due to excess asphalt, individual length ≥ 10 feet and ≥ 1 foot in width	Remove and replace the distressed area to the full distressed depth and to a minimum surface area of 150% of the distressed area.
Pot Holes and Slippage	Any potholes	Remove and replace the distressed area to the full distressed depth and to a minimum surface area of 150% of each distressed area
Raveling and/or Delamination	<p>Any delamination or any moderate or severe raveling Individual Length ≥ 10 feet</p> <p>Any delamination or any moderate or severe raveling Individual Length < 10 feet</p>	<p>Remove and replace distressed area, and 50 feet in both directions from the distressed area, to the full distressed depth and full lane width</p> <p>Patch the distressed area to the full distressed depth and to a minimum of 150% of each distressed area</p>

If a measured distress value indicates remedial action is required per Table 1, the Contractor shall begin remedial work within 45 calendar days of written notification by the Department. The Engineer shall determine the allowable duration for the completion of the remedial work, not to exceed 6 months. However, after each survey, if the Engineer determines the extent and magnitude of pavement distresses in any areas shall not affect the traffic safety, strength of pavement structure or highway aesthetics, the remedial work associated with the pavement distresses may be deferred until later in the guarantee period.

If remedial action is necessary and forensic information is required to determine the source of the distress, the Department may core and trench the pavement. The Contractor will not be responsible for damages to the pavement as a result of any forensic activities conducted by the Department.

The Contractor shall perform remedial work that is determined by the Department to be their responsibility. If, in the opinion of the Engineer, the problem poses an immediate danger to the traveling public and the Contractor cannot begin remedial work within 72 hours of written notification, the Engineer shall have the remedial work performed by other forces. The Contractor shall be responsible for all incurred costs of the work performed by other forces should the problem (remedial work) be determined to be the responsibility of the Contractor. Remedial work performed by other forces does not alter any of the requirements, responsibilities or obligations of the Contractor.

The Contractor shall complete all remedial work to the satisfaction of the Engineer. Approval of remedial work does not relieve the Contractor from continuing responsibility under the provisions of this Specification. The 3 year guarantee period will not be considered to be satisfactorily completed until all outstanding remedial work directed to be performed during the 3 year period has been completed.

Notify the Engineer in writing prior to beginning any remedial work. Perform all signing, pavement markings, markers, and traffic control in accordance with the *Standard Drawings*. Lane closure restrictions listed in the original contract shall apply to remedial work. Written requests to obtain permission for lane closures for either forensic investigation or remedial work shall be made to the Engineer 48 hours in advance of any lane closures. Do not perform any lane closures until written permission is given by the Engineer.

Materials

If remedial work necessitates a corrective action to overlying asphalt layers, pavement markings, signal loops, adjacent lanes, roadway shoulders, or other affected contract work, perform these corrective actions using materials meeting the material requirements shown in the contract documents.

Materials not meeting the contract requirements shall be removed and replaced with materials meeting the contract requirements.

Contractor's Failure to Perform

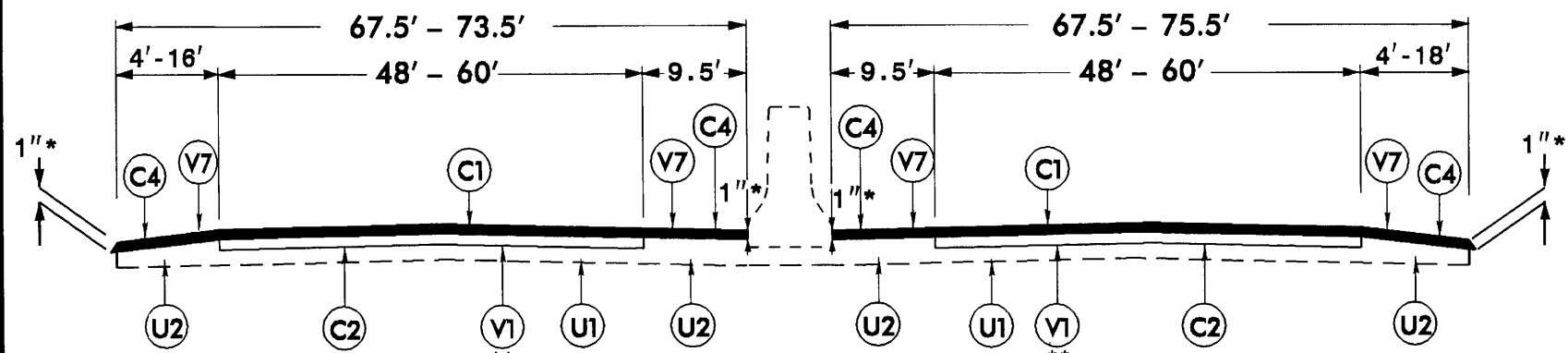
Should the Contractor fail to submit any dispute in a timely manner to the Dispute Review Committee, fail to satisfactorily perform any remedial work, or fail to compensate the Department for any remedial work performed by others which was determined to be the Contractor's responsibility, the Department may remove the Contractor from the Department's prequalified bidder's list, for at least 6 months or until the remedial work has been satisfactorily performed or full and complete payment for remedial work performed by others is made to the Department, whichever is longer.

