

STATE OF NORTH CAROLINA
DIVISION OF HIGHWAYS

STATE PROJECT REFERENCE NO.	SHEET NO.
B-3876	TCP-1

**PLAN FOR PROPOSED
TRAFFIC CONTROL, MARKING & DELINEATION
NASH COUNTY**

B-3876

ROADWAY STANDARD DRAWINGS

THE FOLLOWING ROADWAY STANDARDS AS APPEAR IN "ROADWAY STANDARD DRAWINGS"- PROJECT SERVICES UNIT-N.C. DEPARTMENT OF TRANSPORTATION-RALEIGH, N.C., DATED JULY 2006 ARE APPLICABLE TO THIS PROJECT AND BY REFERENCE HEREBY ARE CONSIDERED A PART OF THESE PLANS:

STD. NO.	TITLE
1101.02	TEMPORARY LANE CLOSURES
1101.04	TEMPORARY SHOULDER CLOSURES
1101.05	WORK ZONE VEHICLE ACCESSES
1101.11	TRAFFIC CONTROL DESIGN TABLES
1110.01	STATIONARY WORK ZONE SIGNS
1110.02	PORTABLE WORK ZONE SIGNS
1130.01	DRUM
1135.01	CONES
1145.01	BARRICADES TYPE III
1150.01	FLAGGING DEVICES
1205.01	PAVEMENT MARKINGS - LINE TYPES & OFFSETS
1205.02	PAVEMENT MARKINGS - 2 LANE & MULTILANE ROADWAYS
1205.04	PAVEMENT MARKINGS - INTERSECTIONS
1205.12	PAVEMENT MARKINGS - BRIDGES
1250.01	PAVEMENT MARKER SPACING
1251.01	RAISED PAVEMENT MARKERS (TEMPORARY & PERMANENT)
1261.01	GUARDRAIL & BARRIER DELINEATOR SPACING
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1262.01	GUARDRAIL END DELINEATION

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 - REMOVAL OF EXISTING PAVEMENT
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 - CRASH CUSHION
 - CHANGEABLE MESSAGE SIGN
 - TRUCK MOUNTED IMPACT ATTENUATOR (TMIA)
 - POLICE
 - FLAGGER

FINAL PAVEMENT MARKING SCHEDULE

SYMBOL	DESCRIPTION	PAY ITEM QUANTITY BREAKDOWN	TOTAL QUANTITY
PAVEMENT MARKINGS			
THERMOPLASTIC (4") 90 MILS			
TA	WHITE EDGELINE	4327 LF	
THERMOPLASTIC (4") 120 MILS			
TD	WHITE MINI-SKIP LINE	25 LF	
TI	YEL DBL CENTER LINE	4294 LF	
			TOTAL 4319 LF
THERMOPLASTIC (24") 120 MILS			
T2	WHITE STOP BAR	44 LF	
PERMANENT RAISED MARKERS			
MA	YELLOW-YELLOW	31 EA	

APPROVED: DATE: 12/13/06	PLAN PREPARED BY: N.C.D.O.T. TRAFFIC CONTROL, MARKING & DELINEATION UNIT
SEAL	J. S. BOURNE, PE TRAFFIC CONTROL ENGINEER
	G. L. GETTIER, PE TRAFFIC CONTROL PROJECT ENGINEER
	J. W. WOOLARD, PE TRAFFIC CONTROL PROJECT DESIGN ENGINEER
	C. S. MOZINGO TRAFFIC CONTROL DESIGN ENGINEER / TECHNICIAN

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woolard J.W.

TIP PROJECT:

GENERAL NOTES

PROJ. REFERENCE NO. B-3876	SHEET NO. TCP-2
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ADAPT THE TRAFFIC CONTROL PLANS, WHEN DIRECTED BY THE ENGINEER, TO MEET FIELD CONDITIONS TO PROVIDE SAFE AND EFFICIENT TRAFFIC MOVEMENT. CHANGES MAY BE REQUIRED WHEN PHYSICAL DIMENSIONS IN THE DETAIL DRAWINGS, STANDARD DETAILS AND ROADWAY DETAILS ARE NOT ATTAINABLE, OR RESULT IN DUPLICATE, OR UNDESIRABLE OVERLAPPING OF DEVICES. MODIFICATION MAY INCLUDE: MOVING, SUPPLEMENTING, COVERING OR REMOVAL OF DEVICES.

THE FOLLOWING GENERAL NOTES APPLY AT ALL TIMES FOR THE DURATION OF THE CONSTRUCTION PROJECT, EXCEPT WHEN OTHERWISE NOTED IN THE PLAN, OR DIRECTED BY THE ENGINEER.

- A) DO NOT CONDUCT ANY HAULING OPERATIONS AGAINST THE FLOW OF TRAFFIC OF AN OPEN TRAVELWAY UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL OR OTHERWISE DIRECTED BY THE ENGINEER.

LANE AND SHOULDER CLOSURE REQUIREMENTS

- B) REMOVE LANE CLOSURE DEVICES FROM THE LANE WHEN WORK IS NOT BEING PERFORMED BEHIND THE LANE CLOSURE OR WHEN A LANE CLOSURE IS NO LONGER NEEDED, OR AS DIRECTED BY THE ENGINEER.
- C) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN 40 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN SHOULDER USING ROADWAY STANDARD DRAWING NO. 1101.04 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.
- D) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING ON THE SHOULDER ADJACENT TO AN UNDIVIDED FACILITY AND WITHIN 5 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN TRAVEL LANE USING ROADWAY STANDARD DRAWING NO. 1101.02 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.
- E) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN A LANE OF TRAVEL OF AN UNDIVIDED OR DIVIDED FACILITY, CLOSE THE LANE ACCORDING TO THE TRAFFIC CONTROL PLANS, ROADWAY STANDARD DRAWINGS OR AS DIRECTED BY THE ENGINEER. CONDUCT THE WORK SO THAT ALL PERSONNEL AND/OR EQUIPMENT REMAIN WITHIN THE CLOSED TRAVEL LANE.
- F) DO NOT WORK SIMULTANEOUSLY, ON BOTH SIDES OF AN OPEN TRAVELWAY, WITHIN THE SAME LOCATION, ON A TWO-LANE, TWO-WAY ROAD.
- G) DO NOT PERFORM WORK INVOLVING HEAVY EQUIPMENT WITHIN 15 FT OF THE EDGE OF TRAVELWAY WHEN WORK IS BEING PERFORMED BEHIND A LANE CLOSURE ON THE OPPOSITE SIDE OF THE TRAVELWAY.
- H) PROVIDE TRAFFIC CONTROL FOR APPROPRIATE LANE CLOSURES FOR SURVEYING DONE BY THE DEPARTMENT.

