

STATE OF NORTH CAROLINA
DIVISION OF HIGHWAYS

STATE PROJECT REFERENCE NO.	SHEET NO.
37857 (MA13063R) & 37645 (MA13025R)	TCP-1

**PLAN FOR PROPOSED
TRAFFIC CONTROL, MARKING & DELINEATION
McDOWELL COUNTY**

WBS: 37645 & 37857

CONTRACT: C201667

ROADWAY STANDARD DRAWINGS

THE FOLLOWING ROADWAY STANDARDS AS APPEAR IN "ROADWAY STANDARD DRAWINGS" - ROADWAY DESIGN UNIT-N.C. DEPARTMENT OF TRANSPORTATION-RALEIGH, N.C., DATED JULY 2006 ARE APPLICABLE TO THIS PROJECT AND BY REFERENCE HEREBY ARE CONSIDERED A PART OF THESE PLANS:

STD. NO.	TITLE
1101.02	TEMPORARY LANE CLOSURES
1101.03	TEMPORARY ROAD CLOSURE
1101.05	WORK ZONE VEHICLE ACCESSES
1101.06	WARNING SIGNS FOR BLASTING ZONES
1101.11	TRAFFIC CONTROL PLAN DESIGN TABLES
1110.01	STATIONARY WORK ZONE SIGNS
1110.02	PORTABLE WORK ZONE SIGNS
1130.01	DRUM
1135.01	CONE
1145.01	BARRICADES - TYPE III
1150.01	FLAGGING DEVICES
1160.01	TEMPORARY CRASH CUSHION - REFLECTIVE END TREATMENT
1165.01	TRUCK MOUNTED IMPACT ATTENUATOR - DELINEATION
1170.01	PORTABLE CONCRETE BARRIER
1205.01	PAVEMENT MARKINGS - LINE TYPES AND OFFSETS
1205.02	PAVEMENT MARKINGS - DIVIDED AND UNDIVIDED ROADWAYS
1205.04	PAVEMENT MARKINGS - INTERSECTIONS
1205.05	PAVEMENT MARKINGS - TURN LANES
1205.06	PAVEMENT MARKINGS - THRU LANE DROPS
1205.08	PAVEMENT MARKINGS - WORDS & SYMBOLS
1205.09	PAVEMENT MARKINGS - PAINTED ISLANDS
1250.01	PAVEMENT MARKER SPACING
1251.01	RAISED PAVEMENT MARKERS - PERMANENT AND TEMPORARY
1253.01	SNOWPLOWABLE RAISED PAVEMENT MARKERS
1261.01	GUARDRAIL & BARRIER DELINEATOR SPACING
1261.02	GUARDRAIL & BARRIER DELINEATOR TYPES
1262.01	GUARDRAIL END DELINEATION
1264.01	OBJECT MARKERS
1264.02	PLACEMENT OF OBJECT MARKERS
1267.01	FLEXIBLE DELINEATOR INSTALLATION
1267.02	FLEXIBLE DELINEATOR SPACING
1267.03	FLEXIBLE DELINEATORS - INTERCHANGES

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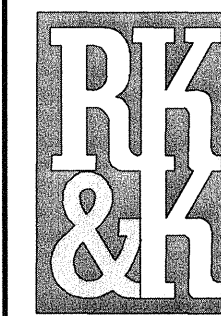
SHEET NO.	TITLE
TCP-1	LIST OF APPLICABLE ROADWAY STANDARD DRAWINGS, LEGEND, FINAL PAVEMENT MARKING SCHEDULE AND INDEX OF SHEETS
TCP-2	PROJECT NOTES
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TCP-12 - TCP-20	PHASE II
TCP-21	PHASE III & TEMPORARY PAVEMENT MARKING SCHEDULE
TCP-22	NC 226 TEMPORARY OFFSITE DETOUR ROUTE
NCMA-1	DETAIL DRAWING FOR ADVANCE WARNING WORK ZONE SIGNS
NCMA-2	SIGN DESIGN FOR CONTRACTORS COMPANY NAME
PM-1	FINAL PAVEMENT MARKING SCHEDULE
PM-2 - PM-6	FINAL PAVEMENT MARKING PLAN

LEGEND

- GENERAL**
- DIRECTION OF TRAFFIC FLOW
 - NORTH ARROW
 - PROPOSED PVMT. EXIST. PVMT.
 - WORK AREA
 - REMOVAL OF EXISTING PAVEMENT
- TRAFFIC CONTROL DEVICES**
- TYPE I BARRICADE
 - TYPE II BARRICADE
 - TYPE III BARRICADE
 - CONE
 - DRUM
 - FLASHING ARROW PANEL (TYPE C)
 - STATIONARY SIGN
 - PORTABLE SIGN
 - STATIONARY OR PORTABLE SIGN
 - WARNING FLAGS
 - CRASH CUSHION
 - CHANGEABLE MESSAGE SIGN
 - TRUCK MOUNTED IMPACT ATTENUATOR (TMIA)
 - POLICE
 - FLAGGER
- PAVEMENT MARKINGS**
- CRYSTAL PAVEMENT MARKER
 - YELLOW/YELLOW PAVEMENT MARKER
 - CRYSTAL/RED PAVEMENT MARKER
 - PAVEMENT MARKING SYMBOLS

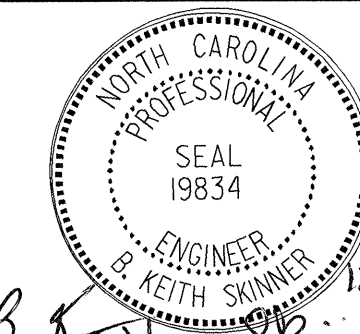
PLAN REVIEWED BY: N.C.D.O.T. DIVISION 13

GREG SHULER DIVISION PROJECT MANAGER



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SEAL



B. K. Skinner

B. K. SKINNER, P.E. TRAFFIC CONTROL ENGINEER

S. E. ROBERTS, P.E. TRAFFIC CONTROL PROJECT ENGINEER

M. A. COLE TRAFFIC CONTROL PROJECT DESIGN ENGINEER

GENERAL NOTES

ADAPT THE TRAFFIC CONTROL PLANS, WHEN DIRECTED BY THE ENGINEER, TO MEET FIELD CONDITIONS TO PROVIDE SAFE AND EFFICIENT TRAFFIC MOVEMENT. CHANGES MAY BE REQUIRED WHEN PHYSICAL DIMENSIONS IN THE DETAIL DRAWINGS, STANDARD DETAILS AND ROADWAY DETAILS ARE NOT ATTAINABLE, OR RESULT IN DUPLICATE, OR UNDESIRE OVERLAPPING OF DEVICES. MODIFICATIONS MAY INCLUDE; MOVING, SUPPLEMENTING, COVERING, OR REMOVAL OF DEVICES.

THE FOLLOWING GENERAL NOTES APPLY AT ALL TIMES FOR THE DURATION OF THE CONSTRUCTION PROJECT, EXCEPT WHEN OTHERWISE NOTED IN THE PLAN, OR DIRECTED BY THE ENGINEER.

TIME RESTRICTIONS

A) DO NOT CLOSE OR NARROW TRAVEL LANES AS FOLLOWS:

ROAD NAME	DAY AND TIME RESTRICTIONS
1. NC 226	7:00 A.M. THRU 6:00 P.M. (SUNDAY THRU SATURDAY)

B) DO NOT CLOSE OR NARROW TRAVEL LANES DURING HOLIDAYS AS FOLLOWS:

ROAD NAME	HOLIDAY
1. NC 226	

- FOR ANY EVENT THAT CREATES UNUSUALLY HIGH TRAFFIC VOLUMES, AS DIRECTED BY THE ENGINEER.
- FOR NEW YEAR'S, BETWEEN THE HOURS OF 7:00 A.M. DECEMBER 31ST TO 600 P.M. JANUARY 2ND. IF NEW YEAR'S DAY IS ON A SATURDAY OR A SUNDAY, THEN UNTIL 6:00 P.M. THE FOLLOWING TUESDAY.
- FOR EASTER, BETWEEN THE HOURS OF 7:00 A.M. THURSDAY AND 6:00 P.M. MONDAY.
- FOR MEMORIAL DAY, BETWEEN THE HOURS OF 7:00 A.M. FRIDAY TO 6:00 P.M. TUESDAY.
- FOR INDEPENDENCE DAY, BETWEEN THE HOURS OF 7:00 A.M. THE DAY BEFORE INDEPENDENCE DAY AND 6:00 P.M. THE DAY AFTER INDEPENDENCE DAY.
IF INDEPENDENCE DAY IS ON A SATURDAY OR SUNDAY, THEN BETWEEN THE HOURS OF 6:00 P.M. THE THURSDAY BEFORE INDEPENDENCE DAY AND 6:00 P.M. THE TUESDAY AFTER INDEPENDENCE DAY.
- FOR LABOR DAY, BETWEEN THE HOURS OF 7:00 A.M. FRIDAY TO 6:00 P.M. TUESDAY.
- FOR THANKSGIVING, BETWEEN THE HOURS OF 7:00 A.M. TUESDAY TO 6:00 P.M. MONDAY.
- FOR CHRISTMAS, BETWEEN THE HOURS OF 7:00 A.M. THE FRIDAY BEFORE THE WEEK OF CHRISTMAS DAY AND 6:00 P.M. THE FOLLOWING MONDAY AFTER THE WEEK OF CHRISTMAS.
- FOR LEAF SEASON, THE ENTIRE MONTH OF OCTOBER AND 1ST WEEK IN NOVEMBER, BETWEEN 7:00 A.M. FRIDAY TO 6:00 P.M. MONDAY.

LANE & SHOULDER CLOSURE REQUIREMENTS

- REMOVE LANE CLOSURE DEVICES FROM THE LANE WHEN WORK IS NOT BEING PERFORMED BEHIND THE LANE CLOSURE OR WHEN A LANE CLOSURE IS NO LONGER NEEDED, OR AS DIRECTED BY THE ENGINEER.
- WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING ON THE SHOULDER ADJACENT TO AN UNDIVIDED FACILITY AND WITHIN 5' (1.5m) OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN TRAVEL LANE USING ROADWAY STANDARD DRAWING NO. 1101.02, UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.
- WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN A LANE OF TRAVEL OF AN UNDIVIDED OR DIVIDED FACILITY. CLOSE THE LANE ACCORDING TO THE TRAFFIC CONTROL PLANS, ROADWAY STANDARD DRAWINGS OR AS DIRECTED BY THE ENGINEER. CONDUCT THE WORK SO THAT ALL PERSONNEL AND/OR EQUIPMENT REMAIN WITHIN THE CLOSED TRAVEL LANE.
- DO NOT WORK SIMULTANEOUSLY, ON BOTH SIDES OF AN OPEN TRAVELWAY, WITHIN THE SAME LOCATION, ON A TWO-LANE, TWO-WAY ROAD.
- DO NOT PERFORM WORK INVOLVING HEAVY EQUIPMENT WITHIN 15' (5m) OF THE EDGE OF TRAVELWAY WHEN WORK IS BEING PERFORMED BEHIND A LANE CLOSURE ON THE OPPOSITE SIDE OF THE TRAVELWAY.

PAVEMENT EDGE DROP OFF REQUIREMENTS

- BACKFILL AT A 6:1 SLOPE UP TO THE EDGE AND ELEVATION OF EXISTING PAVEMENT IN AREAS ADJACENT TO AN OPENED TRAVEL LANE THAT HAS A DROP-OFF AS FOLLOWS:
 - BACKFILL DROP-OFFS THAT EXCEED 2" (50 mm) ON ROADWAYS WITH POSTED SPEED LIMITS OF 45 MPH OR GREATER.
 - BACKFILL DROP-OFFS THAT EXCEED 3" (75mm) ON ROADWAYS WITH POSTED SPEED LIMITS OF LESS THAN 45 MPH.
 - BACKFILL WITH SUITABLE COMPACTED MATERIAL, AS APPROVED BY THE ENGINEER, AT NO EXPENSE TO THE DEPARTMENT.
- DO NOT EXCEED A DIFFERENCE OF 1.5 INCHES (40mm) IN ELEVATION BETWEEN OPEN LANES OF TRAFFIC. INSTALL ADVANCE WARNING "UNEVEN LANES" SIGNS (W8-11) 500 FT (150m) IN ADVANCE AND A MINIMUM OF ONE EVERY MILE THROUGHOUT THE UNEVEN AREA.

PROJECT NOTES

TRAFFIC PATTERN ALTERATIONS

J) NOTIFY THE ENGINEER 21 CALENDAR DAYS PRIOR TO ANY TRAFFIC PATTERN ALTERATION.

SIGNING

K) INSTALL ADVANCE WORK ZONE WARNING SIGNS WHEN WORK IS WITHIN 100 FT (31m) FROM THE EDGE OF TRAVEL LANE AND NO MORE THAN THREE (3) DAYS PRIOR TO THE BEGINNING OF CONSTRUCTION.

WHEN NO WORK IS BEING CONDUCTED FOR A PERIOD LONGER THAN ONE WEEK, REMOVE OR COVER ALL ADVANCE WORK ZONE WARNING SIGNS, AS DIRECTED BY THE ENGINEER, AT NO COST TO THE DEPARTMENT.

L) PROVIDE PERMANENT SIGNING.

M) PROVIDE DETOUR SIGNING WITHIN AND OFF THE PROJECT LIMITS.

N) COVER OR REMOVE ALL DETOUR SIGNING WITHIN AND OFF THE PROJECT LIMITS WHEN A DETOUR IS NOT IN OPERATION.

O) ENSURE ALL NECESSARY SIGNING IS IN PLACE PRIOR TO ALTERING ANY TRAFFIC PATTERN.

TEMPORARY/FINAL SIGNALS

P) NOTIFY THE ENGINEER TWO (2) MONTHS BEFORE A TRAFFIC SIGNAL INSTALLATION BY OTHERS IS REQUIRED.

Q) OTHERS WILL SHIFT AND REVISE ALL SIGNAL HEADS AS DIRECTED UNDER A SEPERATE CONTRACT ADMINSTERED FROM THE 13TH DIVISION TRAFFIC ENGINEERS OFFICE.

TRAFFIC CONTROL DEVICES

R) PLACE TYPE III BARRICADES, WITH "ROAD CLOSED" SIGN R11-2 ATTACHED, OF SUFFICIENT LENGTH TO CLOSE ENTIRE ROADWAY. STAGGER OR OVERLAP BARRICADES TO ALLOW FOR INGRESS OR EGRESS.

S) SPACE CHANNELIZING DEVICES IN WORK AREAS NO GREATER THAN TWICE THE POSTED SPEED LIMIT (MPH), EXCEPT 10 FT (3m) ON-CENTER IN RADII, AND 3 FT (1m) OFF THE EDGE OF AN OPEN TRAVELWAY, WHEN LANE CLOSURES ARE NOT IN EFFECT.

T) PLACE SETS OF THREE DRUMS PERPENDICULAR TO THE EDGE OF THE TRAVELWAY ON 500 FT. CENTERS WHEN UNOPENED LANES ARE CLOSED TO TRAFFIC. THESE DRUMS SHALL BE IN ADDITION TO THE CHANNELIZING DEVICES.

TRAFFIC BARRIER

U) INSTALL MOVABLE/PORTABLE CONCRETE BARRIER ACCORDING TO THE TRAFFIC CONTROL PLANS A MAXIMUM OF TWO (2) WEEKS PRIOR TO BEGINNING WORK IN ANY LOCATION. ONCE MOVABLE/PORTABLE CONCRETE BARRIER IS INSTALLED AT ANY LOCATION, PROCEED IN A CONTINUOUS MANNER TO COMPLETE THE PROPOSED WORK IN THAT LOCATION UNLESS OTHERWISE STATED IN THE TRAFFIC CONTROL PLANS OR AS DIRECTED BY THE ENGINEER.

ONCE MOVABLE/PORTABLE CONCRETE BARRIER IS INSTALLED AT ANY LOCATION AND NO WORK IS PERFORMED BEHIND THE MOVABLE/PORTABLE CONCRETE BARRIER FOR A PERID LONGER THAN TWO (2) MONTHS, REMOVE/RESET MOVABLE/CONCRETE BARRIER AT NO COST TO THE DEPARTMENT UNLESS OTHERWISE STATED IN THE TRAFFIC CONTROL PLANS, BARRIER IS PROTECTING A HAZARD, OR AS DIRECTED BY THE ENGINEER.

V) PROTECT THE APPROACH END OF MOVABLE/PORTABLE CONCRETE BARRIER AT ALL TIMES DURING THE INSTALLATION AND REMOVAL OF THE BARRIER BY EITHER A TRUCK MOUNTED IMPACT ATTENUATOR (MAXIMUM 72 HOURS) OR A TEMPORARY CRASH CUSHION.

PROTECT THE APPROACH END OF MOVABLE/PORTABLE CONCRETE BARRIER FROM ONCOMING TRAFFIC AT ALL TIMES BY A TEMPORARY CRASH CUSHION UNLESS THE APPROACH END OF MOVABLE/PORTABLE CONCRETE BARRIER IS OFFSET FROM ONCOMING TRAFFIC AS FOLLOWS OR AS SHOWN IN THE PLANS:

POSTED SPEED LIMIT	MINIMUM OFFSET
LESS THAN 50 MPH	20 FT.
50 MPH OR HIGHER	40 FT.

W) INSTALL MOVABLE/PORTABLE CONCRETE BARRIER WITH THE TRAFFIC FLOW, BEGINNING WITH THE UPSTREAM SIDE OF TRAFFIC. REMOVE MOVABLE/PORTABLE CONCRETE BARRIER AGAINST THE TRAFFIC FLOW, BEGINNING WITH THE DOWN STREAM SIDE OF TRAFFIC.

X) INSTALL AND SPACE DRUMS NO GREATER THAN TWICE THE POSTED SPEED LIMIT (MPH) TO CLOSE OR KEEP CLOSED THE SECTION OF THE ROADWAY UNTIL THE BARRIER CAN BE PLACED OR AFTER BARRRIER IS REMOVED.

PAVEMENT MARKINGS AND MARKERS

Y) INSTALL PAVEMENT MARKINGS AND MARKERS ON THE FINAL SURFACE AS FOLLOWS:

ROAD NAME	MARKING	MARKER
1. ALL ROADS	THERMOPLASTIC	SNOWPLOWABLE RAISED REFLECTIVE

Z) INSTALL TEMPORARY PAVEMENT MARKINGS AND TEMPORARY PAVEMENT MARKERS ON INTERIM LAYERS OF PAVEMENT AS FOLLOWS:

ROAD NAME	MARKING	MARKER
1. ALL ROADS	PAINT	RAISED REFLECTIVE (TEMPORARY)

AA) TIE PROPOSED PAVEMENT MARKING LINES TO EXISTING PAVEMENT MARKING LINES.