If the Contractor is removed from the prequalified bidder's list, the Contractor will not be permitted to perform as a subcontractor or material supplier, including bidding or working on Purchase Order contracts.

Measurement and Payment

Measurement will not be made of any of the work required to be performed to satisfy the guarantee requirements. Payment at the contract lump sum price for Pavement Guarantee will be considered full compensation for furnishing all maintenance of traffic during remedial work, materials, labor, tools, equipment, and incidentals necessary to complete the requirements of this provision. Payment for this item will be made as part of the last partial pay estimate made for the other work performed under this contract. The provisions of Article 104-6 of the Standard Specifications may only be used with this item by direct approval of the State Highway Administrator.

PROJECT NO.	SHEET NO.	TOTAL SHEETS
40248.3.GV1 (I-4918)	4	6



TYPICAL SECTION NO. 1

* Taper paved shoulder to 1" depth at median barrier wall and at earth shoulder.

** V1 - Mill travel lanes only, do not mill shoulders

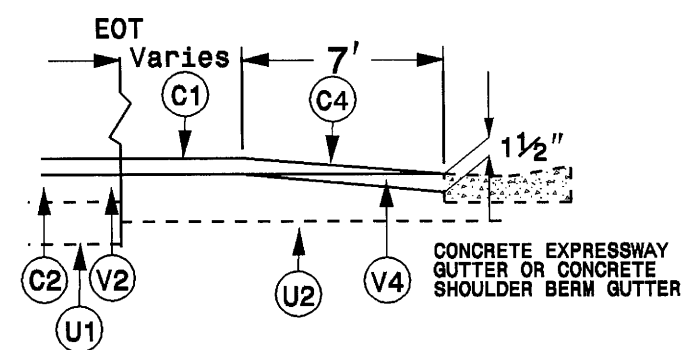
Use Typical Section No. 1 on Maps 1 and 10

NOTE: STA 0+00 EB/NB - STARTS AT NEW PAVEMENT JOINT EAST OF NC 54

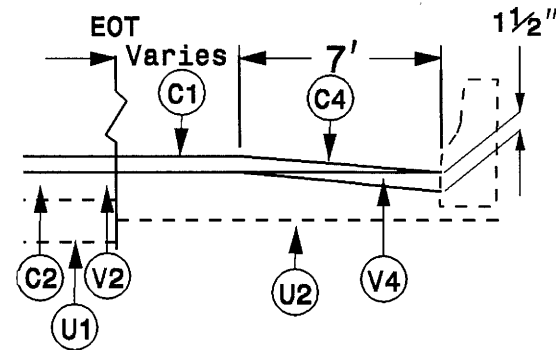
STA 0+00 WBSB - STARTS AT NEW PAVEMENT JOINT 1.1 MILES EAST OF THE ALAMANCE/ORANGE COUNTY LINE

WBSB STA 58+54 LB=STA 0+00 LA AT THE ORANGE/ALAMANCE COUNTY LINE

DO NOT PAVE BRIDGES AT THE FOLLOWING LOCATIONS:
 EB/NB STA 48+07 TO STA 51+80
 EB/NB STA 96+04 TO STA 98+01
 WBSB STA 254+89 TO STA 256+85
 WBSB STA 301+76 TO STA 305+50



