

STATE OF NORTH CAROLINA
DIVISION OF HIGHWAYS

STATE PROJECT REFERENCE NO.	SHEET NO.
U-3612	TCP-1

**PLAN FOR PROPOSED
TRAFFIC CONTROL, MARKING & DELINEATION**

GUILFORD COUNTY

U-3612

ROADWAY STANDARD DRAWINGS

THE FOLLOWING ROADWAY STANDARDS AS APPEAR IN "ROADWAY STANDARD DRAWINGS"- PROJECT SERVICES UNIT-N.C. DEPARTMENT OF TRANSPORTATION-RALEIGH, N.C., DATED JULY 2006 ARE APPLICABLE TO THIS PROJECT AND BY REFERENCE HEREBY ARE CONSIDERED A PART OF THESE PLANS:

STD. NO.	TITLE
1101.02	TEMPORARY LANE CLOSURES
1101.03	TEMPORARY ROAD CLOSURES
1101.04	TEMPORARY SHOULDER CLOSURES
1101.05	WORK ZONE VEHICLE ACCESSES
1101.11	TRAFFIC CONTROL DESIGN TABLES
1110.01	STATIONARY WORK ZONE SIGNS
1110.02	PORTABLE WORK ZONE SIGNS
1115.01	FLASHING ARROW PANELS
1130.01	DRUM
1135.01	CONES
1145.01	BARRICADES
1150.01	FLAGGING DEVICES
1160.01	TEMPORARY CRASH CUSHION
1165.01	TRUCK MOUNTED IMPACT ATTENUATOR
1170.01	PORTABLE CONCRETE BARRIER
1180.01	SKINNY-DRUM
1205.01	PAVEMENT MARKINGS - LINE TYPES & OFFSETS
1205.02	PAVEMENT MARKINGS - 2 LANE & MULTILANE ROADWAYS
1205.04	PAVEMENT MARKINGS - INTERSECTIONS
1205.05	PAVEMENT MARKINGS - TURN LANES
1205.07	PAVEMENT MARKINGS - PEDESTRIAN CROSSWALKS
1205.08	PAVEMENT MARKINGS - SYMBOLS & WORD MESSAGES
1250.01	PAVEMENT MARKER SPACING
1251.01	RAISED PAVEMENT MARKERS (TEMPORARY & PERMANENT)
1253.01	SNOWPLOWABLE RAISED PAVEMENT MARKERS
1261.01	GUARDRAIL & BARRIER DELINEATOR SPACING
1261.02	GUARDRAIL & BARRIER DELINEATOR TYPES
1262.01	GUARDRAIL END DELINEATION

INDEX OF SHEETS

SHEET NO.	TITLE
TCP-1	LIST OF APPLICABLE ROADWAY STANDARD DRAWINGS, LEGEND AND INDEX OF SHEETS
TCP-2	TEMPORARY PAVEMENT MARKING SCHEDULE
TCP-3 & 4	PROJECT NOTES
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TCP-12	DETAIL DRAWING FOR TWO-WAY UNDIVIDED AND URBAN FREEWAYS ADVANCE WORK ZONE WARNING SIGNS DETAIL
PM-1	FINAL PAVEMENT MARKING SCHEDULE
PM-2 & PM-3	FINAL PAVEMENT MARKING PLANS
TCP-7	STEP 2 OVERVIEW

LEGEND

- GENERAL**
- DIRECTION OF TRAFFIC FLOW
 - NORTH ARROW
 - PROPOSED PVMT. EXIST. PVMT.
 - WORK AREA
 - REMOVAL OF EXISTING PAVEMENT
- TRAFFIC CONTROL DEVICES**
- TYPE I BARRICADE
 - TYPE II BARRICADE
 - TYPE III BARRICADE
 - CONE
 - DRUM SKINNY DRUM
 - FLASHING ARROW PANEL (TYPE C)
 - STATIONARY SIGN
 - PORTABLE SIGN
 - STATIONARY OR PORTABLE SIGN
 - WARNING FLAGS
 - CRASH CUSHION
 - CHANGEABLE MESSAGE SIGN
 - TRUCK MOUNTED IMPACT ATTENUATOR (TMIA)
 - POLICE
 - FLAGGER
- PAVEMENT MARKINGS**
- CRYSTAL/CRYSTAL PAVEMENT MARKER
 - YELLOW/YELLOW PAVEMENT MARKER
 - CRYSTAL/RED PAVEMENT MARKER
 - PAVEMENT MARKING SYMBOLS

TIP PROJECT:

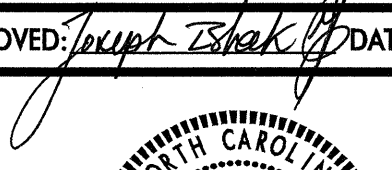

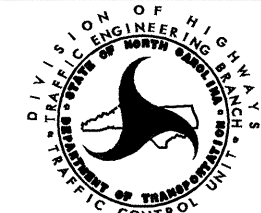
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APPROVED:	PLAN PREPARED BY: N.C.D.O.T. WORK ZONE TRAFFIC CONTROL UNIT
DATE: May 10, 2008	
	J. STUART BOURNE, P.E. TRAFFIC CONTROL ENGINEER
	JOSEPH ISHAK, P.E. TRAFFIC CONTROL PROJECT ENGINEER
	JENNIFER PORTANOVA, P.E. TRAFFIC CONTROL PROJECT DESIGN ENGINEER
	ALLA LYUDMIRSKAYA TRAFFIC CONTROL DESIGN ENGINEER / TECHNICIAN

TEMPORARY PAVEMENT MARKING SCHEDULE

SYMBOL	DESCRIPTION	PAY ITEM	QUANTITY BREAKDOWN	TOTAL QUANTITY
<u>PAVEMENT MARKING LINES</u>				
PAINT (4")				
PA	WHITE EDGELINE 2X		19600 LF	
PD	2 FT. WHITE MINISKIP 2X		250 LF	
PE	WHITE SOLID LANE LINE 2X		700 LF	
PI	YELLOW DOUBLE CENTERLINE 2X		19600 LF	
			TOTAL:	40150 LF
PAINT (24")				
P4	WHITE STOP BAR 2X		200 LF	
			TOTAL:	200 LF
<u>PAVEMENT MARKING SYMBOLS</u>				
PAINT SYMBOLS				
QA	LEFT TURN ARROW 2X		4 EA	
QB	RIGHT TURN ARROW 2X		4 EA	
QC	STRAIGHT ARROW 2X		12 EA	
			TOTAL:	20 EA
<u>TEMPORARY PAVEMENT MARKERS</u>				
TEMPORARY RAISED PAVEMENT MARKERS				
MH	YELLOW & YELLOW		100 EA	
MI	CRYSTAL & RED		170 EA	
			TOTAL:	270 EA
<p>NOTES: - AS DIRECTED BY THE ENGINEER, TEMPORARY PAVEMENT MARKING (PAINT) MAY BE USED TO STRIPE THE FINAL TRAFFIC PATTERN ON -L- AND -Y- LINES. THE TEMPORARY PAVEMENT MARKING SCHEDULE INCLUDES QUANTITIES FOR PLACING TWO APPLICATIONS OF PAINT ON THE FINAL SURFACE OF NEW ASPHALT WITH PERMANENT TRAFFIC PATTERN WHICH WILL REMAIN IN PLACE UNTIL THE PROPOSED FINAL PAVEMENT MARKING (THERMOPLASTIC) IS APPLIED.</p> <p>- FOR EACH PAINT PAVEMENT MARKING ITEM, 1X IMPLIES A SINGLE APPLICATION, 2X IMPLIES TWO APPLICATION, AND 3X IMPLIES THREE APPLICATION.</p>				

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APPROVED:  DATE: 5-31-06 	TEMPORARY PAVEMENT MARKING SCHEDULE	<table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <td>SCALE:</td> <td>NONE</td> </tr> <tr> <td>DATE:</td> <td>04/26/06</td> </tr> <tr> <td>DWG. BY:</td> <td>AYL</td> </tr> <tr> <td>DESIGN BY:</td> <td>AYL</td> </tr> <tr> <td>REVIEWED BY:</td> <td>JJI</td> </tr> </table>	SCALE:	NONE	DATE:	04/26/06	DWG. BY:	AYL	DESIGN BY:	AYL	REVIEWED BY:	JJI
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PROJECT NOTES

ADAPT THE TRAFFIC CONTROL PLANS, WHEN DIRECTED BY THE ENGINEER, TO MEET FIELD CONDITIONS TO PROVIDE SAFE AND EFFICIENT TRAFFIC MOVEMENT. CHANGES MAY BE REQUIRED WHEN PHYSICAL DIMENSIONS IN THE DETAIL DRAWINGS, STANDARD DETAILS AND ROADWAY DETAILS ARE NOT ATTAINABLE OR RESULT IN DUPLICATE, OR UNDESIRED OVERLAPPING OF DEVICES. MODIFICATION MAY INCLUDE: MOVING, SUPPLEMENTING, COVERING OR REMOVAL OF DEVICES.

THE FOLLOWING GENERAL NOTES APPLY AT ALL TIMES FOR THE DURATION OF THE CONSTRUCTION PROJECT, EXCEPT WHEN OTHERWISE NOTED IN THE PLAN OR DIRECTED BY THE ENGINEER.

TIME RESTRICTIONS

A) DO NOT CLOSE OR NARROW TRAVEL LANES AS FOLLOWS:

ROAD NAME	DAY AND TIME RESTRICTIONS
1. -L- (HILLTOP RD.)	MONDAY THROUGH FRIDAY FROM 7:00 A.M. TO 9:00 A.M. AND FROM 4:00 P.M. TO 6:00 P.M.

B) DO NOT CLOSE OR NARROW TRAVEL LANES DURING HOLIDAYS AND SPECIAL EVENTS AS FOLLOWS:

ROAD NAME
1. -L- (HILLTOP RD.)

HOLIDAY

- FOR ANY EVENT THAT CREATES UNUSUALLY HIGH TRAFFIC VOLUMES, AS DIRECTED BY THE ENGINEER.
- FOR NEW YEAR'S, BETWEEN THE HOURS OF 7:00 A.M. DECEMBER 31ST TO 6:00 P.M. JANUARY 2ND. IF NEW YEAR'S DAY IS ON A SATURDAY OR A SUNDAY, THEN UNTIL 6:00 P.M. THE FOLLOWING TUESDAY.
- FOR EASTER, BETWEEN THE HOURS OF 7:00 A.M. THURSDAY AND 6:00 P.M. MONDAY.
- FOR MEMORIAL DAY, BETWEEN THE HOURS OF 7:00 A.M. FRIDAY TO 6:00 P.M. TUESDAY.
- FOR INDEPENDENCE DAY, BETWEEN THE HOURS OF 7:00 A.M. THE DAY BEFORE INDEPENDENCE DAY AND 6:00 P.M. THE DAY AFTER INDEPENDENCE DAY.

IF INDEPENDENCE DAY IS ON A SATURDAY OR SUNDAY, THEN BETWEEN THE HOURS OF 7:00 A.M. THE THURSDAY BEFORE INDEPENDENCE DAY AND 6:00 P.M. THE TUESDAY AFTER INDEPENDENCE DAY.
- FOR LABOR DAY, BETWEEN THE HOURS OF 7:00 A.M. FRIDAY TO 6:00 P.M. TUESDAY.
- FOR THANKSGIVING, BETWEEN THE HOURS OF 7:00 A.M. TUESDAY TO 6:00 P.M. MONDAY.

8. FOR CHRISTMAS, BETWEEN THE HOURS OF 7:00 A.M. THE FRIDAY BEFORE THE WEEK OF CHRISTMAS DAY AND 6:00 P.M. THE FOLLOWING MONDAY AFTER THE WEEK OF CHRISTMAS.