BB) REPLACE ANY PAVEMENT MARKINGS THAT HAVE BEEN OBLITERATED BY THE END OF EACH DAY'S OPERATION.

CC) PLACE AT LEAST TWO APPLICATIONS OF PAINT ON NEW ASPHALT WITH TEMPORARY TRAFFIC PATTERNS WHICH WILL REMAIN IN PLACE OVER THREE (3) MONTHS. PLACE ADDITIONAL APPLICATIONS OF PAINT UPON SUFFICIENT DRYING TIME, AS DETERMINED BY THE ENGINEER.

MISCELLANEOUS

DD) LOCATE AND CONSTRUCT PROPOSED CURB CUT FOR FUTURE WHEELCHAIR RAMPS AND/OR PROPOSED WHEELCHAIR RAMPS AS SHOWN IN ROADWAY STANDARD DRAWING 848.05 AND AS APPROVED OR DIRECTED BY THE ENGINEER.

EE) POLICE MAY BE USED TO MAINTAIN TRAFFIC THROUGH INTERSECTIONS.

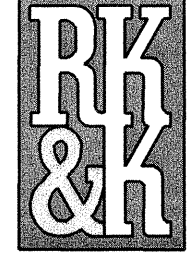
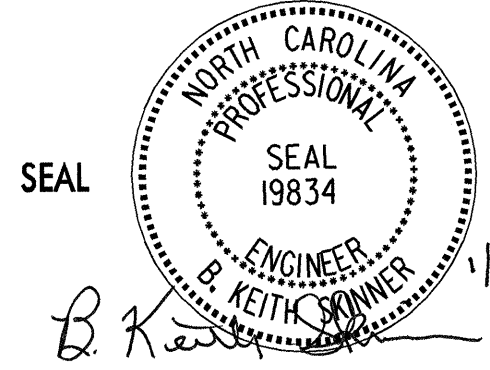
FF) ANY GUARDRAIL REMOVED WILL BE REPLACED BEFORE TRAFFIC IS RESTORED TO ITS ORIGINAL TRAFFIC PATTERN.

GG) WEDGE ALONG NC 226 PROPOSED WIDENING TO PREVENT PONDING OF WATER ADJACENT TO AN OPEN TRAVEL LANE AS DIRECTED AND APPROVED BY THE ENGINEER.

LOCAL NOTES

1. DO NOT STOP TRAFFIC FOR MORE THAN 10 MINUTES AS FOLLOWS:

OPERATION	ROAD NAME
SHIFTING TRAFFIC AND ANY OPERATION THAT REQUIRES TRAFFIC TO BE STOPPED	NC 226
INSTALL PROPOSED DRAINAGE PIPES LOCATED ACROSS NC 226 ONLY DURING THE HOURS OF 6:00 P.M. THROUGH 7:00 A.M. (SUNDAY THROUGH SATURDAY).	
PROPOSED CONSTRUCTION ALONG THE I-40 EASTBOUND/WESTBOUND RAMPS AND LOOPS AT NC 226 SHALL BE CONSTRUCTED BETWEEN THE HOURS OF 6:00 P.M. THROUGH 7:00 A.M. (SUNDAY THROUGH SATURDAY).	

 <p>PLANS PREPARED BY : RUMMEL KLEPPER & KAHL, LLP <i>consulting engineers</i> 900 RIDGEFIELD DRIVE SUITE 350 RALEIGH, NORTH CAROLINA 27609-3960</p> <p>FOR DIVISION OF HIGHWAYS</p>	 <p>SEAL 1/3/07</p>	<h2>PROJECT NOTES</h2>							
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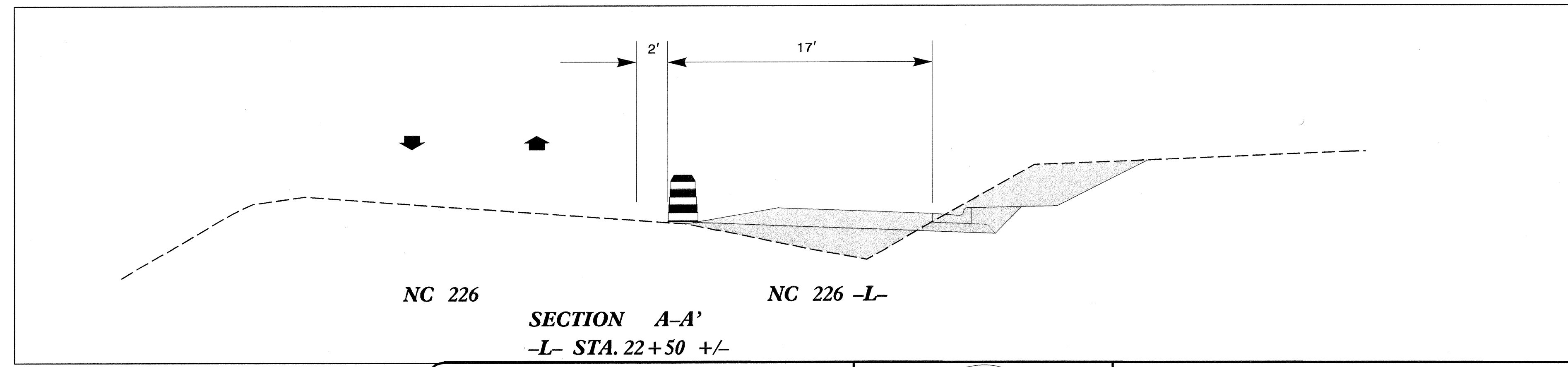
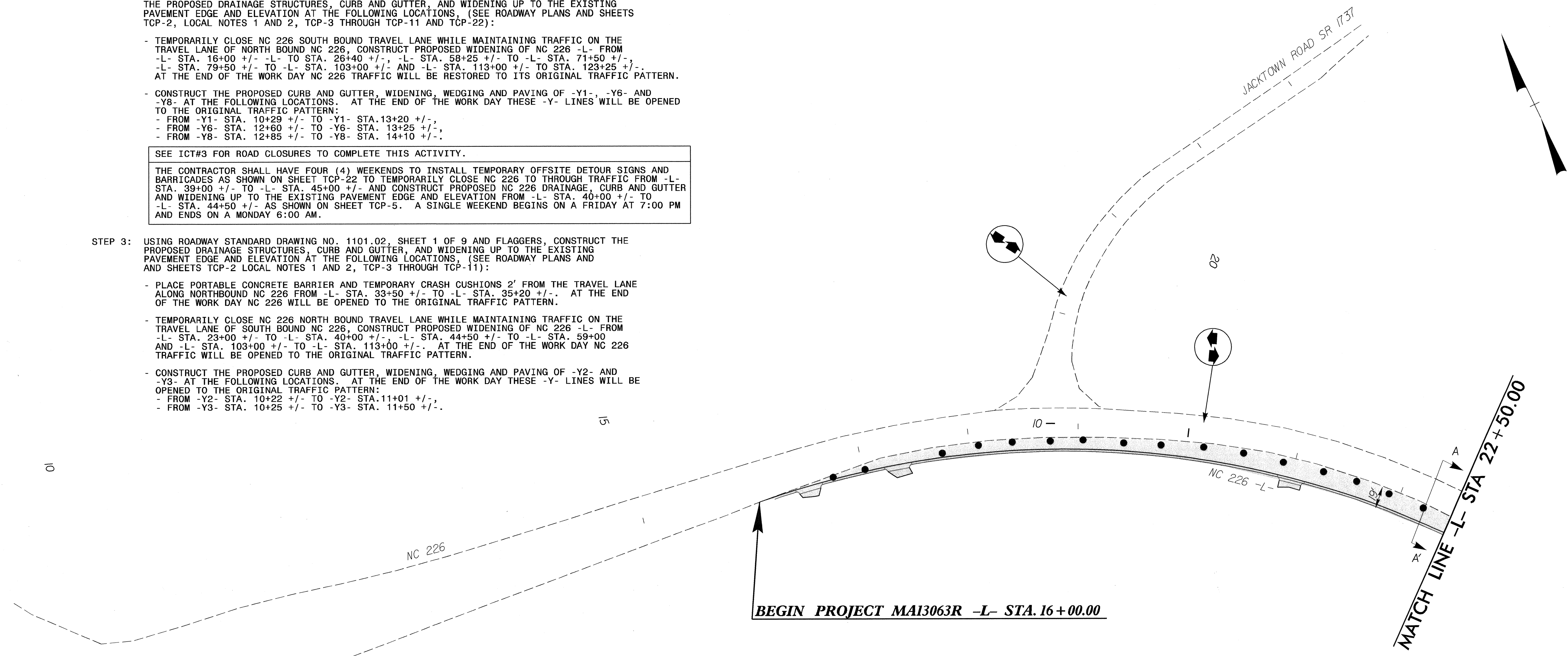
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- STEP 1: USING ROADWAY STANDARD DRAWING NO. 1101.02 SHEET 1 OF 9, PLACE ADVANCED WORK ZONE WARNING SIGNS ON ALL NC 226 -L- AND -Y- LINE APPROACHES TO THE PROJECT, (SEE SHEETS TCP-2, NCMA-1 AND NCMA-2).
- STEP 2: USING ROADWAY STANDARD DRAWING NO. 1101.02, SHEET 1 OF 9 AND FLAGGERS, CONSTRUCT THE PROPOSED DRAINAGE STRUCTURES, CURB AND GUTTER, AND WIDENING UP TO THE EXISTING PAVEMENT EDGE AND ELEVATION AT THE FOLLOWING LOCATIONS, (SEE ROADWAY PLANS AND SHEETS TCP-2, LOCAL NOTES 1 AND 2, TCP-3 THROUGH TCP-11 AND TCP-22):
- TEMPORARILY CLOSE NC 226 SOUTH BOUND TRAVEL LANE WHILE MAINTAINING TRAFFIC ON THE TRAVEL LANE OF NORTH BOUND NC 226, CONSTRUCT PROPOSED WIDENING OF NC 226 -L- FROM -L- STA. 16+00 +/- -L- TO STA. 26+40 +/-, -L- STA. 58+25 +/- TO -L- STA. 71+50 +/-, -L- STA. 79+50 +/- TO -L- STA. 103+00 +/- AND -L- STA. 113+00 +/- TO STA. 123+25 +/-, AT THE END OF THE WORK DAY NC 226 TRAFFIC WILL BE RESTORED TO ITS ORIGINAL TRAFFIC PATTERN.
 - CONSTRUCT THE PROPOSED CURB AND GUTTER, WIDENING, WEDGING AND PAVING OF -Y1-, -Y6- AND -Y8- AT THE FOLLOWING LOCATIONS. AT THE END OF THE WORK DAY THESE -Y- LINES WILL BE OPENED TO THE ORIGINAL TRAFFIC PATTERN:
 - FROM -Y1- STA. 10+29 +/- TO -Y1- STA. 13+20 +/-,
 - FROM -Y6- STA. 12+60 +/- TO -Y6- STA. 13+25 +/-,
 - FROM -Y8- STA. 12+85 +/- TO -Y8- STA. 14+10 +/-.

SEE ICT#3 FOR ROAD CLOSURES TO COMPLETE THIS ACTIVITY.

THE CONTRACTOR SHALL HAVE FOUR (4) WEEKENDS TO INSTALL TEMPORARY OFFSITE DETOUR SIGNS AND BARRICADES AS SHOWN ON SHEET TCP-22 TO TEMPORARILY CLOSE NC 226 TO THROUGH TRAFFIC FROM -L- STA. 39+00 +/- TO -L- STA. 45+00 +/- AND CONSTRUCT PROPOSED NC 226 DRAINAGE, CURB AND GUTTER AND WIDENING UP TO THE EXISTING PAVEMENT EDGE AND ELEVATION FROM -L- STA. 40+00 +/- TO -L- STA. 44+50 +/- AS SHOWN ON SHEET TCP-5. A SINGLE WEEKEND BEGINS ON A FRIDAY AT 7:00 PM AND ENDS ON A MONDAY 6:00 AM.

- STEP 3: USING ROADWAY STANDARD DRAWING NO. 1101.02, SHEET 1 OF 9 AND FLAGGERS, CONSTRUCT THE PROPOSED DRAINAGE STRUCTURES, CURB AND GUTTER, AND WIDENING UP TO THE EXISTING PAVEMENT EDGE AND ELEVATION AT THE FOLLOWING LOCATIONS, (SEE ROADWAY PLANS AND SHEETS TCP-2 LOCAL NOTES 1 AND 2, TCP-3 THROUGH TCP-11):
- PLACE PORTABLE CONCRETE BARRIER AND TEMPORARY CRASH CUSHIONS 2' FROM THE TRAVEL LANE ALONG NORTHBOUND NC 226 FROM -L- STA. 33+50 +/- TO -L- STA. 35+20 +/-, AT THE END OF THE WORK DAY NC 226 WILL BE OPENED TO THE ORIGINAL TRAFFIC PATTERN.
 - TEMPORARILY CLOSE NC 226 NORTH BOUND TRAVEL LANE WHILE MAINTAINING TRAFFIC ON THE TRAVEL LANE OF SOUTH BOUND NC 226, CONSTRUCT PROPOSED WIDENING OF NC 226 -L- FROM -L- STA. 23+00 +/- TO -L- STA. 40+00 +/-, -L- STA. 44+50 +/- TO -L- STA. 59+00 AND -L- STA. 103+00 +/- TO -L- STA. 113+00 +/-, AT THE END OF THE WORK DAY NC 226 TRAFFIC WILL BE OPENED TO THE ORIGINAL TRAFFIC PATTERN.
 - CONSTRUCT THE PROPOSED CURB AND GUTTER, WIDENING, WEDGING AND PAVING OF -Y2- AND -Y3- AT THE FOLLOWING LOCATIONS. AT THE END OF THE WORK DAY THESE -Y- LINES WILL BE OPENED TO THE ORIGINAL TRAFFIC PATTERN:
 - FROM -Y2- STA. 10+22 +/- TO -Y2- STA. 11+01 +/-,
 - FROM -Y3- STA. 10+25 +/- TO -Y3- STA. 11+50 +/-.



NOTE: MAINTAIN EGRESS/INGRESS TO ALL PROPERTIES DURING CONSTRUCTION WITH INCIDENTAL STONE BASE OR MATERIAL SUITABLE AND APPROVED BY THE ENGINEER.

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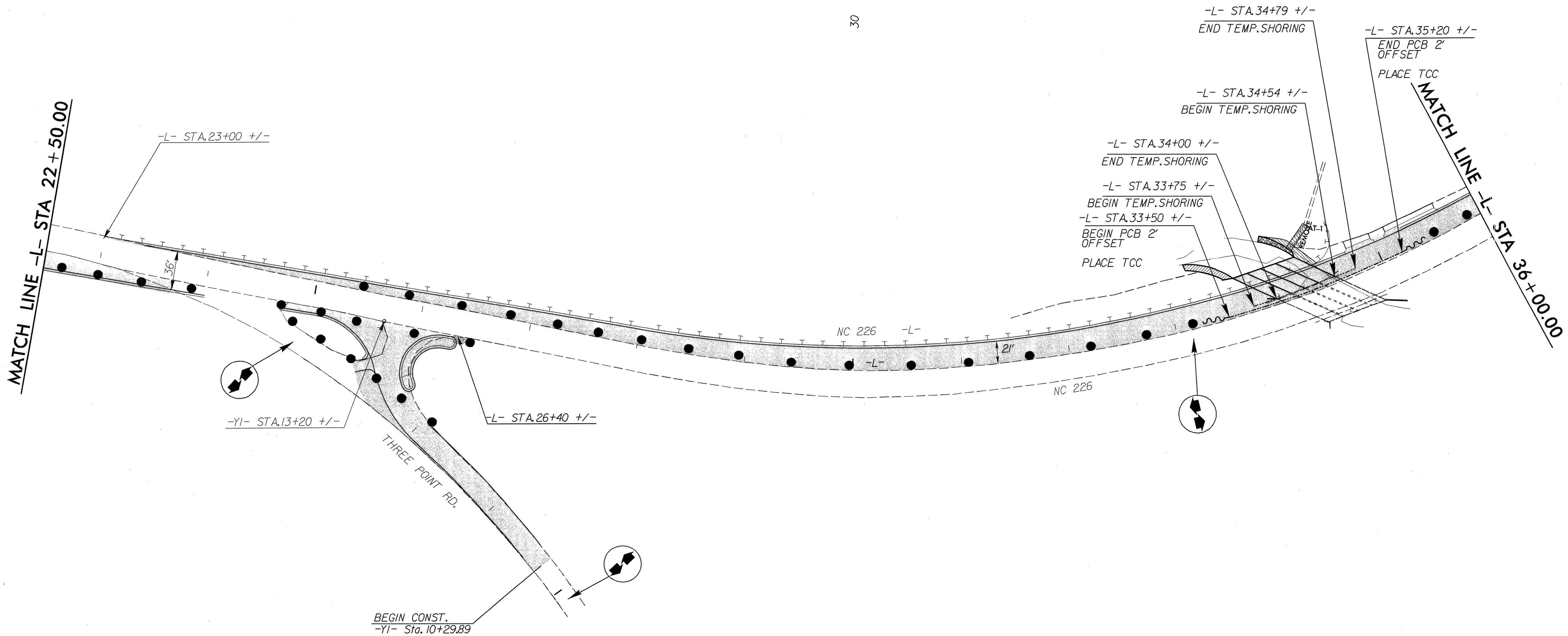
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THE FOLLOWING SOIL PARAMETERS WERE FURNISHED BY NCDOT-DESIGN SERVICES SOILS AND FOUNDATION SECTION. THE FOLLOWING SOIL PARAMETERS SHALL BE USED FOR THE DESIGN OF TEMPORARY SHORING.

UNIT WEIGHT OF SOIL, = 120 lb/ft³ ABOVE THE WATER TABLE
 UNIT WEIGHT OF SOIL, = 60 lb/ft³ BELOW THE WATER TABLE
 FRICTION ANGLE, = 30
 COHESION, c = 0 lb/ft²



NOTE: MAINTAIN EGRESS/INGRESS TO ALL PROPERTIES DURING CONSTRUCTION WITH INCIDENTAL STONE BASE OR MATERIAL SUITABLE AND APPROVED BY THE ENGINEER.