PAVEMENT EDGE DROP OFF REQUIREMENTS

- I) BACKFILL AT A 6:1 SLOPE UP TO THE EDGE AND ELEVATION OF EXISTING PAVEMENT IN AREAS ADJACENT TO AN OPENED TRAVEL LANE THAT HAS A DROP-OFF AS FOLLOWS:

BACKFILL DROP-OFFS THAT EXCEED 2 INCHES ON ROADWAYS WITH POSTED SPEED LIMITS OF 45 MPH OR GREATER.

BACKFILL DROP-OFFS THAT EXCEED 3 INCHES ON ROADWAYS WITH POSTED SPEED LIMITS LESS THAN 45 MPH.

BACKFILL WITH SUITABLE COMPACTED MATERIAL, AS APPROVED BY THE ENGINEER, AT NO EXPENSE TO THE DEPARTMENT.

- J) DO NOT EXCEED A DIFFERENCE OF 1.5 inches IN ELEVATION BETWEEN OPEN LANES OF TRAFFIC. INSTALL ADVANCE WARNING "UNEVEN LANES" SIGNS (W8-11) 500 FT IN ADVANCE AND A MINIMUM OF ONCE EVERY MILE THROUGHOUT THE UNEVEN AREA.

TRAFFIC PATTERN ALTERATIONS

- K) NOTIFY THE ENGINEER TWENTY ONE (21) CALENDAR DAYS PRIOR TO ANY TRAFFIC PATTERN ALTERATION.

SIGNING

- L) INSTALL ADVANCE WORK ZONE WARNING SIGNS WHEN WORK IS WITHIN 100 FT FROM THE EDGE OF TRAVEL LANE AND NO MORE THAN THREE (3) DAYS PRIOR TO THE BEGINNING OF CONSTRUCTION.

WHEN NO WORK IS BEING CONDUCTED FOR A PERIOD LONGER THAN ONE WEEK, REMOVE OR COVER ALL ADVANCE WORK ZONE WARNING SIGNS, AS DIRECTED BY THE ENGINEER, AT NO COST TO THE DEPARTMENT.

- M) PROVIDE PERMANENT SIGNING.
- N) ENSURE ALL NECESSARY SIGNING IS IN PLACE PRIOR TO ALTERING ANY TRAFFIC PATTERN.
- O) INSTALL BLACK ON ORANGE "DIP" SIGNS (W8-2) 500 FT IN ADVANCE OF THE UNEVEN AREA.
- P) INSTALL BLACK ON ORANGE "BUMP" SIGNS (W8-1) 500 FT IN ADVANCE OF THE UNEVEN AREA.

TRAFFIC CONTROL DEVICES

- Q) WHEN USING ROADWAY STANDARD NO. 1101.02, DRUMS MAY BE USED IN LIEU OF CONES ON TAYLOR STORE RD & CORINTH RD.
- R) SPACE CHANNELIZING DEVICES IN WORK AREAS NO GREATER THAN TWICE THE POSTED SPEED LIMIT (55 MPH), EXCEPT 10 FT ON-CENTER IN RADII, AND 3 FT OFF THE EDGE OF AN OPEN TRAVELWAY, WHEN LANE CLOSURES ARE NOT IN EFFECT.
- S) PLACE TYPE III BARRICADES, WITH "ROAD CLOSED" SIGN R11-2 ATTACHED, OF SUFFICIENT LENGTH TO CLOSE ENTIRE ROADWAY. STAGGER OR OVERLAP BARRICADES TO ALLOW FOR INGRESS OR EGRESS.

PAVEMENT MARKINGS AND MARKERS

- T) INSTALL PAVEMENT MARKINGS AND PAVEMENT MARKERS ON THE FINAL SURFACE AS FOLLOWS:

ROAD NAME	MARKING	MARKER
1) TAYLOR STORE RD	THERMOPLASTIC	PERMANENT RAISED
2) CORINTH RD	THERMOPLASTIC	PERMANENT RAISED

- U) INSTALL TEMPORARY PAVEMENT MARKINGS AND TEMPORARY PAVEMENT MARKERSON INTERIM LAYERS OF PAVEMENT AS FOLLOWS:

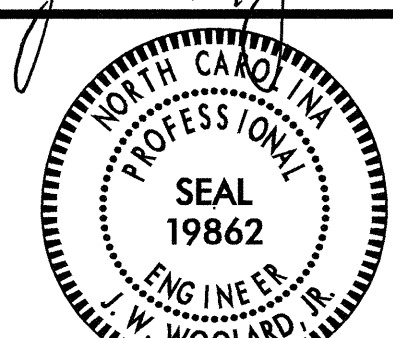
ROAD NAME	MARKING	MARKER
1) TAYLOR STORE RD	PAINT	TEMPORARY RAISED
2) CORINTH RD	PAINT	TEMPORARY RAISED

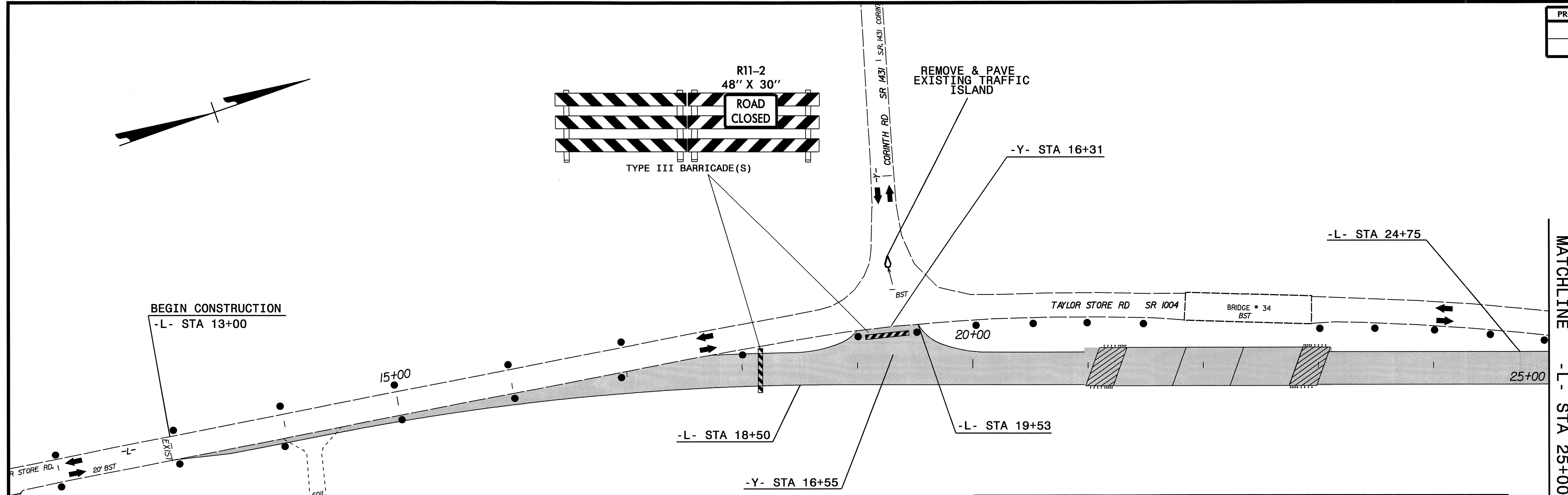
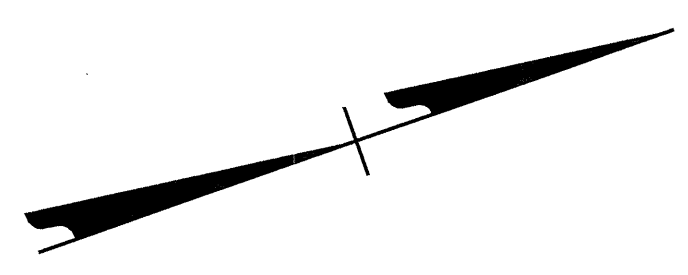
- V) TIE PROPOSED PAVEMENT MARKING LINES TO EXISTING PAVEMENT MARKING LINES.
- W) REPLACE ANY PAVEMENT MARKINGS THAT HAVE BEEN DAMAGED BY THE END OF EACH DAY'S OPERATION.
- X) PLACE AT LEAST TWO APPLICATIONS OF PAINT ON NEW ASPHALT WITH TEMPORARY TRAFFIC PATTERNS WHICH WILL REMAIN IN PLACE OVER THREE (3) MONTHS. PLACE ADDITIONAL APPLICATIONS OF PAINT UPON SUFFICIENT DRYING TIME, AS DETERMINED BY THE ENGINEER.