9. FOR THE HIGH POINT FURNITURE MARKET, BETWEEN THE HOURS OF 7:00 A.M. THE FRIDAY OF THE WEEK OF THE HIGH POINT FURNITURE MARKET AND 9:00 A.M. THE FOLLOWING MONDAY AFTER THE WEEK OF THE HIGH POINT FURNITURE MARKET.

C) DO NOT STOP TRAFFIC FOR MORE THAN 15 MINUTES AS FOLLOWS:

ROAD NAME	OPERATION
1. ALL ROADS	SHIFTING TRAFFIC

D) DO NOT CONDUCT ANY HAULING OPERATIONS AGAINST THE FLOW OF TRAFFIC OF AN OPEN TRAVELWAY UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL OR OTHERWISE DIRECTED BY THE ENGINEER.

LANE AND SHOULDER CLOSURE REQUIREMENTS

- REMOVE LANE CLOSURE DEVICES FROM THE LANE WHEN WORK IS NOT BEING PERFORMED BEHIND THE LANE CLOSURE OR WHEN A LANE CLOSURE IS NO LONGER NEEDED, OR AS DIRECTED BY THE ENGINEER.
- WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN 40 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN SHOULDER USING RSD 1101.04 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.
- WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING ON THE SHOULDER ADJACENT TO AN UNDIVIDED FACILITY AND WITHIN 5 FT OF AN OPEN TRAVEL LANE, CLOSE THE NEAREST OPEN TRAVEL LANE USING ROADWAY STANDARD DRAWING NO. 1101.02 UNLESS THE WORK AREA IS PROTECTED BY BARRIER OR GUARDRAIL.
- WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN A LANE OF TRAVEL OF AN UNDIVIDED OR DIVIDED FACILITY, CLOSE THE LANE ACCORDING TO THE TRAFFIC CONTROL PLANS, ROADWAY STANDARD DRAWINGS OR AS DIRECTED BY THE ENGINEER. CONDUCT THE WORK SO THAT ALL PERSONNEL AND/OR EQUIPMENT REMAIN WITHIN THE CLOSED TRAVEL LANE.
- DO NOT WORK SIMULTANEOUSLY, ON BOTH SIDES OF AN OPEN TRAVELWAY, WITHIN THE SAME LOCATION, ON A TWO-LANE, TWO-WAY ROAD.
- DO NOT PERFORM WORK INVOLVING HEAVY EQUIPMENT WITHIN 15 FT OF THE EDGE OF TRAVELWAY WHEN WORK IS BEING PERFORMED BEHIND A LANE CLOSURE ON THE OPPOSITE SIDE OF THE TRAVELWAY.
- DO NOT INSTALL MORE THAN 0.5 MILES OF LANE CLOSURE ON HILLTOP RD., MEASURED FROM THE BEGINNING OF THE MERGE TAPER TO THE END OF THE LANE CLOSURE.
- DO NOT INSTALL MORE THAN ONE LANE CLOSURE, IN ANY ONE DIRECTION, ON HILLTOP ROAD.

PAVEMENT EDGE DROP OFF REQUIREMENTS

M) BACKFILL AT A 6:1 SLOPE UP TO THE EDGE AND ELEVATION OF EXISTING PAVEMENT IN AREAS ADJACENT TO AN OPENED TRAVEL LANE THAT HAS A DROP-OFF AS FOLLOWS:

BACKFILL DROP-OFFS THAT EXCEED 2 INCHES ON ROADWAYS WITH POSTED SPEED LIMITS OF 45 MPH OR GREATER.

BACKFILL DROP-OFFS THAT EXCEED 3 INCHES ON ROADWAYS WITH POSTED SPEED LIMITS LESS THAN 45 MPH.

BACKFILL WITH SUITABLE COMPACTED MATERIAL, AS APPROVED BY THE ENGINEER, AT NO EXPENSE TO THE DEPARTMENT.

N) DO NOT EXCEED A DIFFERENCE OF 2 INCHES IN ELEVATION BETWEEN OPEN LANES OF TRAFFIC. INSTALL ADVANCE WARNING "UNEVEN LANES" SIGNS (W8-11) 500 FT IN ADVANCE AND A MINIMUM OF ONCE EVERY MILE THROUGHOUT THE UNEVEN AREA.

TRAFFIC PATTERN ALTERATIONS

O) NOTIFY THE ENGINEER TWENTY ONE (21) CALENDAR DAYS PRIOR TO ANY TRAFFIC PATTERN ALTERATION.

SIGNING

P) INSTALL ADVANCE WORK ZONE WARNING SIGNS WHEN WORK IS WITHIN 100 FT FROM THE EDGE OF TRAVEL LANE AND NO MORE THAN THREE (3) DAYS PRIOR TO THE BEGINNING OF CONSTRUCTION.

WHEN NO WORK IS BEING CONDUCTED FOR A PERIOD LONGER THAN ONE WEEK, REMOVE OR COVER ALL ADVANCE WORK ZONE WARNING SIGNS, AS DIRECTED BY THE ENGINEER, AT NO COST TO THE DEPARTMENT.

Q) PROVIDE PERMANENT SIGNING WITHIN THE PROJECT LIMITS.

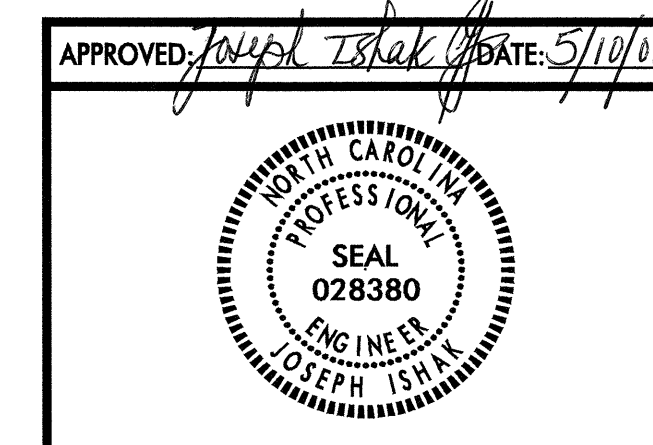
R) ENSURE ALL NECESSARY SIGNING IS IN PLACE PRIOR TO ALTERING ANY TRAFFIC PATTERN.

S) INSTALL BLACK ON ORANGE "DIP" SIGNS (W8-2) 500 FT IN ADVANCE OF THE UNEVEN AREA.

T) INSTALL BLACK ON ORANGE "BUMP" SIGNS (W8-1) 500 FT IN ADVANCE OF THE UNEVEN AREA.

TRAFFIC BARRIER

U) INSTALL TEMPORARY BARRIER ACCORDING TO THE TRAFFIC CONTROL PLANS A MAXIMUM OF TWO (2) WEEKS PRIOR TO BEGINNING WORK IN ANY LOCATION. ONCE TEMPORARY BARRIER IS INSTALLED AT ANY LOCATION, PROCEED IN CONTINUOUS MANNER TO COMPLETE THE PROPOSED WORK IN THAT LOCATION UNLESS OTHERWISE STATED IN THE TRAFFIC CONTROL PLANS OR AS DIRECTED BY THE ENGINEER.



APPROVED: <i>Joseph Ishak</i> DATE: 5/14/06		PROJECT NOTES SHEET 1 OF 2	
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PROJECT NOTES

ONCE TEMPORARY BARRIER INSTALLED AT ANY LOCATION AND NO WORK IS PERFORMED BEHIND THE TEMPORARY BARRIER FOR A PERIOD LONGER THAN TWO (2) MONTHS, REMOVE/RESET TEMPORARY BARRIER AT NO COST TO THE DEPARTMENT UNLESS OTHERWISE STATED IN THE TRAFFIC CONTROL PLANS, BARRIER IS PROTECTING A HAZARD, OR AS DIRECTED BY THE ENGINEER.

- V) PROTECT THE APPROACH END OF TEMPORARY BARRIER AT ALL TIMES DURING THE INSTALLATION AND REMOVAL OF THE BARRIER BY EITHER A TRUCK MOUNTED IMPACT ATTENUATOR (MAXIMUM 72 HOURS) OR A TEMPORARY CRASH CUSHION.

PROTECT THE APPROACH END OF TEMPORARY BARRIER FROM ONCOMING TRAFFIC AT ALL TIMES BY A TEMPORARY CRASH CUSHION UNLESS THE APPROACH END OF TEMPORARY BARRIER IS OFFSET FROM ONCOMING TRAFFIC AS FOLLOWS OR AS SHOWN IN THE PLANS:

POSTED SPEED LIMIT	MINIMUM OFFSET
LESS THAN 50 MPH	20 FT
50 MPH or HIGHER	30 FT

INSTALL TEMPORARY BARRIER WITH THE TRAFFIC FLOW, BEGINNING WITH THE UPSTREAM SIDE OF TRAFFIC. REMOVE TEMPORARY BARRIER AGAINST THE TRAFFIC FLOW, BEGINNING WITH THE DOWNSTREAM SIDE OF TRAFFIC.

INSTALL AND SPACE DRUMS NO GREATER THAN TWICE THE POSTED SPEED LIMIT (MPH) TO CLOSE OR KEEP CLOSED THE SECTION OF THE ROADWAY UNTIL THE BARRIER CAN BE PLACED OR AFTER BARRIER IS REMOVED.

TRAFFIC CONTROL DEVICES

- W) SPACE CHANNELIZING DEVICES IN WORK AREAS NO GREATER THAN TWICE THE POSTED SPEED LIMIT (MPH), EXCEPT 10 FT ON-CENTER IN RADII, AND 3FT OFF THE EDGE OF AN OPEN TRAVELWAY, WHEN LANE CLOSURES ARE NOT IN EFFECT.
- X) PLACE TYPE III BARRICADES, WITH "ROAD CLOSED" SIGN R11-2 ATTACHED, OF SUFFICIENT LENGTH TO CLOSE THE ENTIRE ROADWAY. STAGGER OR OVERLAP BARRICADES TO ALLOW FOR INGRESS OR EGRESS.
- Y) PLACE SETS OF THREE DRUMS PERPENDICULAR TO THE EDGE OF THE TRAVELWAY ON 500 FT CENTERS WHEN UNOPENED LANES ARE CLOSED TO TRAFFIC. THESE DRUMS SHALL BE IN ADDITION TO CHANNELIZING DEVICES.

PAVEMENT MARKINGS AND MARKERS

- Z) INSTALL PAVEMENT MARKINGS AND PAVEMENT MARKERS ON THE FINAL SURFACE AS FOLLOWS:

ROAD NAME	MARKING	MARKER
1. -L- SR 1424 (HILLTOP RD.)	THERMOPLASTIC	SNOWPLOWABLE
2. ALL -Y- LINES	THERMOPLASTIC	PERMANENT RAISED

- AA) INSTALL TEMPORARY PAVEMENT MARKINGS AND TEMPORARY PAVEMENT MARKERS ON INTERIM LAYERS OF PAVEMENT AS FOLLOWS:

ROAD NAME	MARKING	MARKER
1. -L- AND ALL -Y- LINES	PAINT	TEMPORARY RAISED

- BB) PLACE AT LEAST TWO APPLICATIONS OF PAINT PAVEMENT MARKINGS ON THE FINAL WEARING SURFACE ON NEW ASPHALT PAVEMENT. PLACE ADDITIONAL APPLICATIONS OF PAINT UPON SUFFICIENT DRYING TIME, AS DETERMINED BY THE ENGINEER.
- CC) TIE PROPOSED PAVEMENT MARKING LINES TO EXISTING PAVEMENT MARKING LINES.
- DD) REPLACE ANY PAVEMENT MARKINGS THAT HAVE BEEN DAMAGED BY THE END OF EACH DAY'S OPERATION.
- EE) PLACE AT LEAST TWO APPLICATIONS OF PAINT ON NEW ASPHALT WITH TEMPORARY TRAFFIC PATTERNS WHICH WILL REMAIN IN PLACE OVER THREE (3) MONTHS. PLACE ADDITIONAL APPLICATIONS OF PAINT UPON SUFFICIENT DRYING TIME, AS DETERMINED BY THE ENGINEER.