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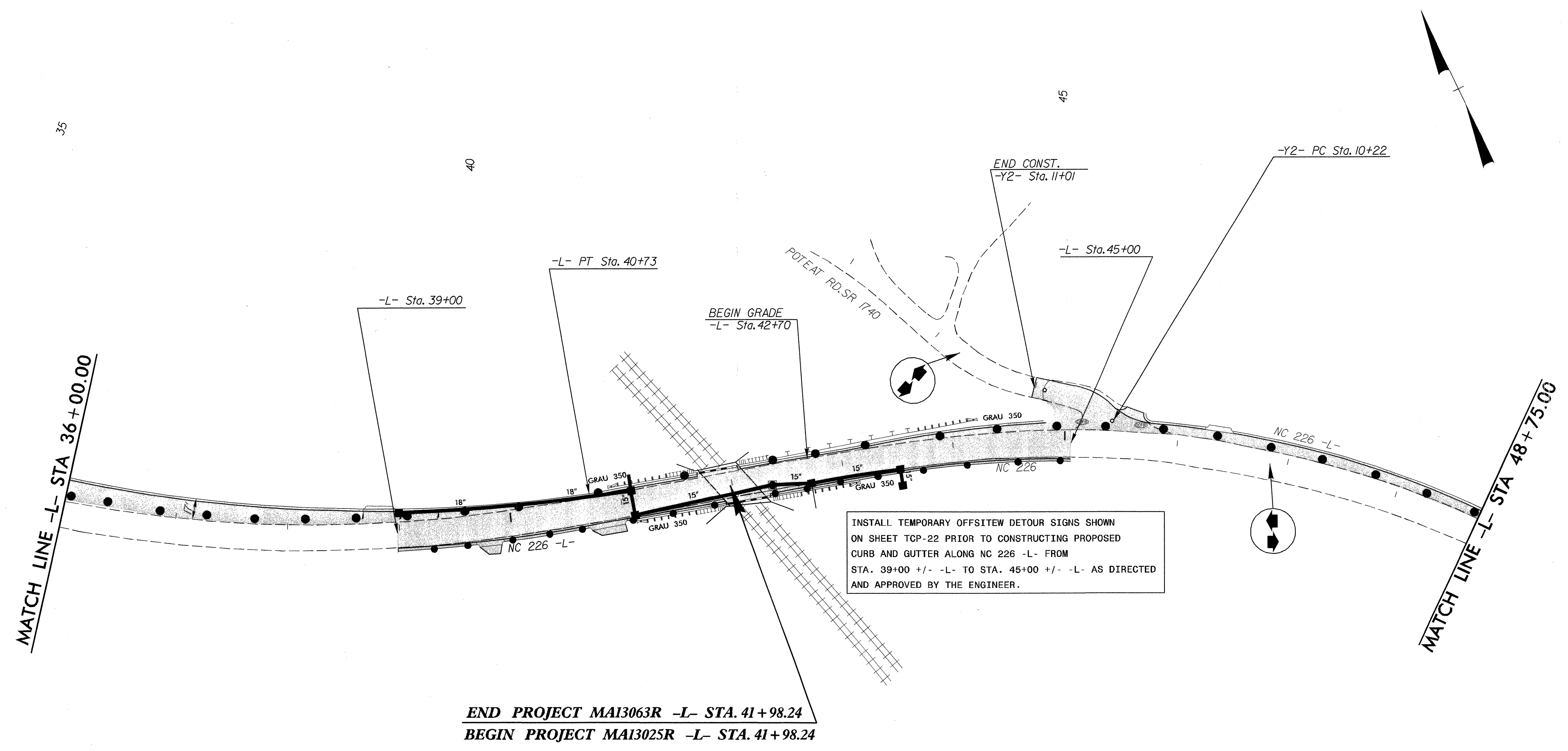
FOR
DIVISION OF HIGHWAYS

SEAL

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 11/8/06

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INSTALL TEMPORARY OFFSITE DETOUR SIGNS SHOWN ON SHEET TCP-22 PRIOR TO CONSTRUCTING PROPOSED CURB AND GUTTER ALONG NC 226 -L- FROM STA. 39+00 +/- -L- TO STA. 45+00 +/- -L- AS DIRECTED AND APPROVED BY THE ENGINEER.

NOTE: MAINTAIN EGRESS/INGRESS TO ALL PROPERTIES DURING CONSTRUCTION WITH INCIDENTAL STONE BASE OR MATERIAL SUITABLE AND APPROVED BY THE ENGINEER.

END PROJECT MAI3063R -L- STA. 41+98.24
BEGIN PROJECT MAI3025R -L- STA. 41+98.24

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DIVISION OF HIGHWAYS

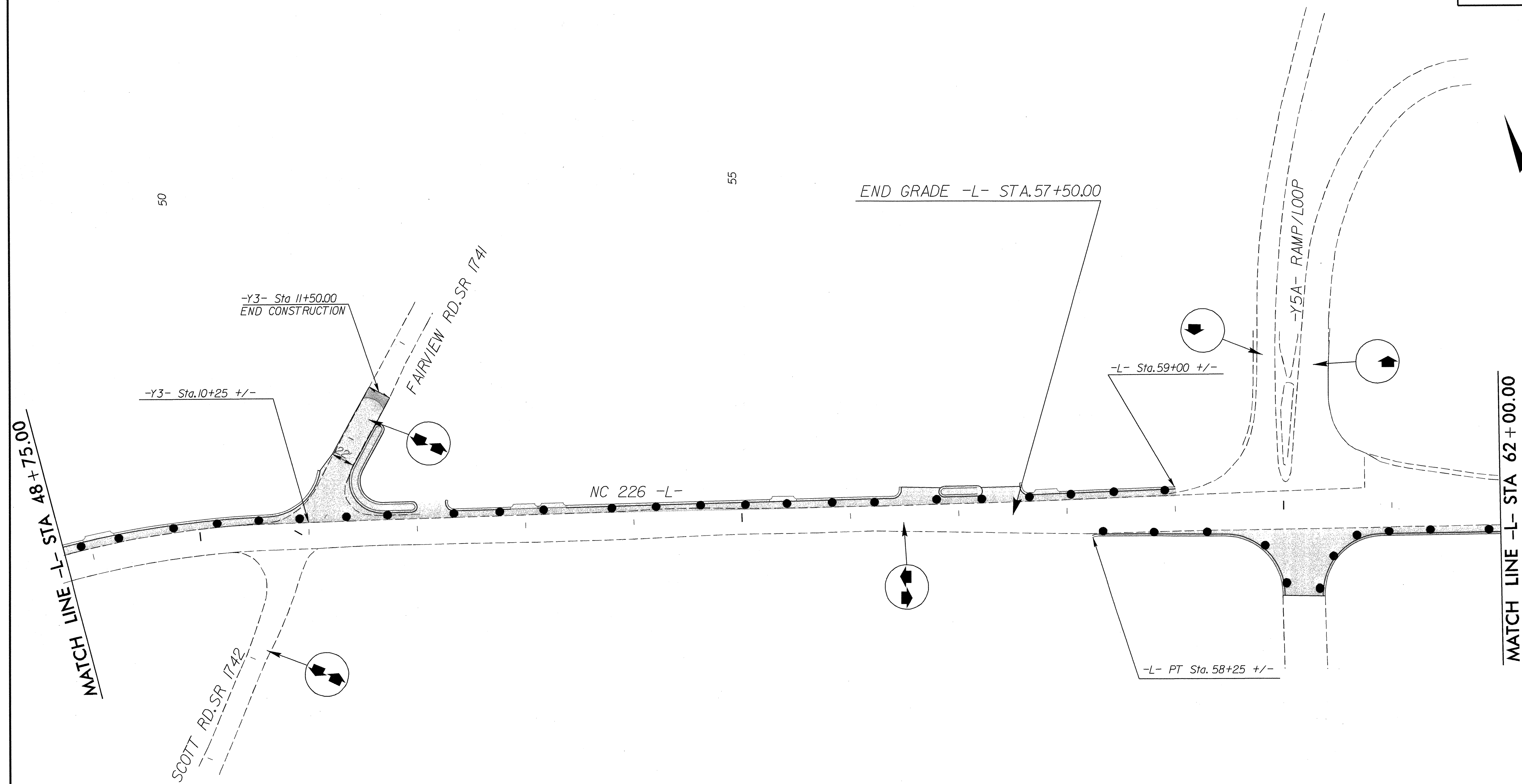
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NOTE: MAINTAIN EGRESS/INGRESS TO ALL PROPERTIES DURING CONSTRUCTION WITH INCIDENTAL STONE BASE OR MATERIAL SUITABLE AND APPROVED BY THE ENGINEER.

11/2/2006
 I:\Projects\11-06\11-06-TCP\TCP\PHASE I\TCP06.DGN
 mcole

PLANS PREPARED BY :
RUMMEL KLEPPER & KAHL, LLP
consulting engineers
 900 RIDGEFIELD DRIVE SUITE 350
 RALEIGH, NORTH CAROLINA 27609-3960
FOR
DIVISION OF HIGHWAYS

SEAL

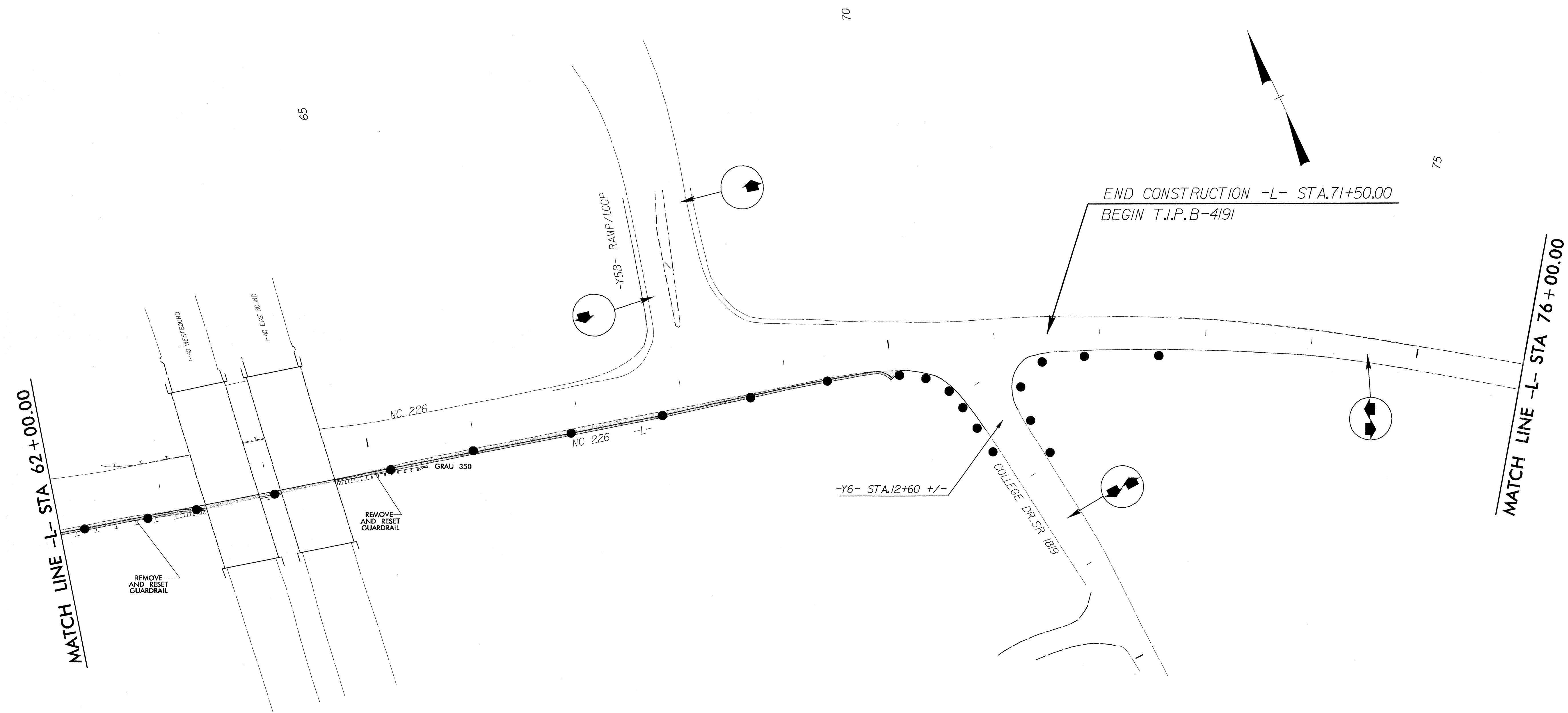
B. Kelly Sk
 11/8/06

PHASE I

SCALE: NONE		REVISIONS
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DESIGN BY: MAC		
REVIEWED BY: BKS		

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PROJ. REFERENCE NO.	SHEET NO.
37857 & 37645	TCP-7
MA13063R & MA13025R	



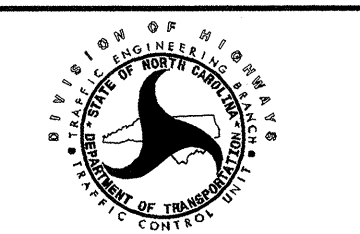
NOTE: MAINTAIN EGRESS/INGRESS TO ALL PROPERTIES DURING CONSTRUCTION WITH INCIDENTAL STONE BASE OR MATERIAL SUITABLE AND APPROVED BY THE ENGINEER.

PLANS PREPARED BY :
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FOR
DIVISION OF HIGHWAYS

SEAL
 NORTH CAROLINA PROFESSIONAL ENGINEER
 SEAL 19834
 B. Keiser
 11/8/06

PHASE I

SCALE: NONE
 DATE: 11/06
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 DESIGN BY: MAC
 REVIEWED BY: BKS



REVISIONS	

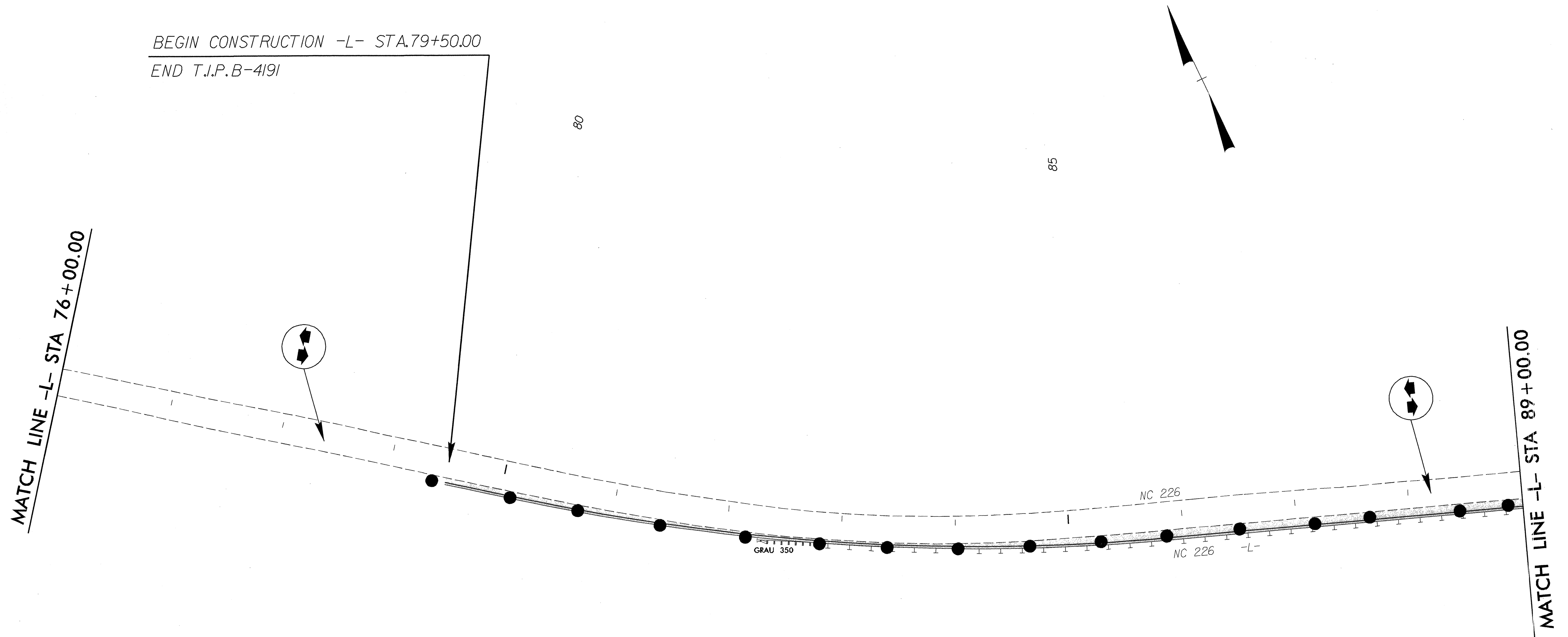
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PROJ. REFERENCE NO.	SHEET NO.
37857 & 37645	TCP-8
MA13063R & MA13025R	

BEGIN CONSTRUCTION -L- STA.79+50.00
 END T.I.P.B-4191

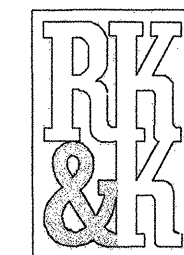
MATCH LINE -L- STA 76+00.00

MATCH LINE -L- STA 89+00.00



NOTE: MAINTAIN EGRESS/INGRESS TO ALL PROPERTIES DURING CONSTRUCTION WITH INCIDENTAL STONE BASE OR MATERIAL SUITABLE AND APPROVED BY THE ENGINEER.