MISCELLANEOUS

- Y) IN THE EVENT A TIE-IN CANNOT BE MADE IN ONE DAYS TIME, BRING THE TIE-IN AREA TO AN APPROPRIATE ROADWAY ELEVATION, AS DETERMINED BY THE ENGINEER. PLACE BLACK ON ORANGE "LOOSE GRAVEL" SIGNS (W8-7) AND BLACK ON ORANGE "PAVEMENT ENDS" SIGNS (W8-3) 500 FT AND 1000 FT RESPECTIVELY IN ADVANCE OF THE UNEVEN AREAS. USE DRUMS TO DELINEATE THE EDGE OF ROADWAY ALONG UNPAVED AREAS.

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APPROVED: <i>J. W. Woolard</i> DATE: 12/13/06 	<h2 style="margin: 0;">PROJECT NOTES</h2>	<table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <td style="font-size: small;">SCALE:</td> <td>NONE</td> </tr> <tr> <td style="font-size: small;">DATE:</td> <td>2006 NOV</td> </tr> <tr> <td style="font-size: small;">DWG. BY:</td> <td>CSM</td> </tr> <tr> <td style="font-size: small;">DESIGN BY:</td> <td>CSM</td> </tr> <tr> <td style="font-size: small;">REVIEWED BY:</td> <td>JWW</td> </tr> </table>	SCALE:	NONE	DATE:	2006 NOV	DWG. BY:	CSM	DESIGN BY:	CSM	REVIEWED BY:	JWW
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PHASE I

NOTE: RETURN TRAFFIC TO EXISTING TWO-LANE TWO-WAY PATTERN AT THE END OF THE WORK DAY.

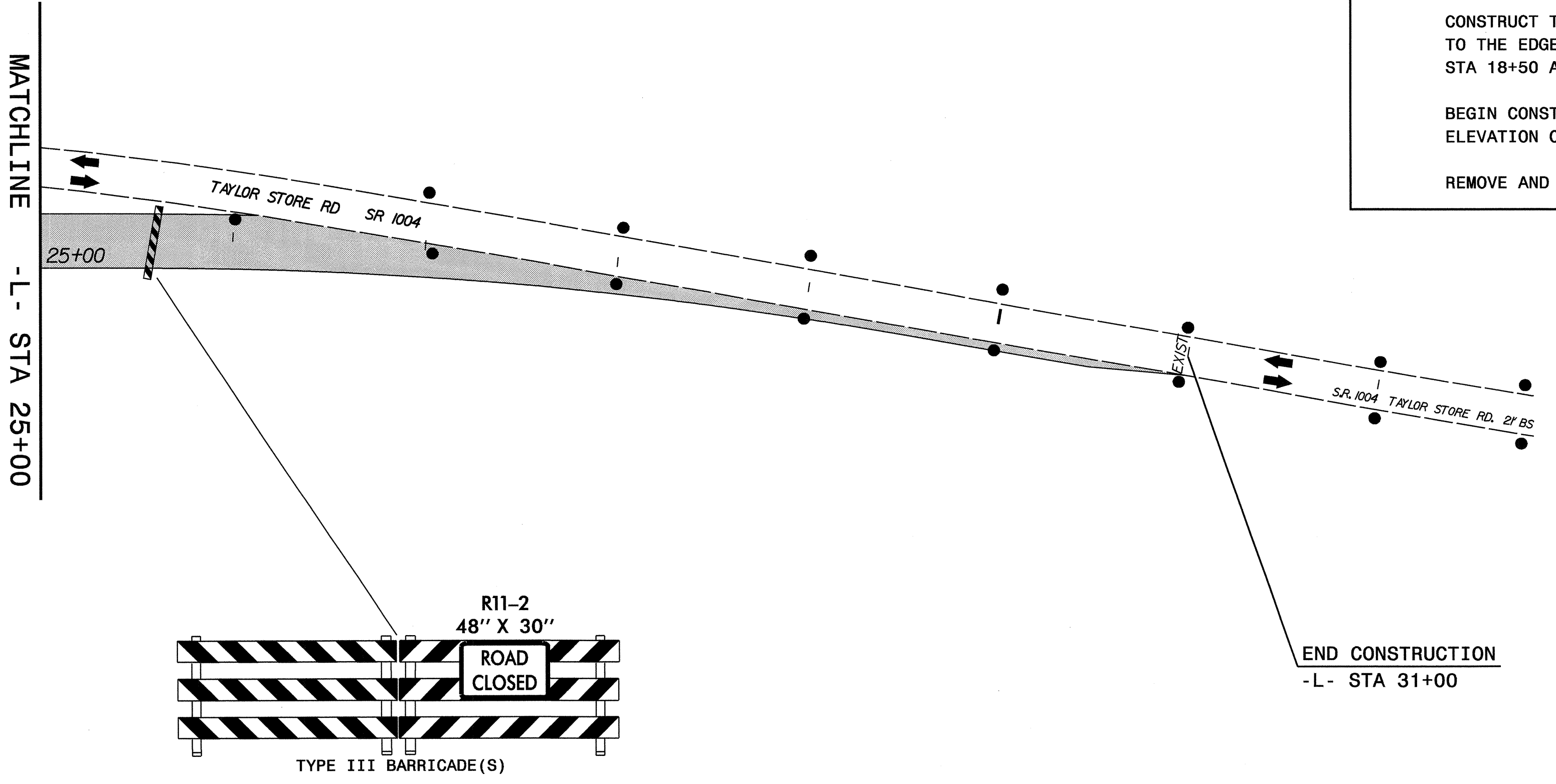
STEP 1: INSTALL ALL WORK ZONE ADVANCE WARNING SIGNS PRIOR TO ANY CONSTRUCTION ACTIVITY AS SHOWN ON TCP-7.