TEMPORARY/FINAL SIGNALS

- FF) SHIFT AND REVISE ALL SIGNAL HEADS AS SHOWN ON THE SIGNAL PLANS.

MISCELLANEOUS


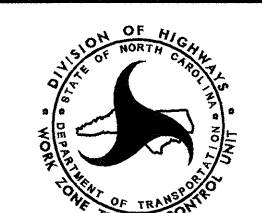
- GG) IN THE EVENT A TIE-IN CANNOT BE MADE IN ONE DAYS TIME, BRING THE TIE-IN AREA TO AN APPROPRIATE ROADWAY ELEVATION, AS DETERMINED BY THE ENGINEER. PLACE BLACK ON ORANGE "LOOSE GRAVEL" SIGNS (W8-7) AND BLACK ON ORANGE "PAVEMENT ENDS" SIGNS (W8-3) 500 FT (150m) AND 1000 FT (300m) RESPECTIVELY IN ADVANCE OF THE UNEVEN AREAS. USE DRUMS TO DELINEATE THE EDGE OF ROADWAY ALONG UNPAVED AREAS.

- HH) REFER TO MUTCD, PARTS 6D AND 6H1, FOR PEDESTRIAN DETOUR CONSIDERATIONS.

LOCAL NOTES

- II) FOR STAGING OF PROPOSED 7' X 8' RCBC AT -L- STA.23+12.30. SEE STRUCTURE AND ROADWAY PLANS.
- JJ) COORDINATE WITH THE CITY OF GREENSBORO FOR SIGNAL AND PAVEMENT MARKING WORK AT THE INTERSECTION SR 1424 (HILLTOP RD.) AND ADAMS FARM PARKWAY. SEE SHEETS TCP-10 & PM-3.

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APPROVED: <i>Joseph Ishai</i> DATE: 5/19/06 	PROJECT NOTES SHEET 2 OF 2		<table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th style="text-align: center;">REVISIONS</th> </tr> </thead> <tbody> <tr><td> </td></tr> <tr><td> </td></tr> <tr><td> </td></tr> <tr><td> </td></tr> </tbody> </table>	REVISIONS				
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AT THE END OF EACH WORK DAY RETURN TRAFFIC TO A TWO-LANE TWO-WAY TRAFFIC PATTERN UNLESS OTHERWISE IS MENTIONED IN THE PHASING.

ENSURE DRIVEWAY ACCESS IS MAINTAINED TO ALL RESIDENCES AND BUSINESSES WITHIN THE PROJECT LIMITS DURING CONSTRUCTION.

INSTALL ALL REQUIRED PEDESTRIAN WARNING SIGNS PER PROJECT NOTE "HH" BEFORE BEGINNING ANY SIDEWALK CONSTRUCTION.

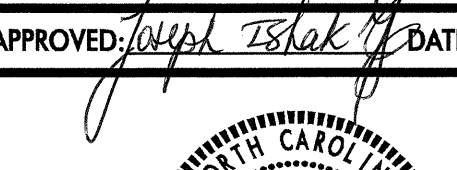

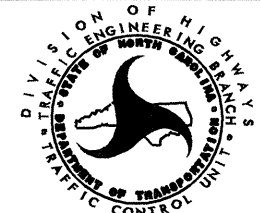
PHASING

- STEP 1: INSTALL ADVANCE WORK ZONE WARNING SIGNS. SEE SHEET TCP-12.
- STEP 2: -USING RSD 1101.02, SHEET 1 OF 9, AND RSD 1101.03, SHEET 3 OF 9, PERFORM THE FOLLOWING:
SEE SHEETS TCP-6, TCP-7 AND ROADWAY PLANS.
- 1.-BEGIN CONSTRUCTION OF PROPOSED DRAINAGE WORK.
 - 2.-INSTALL TEMPORARY SHORING, BARRIER SUPPORTED FROM -DET- STA.14+71+/- TO -DET- STA.15+21+/- AND TEMPORARY 84" CSP RIGHT OF EXISTING CULVERT AT -L- STA.23+12.30.
- CONSTRUCT THE TEMPORARY ONSITE DETOUR RIGHT OF EXISTING HILLTOP RD. FROM -L- STA.18+20+/- TO -L- STA.28+71+/-, TYING TO THE EDGE AND ELEVATION OF EXISTING HILLTOP RD. (SR 1424). SEE ROADWAY PLANS.
- MAY BEGIN WIDENING ON BOTH SIDES OF PROPOSED -L- UP TO THE EDGE AND ELEVATION OF EXISTING HILLTOP RD. (SR 1424).
- MAY BEGIN WIDENING OF PROPOSED -Y1- AND -Y2- UP TO THE EDGE AND ELEVATION OF EXISTING SHIMER DR. (SR 4108) AND ALFRED DR. (SR 1601).
- MAY BEGIN CONSTRUCTION OF CURB & GUTTER AND SIDEWALK. SEE PROJECT NOTE "HH".
- AWAY FROM TRAFFIC INSTALL TEMPORARY GUARDRAIL ON RIGHT SIDE OF A TEMPORARY ONSITE DETOUR. SEE ROADWAY PLANS FOR STATION LOCATIONS.
- STEP 3: -USING RSD 1101.02, SHEET 1 OF 9, AND RSD 1101.03, SHEET 3 OF 9, PERFORM THE FOLLOWING:
SEE SHEETS TCP-6, TCP-7, TCP-8 AND ROADWAY PLANS.
1. INSTALL ANCHORED PORTABLE CONCRETE BARRIER (PCB) AND CRASH CUSHION ON LEFT SIDE OF TEMPORARY ONSITE DETOUR FROM -DET- STA.13+87+/- TO -DET- STA.16+00+/-.
 2. WORK IN CONTINUOUS MANNER TO SHIFT TRAFFIC ON TEMPORARY DETOUR AS FOLLOW:
-CLOSE EXISTING EB LANE, AND PLACE TRAFFIC ON EXISTING WB IN ONE-LANE TWO-WAY TEMPORARY TRAFFIC PATTERN FROM -L- STA.18+20+/- TO -L- STA.28+71.
- CONSTRUCT TIE INS EB -DET- WITH EXISTING HILLTOP RD. UP TO INCLUDING FINAL LAYER OF SURFACE COURSE AT -DET- STA.10+00+/- AND -DET- STA.20+62+/-.
- PLACE TEMPORARY PAVEMENT MARKINGS (PAINT) AND TEMPORARY PAVEMENT MARKERS (TEMPORARY RAISED) ON TEMPORARY -DET- FROM STA.10+00+/- TO STA.20+62+/-, AND TIE IN TO EXISTING MARKINGS AND MARKERS.
- SHIFT TRAFFIC ON EB -DET- IN ONE-LANE TWO-WAY TEMPORARY TRAFFIC PATTERN.
- CONSTRUCT TIE INS WB -DET- WITH EXISTING HILLTOP RD. UP TO INCLUDING FINAL LAYER OF SURFACE COURSE AT -DET- STA.10+00+/- AND -DET- STA.20+62+/-.
- COMPLETE INSTALLATION OF TEMPORARY PAVEMENT MARKINGS (PAINT) AND TEMPORARY PAVEMENT MARKERS (TEMPORARY RAISED) ON TEMPORARY -DET- FROM STA.10+00+/- TO STA.20+62+/-, AND TIE IN TO EXISTING MARKINGS AND MARKERS.
- CLOSE PORTION OF EXISTING HILLTOP RD. FROM STA. 18+20+/- TO STA. 28+71+/-, AND OPEN TEMPORARY ONSITE DETOUR IN TWO-LANE, TWO-WAY TRAFFIC PATTERN.