11/2/2006 \\c:\trafficcontrol\tcp\tcpmp\phase\tcp08.dgn

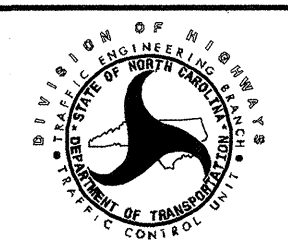


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SEAL

PHASE I

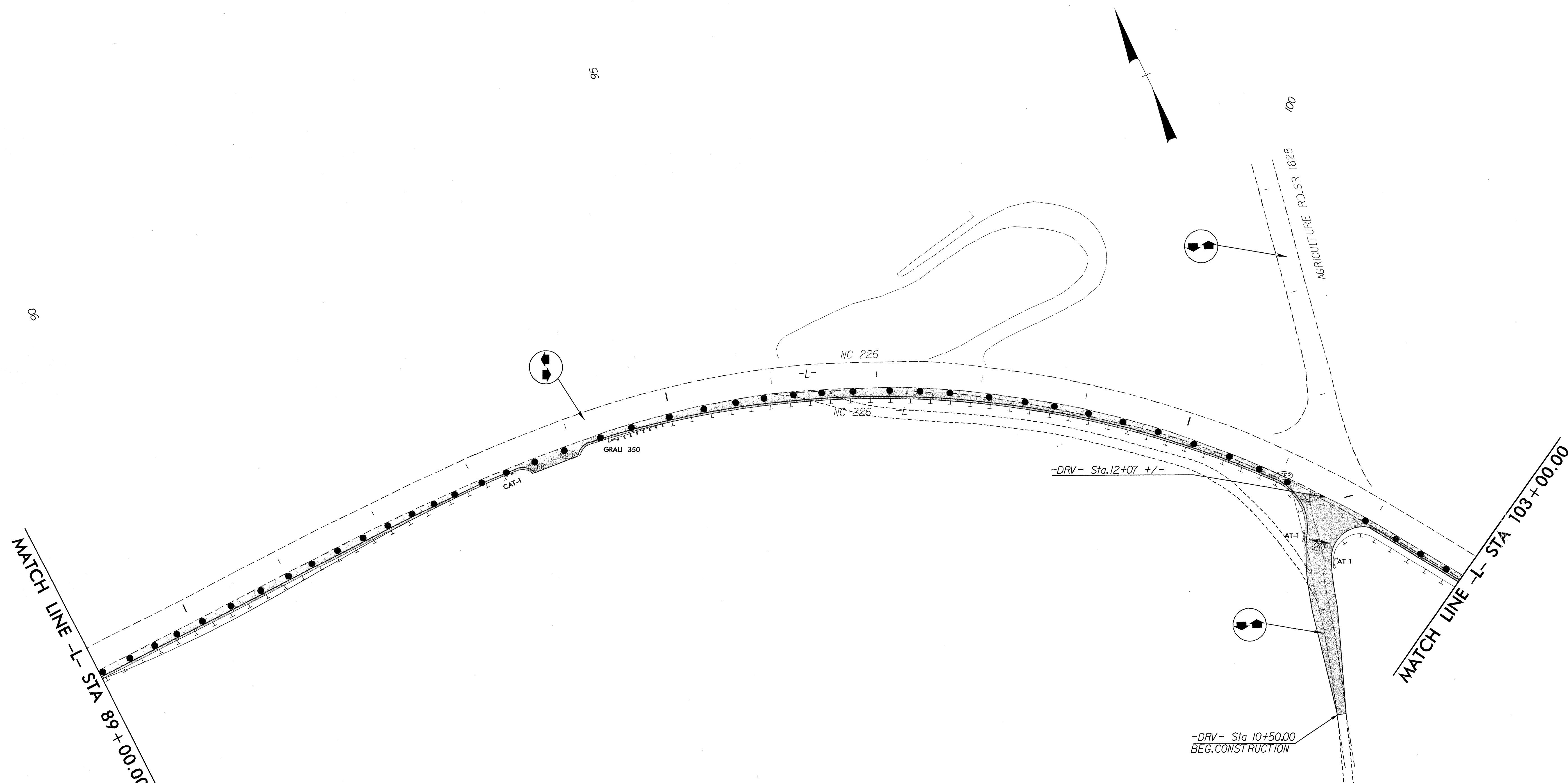
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REVISIONS

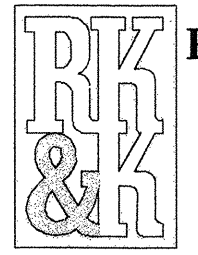
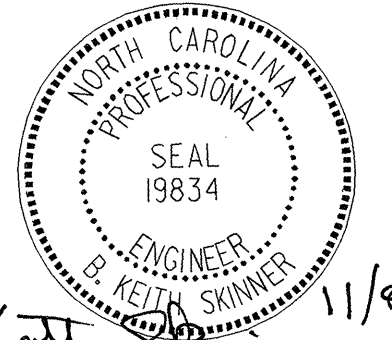
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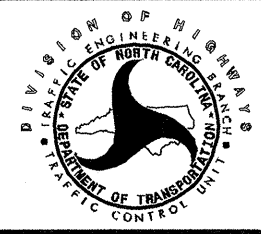
PROJ. REFERENCE NO.	SHEET NO.
37857 & 37645	TCP-9
MA13063R & MA13025R	

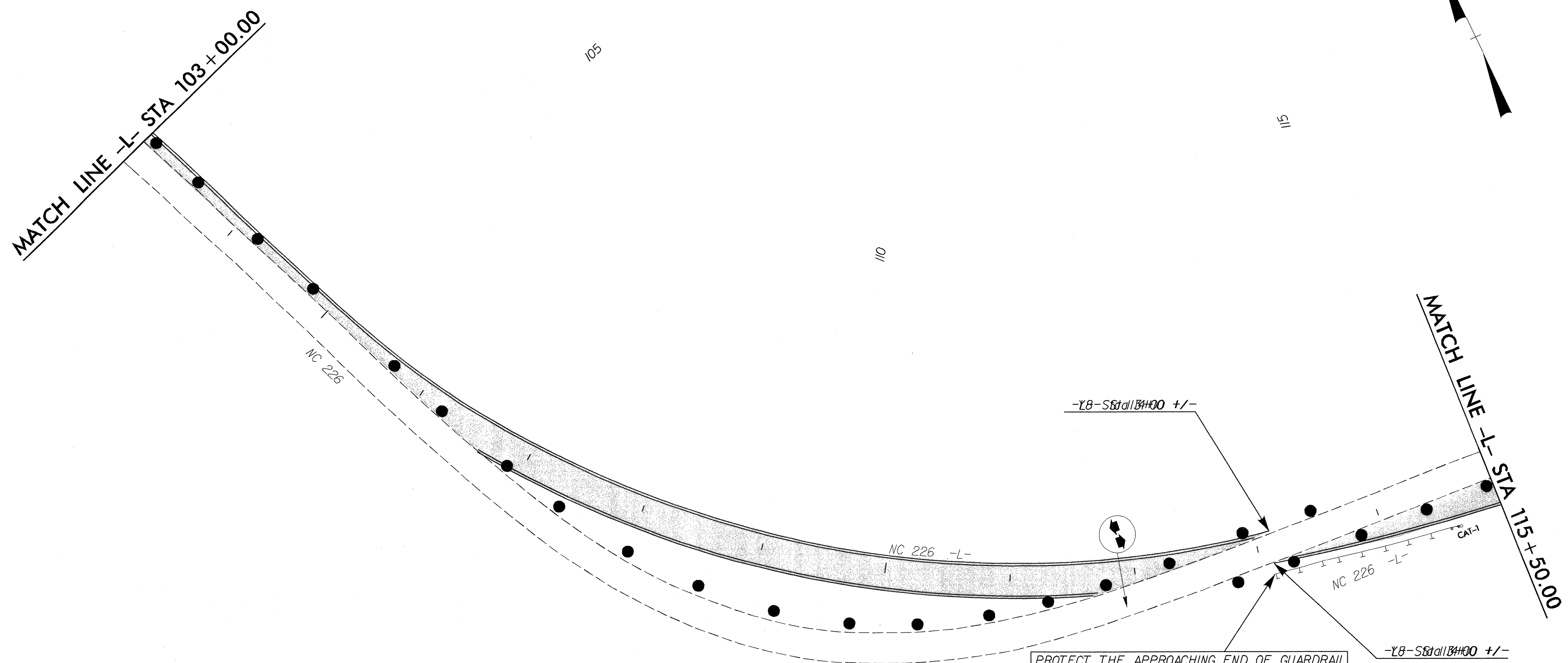


NOTE: MAINTAIN EGRESS/INGRESS TO ALL PROPERTIES DURING CONSTRUCTION WITH INCIDENTAL STONE BASE OR MATERIAL SUITABLE AND APPROVED BY THE ENGINEER.

11/2/2006 R:\Traffic\TCP\TCPMP\PHASE1\top09.dgn mcole

 PLANS PREPARED BY : RUMMEL KLEPPER & KAHL, LLP <i>consulting engineers</i> 900 RIDGEFIELD DRIVE SUITE 350 RALEIGH, NORTH CAROLINA 27609-3960 FOR DIVISION OF HIGHWAYS	 SEAL 19834 KEVIN SKINNER ENGINEER B. K. Skinner 11/8/06	PHASE I							
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REVISIONS									

	CADD FILE:	TCP09.DGN
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PROTECT THE APPROACHING END OF GUARDRAIL WITH APPROPRIATE END TREATMENT OR A TRUCK MOUNTED ATTENUATOR AT ALL TIMES UNTIL REMAINING GUARDRAIL CAN BE INSTALLED.

NOTE: MAINTAIN EGRESS/INGRESS TO ALL PROPERTIES DURING CONSTRUCTION WITH INCIDENTAL STONE BASE OR MATERIAL SUITABLE AND APPROVED BY THE ENGINEER.

11/2/2006
 R:\Projects\TrafficControl\TCP\TCPMP\PHASE1\top10.dgn
 BKS

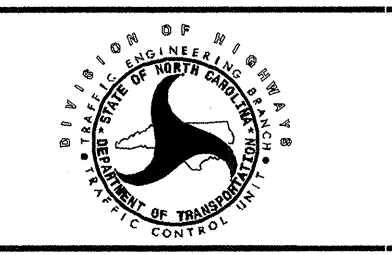
PLANS PREPARED BY :
RUMMEL KLEPPER & KAHL, LLP
consulting engineers
 900 RIDGEFIELD DRIVE SUITE 350
 RALEIGH, NORTH CAROLINA 27609-3960
FOR
DIVISION OF HIGHWAYS

SEAL

 B. Keith Summer 11/8/06

PHASE I

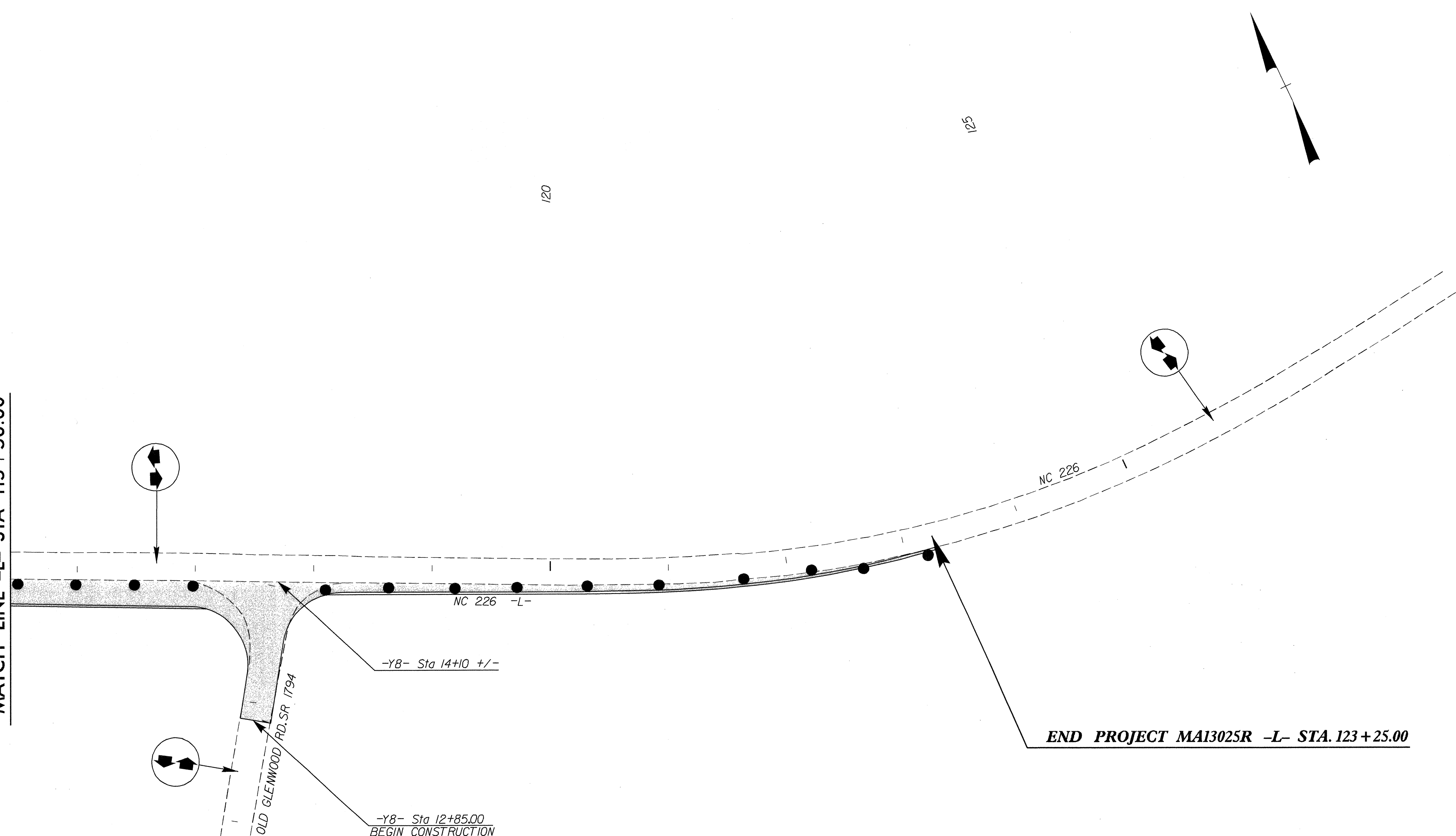
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 DESIGN BY: MAC
 REVIEWED BY: BKS



REVISIONS	

CADD FILE: TCP10.DGN

MATCH LINE -L- STA 115 + 50.00



END PROJECT MA13025R -L- STA. 123 + 25.00

NOTE: MAINTAIN EGRESS/INGRESS TO ALL PROPERTIES DURING CONSTRUCTION WITH INCIDENTAL STONE BASE OR MATERIAL SUITABLE AND APPROVED BY THE ENGINEER.

11/2/2006
 F:\T\off\c\TrafficControl\TCP\TCPMP\PHASE1\TCP11.dgn
 mcole

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 RALEIGH, NORTH CAROLINA 27609-3960
FOR
DIVISION OF HIGHWAYS

SEAL
 NORTH CAROLINA
 PROFESSIONAL
 ENGINEER
 SEAL
 19834
 B. Keith Skinner
 11/08/06

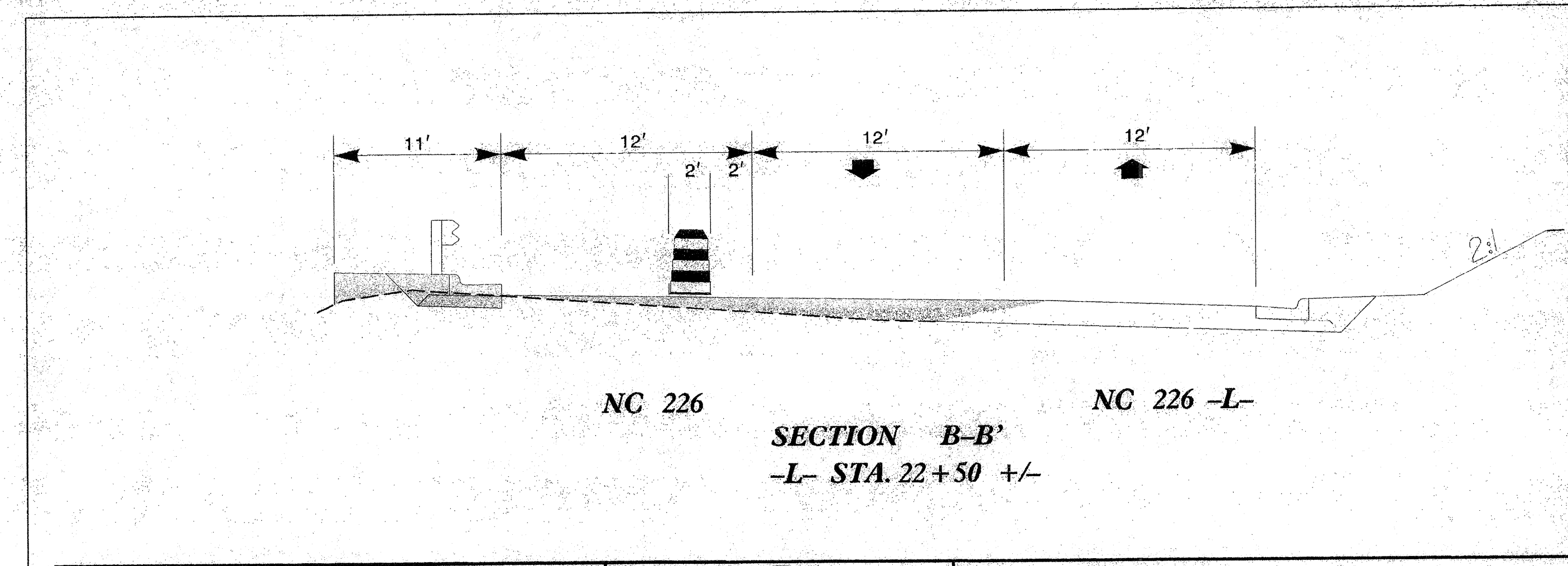
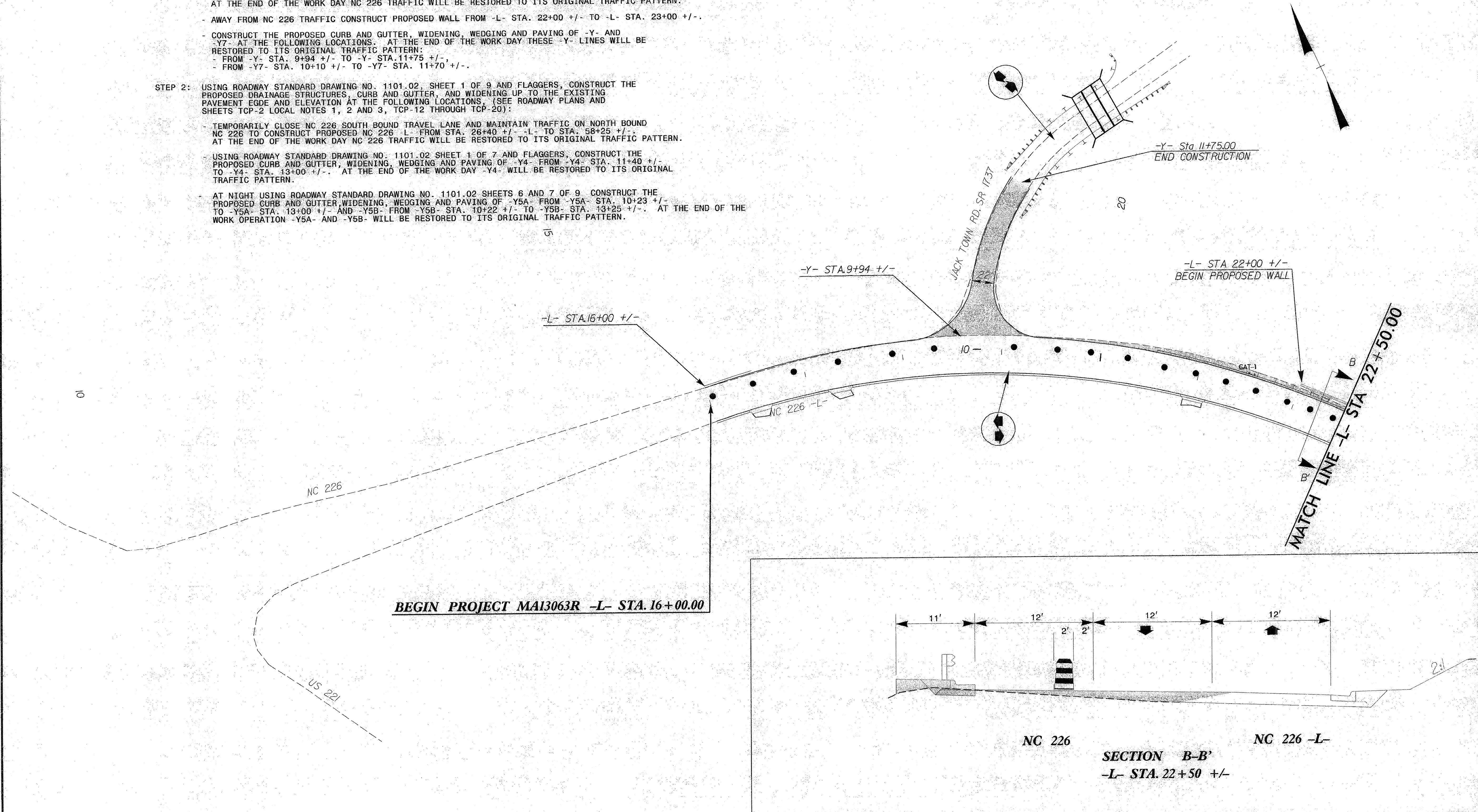
PHASE I