STEP 2: USING SHOULDER CLOSURES AND LANE CLOSURES AND FLAGGERS AS DESCRIBED IN ROADWAY STANDARD DRAWING 1101.04, SHEET 1 OF 1, AND 1101.02, SHEET 1 OF 9, CONSTRUCT THE PROPOSED RE-ALIGNMENT OF TAYLOR STORE RD. (SR 1004) (-L-), INCLUDING THE PROPOSED STRUCTURE, UP TO BUT NOT INCLUDING THE FINAL LIFT OF THE SURFACE COURSE FROM -L- STA 18+50 TO STA 24+75.

CONSTRUCT THE PROPOSED RE-ALIGNMENT OF TAYLOR STORE RD. (SR 1004) (-L-), UP TO THE EDGE AND ELEVATION OF EXISTING TAYLOR STORE RD FROM -L- STA 13+00 TO STA 18+50 AND -L- STA 24+75 TO STA 31+00.

BEGIN CONSTRUCTION OF CORINTH RD (SR 1431) (-Y-) FROM THE EDGE AND ELEVATION OF EXISTING TAYLOR STORE RD FROM -Y- STA 16+31 TO STA 16+55.

REMOVE AND PAVE THE EXISTING TRAFFIC ISLAND ON CORINTH RD (SR 1431) (-Y-).



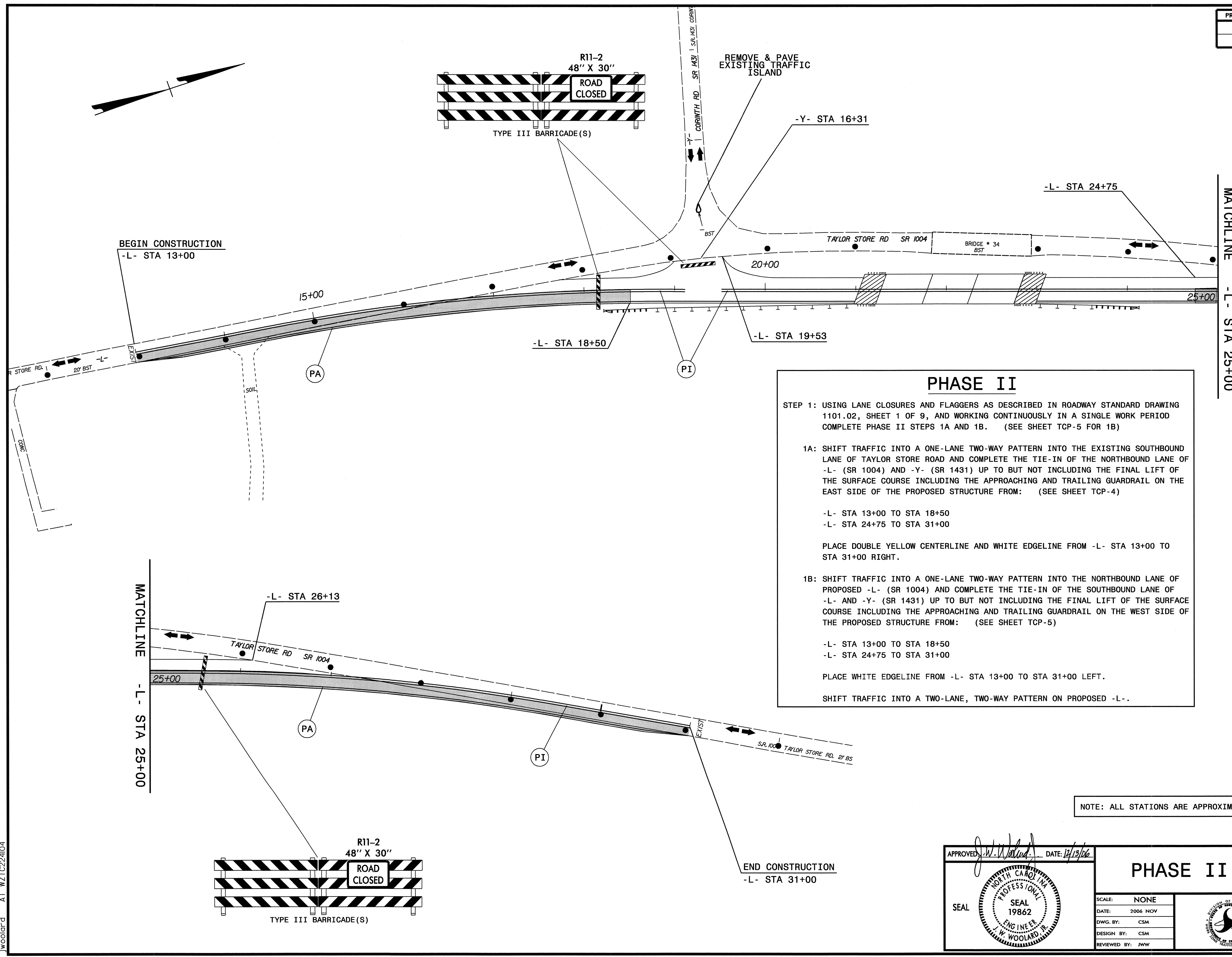
NOTE: ALL STATIONS ARE APPROXIMATE.