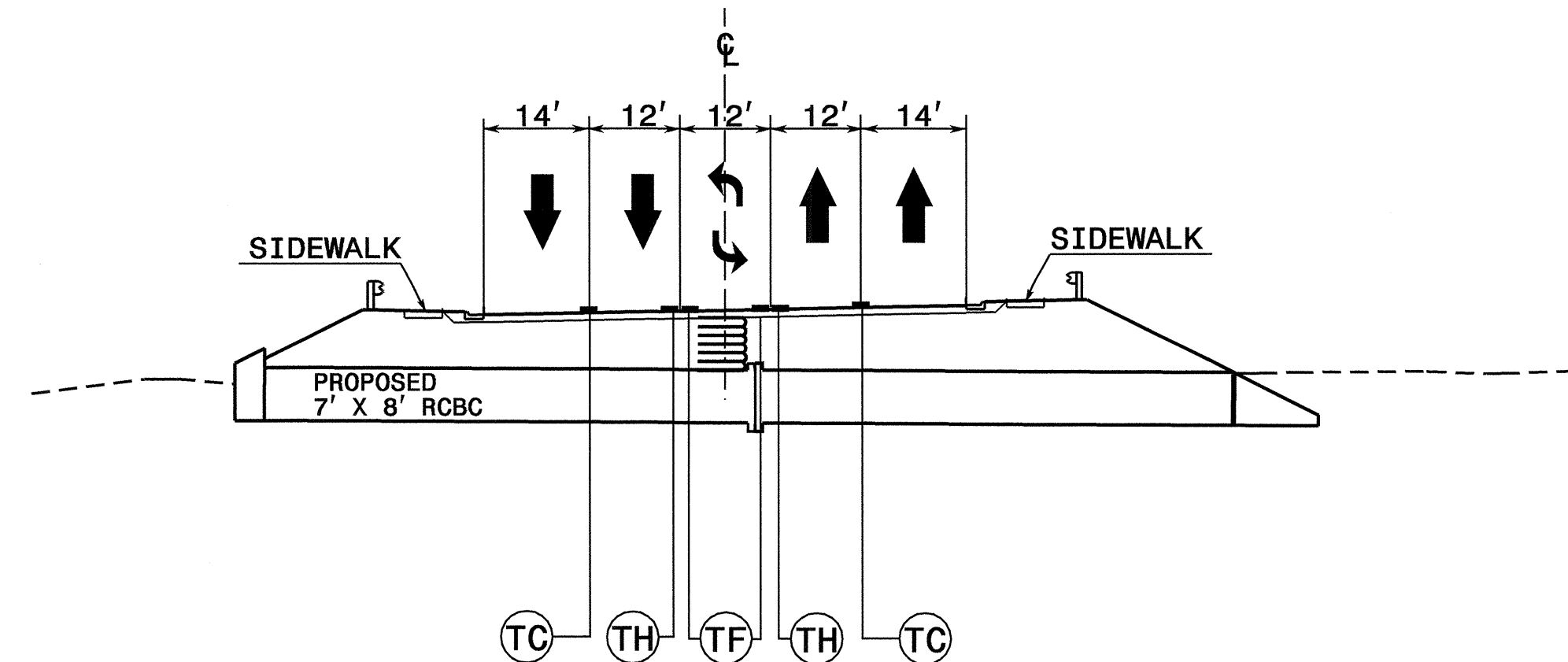
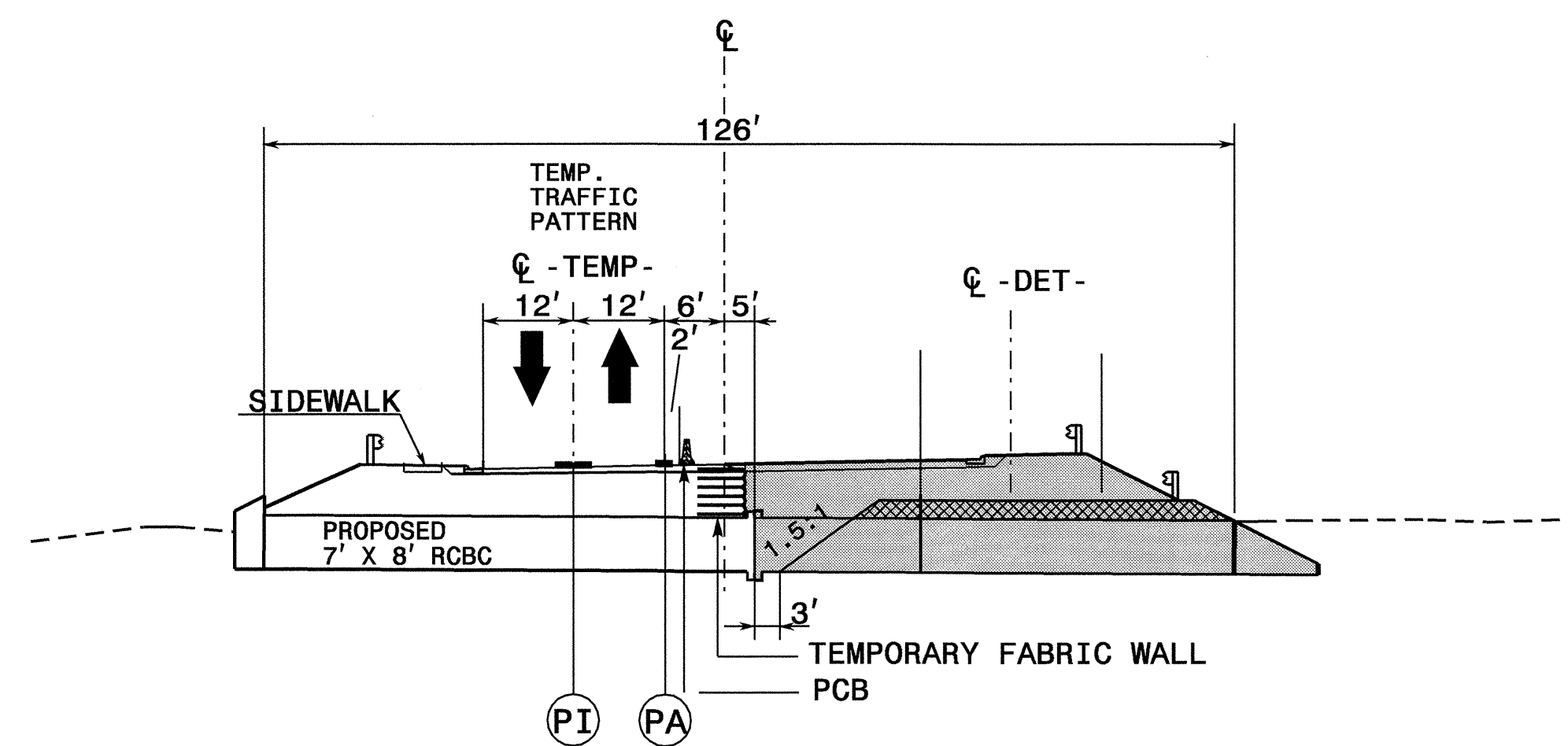
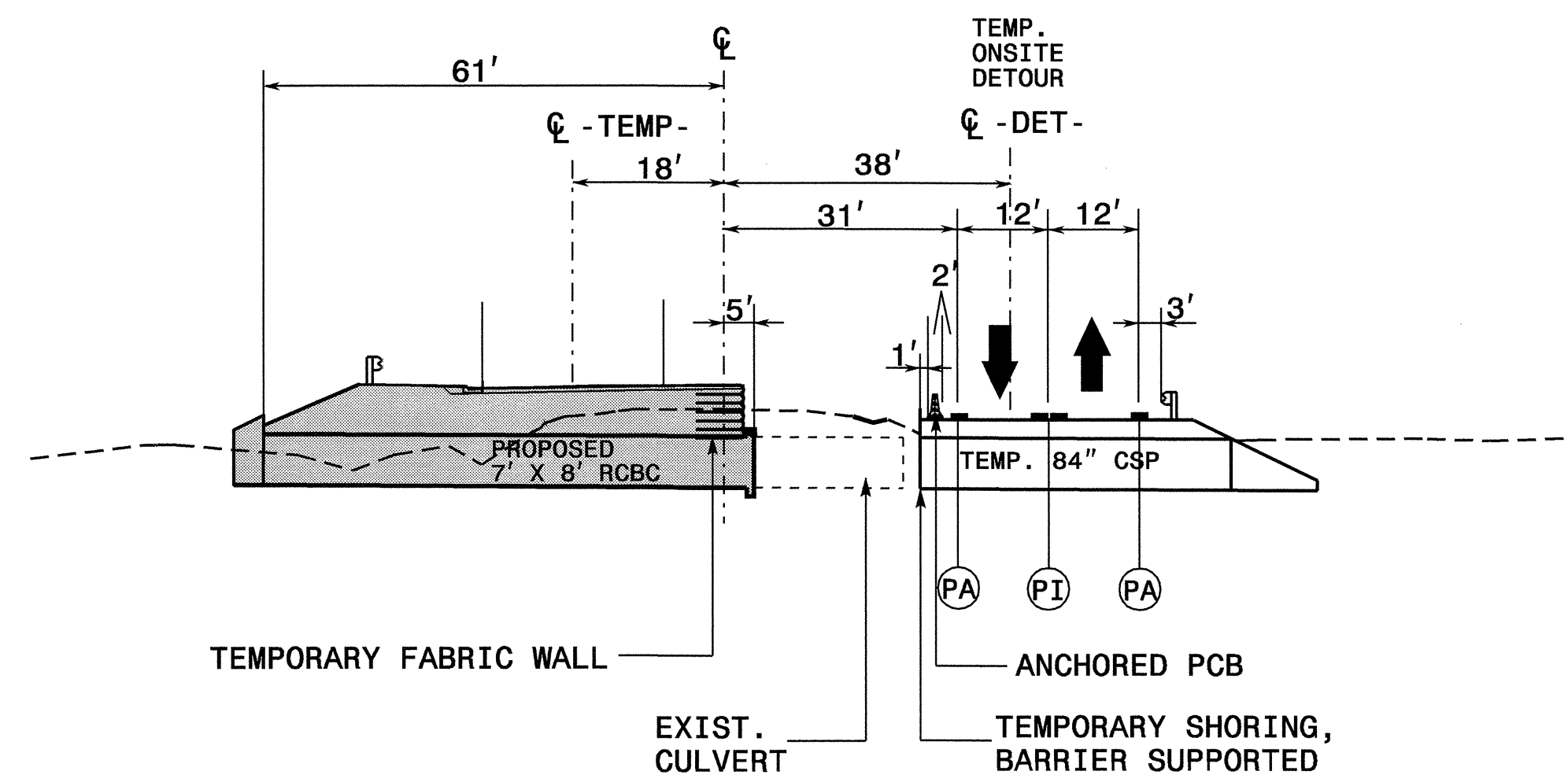
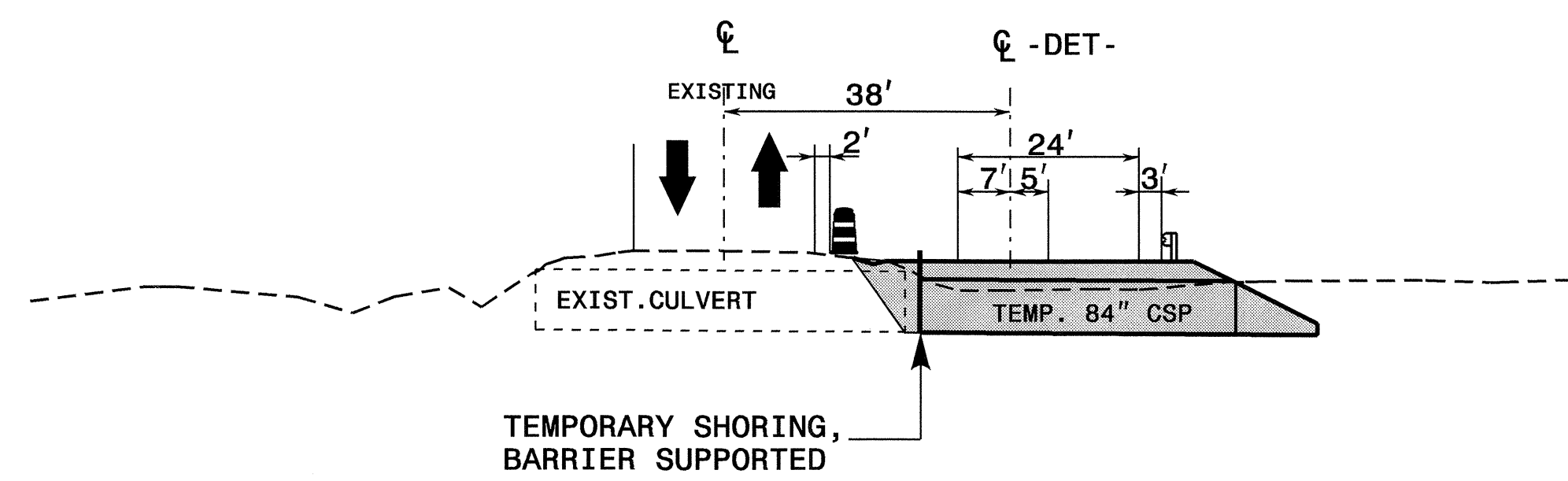
- AWAY FROM TRAFFIC, PERFORM THE FOLLOWING:
- 1.-COMPLETE STAGE I CONSTRUCTION OF THE PROPOSED CULVERT AT -L- STA.23+12.30. SEE HYDRO AND STRUCTURE PLANS.
 - 2.-CONSTRUCT TEMPORARY FABRIC WALL FROM -L- STA.22+73.30 TO -L- STA.23+51.30. SEE SHEET TCP-9 AND ROADWAY PLANS.
 3. CONSTRUCT LEFT SIDE OF -L- UP TO BUT NOT INCLUDING THE FINAL LAYER OF SURFACE COURSE FROM -L- STA.18+20+/- TO -L- STA.28+71+/-.
 4. INSTALL PERMANENT GUARDRAIL ON THE LEFT SIDE OF -L-. SEE ROADWAY PLAN FOR STATION LOCATIONS.
 5. COMPLETE CONSTRUCTION OF PROPOSED -Y1- AND PROPOSED -Y2- UP TO BUT NOT INCLUDING THE FINAL LAYER OF SURFACE COURSE, TYING TO THE EDGE AND ELEVATION OF THE LEFT SIDE OF PROPOSED -L-.
- STEP 4: -AWAY FROM TRAFFIC, INSTALL PORTABLE CONCRETE BARRIER (PCB) ON RIGHT SIDE OF TEMPORARY TRAFFIC PATTERN -TEMP- FROM -L- STA.22+28+/- TO -L- STA.23+95+/- . SEE SHEETS TCP-10 AND TCP-11.
- USING RSD 1101.02, SHEET 1 OF 9, PERFORM THE FOLLOWING:
SEE SHEETS TCP-6, TCP-10, TCP-11, PM-2, PM-3 AND ROADWAY PLANS.
- 1.-PLACE TEMPORARY MARKINGS (PAINT) AND TEMPORARY MARKERS (TEMPORARY RAISED) ON THE TOP OF PROPOSED -L- FROM STA.16+50+/- TO -L- STA.44+25+/- AS SHOWN ON SHEETS TCP-10.
 - PLACE TEMPORARY MARKINGS (PAINT) AND TEMPORARY MARKERS (TEMPORARY RAISED) ONTO -Y1- AND -Y2- IN THE FINAL TRAFFIC PATTERN AS FOLLOW:
SEE SHEETS PM-2 AND PM-3.
-FROM -Y1- STA.13+35+/- TO -Y1- STA.14+85+/-;
-FROM -Y2- STA.11+03+/- TO -Y2- STA.12+40+/-.
 2. WORK IN CONTINUOUS MANNER TO SHIFT TRAFFIC ON THE TOP OF -L- AS FOLLOW:
-CLOSE WB -DET-, AND PLACE TRAFFIC ON EB -DET- IN ONE-LANE TWO-WAY TEMPORARY TRAFFIC PATTERN.
 - CONSTRUCT TIE INS WB -DET- WITH LEFT SIDE OF -L- UP TO BUT NOT INCLUDING FINAL LAYER OF SURFACE COURSE AT -L- STA.18+20+/- AND -L- STA.28+71+/-.
 - PLACE TEMPORARY PAVEMENT MARKINGS (PAINT) AND MARKERS (TEMPORARY RAISED) ONTO THE TOP OF PROPOSED -L- FROM STA.16+50+/- TO STA.44+25+/-, AND TIE IN TO EXISTING MARKINGS AND MARKERS.
 - RESET CRASH CUSHION ON PCB AT -L- STA.22+28+/- AND -L- STA.23+95+/- . SEE SHEETS TCP-10 & TCP-11.
 - SHIFT TRAFFIC ON THE TOP OF PROPOSED -L- IN TWO-LANE TWO-WAY TEMPORARY TRAFFIC PATTERN. MAINTAIN ACCESS TO -Y1- & -Y2-.
- AWAY FROM TRAFFIC, PERFORM THE FOLLOWING:
- 1.-REMOVE TEMPORARY SHORING, BARRIER SUPPORTED FROM -DET- STA.14+71+/- TO -DET- STA.15+21+/-, AND ANCHORED PCB FROM LEFT SIDE OF TEMPORARY ONSITE DETOUR FROM -DET- STA.13+87+/- TO -DET- STA.16+00+/-.
 - REMOVE TEMPORARY ONSITE DETOUR FROM -L- STA.18+20+/- TO -L- STA. 28+71+/-.