SCALE: NONE		REVISIONS
DATE: 11/06		
DWG. BY: MAC		
DESIGN BY: MAC		
REVIEWED BY: BKS		

CADD FILE: TCP11.DGN

PHASE II

- STEP 1: USING ROADWAY STANDARD DRAWING NO. 1101.02, SHEET 1 OF 9 AND FLAGGERS, CONSTRUCT THE PROPOSED DRAINAGE STRUCTURES, CURB AND GUTTER, AND WIDENING UP TO THE EXISTING PAVEMENT EDGE AND ELEVATION AT THE FOLLOWING LOCATIONS. (SEE ROADWAY PLANS AND SHEETS TCP-2 LOCAL NOTES 1, 2 AND 3, TCP-12 THROUGH TCP-20):
- TEMPORARILY CLOSE NC 226 NORTH BOUND TRAVEL LANE AND MAINTAIN TRAFFIC ON SOUTH BOUND NC 226 TO CONSTRUCT PROPOSED NC 226 -L- FROM STA. 16+00 +/- -L- TO STA. 23+00 +/-, -L- STA. 59+00 +/- TO -L- STA. 71+50 +/- AND -L- STA. 79+50 +/- TO -L- STA. 123+25 +/-, AT THE END OF THE WORK DAY NC 226 TRAFFIC WILL BE RESTORED TO ITS ORIGINAL TRAFFIC PATTERN.
 - AWAY FROM NC 226 TRAFFIC CONSTRUCT PROPOSED WALL FROM -L- STA. 22+00 +/- TO -L- STA. 23+00 +/-.
 - CONSTRUCT THE PROPOSED CURB AND GUTTER, WIDENING, WEDGING AND PAVING OF -Y- AND -Y7- AT THE FOLLOWING LOCATIONS. AT THE END OF THE WORK DAY THESE -Y- LINES WILL BE RESTORED TO ITS ORIGINAL TRAFFIC PATTERN:
 - FROM -Y- STA. 9+94 +/- TO -Y- STA. 11+75 +/-,
 - FROM -Y7- STA. 10+10 +/- TO -Y7- STA. 11+70 +/-.
- STEP 2: USING ROADWAY STANDARD DRAWING NO. 1101.02, SHEET 1 OF 9 AND FLAGGERS, CONSTRUCT THE PROPOSED DRAINAGE STRUCTURES, CURB AND GUTTER, AND WIDENING UP TO THE EXISTING PAVEMENT EDGE AND ELEVATION AT THE FOLLOWING LOCATIONS. (SEE ROADWAY PLANS AND SHEETS TCP-2 LOCAL NOTES 1, 2 AND 3, TCP-12 THROUGH TCP-20):
- TEMPORARILY CLOSE NC 226 SOUTH BOUND TRAVEL LANE AND MAINTAIN TRAFFIC ON NORTH BOUND NC 226 TO CONSTRUCT PROPOSED NC 226 -L- FROM STA. 26+40 +/- -L- TO STA. 58+25 +/-, AT THE END OF THE WORK DAY NC 226 TRAFFIC WILL BE RESTORED TO ITS ORIGINAL TRAFFIC PATTERN.
 - USING ROADWAY STANDARD DRAWING NO. 1101.02 SHEET 1 OF 7 AND FLAGGERS, CONSTRUCT THE PROPOSED CURB AND GUTTER, WIDENING, WEDGING AND PAVING OF -Y4- FROM -Y4- STA. 11+40 +/- TO -Y4- STA. 13+00 +/-, AT THE END OF THE WORK DAY -Y4- WILL BE RESTORED TO ITS ORIGINAL TRAFFIC PATTERN.
 - AT NIGHT USING ROADWAY STANDARD DRAWING NO. 1101.02 SHEETS 6 AND 7 OF 9 CONSTRUCT THE PROPOSED CURB AND GUTTER, WIDENING, WEDGING AND PAVING OF -Y5A- FROM -Y5A- STA. 10+23 +/- TO -Y5A- STA. 13+00 +/- AND -Y5B- FROM -Y5B- STA. 10+22 +/- TO -Y5B- STA. 13+25 +/-, AT THE END OF THE WORK OPERATION -Y5A- AND -Y5B- WILL BE RESTORED TO ITS ORIGINAL TRAFFIC PATTERN.



NOTE: MAINTAIN EGRESS/INGRESS TO ALL PROPERTIES DURING CONSTRUCTION WITH INCIDENTAL STONE BASE OR MATERIAL SUITABLE AND APPROVED BY THE ENGINEER.

PLANS PREPARED BY :
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consulting engineers
 900 RIDGEFIELD DRIVE SUITE 350
 RALEIGH, NORTH CAROLINA 27609-3960
 FOR
DIVISION OF HIGHWAYS

SEAL

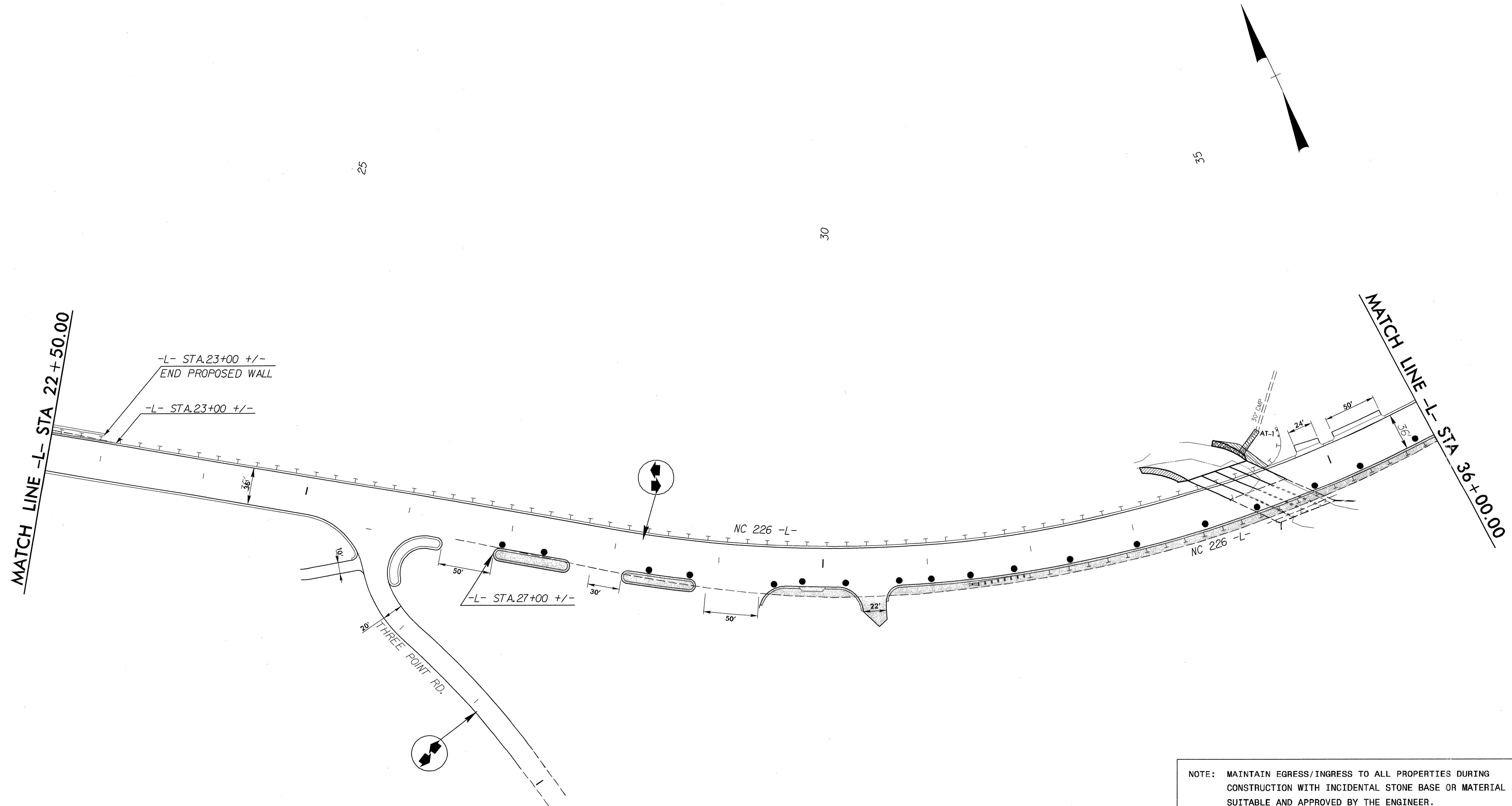
 12/7/06

PHASE II

SCALE: NONE		REVISIONS
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DWG. BY: MAC		
DESIGN BY: MAC		
REVIEWED BY: BKS		

CADD FILE: TCP12.DGN

P:\7-2506-1\PROJECTS\37857 & 37645\TCP-12\MA13063R.dwg
 11/15/06 10:41 AM
 11/15/06 10:41 AM
 11/15/06 10:41 AM



-L- STA.23+00 +/-
END PROPOSED WALL

-L- STA.23+00 +/-

-L- STA.27+00 +/-

NC 226 -L-

NC 226 -L-

NOTE: MAINTAIN EGRESS/INGRESS TO ALL PROPERTIES DURING CONSTRUCTION WITH INCIDENTAL STONE BASE OR MATERIAL SUITABLE AND APPROVED BY THE ENGINEER.

11/3/2006
F:\TrafficControl\TCP\TCPMP\PhaseII\tcp3.dgn
moole

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consulting engineers
 900 RIDGEFIELD DRIVE SUITE 350
 RALEIGH, NORTH CAROLINA 27609-3960
FOR
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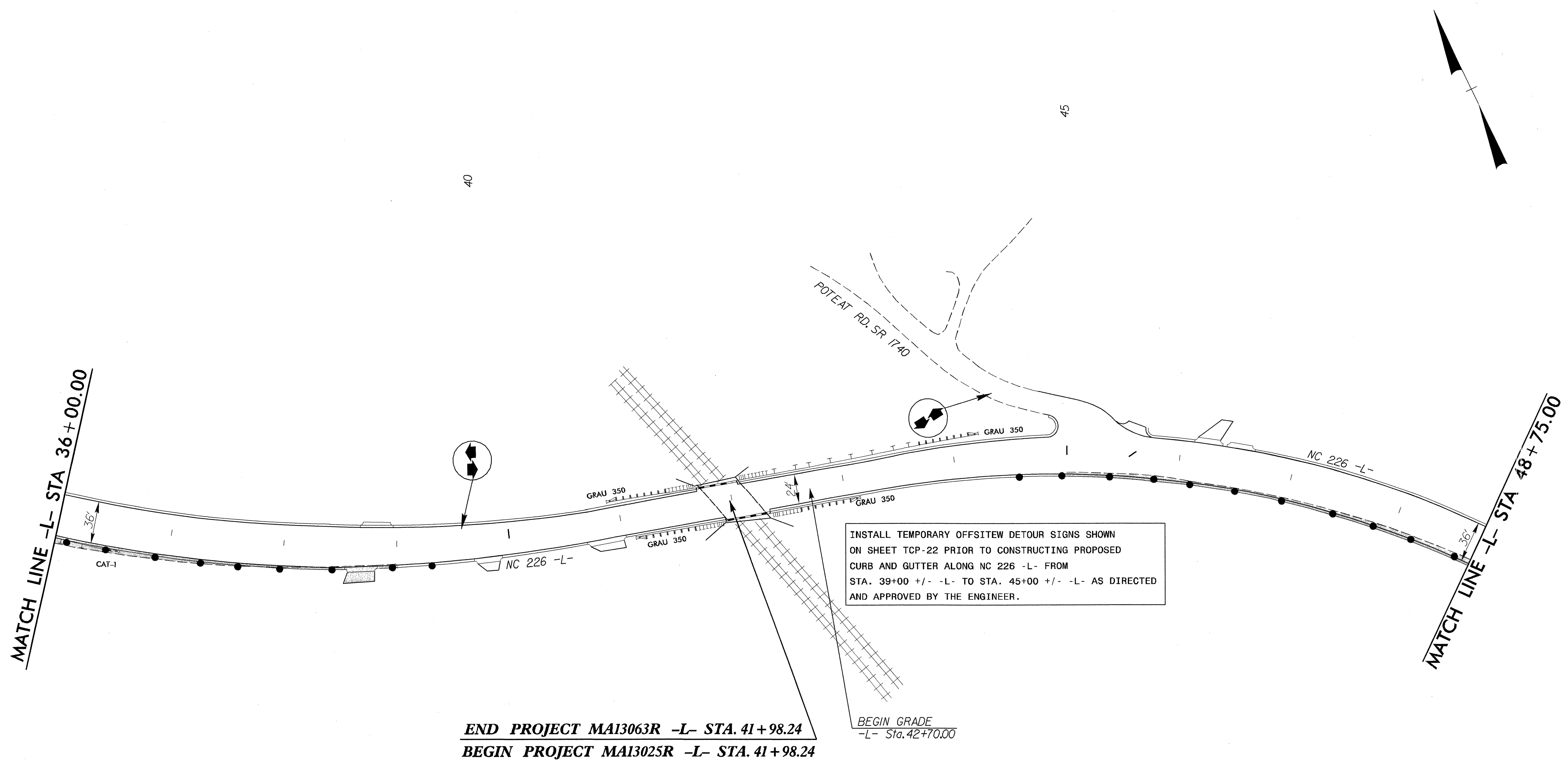
SEAL

B. Keith

PHASE II

SCALE: NONE		REVISIONS
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REVIEWED BY: BKS		
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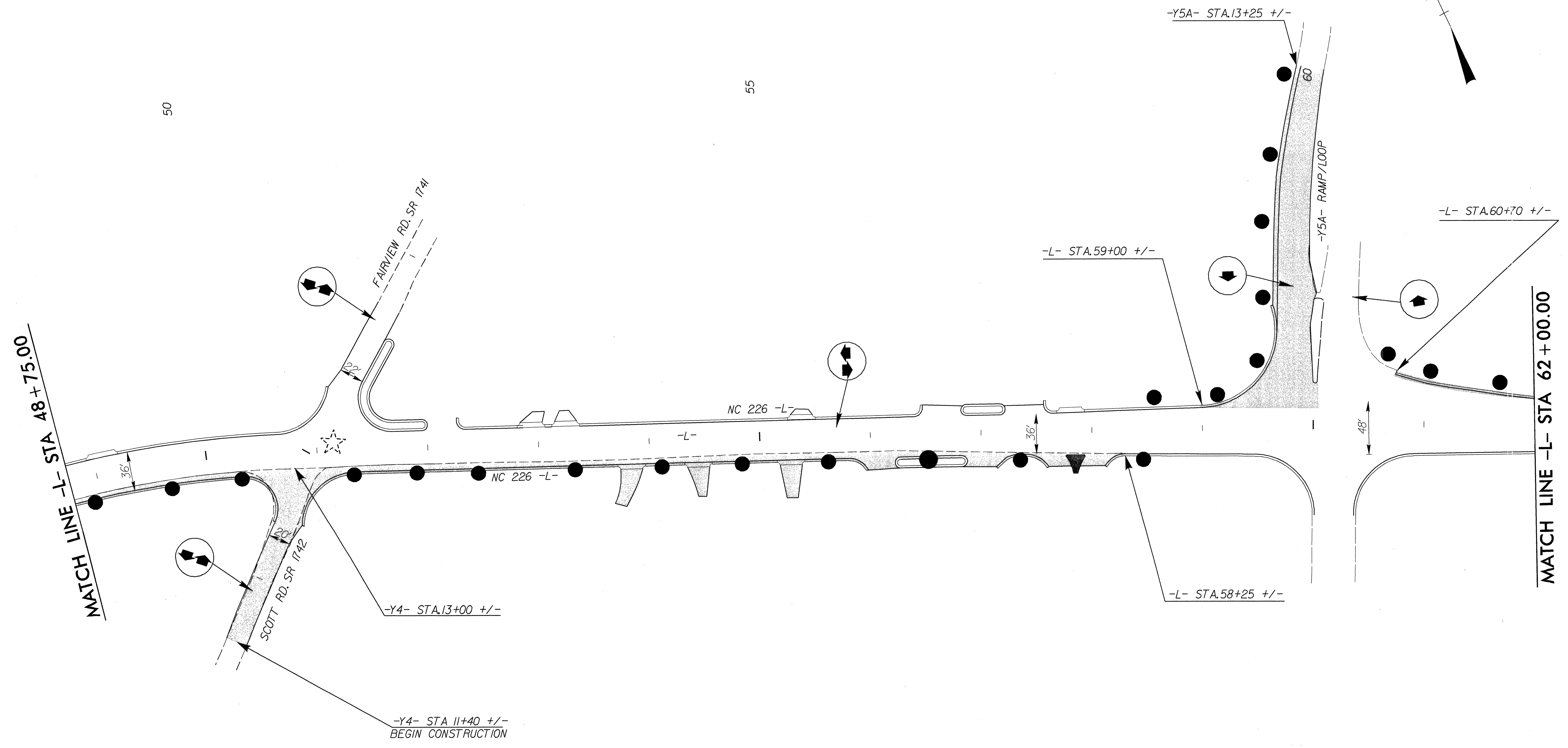
PROJ. REFERENCE NO.	SHEET NO.
37857 & 37645	TCP-14
MA13063R & MA13025R	



NOTE: MAINTAIN EGRESS/INGRESS TO ALL PROPERTIES DURING CONSTRUCTION WITH INCIDENTAL STONE BASE OR MATERIAL SUITABLE AND APPROVED BY THE ENGINEER.