APPROVED: *J.W. Woolard* DATE: 12/13/06

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PHASE II

STEP 1: USING LANE CLOSURES AND FLAGGERS AS DESCRIBED IN ROADWAY STANDARD DRAWING 1101.02, SHEET 1 OF 9, AND WORKING CONTINUOUSLY IN A SINGLE WORK PERIOD COMPLETE PHASE II STEPS 1A AND 1B. (SEE SHEET TCP-5 FOR 1B)

1A: SHIFT TRAFFIC INTO A ONE-LANE TWO-WAY PATTERN INTO THE EXISTING SOUTHBOUND LANE OF TAYLOR STORE ROAD AND COMPLETE THE TIE-IN OF THE NORTHBOUND LANE OF -L- (SR 1004) AND -Y- (SR 1431) UP TO BUT NOT INCLUDING THE FINAL LIFT OF THE SURFACE COURSE INCLUDING THE APPROACHING AND TRAILING GUARDRAIL ON THE EAST SIDE OF THE PROPOSED STRUCTURE FROM: (SEE SHEET TCP-4)

-L- STA 13+00 TO STA 18+50
-L- STA 24+75 TO STA 31+00

PLACE DOUBLE YELLOW CENTERLINE AND WHITE EDGELINE FROM -L- STA 13+00 TO STA 31+00 RIGHT.

1B: SHIFT TRAFFIC INTO A ONE-LANE TWO-WAY PATTERN INTO THE NORTHBOUND LANE OF PROPOSED -L- (SR 1004) AND COMPLETE THE TIE-IN OF THE SOUTHBOUND LANE OF -L- AND -Y- (SR 1431) UP TO BUT NOT INCLUDING THE FINAL LIFT OF THE SURFACE COURSE INCLUDING THE APPROACHING AND TRAILING GUARDRAIL ON THE WEST SIDE OF THE PROPOSED STRUCTURE FROM: (SEE SHEET TCP-5)

-L- STA 13+00 TO STA 18+50
-L- STA 24+75 TO STA 31+00

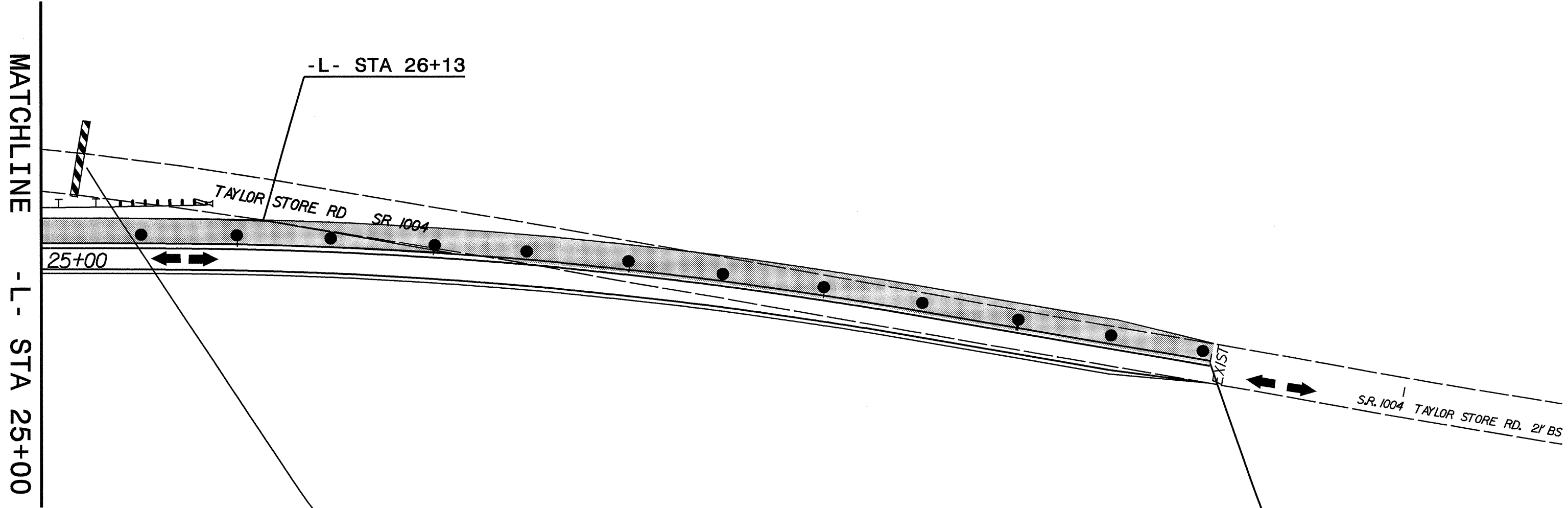
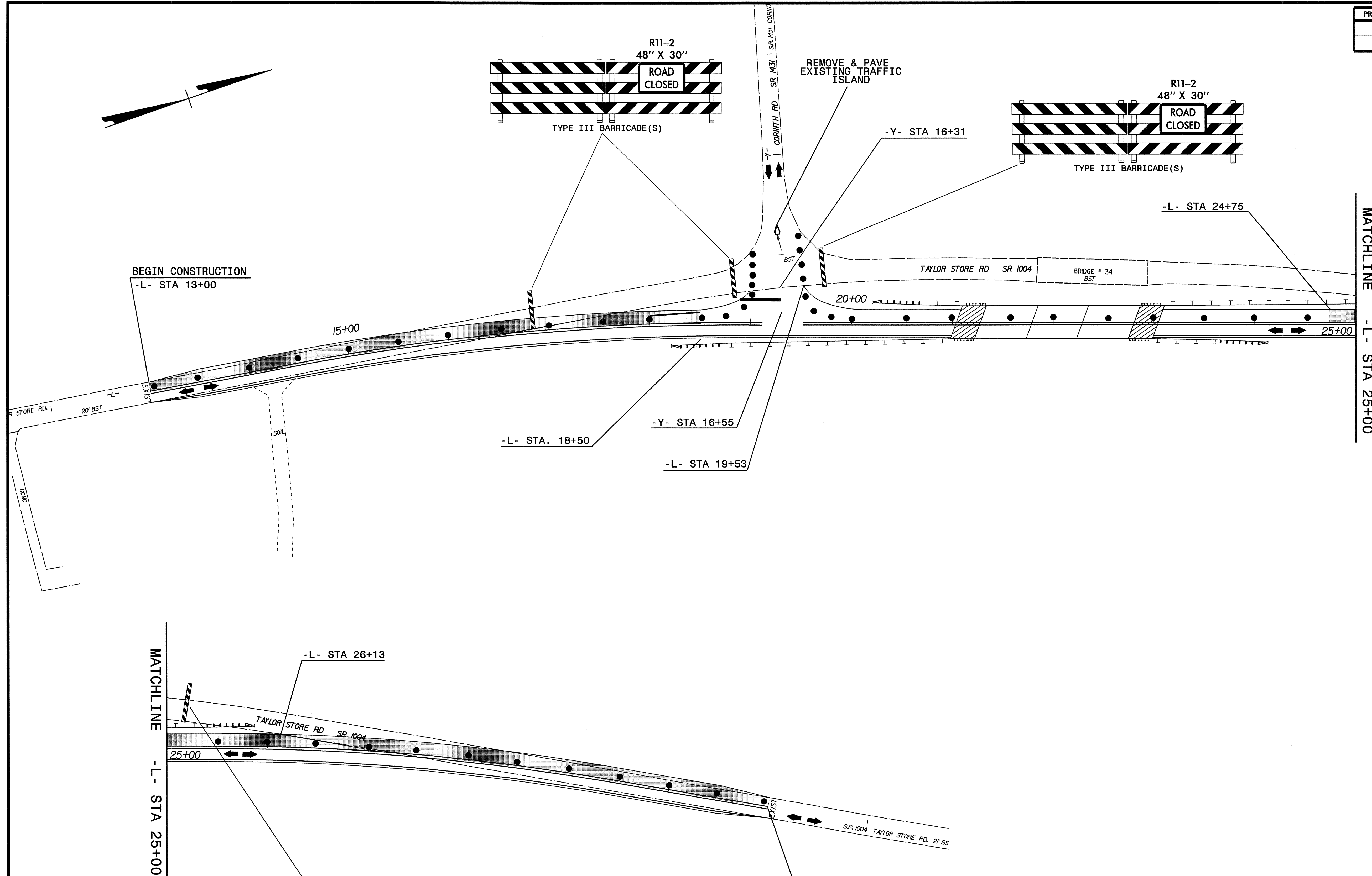
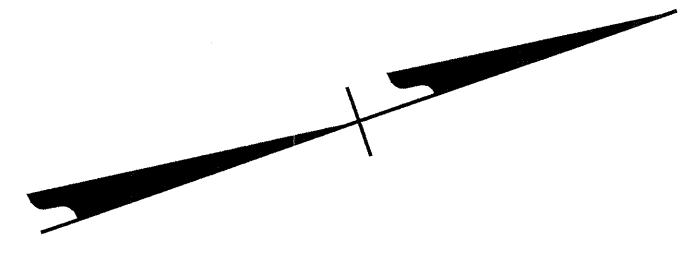
PLACE WHITE EDGELINE FROM -L- STA 13+00 TO STA 31+00 LEFT.