2. COMPLETE STAGE II CONSTRUCTION OF THE PROPOSED CULVERT AT -L- STA.23+12.30. SEE HYDRO AND STRUCTURE PLANS.
 3. CONSTRUCT RIGHT SIDE OF PROPOSED -L- UP TO BUT NOT INCLUDE THE FINAL LAYER OF SURFACE COURSE FROM STA.14+59+/- TO STA.45+00+/. SEE ROADWAY PLANS.
 4. INSTALL FINAL SIGNAL AND COVER SIGNAL HEADS.
- STEP 5: USING RSD 1101.02, SHEET 1 OF 9, PERFORM THE FOLLOWING:
1. COMPLETE PROPOSED DRAINAGE WORK.
 2. REMOVE PCB AND CRASH CUSHION INSTALLED IN STEP 4 FROM -L- STA.22+28+/- TO -L- STA.23+95+/-.
 3. COMPLETE INSTALLATION OF CURB & GUTTER AND SIDEWALK ON -L- AND -Y- LINES.
 4. WORK IN CONTINUOUS MANNER TO SHIFT TRAFFIC ON PROPOSED -L- IN THE FINAL TRAFFIC PATTERN AS FOLLOW:
-PLACE FINAL LAYER OF SURFACE COURSE AND TEMP. PAVEMENT MARKINGS (PAINT) AND MARKERS (TEMPORARY RAISED) ON TWO EB LANES AND TWO-WAY LEFT TURN LANE OF -L- FROM STA.16+50+/- TO STA.42+70+/- .SEE SHEETS PM-2 & PM-3.
-SHIFT EB LANE OF -TEMP- ONTO TWO EB LANES OF -L- .
-PLACE FINAL SURFACE COURSE AND TEMP. PAVEMENT MARKINGS (PAINT) AND MARKERS (TEMPORARY RAISED) ON PROPOSED INSIDE WB LANE OF -L- FROM STA.16+50+/- TO -L- STA.42+70+/- .
-SHIFT WB LANE OF -TEMP- ON PROPOSED INSIDE WB LANE OF -L- .
-PLACE FINAL SURFACE COURSE AND TEMP. PAVEMENT MARKINGS (PAINT) AND MARKERS (TEMPORARY RAISED) ON PROPOSED OUTSIDE WB LANE OF -L- FROM -L- STA.16+50+/- TO -L- STA.42+70+/- .
-REMOVE CONFLICTING PAVEMENT MARKINGS AND MARKERS, AND INSTALL FINAL PAVEMENT MARKINGS FOR PROPOSED PATTERN EAST OF HILLTOP RD. AND ADAMS FARM PARKWAY INTERSECTION. SEE SHEETS TCP-10 & PM-3.
-UNCOVER AND ACTIVATE FINAL TRAFFIC SIGNAL HEADS AT -L- / -Y3- INTERSECTION.
 5. -COMPLETE PROPOSED -L-, -Y1 AND -Y2- UP TO AND INCLUDING FINAL LAYER OF SURFACE COURSE.
-PLACE FINAL PAVEMENT MARKINGS (THERMOPLASTIC) AND MARKERS (SNOWPLOWABLE RAISED) IN THE FINAL TRAFFIC PATTERN IN THE FOLLOWING LOCATIONS:
SEE SHEETS PM-2 & PM-3.
-FROM -L- STA.14+49+/- TO -L- STA.45+00+/-,
-FROM STA.13+35+/- TO -Y1- STA.14+85+/-,
-FROM -Y2 STA.11+03+/- TO -Y2- STA.14+55+/- .
 6. REMOVE ALL TRAFFIC CONTROL DEVICES.
 7. OPEN ALL ROADS TO THE FINAL TRAFFIC PATTERN. SEE SHEETS PM-2 & PM-3.

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APPROVED:  DATE: 5/10/06		PHASING	
	SCALE: NONE		REVISIONS
	DATE: 05/01/06		
	DWG. BY: AYL		
	DESIGN BY: AYL		
REVIEWED BY: JI			

**TRAFFIC MAINTAINANCE
DURING CULVERT CONSTRUCTION
REFER TO PHASING FOR DETAILS**



- MAINTAIN TRAFFIC ON EXISTING HILLTOP RD. WHILE INSTALLING TEMPORARY SHORING, BARRIER SUPPORTED AND TEMPORARY 84" CSP RIGHT OF EXISTING CULVERT AT -L- STA.23+12.30. SEE HYDRO AND STRUCTURE PLANS FOR DETAILS.

- CONSTRUCT TEMPORARY ONSITE DETOUR.

- SHIFT TRAFFIC ONTO TEMPORARY ONSITE DETOUR IN 2-L/2-W TEMPORARY TRAFFIC PATTERN.

- COMPLETE STAGE I CONSTRUCTION OF PROPOSED CULVERT, AND CONSTRUCT TEMPORARY FABRIC WALL. SEE HYDRO AND STRUCTURE PLANS FOR DETAILS.

- CONSTRUCT PROPOSED LEFT SIDE OF -L-.

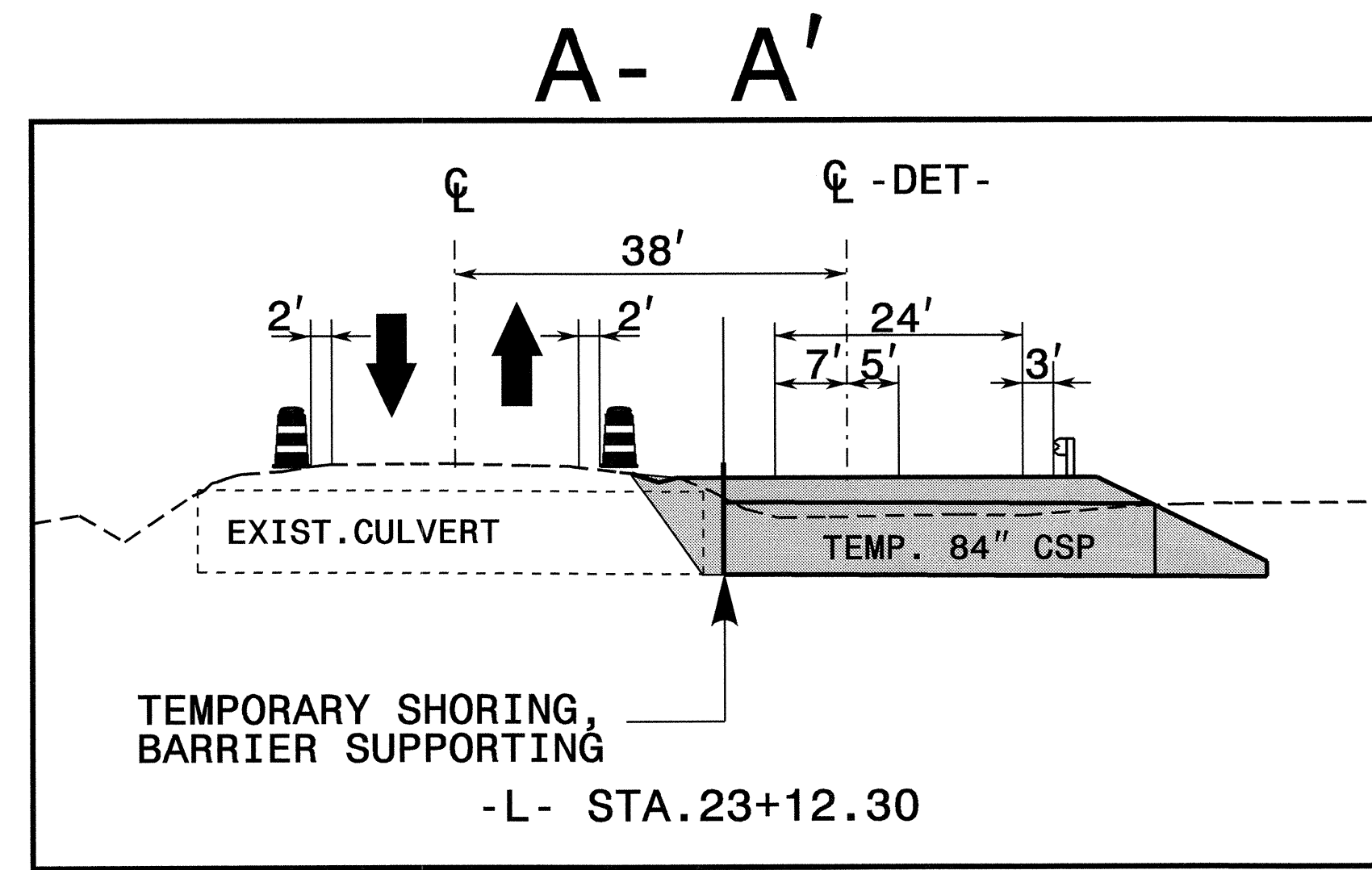
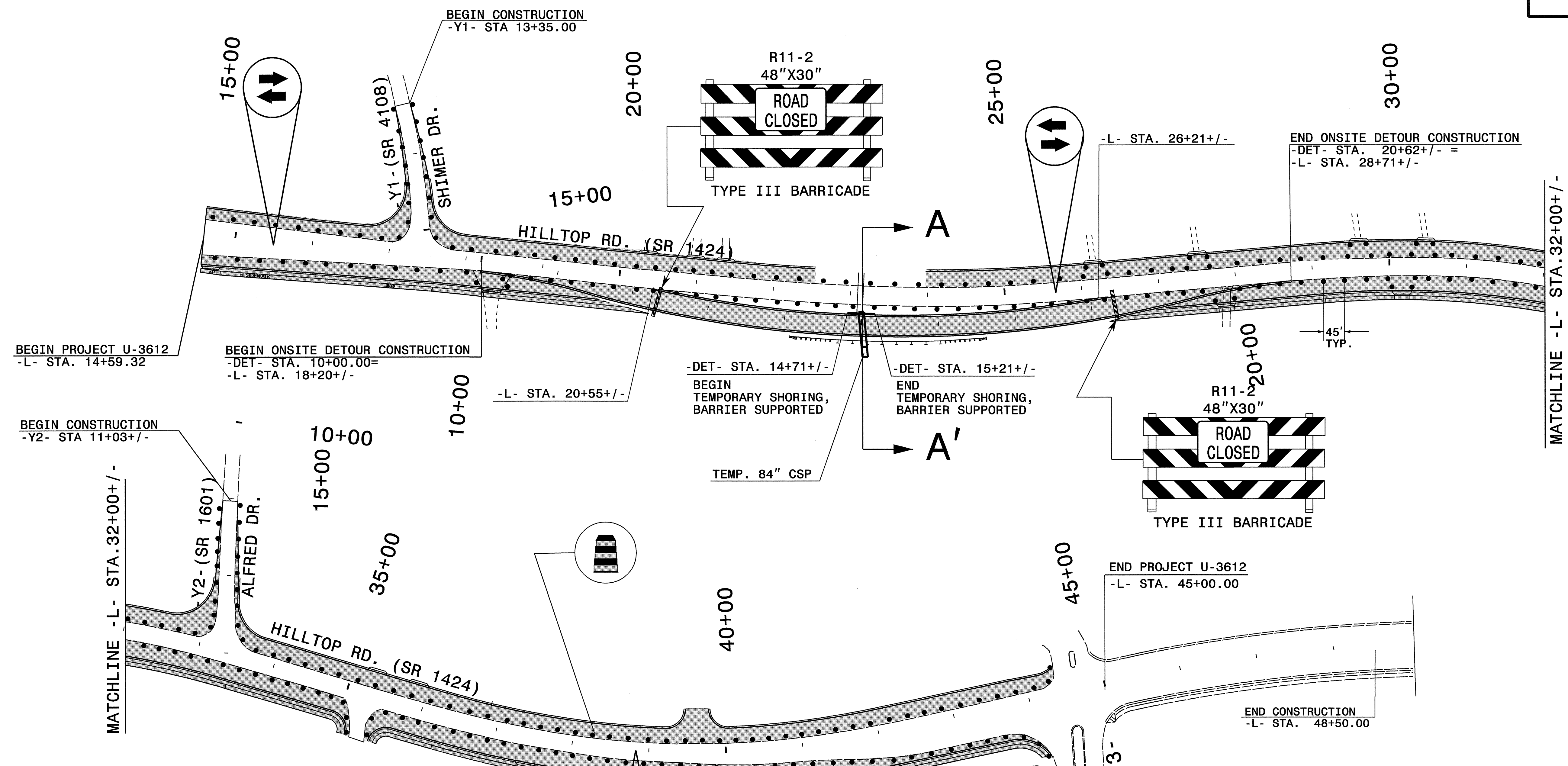
- SHIFT TRAFFIC ONTO LEFT SIDE OF -L- IN 2-L/2-W TEMPORARY TRAFFIC PATTERN.