11/7/2006 \\p0111c\TrafficControl\TCP\TCPPMP\PhaseII\top14.dgn mcdib

<p>PLANS PREPARED BY : RUMMEL KLEPPER & KAHL, LLP <i>consulting engineers</i> 900 RIDGEFIELD DRIVE SUITE 350 RALEIGH, NORTH CAROLINA 27609-3960</p> <p>FOR DIVISION OF HIGHWAYS</p>	<p>SEAL B. Keith Skinner 11/8/06</p>	PHASE II					
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REVISIONS							



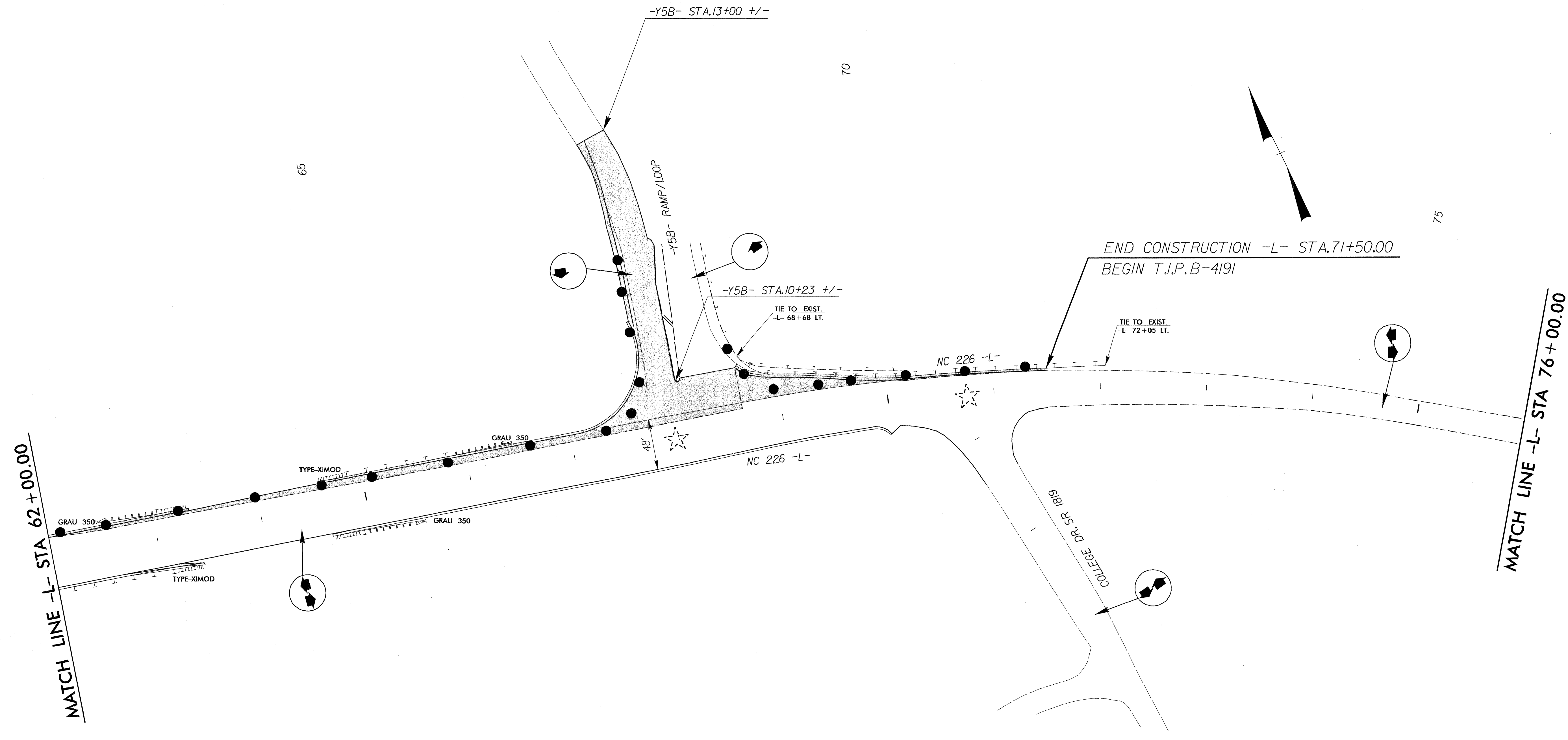
NOTE: MAINTAIN EGRESS/INGRESS TO ALL PROPERTIES DURING CONSTRUCTION WITH INCIDENTAL STONE BASE OR MATERIAL SUITABLE AND APPROVED BY THE ENGINEER.

11/3/2006 R:\TrafficControl\TCP\TCPPMP\PhaseII\Top15.dgn mcbale

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FOR
DIVISION OF HIGHWAYS

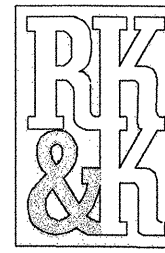
SEAL
 NORTH CAROLINA
 PROFESSIONAL
 SEAL
 19834
 ENGINEER
 B. KEITH SKINNER
 R. Keith Skinner 11/08/06

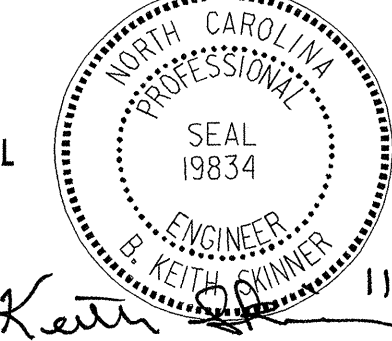
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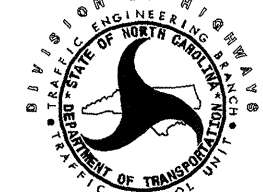
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11/7/2006
I:\TrafficControl\TCP\TCPPMP\PhaseII\top16.dgn
mcb

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consulting engineers
 900 RIDGEFIELD DRIVE SUITE 350
 RALEIGH, NORTH CAROLINA 27609-3960
FOR
DIVISION OF HIGHWAYS

SEAL

 B. Keith Swimmer
 11/08/06

PHASE II

SCALE: NONE		REVISIONS
DATE: 11/06		
DWG. BY: MAC		
DESIGN BY: MAC		
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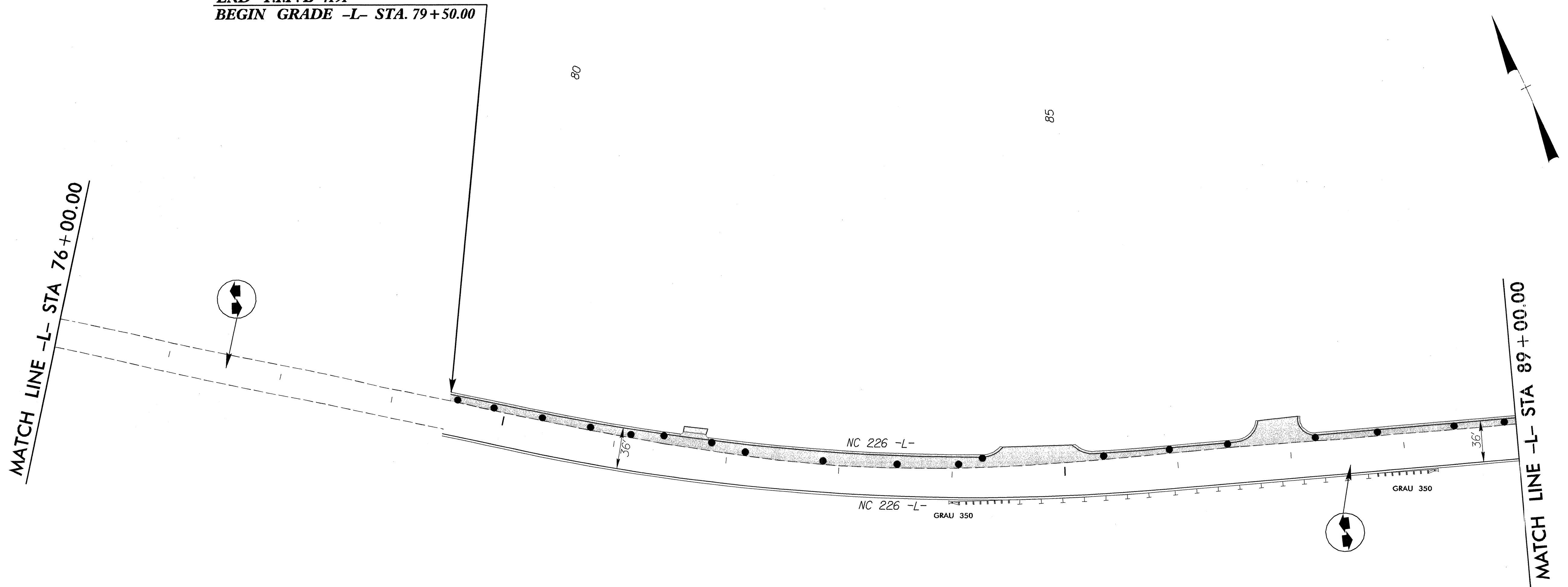
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PROJ. REFERENCE NO.	SHEET NO.
37857 & 37645	TCP-17
MA13063R & MA13025R	

END T.I.P. B-4191
 BEGIN GRADE -L- STA. 79+50.00

MATCH LINE -L- STA 76+00.00

MATCH LINE -L- STA 89+00.00



NOTE: MAINTAIN EGRESS/INGRESS TO ALL PROPERTIES DURING CONSTRUCTION WITH INCIDENTAL STONE BASE OR MATERIAL SUITABLE AND APPROVED BY THE ENGINEER.

11/3/2006
 I:\TrafficControl\TCP\TCPMP\PhaseII\Top17.dgn
 mc016

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consulting engineers
 900 RIDGEFIELD DRIVE SUITE 350
 RALEIGH, NORTH CAROLINA 27609-3960

FOR
DIVISION OF HIGHWAYS

SEAL

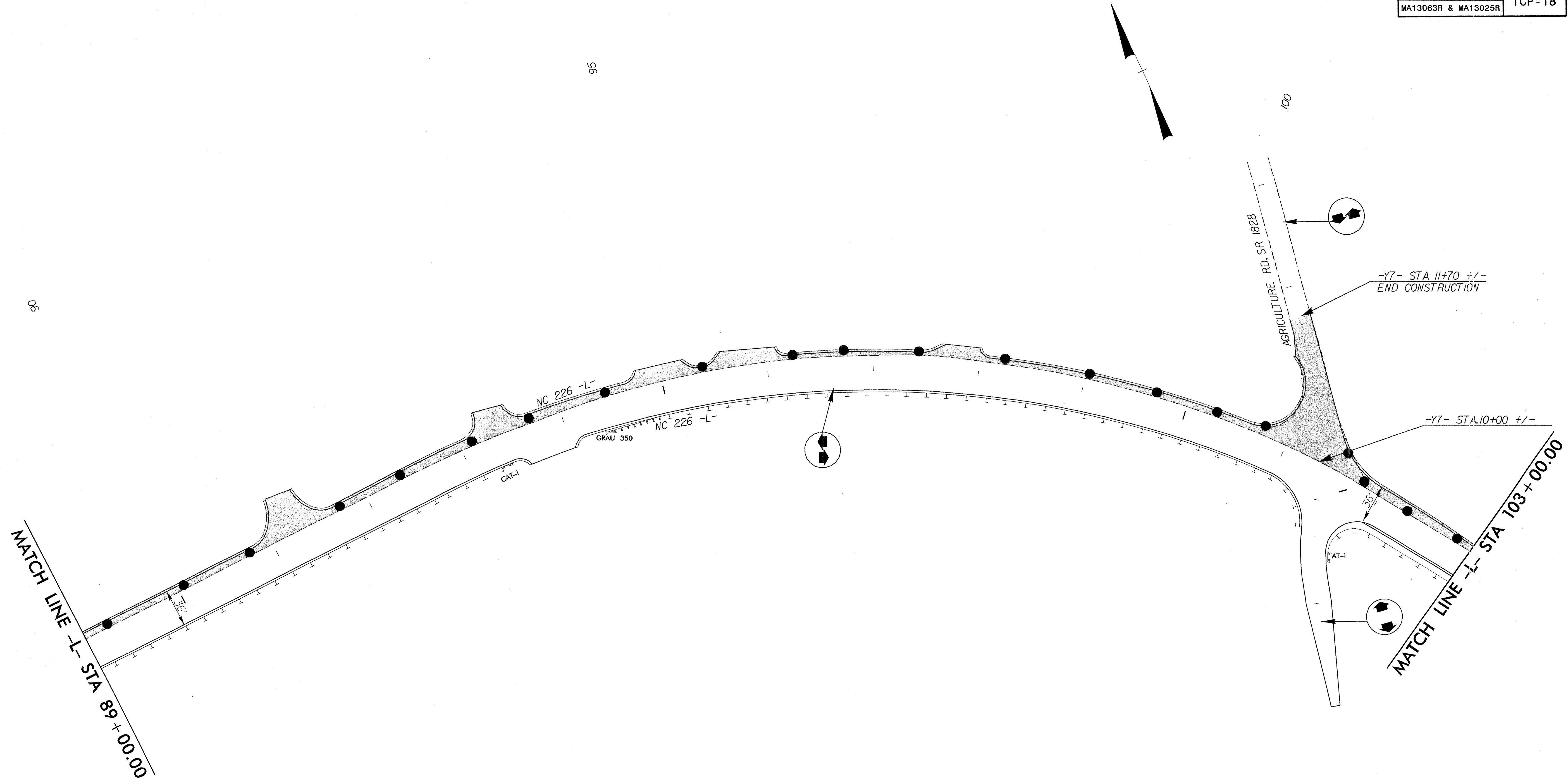
11/08/06

PHASE II

SCALE: NONE		REVISIONS
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DWG. BY: MAC		
DESIGN BY: MAC		
REVIEWED BY: BKS		

CADD FILE: TCP17.DGN

PROJ. REFERENCE NO.	SHEET NO.
37857 & 37645	TCP-18
MA13063R & MA13025R	



NOTE: MAINTAIN EGRESS/INGRESS TO ALL PROPERTIES DURING CONSTRUCTION WITH INCIDENTAL STONE BASE OR MATERIAL SUITABLE AND APPROVED BY THE ENGINEER.

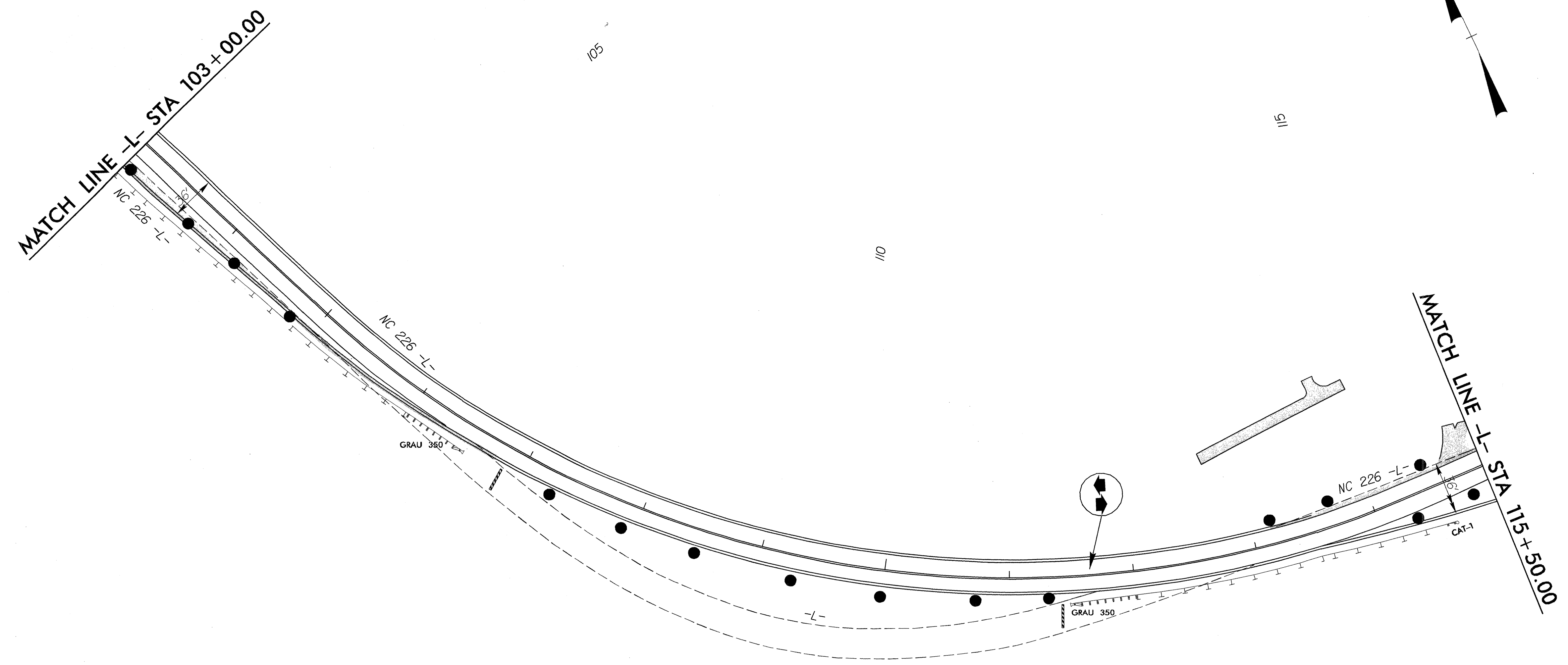
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consulting engineers
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FOR
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 B. Keith Swinner 11/06/06

PHASE II		REVISIONS	
SCALE: NONE			
DATE: 11/06			
DWG. BY: MAC			
DESIGN BY: MAC			
REVIEWED BY: BKS			
CADD FILE: TCP18.DGN			



NOTE: MAINTAIN EGRESS/INGRESS TO ALL PROPERTIES DURING CONSTRUCTION WITH INCIDENTAL STONE BASE OR MATERIAL SUITABLE AND APPROVED BY THE ENGINEER.

