



STATE OF NORTH CAROLINA  
DEPARTMENT OF TRANSPORTATION

MICHAEL F. EASLEY  
GOVERNOR

LYNDO TIPPETT  
SECRETARY

June 12, 2007

**Addendum No. 1**

RE: Contract ID: C201793

WBS# 41454.3.GV1

F. A. # IMS-040-1 (176) 27

**Haywood County (I-5004)**

I-40 From US-74 (MP 27 ) to Buncombe County Line

**June 19, 2007 Letting**

To Whom It May Concern:

Reference is made to the Sketch Maps and Proposal Form recently furnished to you on this project.

The following revisions have been made to the Proposal Form:

On Page No. 1, the first paragraph of the Project Special Provision entitled "Intermediate Contract Time Number 1 and Liquidated Damages" has been revised. Please void Page No. 1 in your proposal and staple the revised Page No. 1 thereto.

On Page Nos. 19 thru 22, the Project Special Provision entitled "Final Surface Testing - Asphalt Pavements (Rideability)" has been deleted. Please void Page Nos. 19 thru 22 in your proposal and staple the revised Page Nos. 19 thru 22 thereto.

On Page No. 39, the Project Special Provision entitled " Convert Existing Drop Inlet to Traffic Bearing Drop Inlet" has been revised. Please void Page No. 39 in your proposal and staple the revised Page No. 39 thereto.

**MAILING ADDRESS:**  
NC DEPARTMENT OF TRANSPORTATION  
DESIGN SERVICES UNIT  
1591 MAIL SERVICE CENTER  
RALEIGH NC 27699-1591

TELEPHONE: 919-250-4128  
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WEBSITE: [WWW.DOH.DOT.STATE.NC.US](http://WWW.DOH.DOT.STATE.NC.US)

**LOCATION:**  
CENTURY CENTER COMPLEX  
ENTRANCE B2  
1020 BIRCH RIDGE DRIVE  
RALEIGH NC

Page 2

RE: Contract ID: C201793

WBS# 41454.3.GV1

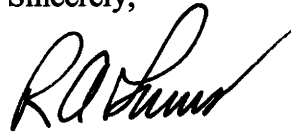
F. A. # IMS-040-1 (176)27

**Haywood County (I-5004)**

The following revision has been made to the Sketch Maps:

In the project Sketch Maps, after Page No. 8, the Detail to Convert Existing Drop Inlet to Traffic Bearing Drop Inlet has been revised. Please void the current sheet in the Sketch Maps and staple the revised sheet thereto.

Sincerely,



R. A. Garris, PE  
Contract Officer

RAG/jag/blr  
Attachment

cc: Mr. W. S. Varnedoe, PE  
Mr. E. C. Powell, PE  
Mr. J. B. Setzer, PE  
Ms. D. M. Barbour, PE  
Mr. Art McMillan, PE  
Mr. J. V. Barbour, PE  
Mr. Mark Staley (2)  
Mr. Robert Memory  
Mr. R. E. Davenport, Jr., PE  
Ms. Norma Smith  
Ms. Marsha Sample  
Ms. Marsha Byrd  
Mr. Ronnie Higgins  
Ms. Taylor Mishoe  
Project File (2)

**PROJECT SPECIAL PROVISIONS**

**GENERAL**

**CONTRACT TIME AND LIQUIDATED DAMAGES:**

(7-1-95)(Rev. 12-19-06)

RG 06

The date of availability for this contract is **August 6, 2007**.

The completion date for this contract is **August 1, 2008**.

When observation periods are required by the contract, they are not a part of the work to be completed by the completion date and/or intermediate contract times stated in the contract. Should an observation period extend beyond the final completion date, the acceptable completion of the observation period shall be a part of the work covered by the performance and payment bonds.

The liquidated damages for this contract are **Eight Hundred Dollars (\$800.00)** per calendar day.

**INTERMEDIATE CONTRACT TIME NUMBER 1 AND LIQUIDATED DAMAGES:**

Except for the work required for the placement of the Open Graded Asphalt Friction Course FC-1 Modified, Polyurea Pavement Marking Lines, Polyurea Pavement Marking Symbols, snowplowable pavement markers, and Sign Erection, the Contractor will be required to complete all work included in this contract and shall place and maintain traffic on same by **November 15, 2007**.

The date of availability for this intermediate contract time is **August 6, 2007**.

The liquidated damages for this intermediate contract time are **One Thousand Dollars (\$1,000.00)** per calendar day.

**INTERMEDIATE CONTRACT TIME NUMBER 2 AND LIQUIDATED DAMAGES**

(2-20-07)

RG 07 A

The Contractor shall complete the required work of installing, maintaining, and removing the traffic control devices for lane closures and restoring traffic to a **four-lane, two-way** traffic pattern. The Contractor shall not close or narrow a lane of traffic on **I-40 and Ramps** during the following time restrictions:

**DAY AND TIME RESTRICTIONS**

**MONDAY THRU SUNDAY 6:00 A.M. TO 7:00 P.M.**

**19**

**INTENTIONALLY**

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**INTENTIONALLY**

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**INTENTIONALLY**

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The 1 inch milling on map 1, I-40 East, is for removal of the existing OGAFc in the travel lanes from mile marker 34.46 to the Buncombe County Line.

**RESURFACING EXISTING BRIDGES:**

(7-1-95)

RR 61

The Contractor's attention is directed to the fact that he will be required to resurface the bridges on this project if directed by the Engineer.

Place the surface so as to follow a grade line set by the Engineer with the minimum thickness as shown on the sketch herein or as directed by the Engineer. State Forces will make all necessary repairs to the bridge floors prior to the time that the Contractor places the proposed surfacing. Give the Engineer at least 15 days notice prior to the expected time to begin operations so that State Forces will have sufficient time to complete their work.

At all bridges that are not to be resurfaced, taper out the proposed resurfacing layer adjacent to the bridges to insure a proper tie-in with the bridge surface.

**CONVERT EXISTING DROP INLET TO TRAFFIC BEARING DROP INLET:**

(4-12-07)

SP8 R50Rev

At the proper phase of construction, convert the existing drop inlets at locations indicated in the plans or where directed, to traffic bearing drop inlets in accordance with the details in the plans and the applicable requirements of Sections 840 and 859 of the *Standard Specifications*.

Construct precast concrete caps containing steel frame and grate according to this provision and the special detail shown in the plans. The Contractor shall obtain measurements of all drop inlets prior to construction of the precast caps to ensure proper fit. Remove the existing cast iron grates, excavate around the drainage structure to a sufficient depth, remove up to 12" of the existing drainage structure, place new brick as needed, install the precast cap, and place full depth asphalt pavement repair adjacent to the drainage structure within the same night's operation. At no time shall a drainage structure remain open adjacent to traffic.

At any location where drop inlets are in conflict with existing barrier, the precast cap shall be modified to fit the field conditions. The contractor shall identify any such conflicts and be prepared to discuss the necessary modifications at the preconstruction meeting.

The existing cast iron grates and frames shall remain the property of the NCDOT and shall be delivered to the NCDOT's Haywood County Maintenance Facility.

*Convert Existing Drop Inlets to Traffic Bearing* will be measured and paid for as each, completed and accepted. Such price and payment is considered full compensation for all equipment, materials, labor, tools, asphalt pavement repair, and other incidentals necessary to complete each conversion to the satisfaction of the Engineer.

Payment will be made under:

<b>Pay Item</b>	<b>Pay Unit</b>
Convert Existing Drop Inlets to Traffic Bearing Drop Inlets	Each