- COMPLETE STAGE II OF PROPOSED CULVERT AT -L- STA.23+12.30, AND CONSTRUCT RIGHT SIDE OF PROPOSED -L-. SEE HYDRO AND STRUCTURE PLANS FOR DETAILS.

- PLACE TRAFFIC ON NEWLY CONSTRUCTED -L- INTO FINAL TRAFFIC PATTERN.

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 ayudmi AT WZTC224239

APPROVED: <i>Joseph Isiah</i> DATE: 5/10/06	TRAFFIC MAINTAINANCE DURING CULVERT CONSTRUCTION AT -L- STA.23+12.30				
	SCALE: NONE				
	DATE: 05/01/06				
	DWG. BY: AYL				
	DESIGN BY: AYL				
REVIEWED BY: JI	<table border="1"> <thead> <tr> <th>REVISIONS</th> </tr> </thead> <tbody> <tr><td> </td></tr> <tr><td> </td></tr> <tr><td> </td></tr> </tbody> </table>	REVISIONS			
REVISIONS					

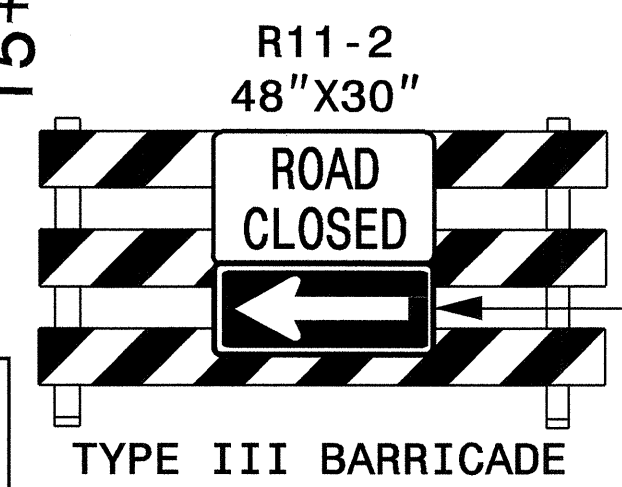
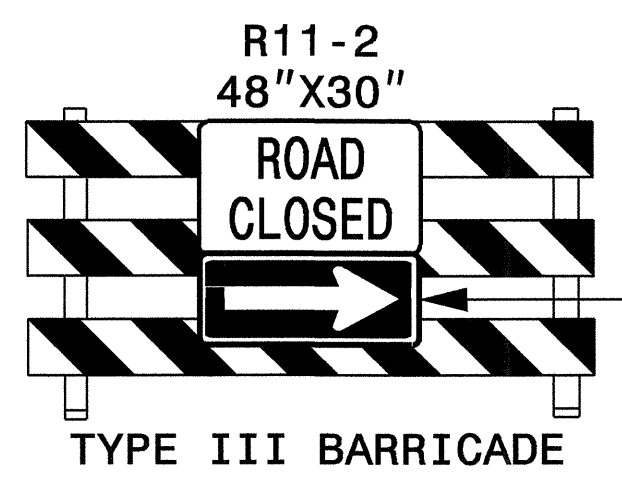
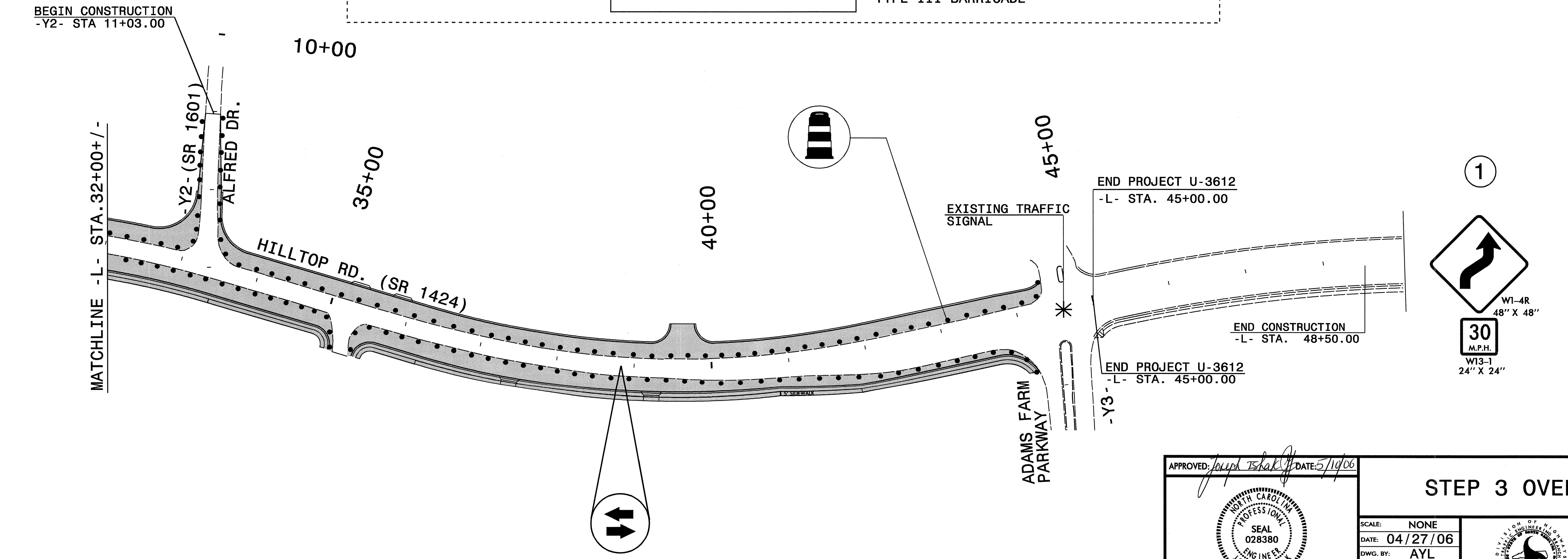
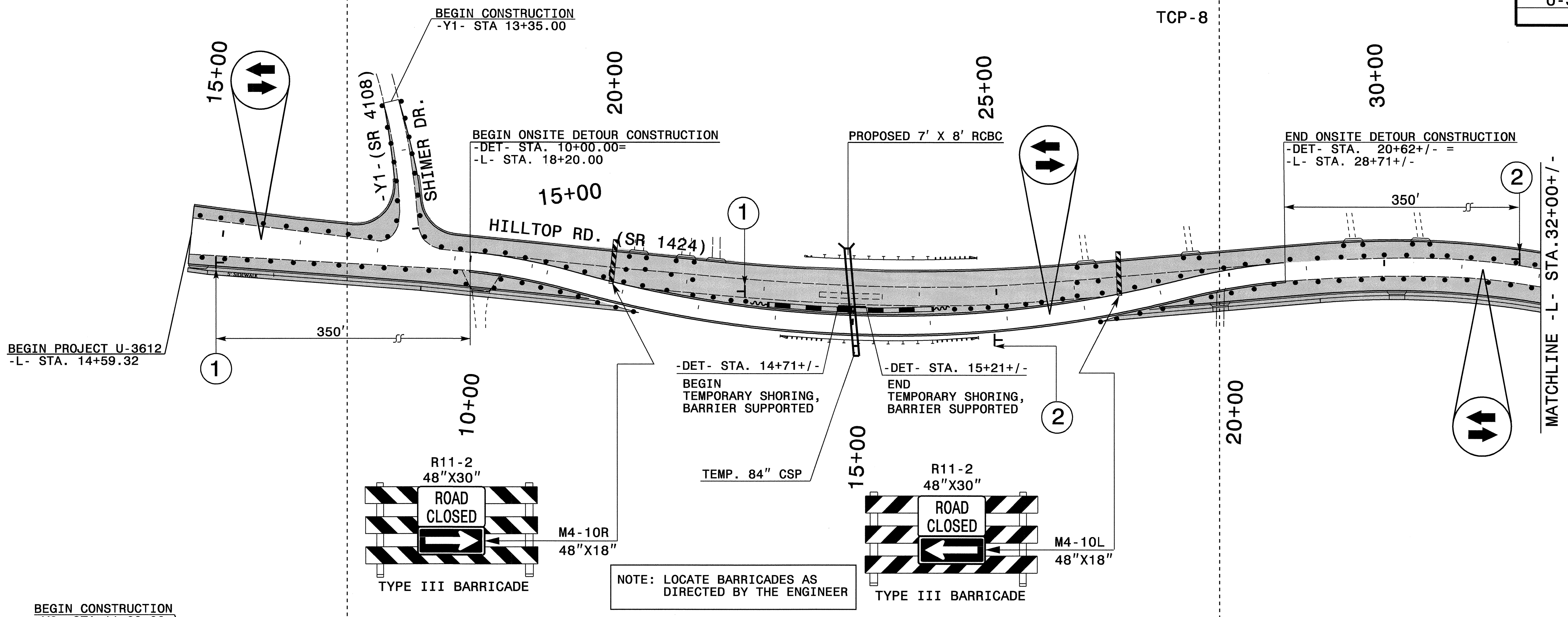


NOTES: SEE ROADWAY PLANS FOR ONSITE DETOUR PLANS. SEE HYDRO AND STRUCTURE PLANS FOR CULVERT CONSTRUCTIONS.