11/3/2006 \\fs1\office\TrafficControl\TCP\TCPMP\PhaseII\Tcp19.dgn mcole

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consulting engineers
 900 RIDGEFIELD DRIVE SUITE 350
 RALEIGH, NORTH CAROLINA 27609-3960

FOR
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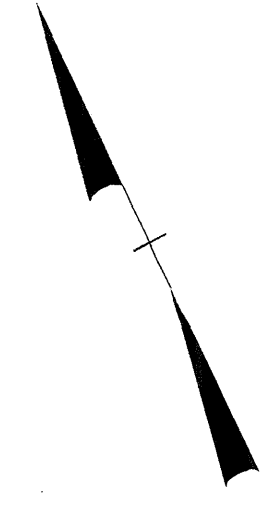
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B. Kern 11/08/06

PHASE II

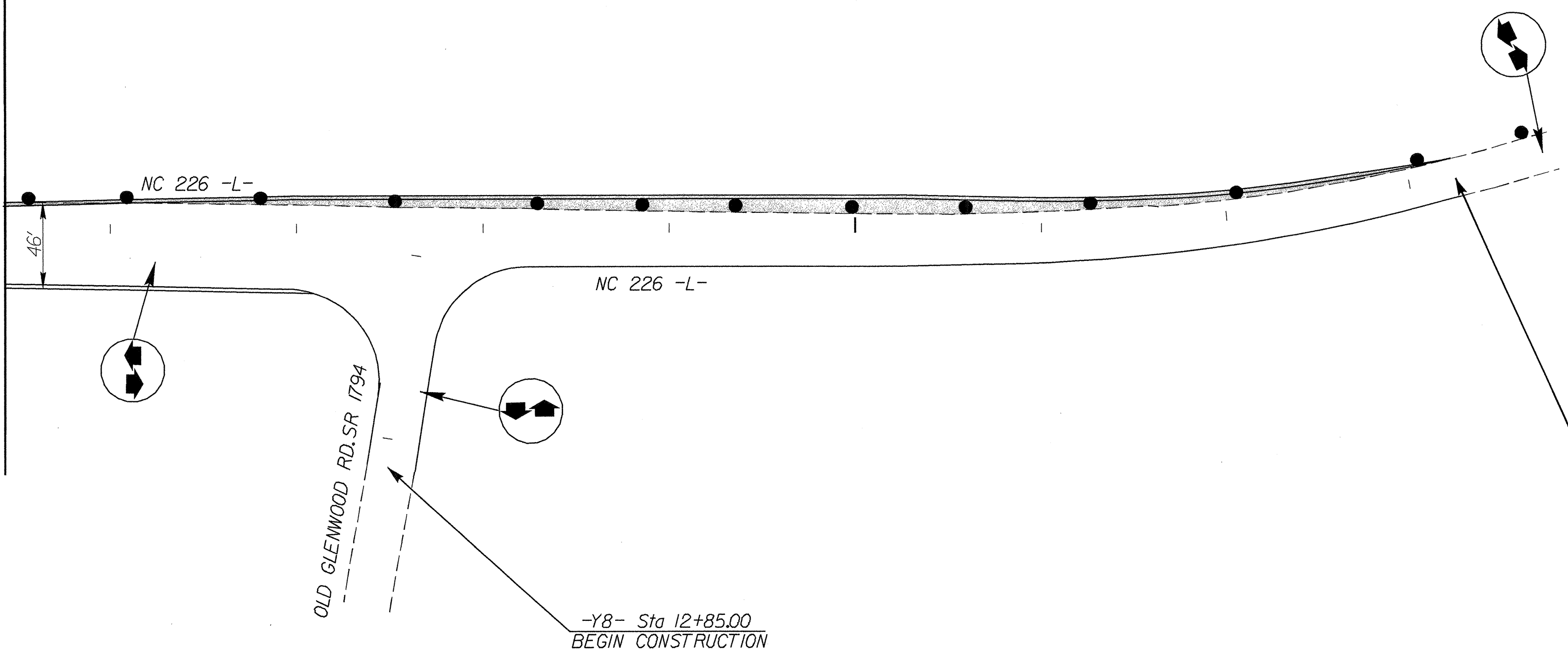
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PROJ. REFERENCE NO.	SHEET NO.
37857 & 37645	TCP-20
MA13063R & MA13025R	



120

MATCH LINE -L- STA 115 + 50.00



END PROJECT MA13025R -L- STA. 123 + 25.00

NOTE: MAINTAIN EGRESS/INGRESS TO ALL PROPERTIES DURING CONSTRUCTION WITH INCIDENTAL STONE BASE OR MATERIAL SUITABLE AND APPROVED BY THE ENGINEER.

11/3/2006
I:\Projects\TrafficControl\TCP\TCPMP\PhaseII\Tcp20.dgn
10:02

PLANS PREPARED BY :
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consulting engineers
 900 RIDGEFIELD DRIVE SUITE 350
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FOR
DIVISION OF HIGHWAYS

SEAL

 B. Skinner 11/06/06

PHASE II

SCALE: NONE		REVISIONS
DATE: 11/06		
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DESIGN BY: MAC		
REVIEWED BY: BKS		

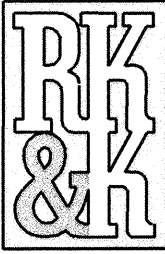
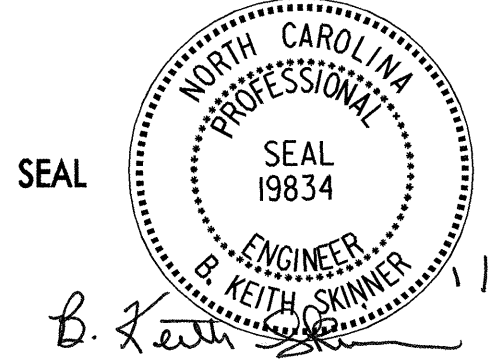
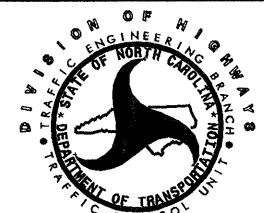
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PHASE III

- STEP 1: USING ROADWAY STANDARD DRAWING NO. 1101.02 SHEET 1 OF 9 AND FLAGGERS, WEDGE/PAVE THE PROPOSED NC 226 -L- UP TO AND INCLUDING THE FINAL LAYER OF SURFACE COURSE AT THE FOLLOWING LOCATIONS, (SEE ROADWAY PLANS AND SHEETS TCP-2 LOCAL NOTES 1 AND 3):
- TEMPORARILY CLOSE NC 226 SOUTH BOUND TRAVEL LANE AND MAINTAIN TRAFFIC ON NORTH BOUND NC 226 TO WEDGE/PAVE PROPOSED NC 226 -L- AND PLACE PROPOSED PAVEMENT MARKINGS, (THERMOPLASTIC) AND SNOWPLOWABLE RAISED PAVEMENT MARKERS FROM STA. 16+00 +/- -L- TO STA. 71+50 +/- AND FROM -L- STA. 79+50 +/- TO -L- STA. 123+25 +/- AT THE END OF THE WORK DAY NC 226 TRAFFIC WILL BE RESTORED TO ITS ORIGINAL TRAFFIC PATTERN.
 - WEDGE/PAVE THE PROPOSED -Y- LINES AND PLACE PROPOSED PAVEMENT MARKINGS, (THERMOPLASTIC) AND SNOWPLOWABLE RAISED PAVEMENT MARKERS AT THE FOLLOWING LOCATIONS. AT THE END OF THE WORK DAY THESE -Y- LINES WILL BE OPENED TO THE ORIGINAL TRAFFIC PATTERN:
 - FROM -Y1- STA. 10+29 +/- TO -Y1- STA. 13+20 +/-,
 - FROM -Y6- STA. 12+60 +/- TO -Y6- STA. 13+25 +/-,
 - FROM -Y8- STA. 12+85 +/- TO -Y8- STA. 14+10 +/-.
- STEP 2: USING ROADWAY STANDARD DRAWING NO. 1101.02 SHEET 1 OF 9 AND FLAGGERS, WEDGE/PAVE PROPOSED UP TO AND INCLUDING THE FINAL LAYER OF SURFACE COURSE AT THE FOLLOWING LOCATIONS, (SEE ROADWAY PLANS AND SHEET TCP-2 LOCAL NOTES 1 AND 3):
- TEMPORARILY CLOSE NC 226 NORTH BOUND TRAVEL LANE AND MAINTAIN TRAFFIC ON SOUTH BOUND NC 226 TO CONSTRUCT WEDGE/PAVE PROPOSED NC 226 -L- FROM STA. 16+00 +/- -L- TO STA. 71+50 +/- AND FROM -L- STA. 79+50 +/- TO -L- STA. 123+25 +/- AT THE END OF THE WORK DAY NC 226 TRAFFIC WILL BE RESTORED TO ITS ORIGINAL TRAFFIC PATTERN.
 - WEDGE/PAVE THE FINAL LAYER OF SURFACE COURSE OF -Y1-, -Y6- AND -Y8- AT THE FOLLOWING LOCATIONS. AT THE END OF THE WORK DAY THESE -Y- LINES WILL BE OPENED TO THE ORIGINAL TRAFFIC PATTERN:
 - FROM -Y2- STA. 10+22 +/- TO -Y2- STA. 11+01 +/-,
 - FROM -Y3- STA. 10+25 +/- TO -Y3- STA. 11+50 +/-.
- STEP 3: USING ROADWAY STANDARD DRAWING NO. 1101.02 SHEET SHEET 2 OF 9 AND FLAGGERS, PLACE PAVEMENT MARKINGS, (THERMOPLASTIC) AND PAVEMENT MARKERS, (PERMANENT) ON THE FINAL WEARING SURFACE OF NC 226 FROM -L- STA. 16+00 +/- TO -L- STA. 71+50 +/-, FROM STA. 79+50 +/- TO -L- STA. 123+25 +/- AND ON THE FOLLOWING -Y- LINE LOCATIONS, (SEE SHEETS TCP-2 LOCAL NOTES 1 AND 3, PM-1 THROUGH PM-4):
- FROM -Y- STA. 10+00 +/- TO -Y- STA. 11+75 +/-,
 - FROM -Y1- STA. 10+29 +/- TO -Y1- STA. 13+06 +/-,
 - FROM -Y2- STA. 10+22 +/- TO -Y2- STA. 11+01 +/-,
 - FROM -Y3- STA. 10+20 +/- TO -Y3- STA. 11+50 +/-,
 - FROM -Y4- STA. 11+40 +/- TO -Y4- STA. 13+00 +/-,
 - FROM -Y5A- STA. 10+23 +/- TO -Y5A- STA. 13+00,
 - FROM -Y5B- STA. 10+22 +/- TO -Y5B- 13+25 +/-,
 - FROM -Y6- STA. 12+60 +/- TO -Y6- STA. 13+35 +/-,
 - FROM -Y7- STA. 10+25 +/- TO -Y7- STA. 11+70 +/-,
 - FROM -Y8- STA. 12+85 +/- TO -Y8- STA. 14+00 +/-,
- STEP 4: REMOVE ANY REMAINING TRAFFIC CONTROL DEVICES FROM THE PROJECT LIMITS AND OPEN NC 226 AND ALL -Y- LINES TO THEIR PROPOSED TRAFFIC PATTERNS.

SYMBOL	DESCRIPTION	TEMPORARY PAVEMENT MARKING SCHEDULE	
		QUANTITY BREAKDOWN	TOTAL QUANTITY
		PAVEMENT MARKING LINES	
		PAINT (4")	91,988 LF
PA	WHITE EDGELINE 2X	1700 LF	
PE	WHITE SOLID LANE LINE 2X	2400 LF	
PF	10 FT YELLOW SKIP 2X	5338 LF	
PH	YELLOW SINGLE CENTER 2X	64050 LF	
PI	DOUBLE YELLOW CENTERLINE 2X		
		PAINT (8")	600 LF
PV	YELLOW DIAGONAL 2X	600 LF	
		PAINT (24")	400 LF
P4	WHITE STOP BAR 2X	400 LF	
		PAVEMENT MARKING SYMBOLS	
		PAINT SYMBOL	138 EA
QA	LEFT TURN ARROW 2X	80 EA	
QB	RIGHT TURN ARROW 2X	16 EA	
QC	STRAIGHT ARROW 2X	20 EA	
QD	COMBINATION STRAIGHT & LEFT TURN ARROW 2X	4 EA	
QE	COMBINATION STRAIGHT & RIGHT TURN ARROW 2X	18 EA	
		PAVEMENT MARKING CHARACTERS	
		PAINT CHARACTERS	8 EA
QI	ALPHANUMERIC 1X	8 EA	
		PAVEMENT MARKERS	
		TEMPORARY RAISED	312 EA
MH	YELLOW & YELLOW	282 EA	
MI	CRYSTAL & RED	30 EA	

NOTE: FOR EACH PAINT PAVEMENT MARKING ITEM, 1X IMPLIES A SINGLE APPLICATION, 2X IMPLIES TWO APPLICATIONS, AND 3X IMPLIES THREE APPLICATIONS.

	PLANS PREPARED BY : RUMMEL KLEPPER & KAHL, LLP <i>consulting engineers</i> 900 RIDGEFIELD DRIVE SUITE 350 RALEIGH, NORTH CAROLINA 27609-3960 FOR DIVISION OF HIGHWAYS		PHASE III & TEMPORARY PAVEMENT MARKING SCHEDULE					
	SCALE: NONE DATE: 11/06 DWG. BY: MAC DESIGN BY: MAC REVIEWED BY: BKS		REVISIONS <table border="1"> <tr><td> </td><td> </td></tr> <tr><td> </td><td> </td></tr> <tr><td> </td><td> </td></tr> </table>					

LEGEND

A ROAD CLOSED TO THRU TRAFFIC R11-4 60" X 30"
TYPE III BARRIAGE

B NC 226 18" X 24"
DETOUR ← M4-9 L 30" X 24"

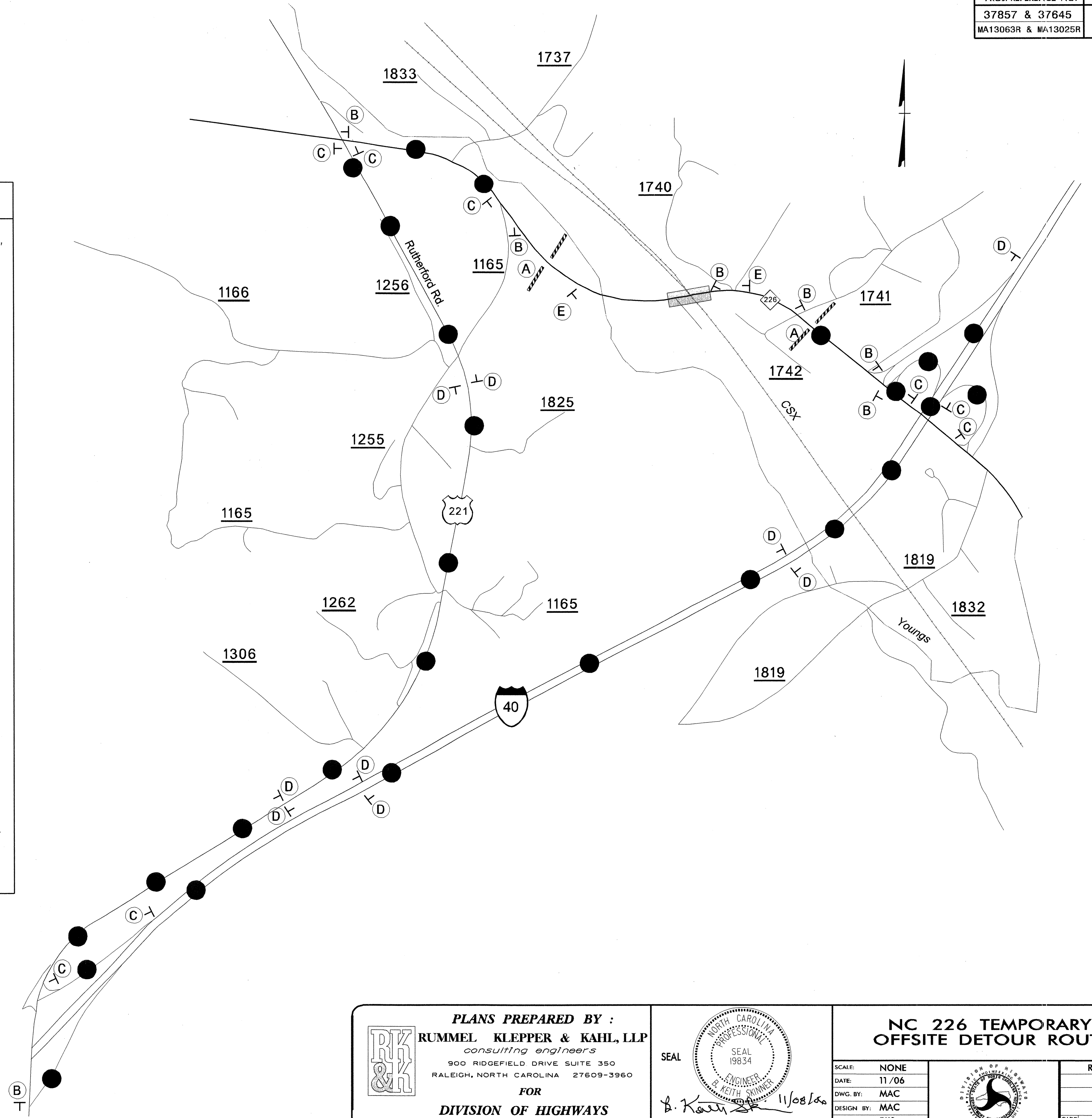
C NC 226 18" X 24"
DETOUR → M4-9 R 30" X 24"