SHIFT TRAFFIC INTO A TWO-LANE, TWO-WAY PATTERN ON PROPOSED -L-.

NOTE: ALL STATIONS ARE APPROXIMATE.

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NOTE: ALL STATIONS ARE APPROXIMATE.

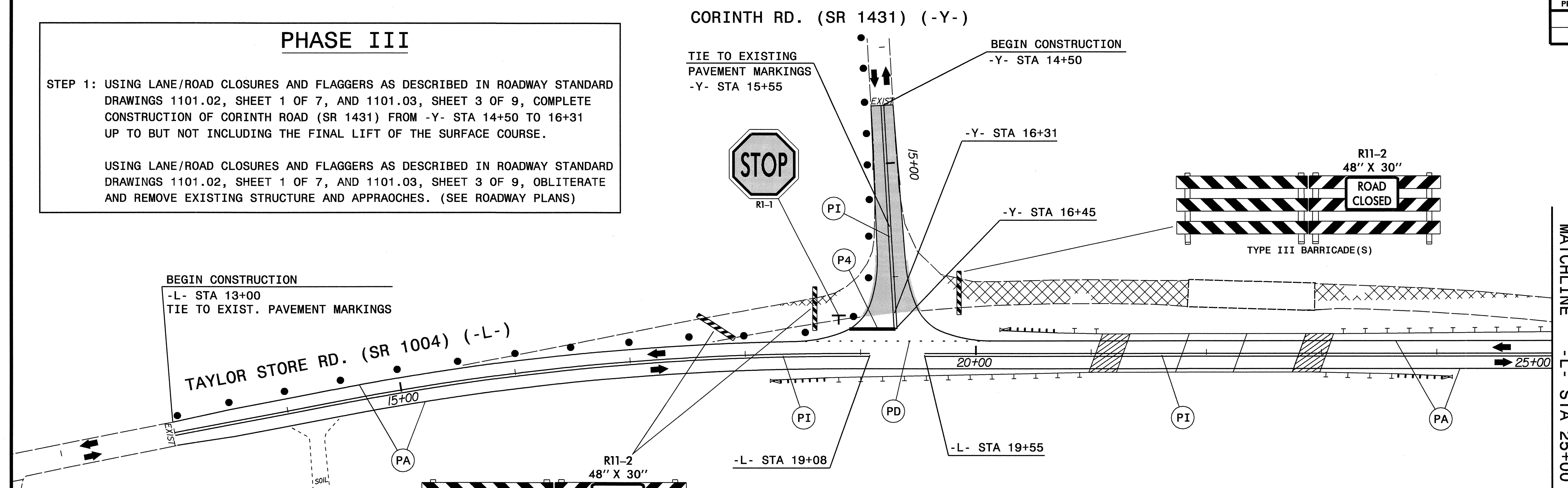
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PHASE III

STEP 1: USING LANE/ROAD CLOSURES AND FLAGGERS AS DESCRIBED IN ROADWAY STANDARD DRAWINGS 1101.02, SHEET 1 OF 7, AND 1101.03, SHEET 3 OF 9, COMPLETE CONSTRUCTION OF CORINTH ROAD (SR 1431) FROM -Y- STA 14+50 TO 16+31 UP TO BUT NOT INCLUDING THE FINAL LIFT OF THE SURFACE COURSE.

USING LANE/ROAD CLOSURES AND FLAGGERS AS DESCRIBED IN ROADWAY STANDARD DRAWINGS 1101.02, SHEET 1 OF 7, AND 1101.03, SHEET 3 OF 9, OBLITERATE AND REMOVE EXISTING STRUCTURE AND APPROACHES. (SEE ROADWAY PLANS)