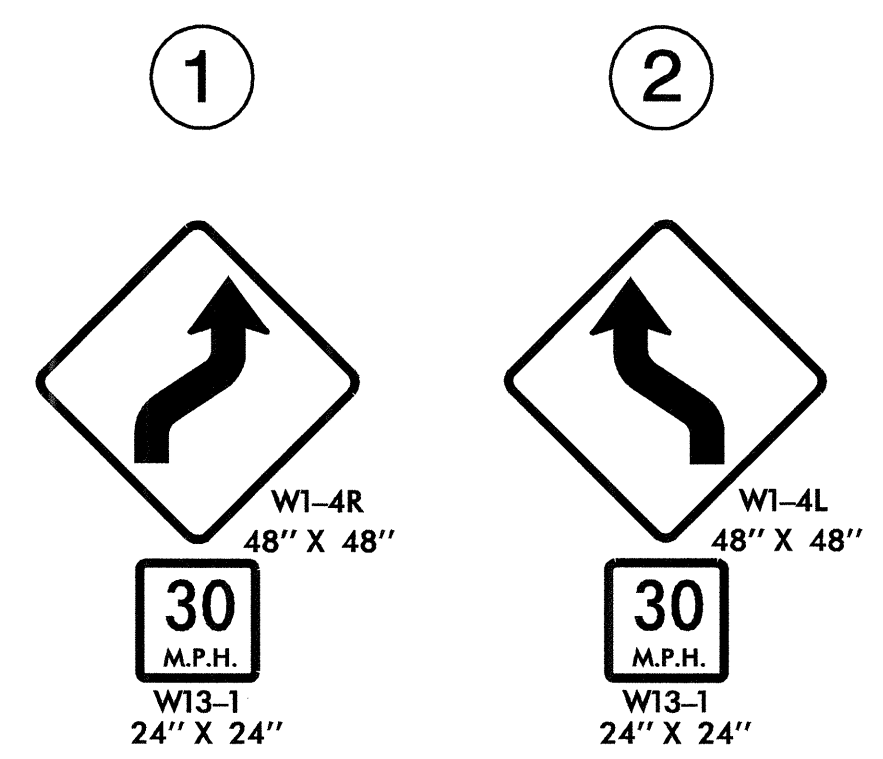
- FOR DESIGN OF TEMPORARY SHORING - BARRIER SUPPORTED, USE THE FOLLOWING SOIL PARAMETERS:
UNIT WEIGHT OF SOIL ABOVE WATER TABLE, $\gamma' = 120$ psf
UNIT WEIGHT OF SOIL BELOW WATER TABLE, $\gamma' = 60$ psf
FRICTION ANGLE, $\phi = 30^\circ$
COHESION, $c = 0$ psf
- SHORING EMBEDMENT MAY BE DIFFICULT TO OBTAIN DUE TO THE PRESENCE OF WEATHERED ROCK AT SHALLOW DEPTHS.
- TEMPORARY SHORING SHALL NOT EMPEDE FLOW IN THE PROPOSED TEMPORARY PIPE.
- TEMPORARY SHORING SHALL INCLUDE A SEAL AROUND THE PROPOSED TEMPORARY PIPE TO AID IN RETAINING BACKFILL MATERIAL. THE COST FOR THE SEAL SHALL BE INCIDENTAL TO THE COST OF THE TEMPORARY SHORING.

APPROVED: <i>[Signature]</i> DATE: 5/10/06	STEP 2 OVERVIEW	
	SCALE: NONE	
	DATE: 04/27/06	
	DWG. BY: AYL	
	DESIGN BY: AYL	
REVIEWED BY: JI	REVISIONS	

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NOTE: LOCATE BARRICADES AS DIRECTED BY THE ENGINEER

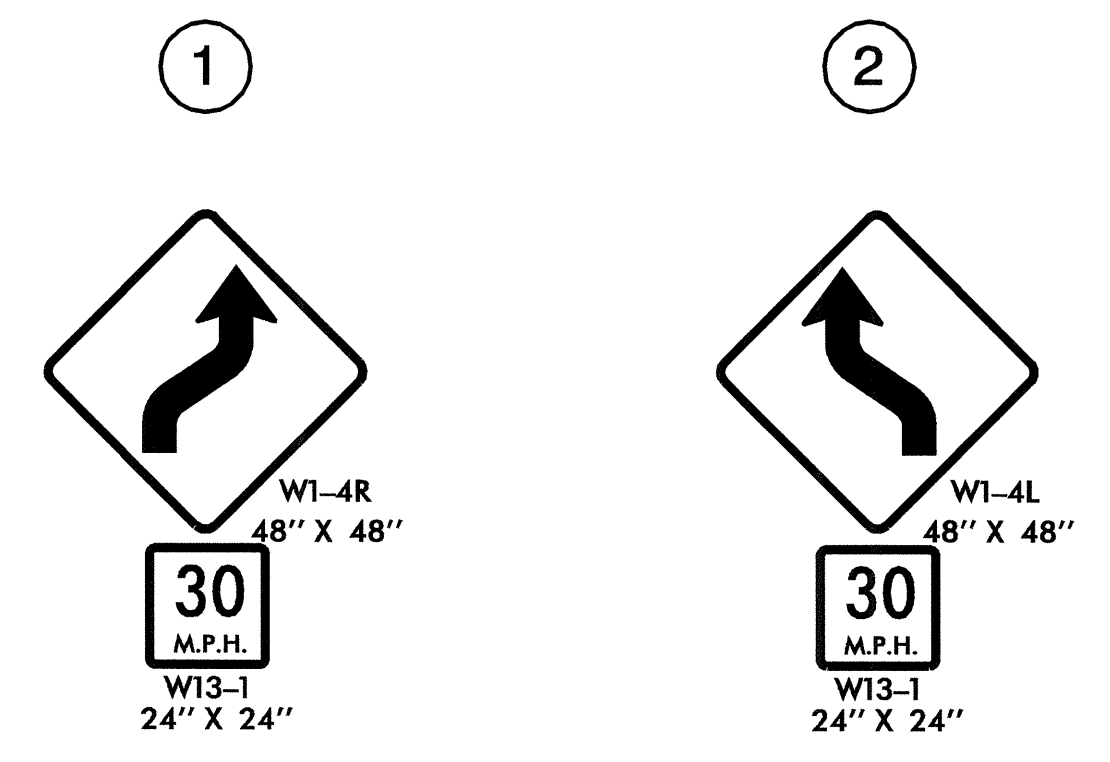
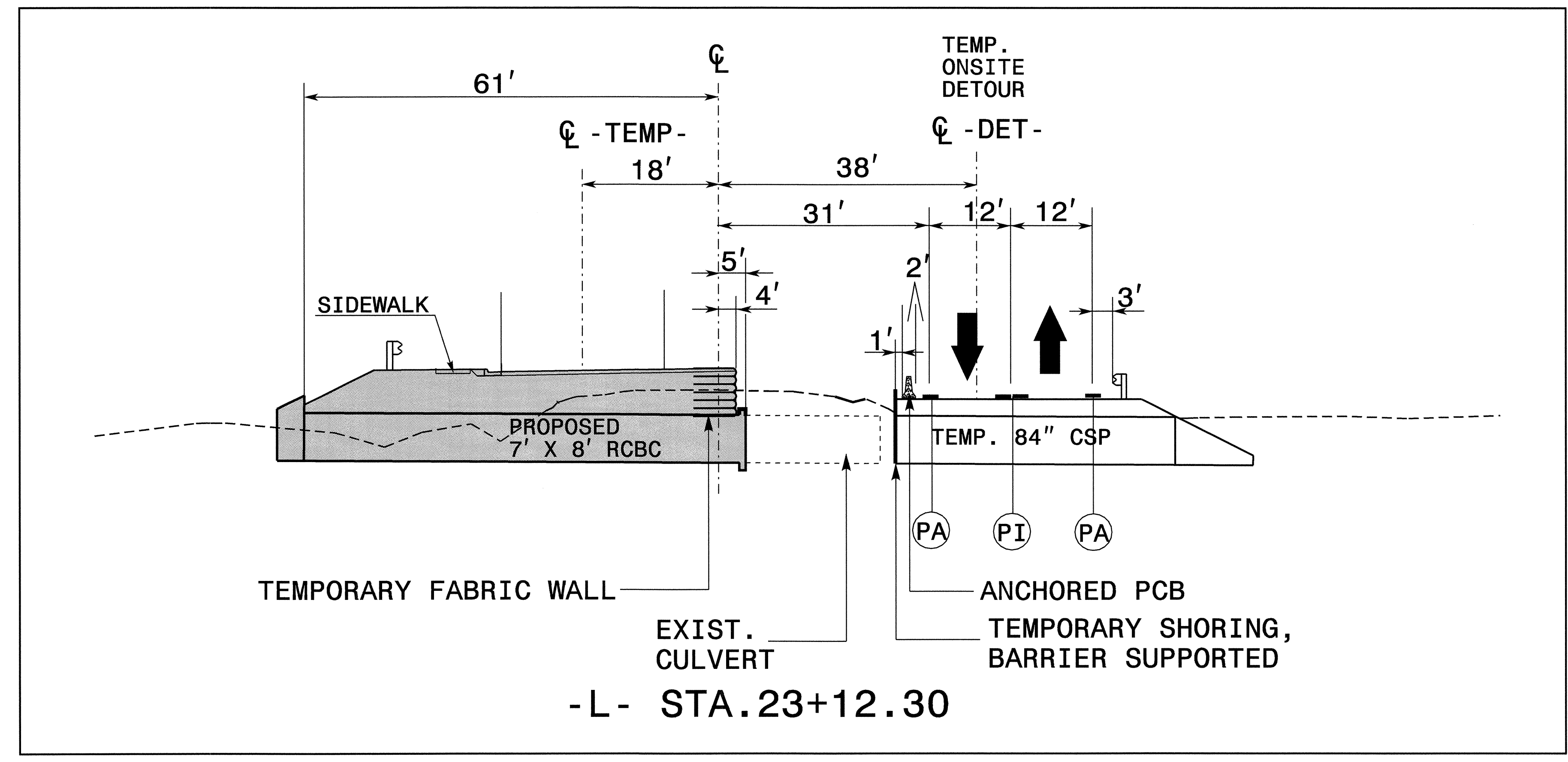
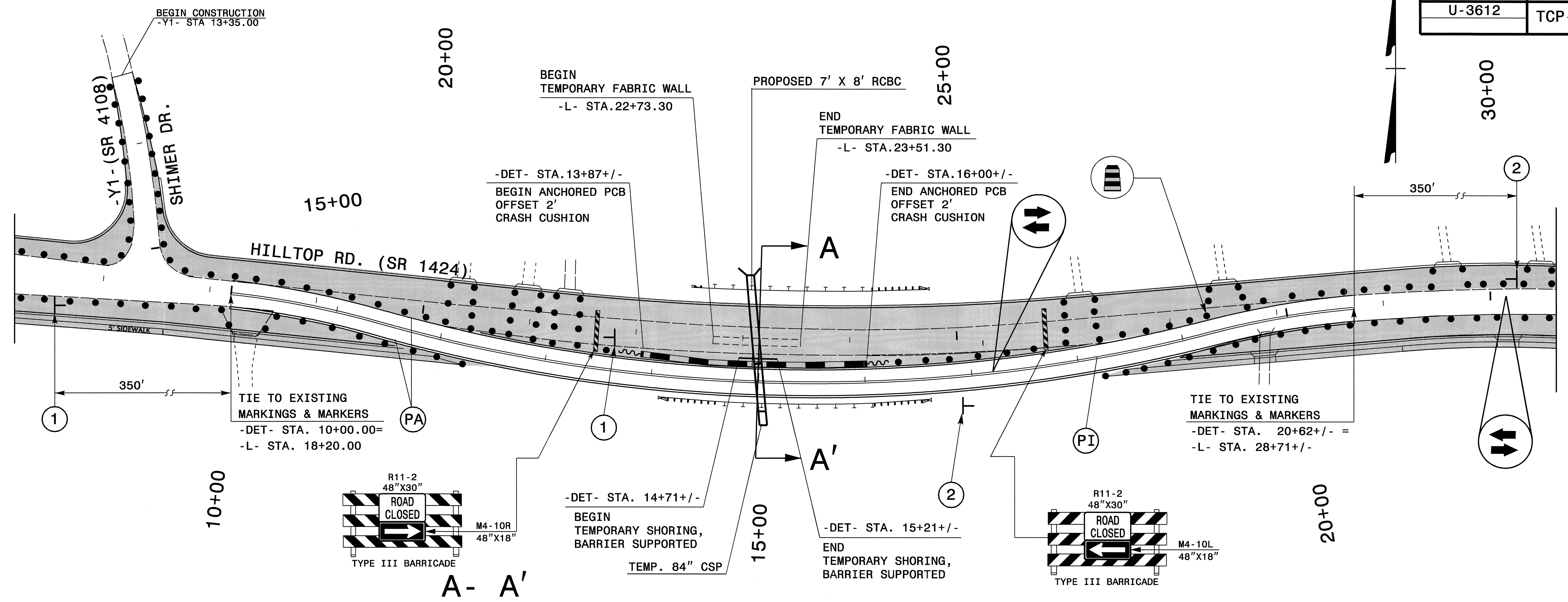