MILLING TYPICAL SECTION 1
TO BE USED IN CONJUNCTION WITH TS # 1 & 3



MILLING TYPICAL SECTION 2
TO BE USED IN CONJUNCTION WITH TS # 1

PAVEMENT SCHEDULE		
C1	PROP. APPROX. 1 1/2" ASPHALT. CONC. SURFACE COURSE, TYPE S9.5D, AT AN AVERAGE RATE OF 188 LBS. PER SQ. YD.	
C2	PROP. APPROX. 2" ASPHALT. CONC. SURFACE COURSE, TYPE S9.5D, AT AN AVERAGE RATE OF 224 LBS. PER SQ. YD.	
C3	PROP. APPROX. 3" ASPHALT. CONC. SURFACE COURSE, TYPE S9.5D, AT AN AVERAGE RATE OF 188 LBS. PER SQ. YD. IN EACH OF TWO LAYERS	
C4	PROP. APPROX. 1 1/2" ASPHALT. CONC. SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 188 LBS. PER SQ. YD.	
D1	PROP. APPROX. 2 1/2" ASPHALT. CONC. INTERMEDIATE COURSE, TYPE I19.0D, AT AN AVERAGE RATE OF 285 LBS. PER SQ. YD.	
D2	PROP. APPROX. 3" ASPHALT. CONC. INTERMEDIATE COURSE, TYPE I19.0D, AT AN AVERAGE RATE OF 342 LBS. PER SQ. YD.	
D3	PROP. APPROX. 4" ASPHALT. CONC. INTERMEDIATE COURSE, TYPE I19.0D, AT AN AVERAGE RATE OF 458 LBS. PER SQ. YD.	
E1	PROP. APPROX. 8" ASPHALT. CONC. BASE COURSE, TYPE B25.0C, AT AN AVERAGE RATE OF 458 LBS. PER SQ. YD. IN EACH OF TWO LAYERS	
E2	PROP. APPROX. 5" ASPHALT. CONC. BASE COURSE, TYPE B25.0C, AT AN AVERAGE RATE OF 570 LBS. PER SQ. YD.	
J	EXISTING AGGREGATE BASE COURSE OR CTABC	
R1	EXISTING SHLDR. BERM GUTTER	U1 EXISTING TRAVELWAY.
R2	EXISTING 2-6 CURB AND GUTTER	U2 EXISTING PAVED SHOULDER.
MILLING SCHEDULE		
V1	MILLING BITUMINOUS PAVEMENT, 2" DEPTH.	
V2	MILLING BITUMINOUS PAVEMENT, 2 1/2" DEPTH.	
V3	MILLING BITUMINOUS PAVEMENT, 1 1/2" DEPTH.	
V4	MILLING BITUMINOUS PAVEMENT, 0" to 1 1/2" DEPTH.	
V5	MILLING BITUMINOUS PAVEMENT, 2" to 3 1/2" DEPTH.	
V6	MILLING BITUMINOUS PAVEMENT, 3 1/2" DEPTH.	
V7	MILLED RUMBLE STRIP. use in conjunction with Standard Drawing # 685.01	
V8	MILLING BITUMINOUS PAVEMENT, 11" DEPTH.	

\$\$\$SYTIME\$\$\$
 \$\$\$DGN\$\$\$
 \$\$\$FRM\$\$\$
 \$\$\$APP\$\$\$
 \$\$\$CUL\$\$\$
 \$\$\$UD\$\$\$
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PROJECT NO.	SHEET NO.	TOTAL NO.
40248.3.GV1	2	6