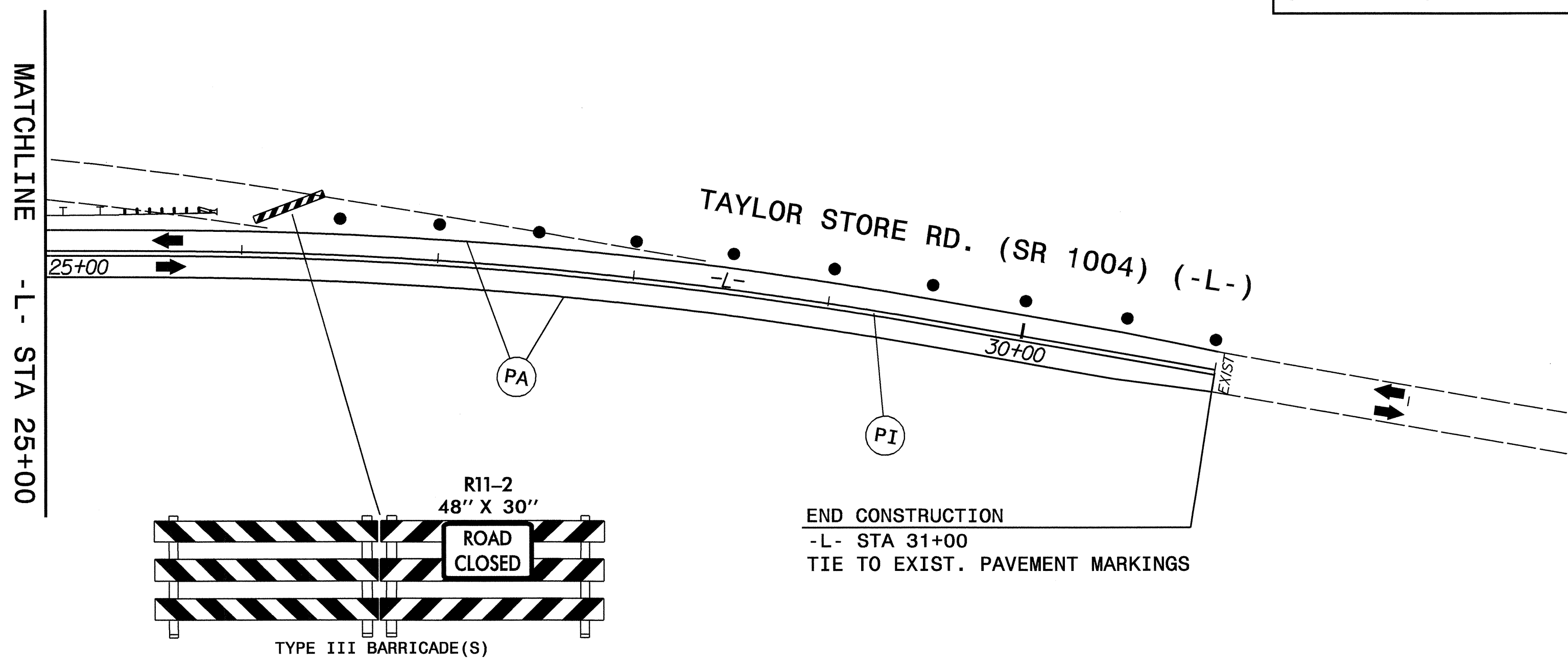


PHASE IV

STEP 1: USING LANE/ROAD CLOSURES & FLAGGERS AS DESCRIBED IN ROADWAY STANDARD DRAWING 1101.02, SHEET 1 OF 7, PLACE THE FINAL LIFT OF THE SURFACE COURSE AND FINAL PAVEMENT MARKINGS (THERMOPLASTIC) AND MARKERS (PERMANENT RAISED) ON:

-L- STA 13+00 TO STA 31+00
 -Y- STA 14+50 TO STA 16+55

STEP 2: REMOVE ALL TRAFFIC CONTROL DEVICES AND OPEN TO FINAL TRAFFIC PATTERN.



TEMPORARY PAVEMENT MARKING SCHEDULE			
SYMBOL	DESCRIPTION	PAY ITEM QUANTITY BREAKDOWN	TOTAL QUANTITY
PAVEMENT MARKINGS			
PAINT (4")			
PA	WHITE EDGELINE	8656 LF	TOTAL 17,277 LF
PD	WHITE MINI-SKIP LINE	49 LF	
PI	YEL DBL CENTER LINE	8572 LF	
PAINT (24")			
P4	WHITE STOP BAR	88 LF	
TEMPORARY RAISED MARKERS			
MH	YELLOW-YELLOW	114 EA	

NOTE: ALL STATIONS ARE APPROXIMATE.

APPROVED: *J.W. Woolard* DATE: 12/13/06

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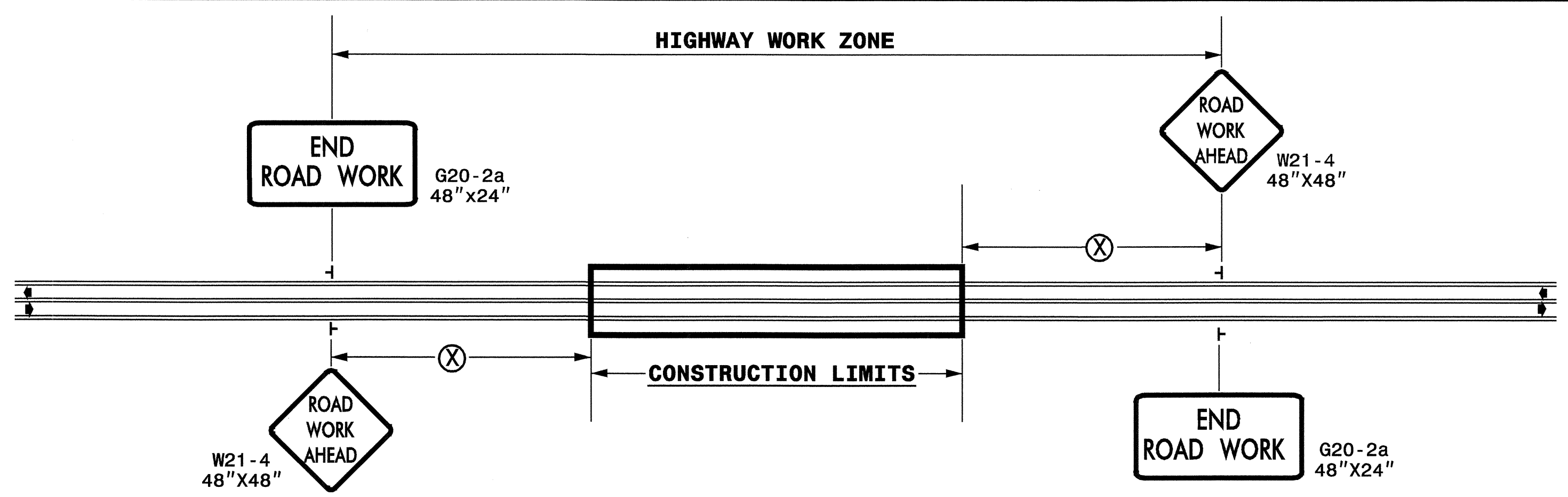
NO. 19862
 PROFESSIONAL ENGINEER
 J.W. WOOLARD, JR.