D NC 226 18" X 24"
DETOUR ↑ M4-9 S 30" X 24"

E NC 226 18" X 24"
END DETOUR M4-8 A 24" X 18"

WORK AREA

DETOUR ROUTE



PLANS PREPARED BY :
RUMMEL KLEPPER & KAHL, LLP
consulting engineers
 900 RIDGEFIELD DRIVE SUITE 350
 RALEIGH, NORTH CAROLINA 27609-3960

FOR
DIVISION OF HIGHWAYS

SEAL

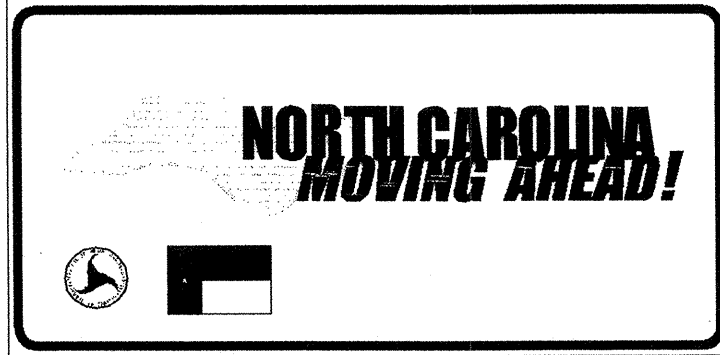
NORTH CAROLINA PROFESSIONAL ENGINEER
 SEAL 19834
 B. KEITH SKINNER
 11/08/06

NC 226 TEMPORARY OFFSITE DETOUR ROUTE

SCALE:	NONE		<table border="1"> <tr> <th colspan="2">REVISIONS</th> </tr> <tr> <td> </td> <td> </td> </tr> <tr> <td> </td> <td> </td> </tr> <tr> <td> </td> <td> </td> </tr> </table>	REVISIONS							
REVISIONS											
DATE:	11/06										
DWG. BY:	MAC										
DESIGN BY:	MAC										
REVIEWED BY:	BKS	CADD FILE:	TCP22.DGN								

TWO LANE, TWO WAY WORK ZONE (L-LINES)

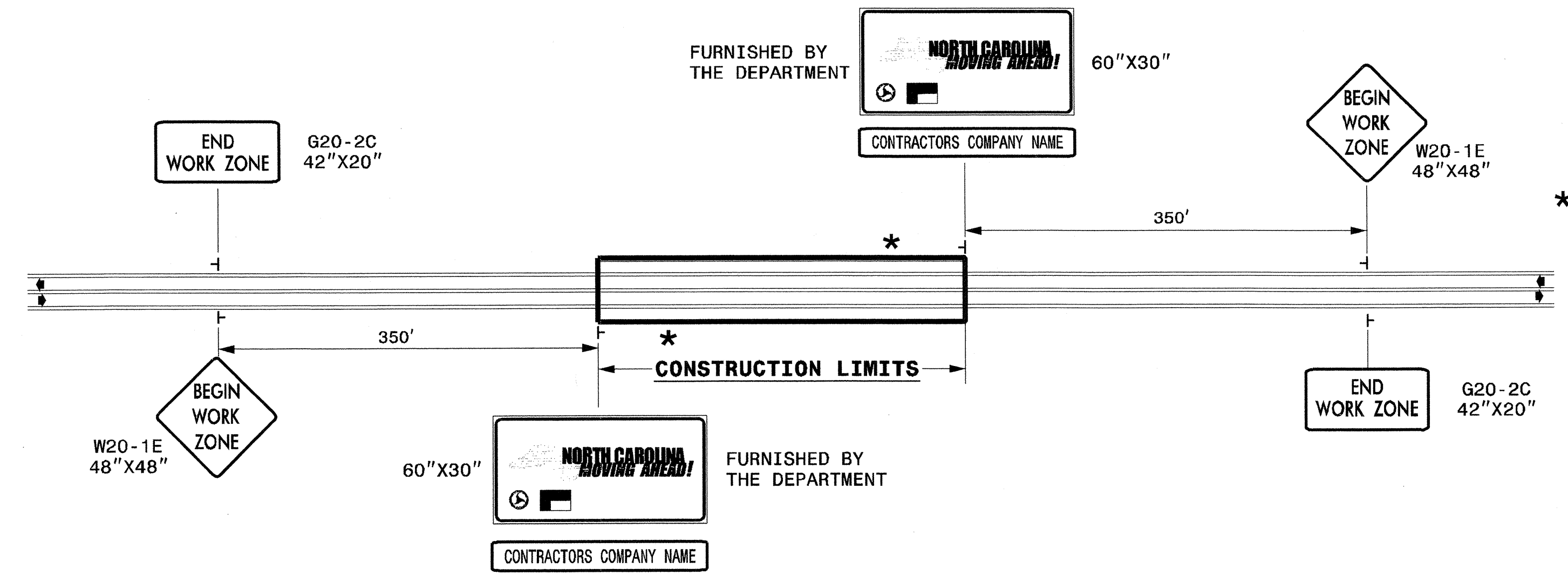
FURNISHED BY THE DEPARTMENT



60"X30"

CONTRACTORS COMPANY NAME

60" Max. X 12"

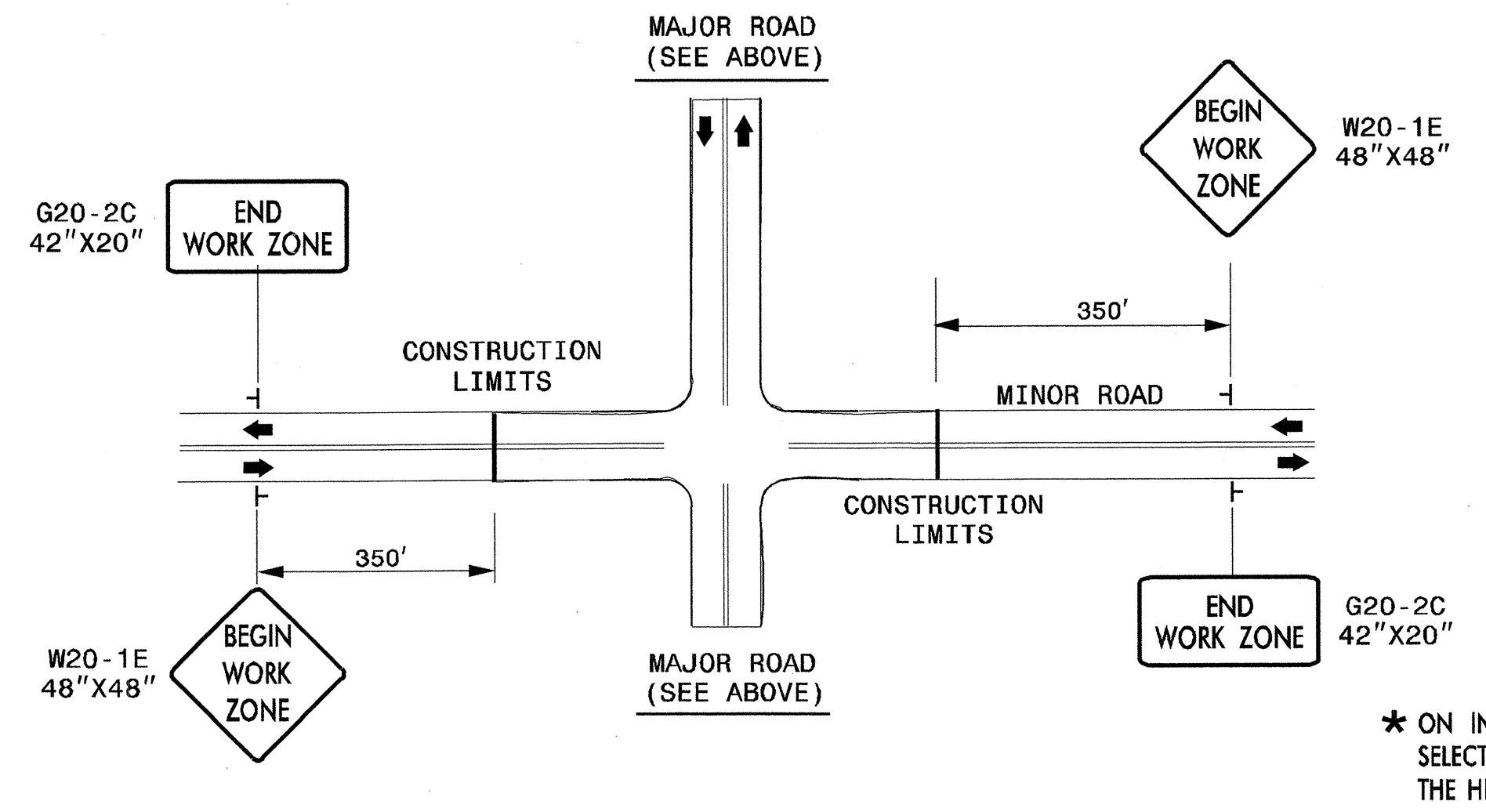


* **WORK ZONE NEXT XX MILES** G20-1 42"X20"

* THIS SIGN TO BE USED ON PROJECTS LONGER THAN 2 MILES. THE NUMBER DISPLAYED ON THE SIGN IS TO BE A WHOLE NUMBER ROUNDED UP TO THE NEXT MILE. IT'S TO BE LOCATED 1,500 FEET INSIDE OF THE CONSTRUCTION LIMITS.

STATE OF NORTH CAROLINA
 DEPT. OF TRANSPORTATION
 DIVISION OF HIGHWAYS
 RALEIGH, N.C.

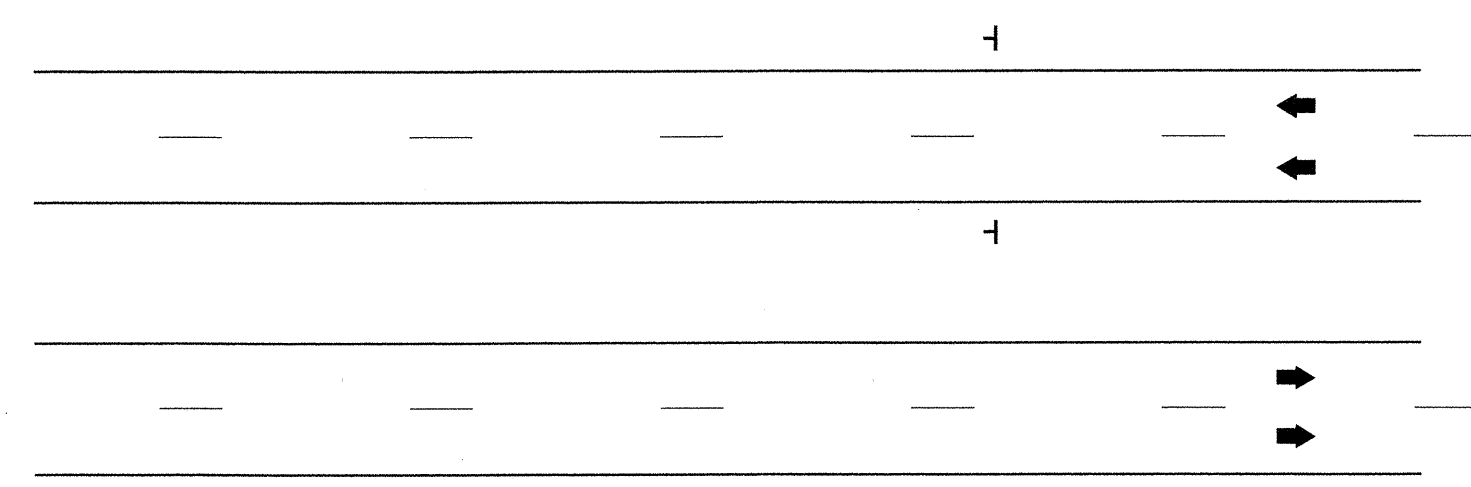
INTERSECTIONS (-Y- LINES)



FREEWAYS / INTERSTATES

DUAL MOUNT "BEGIN WORK ZONE" SIGNS 1,000' IN ADVANCE OF PROJECT LIMITS

DUAL MOUNT "MOVING AHEAD" SIGNS 500' IN ADVANCE OF PROJECT LIMITS



GENERAL NOTES

SEE SPECIAL PROVISIONS FOR "NORTH CAROLINA MOVING AHEAD" REQUIREMENTS.

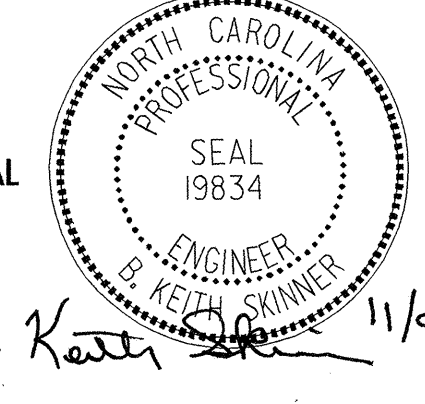
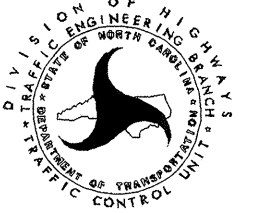
LEGEND

┆ STATIONARY SIGN

◀ DIRECTION OF TRAFFIC FLOW

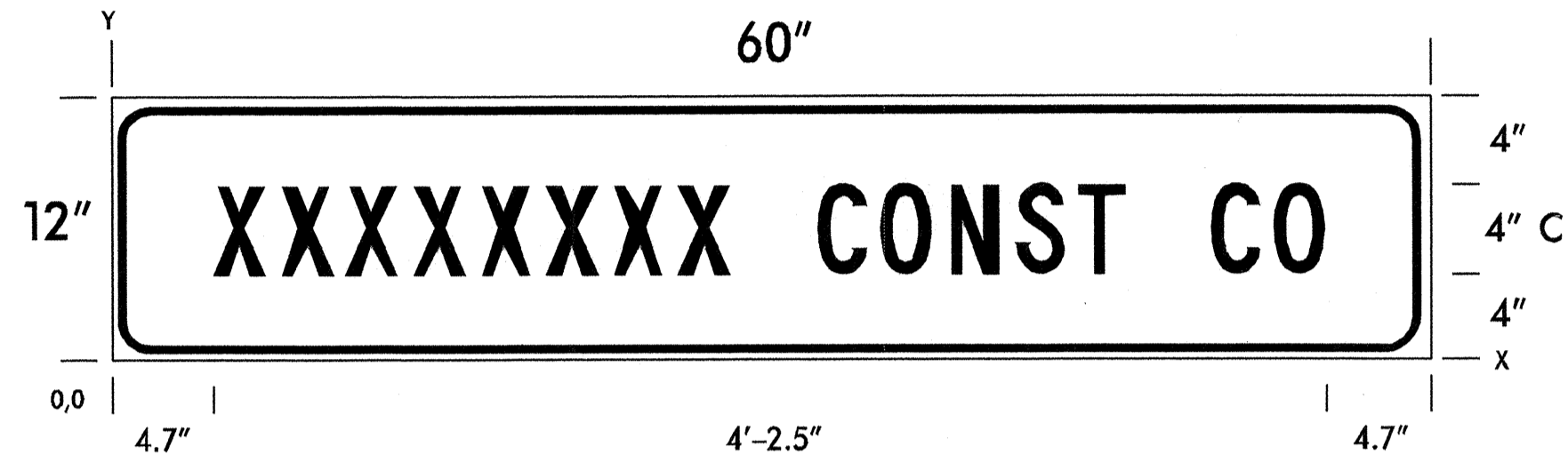
DETAIL DRAWING FOR ADVANCE
 WARNING WORK ZONE SIGNS

SHEET 1 OF 1

SEAL  B. Keith Skinner 11/03/06	ADVANCE WARNING WORK ZONE SIGNS FOR "MOVING AHEAD"		SCALE: NONE DATE: 07/03 DWG. BY: JSK DESIGN BY: JSK REVIEWED BY: BKS			REVISIONS 11/06	
						CAD FILE: NCMA01.DGN	

11/3/2006
 I:\AT\Office\Tr\office\Contr\on\TCP\TCPPMP\PhaseIII\ncma01.dgn
 mcole

SIGN DETAIL
SCALE 1:15



N. C. DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
TRAFFIC ENGINEERING BRANCH

SIGN NUMBER: SP03331 DESIGN BY: D IVES DATE: 8-11-03
 TYPE: D CHECKED BY: STD. #: 0
 QUANTITY: N/A PROJECT ID: DIV: N/A
 WIDTHxHEIGHT: 60" x 12" BACKGROUND COLOR: ORANGE
 SIGN AREA: 5.00 sq.ft. COPY COLOR: BLACK
 BORDER TYPE: FLUSH MAJ COPY SERIES: C
 RECESS: 0.375
 BORDER WIDTH: 0.375" 2 U CHANNEL POST(S)
 RADII: 1.5" MAT'L: 0.125" ALUMINUM

NOTES:
 -Legend and border shall be direct applied non-reflective sheeting.
 -Background shall be Type I reflective sheeting.
 -If needed, letter size for contractor's name shall be reduced so sign shall not exceed 60 inches in width.
 -To be mounted beneath "NC Moving Ahead" sign.

DIMENSIONS IN INCHES

SPACINGS ARE TO START OF NEXT LETTER

Y FONT	LETTER SPACINGS																HT LEN			
4.0	X	X	X	X	X	X	X	X	X	C	O	N	S	T	C	O	4.0			
C	4.7	3.0	3.0	3.0	3.0	3.0	3.0	3.0	2.3	4.0	2.9	3.1	3.0	2.9	2.0	4.0	2.9	2.3	4.7	50.5

file: Const Co Demo.dgn

11/3/2006
 C:\TrafficControl\TCF\TCFMP\PhaseII\ncma02.dgn
 macole

	SIGN DESIGN FOR CONTRACTORS COMPANY NAME	
	SCALE: NONE	REVISIONS
	DATE: 11/06	
	DWG. BY: MAC	
DESIGN BY: MAC		
REVIEWED BY: BKS		
		CADD FILE NCMA02.DGN