SUMMARY OF QUANTITIES

PROJECT NO.	COUNTY	MAP	ROUTE	DESCRIPTION	TYP	LENGTH	WIDTH	COMPREHENSIVE GRADING	REMOVAL OF EXISTING ASPHALT PAVEMENT	FABRIC FOR SOIL STABILIZATION	3.5" MILLING	1 1/2" MILLING	11" MILLING	2.5" MILLING	2" MILLING	0" TO 1.5" MILLING	2" TO 3.5" MILLING	MILLED RUMBLE STRIPS	INCIDENTAL MILLING	BASE COURSE, B25.0C	INTERMEDIATE COURSE, I19.0D	SURFACE COURSE, S9.5B	SURFACE COURSE, S9.5D	PG 64-22 PLANT MIX	PG 70-22 PLANT MIX	PG 76-22 PLANT MIX	ADJ. OF GRATES AND/OR FRAMES ON DROP INLETS	REPAIR OF DROP INLETS	SEED & MULCHING	TRENCHING (UNPAVED) (1 X 2")	PULL BOX (STANDARD)	INDUCTIVE LOOP SAW CUT	LEAD-IN CABLE (18-2)	LEAD-IN CABLE (18-4)	PORTABLE LIGHTING		
NO		NO			NO	MI	FT	LS	SY	SY	SY	SY	SY	SY	SY	SY	SY	LF	SY	TONS	TONS	TON	TON	TONS	TONS	TONS	EA	EA	AC	LF	EA	LF	LF	LF	LF	LS	
40248.3.GV1	Alamance & Orange	1	I-85 NB/I-40 EB	FROM NC 54 IN ALAMANCE TO 1.1 MILES EAST OF THE ALAMANCE/ORANGE COUNTY LINE	1	7.802	67.5 - 75.5				2,734				219,370	4,530	11,146	81,264	6,000			7,879	45,475	473		2,505	56	20				500				"	
	Alamance	2	OFF RAMP EB - JIMMIE KERR ROAD EXIT	EXIT 150	2	0.136	24 - 55							1,797								258		228		12	13					205					"
	Alamance	3	ON RAMP EB - JIMMIE KERR ROAD EXIT	EXIT 150	3	0.199	24					2,972												251			14										"
	Alamance	4	OFF RAMP EB - TROLLINGWOOD RD EXIT	EXIT 152	4	0.178	23	LS	2,958	1,000											1,379	682		567	58	32	31		0.35								"
	Alamance	5	ON RAMP EB - TROLLINGWOOD RD EXIT	EXIT 152	4	0.195	22	LS	3,046	1,000											1,417	700		514	61	33	29		0.35								"
	Alamance	6	OFF RAMP EB - NC 119 EXIT	EXIT 153	5	0.172	23 - 32							1,034										368			20					213				"	
	Alamance	7	ON RAMP EB - NC 119 EXIT	EXIT 153	3	0.187	22 - 38						2,766											233			13									"	
	Alamance	8	OFF RAMP EB - MEBANE OAKS RD EXIT	EXIT 154	5	0.12	22 - 48					458												281			15			10	1	410	10	10		"	
	Alamance	9	ON RAMP EB - MEBANE OAKS RD EXIT	EXIT 154	3	0.138	24 - 25					2,305												195			10									"	
	Orange & Alamance	10	I-85 SB/I-40 WB	FROM 1.1 MILES EAST OF THE ALAMANCE/ORANGE COUNTY LINE TO NC 54 IN ALAMANCE	1	7.855	67.5 - 73.5					2,817		4,547		223,794	4,098	11,361	81,816	6,000	1,296	1,037	7,585	46,871	511	49	2,580						500				"
	Alamance	11	OFF RAMP WB - MEBANE OAKS ROAD	EXIT 154	3	0.169	24 - 45					3,583												302			16					410					"
	Alamance	12	ON RAMP WB - MEBANE OAKS ROAD	EXIT 154	3	0.11	24					1,549												131			7									"	
	Alamance	13	OFF RAMP WB - NC 119	EXIT 153	5	0.158	24 - 36					961												340			19					213				"	
	Alamance	14	ON RAMP WB - NC 119	EXIT 153	3	0.206	24					2,992												253			14								"		
	Alamance	15	OFF RAMP WB - TROLLINGWOOD ROAD	EXIT 152	4	0.156	24 - 28	LS	2,207	1,000											1,038	511		442	44	25	24		0.30							"	
	Alamance	16	ON RAMP WB - TROLLINGWOOD ROAD	EXIT 152	4	0.129	24	LS	1,819	1,000											851	420		308	37	20	17		0.25						"		
	Alamance	17	OFF RAMP WB - JIMMIE KERR ROAD EXIT	EXIT 150	3	0.144	24					2,075												175			10					205				"	
	Alamance	18	ON RAMP WB - JIMMIE KERR ROAD EXIT	EXIT 150	2	0.158	24							2,244									323		189		15	11							"		
TOTAL FOR PROJ NO. 40248.3.GV1						18.212		1	10,030	4,000	5,551	20,695	4,547	4,041	443,164	8,628	22,507	163,080	12,000	5,981	3,931	15,464	97,123	1,184	186	5,348	56	20	1.25	10	1	2,656	10	10	LS		

NOTE: THERE IS NO SEPARATE PAYMENT FOR REMOVAL OF EXISTING ASPHALT PAVEMENT. IT IS INCLUDED IN THE LUMP SUM GRADING.