PHASE III & IV

SCALE: NONE		REVISIONS
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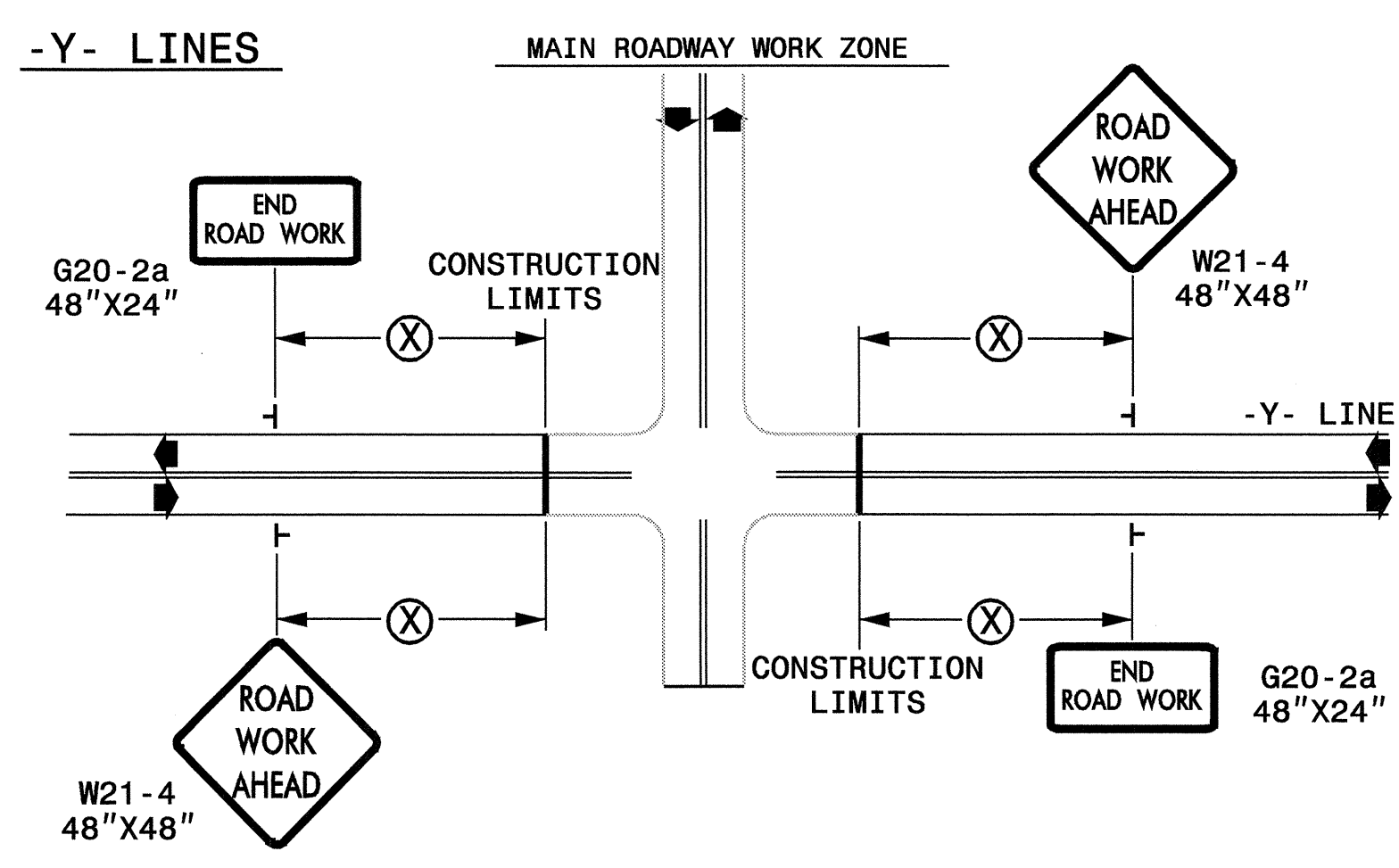
TWO-WAY UNDIVIDED & URBAN FREEWAYS (L-LINES)



POSTED SPEED LIMIT (M.P.H.)	RECOMMENDED MINIMUM SIGN SPACING
≤ 50	500'
≥ 55	1000'

STATE OF NORTH CAROLINA
DEPT. OF TRANSPORTATION
DIVISION OF HIGHWAYS
RALEIGH, N.C.

ROADWAYS INTERSECTING ALONG 2 WAY UNDIVIDED WORK ZONE (Y-LINES)



GENERAL NOTES

- USE FLUORESCENT ORANGE SHEETING (TYPE VII OR HIGHER) ON ALL ADVANCED WORK ZONE SIGNS.
- DO NOT INSTALL ADVANCE WARNING SIGNS MORE THAN 3 DAYS PRIOR TO BEGINNING OF WORK.
- SIGNS SHOWN ARE REQUIRED FOR WORK ZONES THAT WILL REMAIN IN EFFECT OVERNIGHT. FOR SHORT-TERM DAILY MAINTENANCE TYPE OPERATIONS, THIS SIGNING APPLICATION IS OPTIONAL; MAY USE ONLY APPLICABLE ROADWAY STANDARD DRAWINGS INSTEAD. HOWEVER, IF THIS SIGNING APPLICATION IS USED, SIGNS MAY BE PORTABLE MOUNTED.
- ALL SIGN SPACING DIMENSIONS ARE APPROXIMATE, FIELD ADJUST AS NECESSARY OR AS DIRECTED.
- USE 3LB STEEL U-CHANNEL POST OR 4" X 4" WOOD POST FOR ALL WORK ZONE SIGNS. 3LB STEEL U-CHANNEL POSTS MUST MEET THE REQUIREMENTS OF STANDARD SPECIFICATION SECTION 1094-1(B). MAY BE GALVANIZED STEEL, OR MAY BE PAINTED GREEN BY THE POST MANUFACTURER. SQUARE STEEL TUBING POSTS HAVING EQUIVALENT STRENGTH OF THE 3 LB STEEL U-CHANNEL POST ARE ALSO ACCEPTABLE FOR USE. ERECT SIGNS PER ROADWAY STANDARD DRAWING 1110.01. PAYMENT FOR WOOD POSTS, 3LB STEEL U-CHANNEL AND SQUARE STEEL TUBING POSTS WITH SIGNS WILL BE MADE ACCORDING TO STANDARD SPECIFICATION "WORK ZONE SIGNS" SECTION 1110.
- WHEN NECESSARY, USE SPLICING IN ACCORDANCE WITH ROADWAY STANDARD DRAWING NO. 1110.01. REMOVE ENTIRE POST WHEN REMOVING SIGNS WITH SPLICED POSTS.
- DO NOT BACK BRACE SIGN SUPPORTS.

LEGEND

┆ STATIONARY SIGN

◀ DIRECTION OF TRAFFIC FLOW

DETAIL DRAWING FOR
TWO-WAY UNDIVIDED
WORK ZONE WARNING SIGNS

SHEET 1 OF 1

APPROVED: <i>J.W. Woolard</i> DATE: 12/13/06	DETAIL DRAWING FOR TWO-WAY UNDIVIDED AND URBAN FREEWAYS ADVANCED WORK ZONE WARNING SIGNS		
	SCALE: NONE		
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