APPROVED: *Joseph Ishak* DATE: 5/19/06
 SEAL 028380
 ENGINEER JOSEPH ISHAK

STEP 3 OVERVIEW

SCALE: NONE		REVISIONS
DATE: 04/27/06		
DWG. BY: AYL		
DESIGN BY: AYL		
REVIEWED BY: JI		

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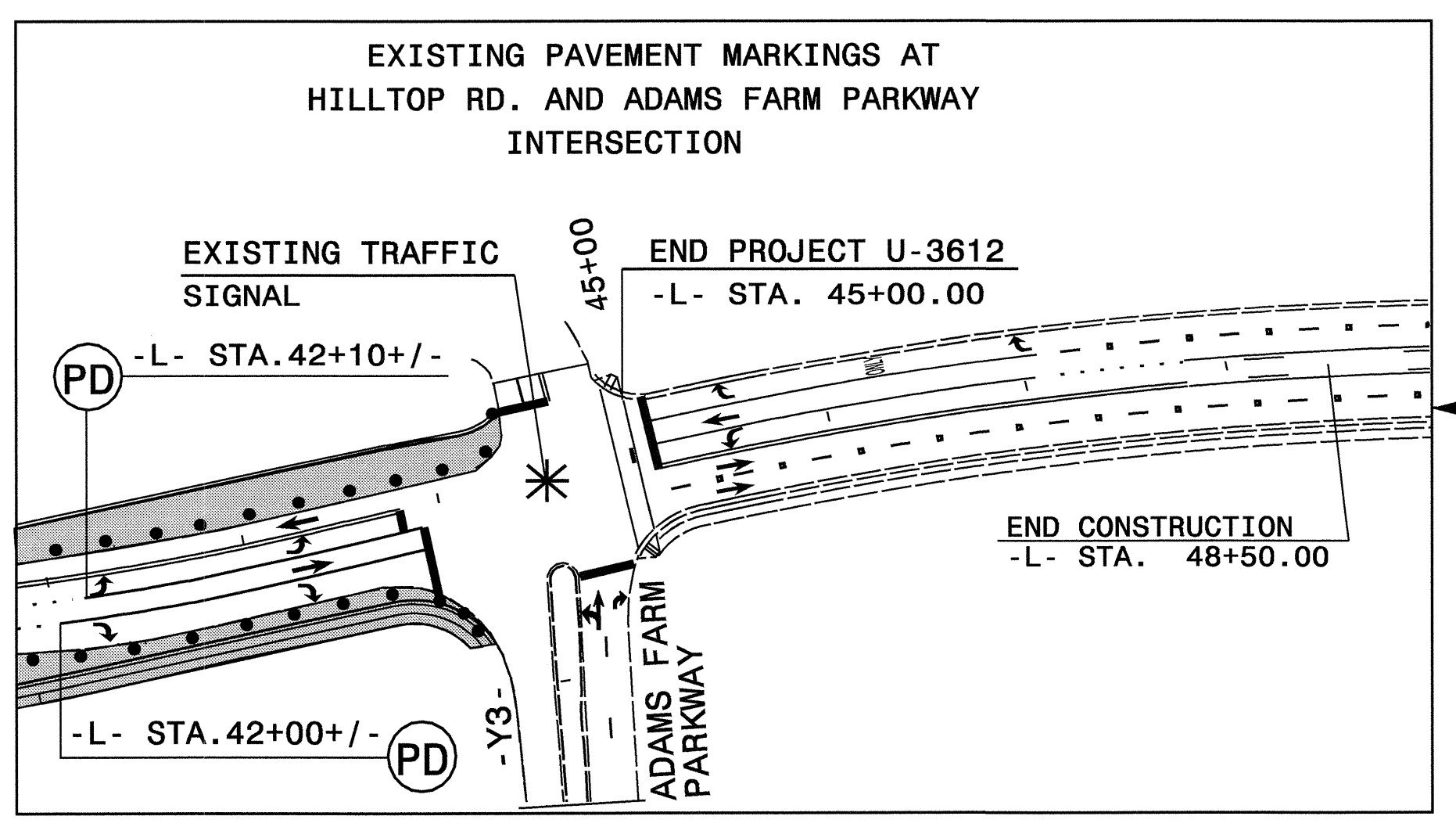
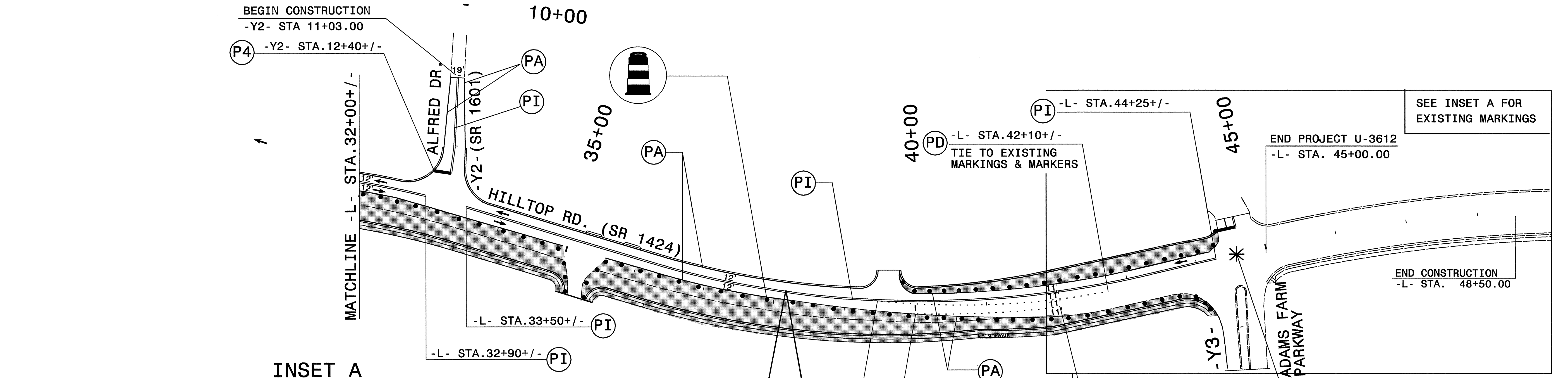
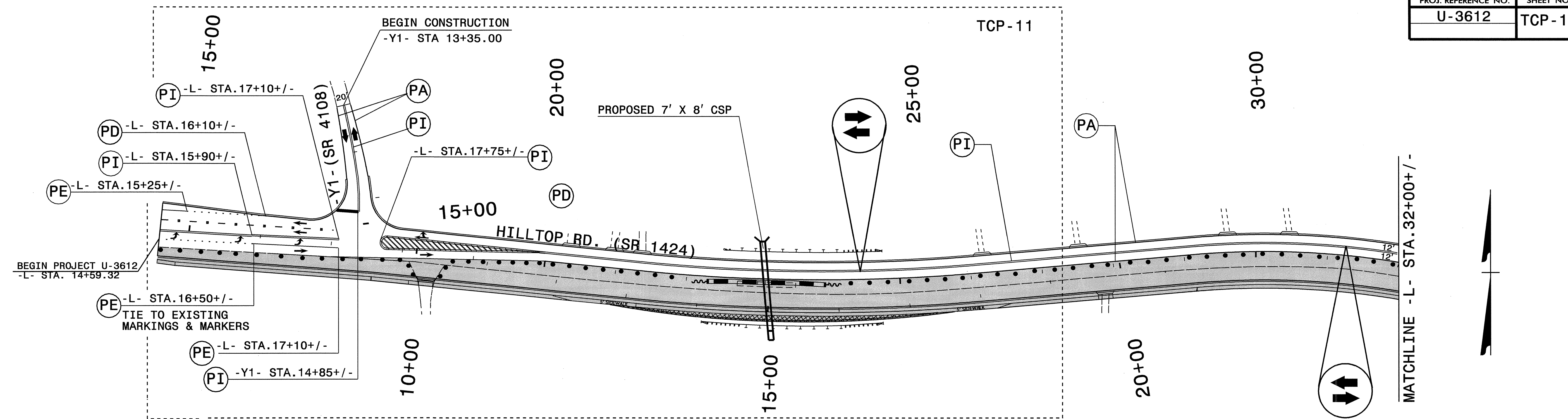


NOTES: 1. USE THIS DETAIL AND RSD 1250.01, 1251.01 TO INSTALL TEMPORARY PAVEMENT MARKERS.

2. ONE TEMPORARY FABRIC WALL WILL BE REQUIRED TO CONSTRUCT STAGE II OF THE PROPOSED CULVERT WHILE MAINTAINING TRAFFIC. SEE ROADWAY PLANS FOR TEMPORARY FABRIC WALL DESIGN.

APPROVED: <i>[Signature]</i> DATE: 5/19/06	STEP 3 DETAIL	
	SCALE: NONE	
	DATE: 04/27/06	
	DESIGN BY: AYL	
	REVIEWED BY: JI	
		REVISIONS

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 alyudmi AT WZTC24239



EXISTING PAVEMENT MARKINGS TO BE REMOVED IN STEP 5. SEE PHASING AND PM-3.

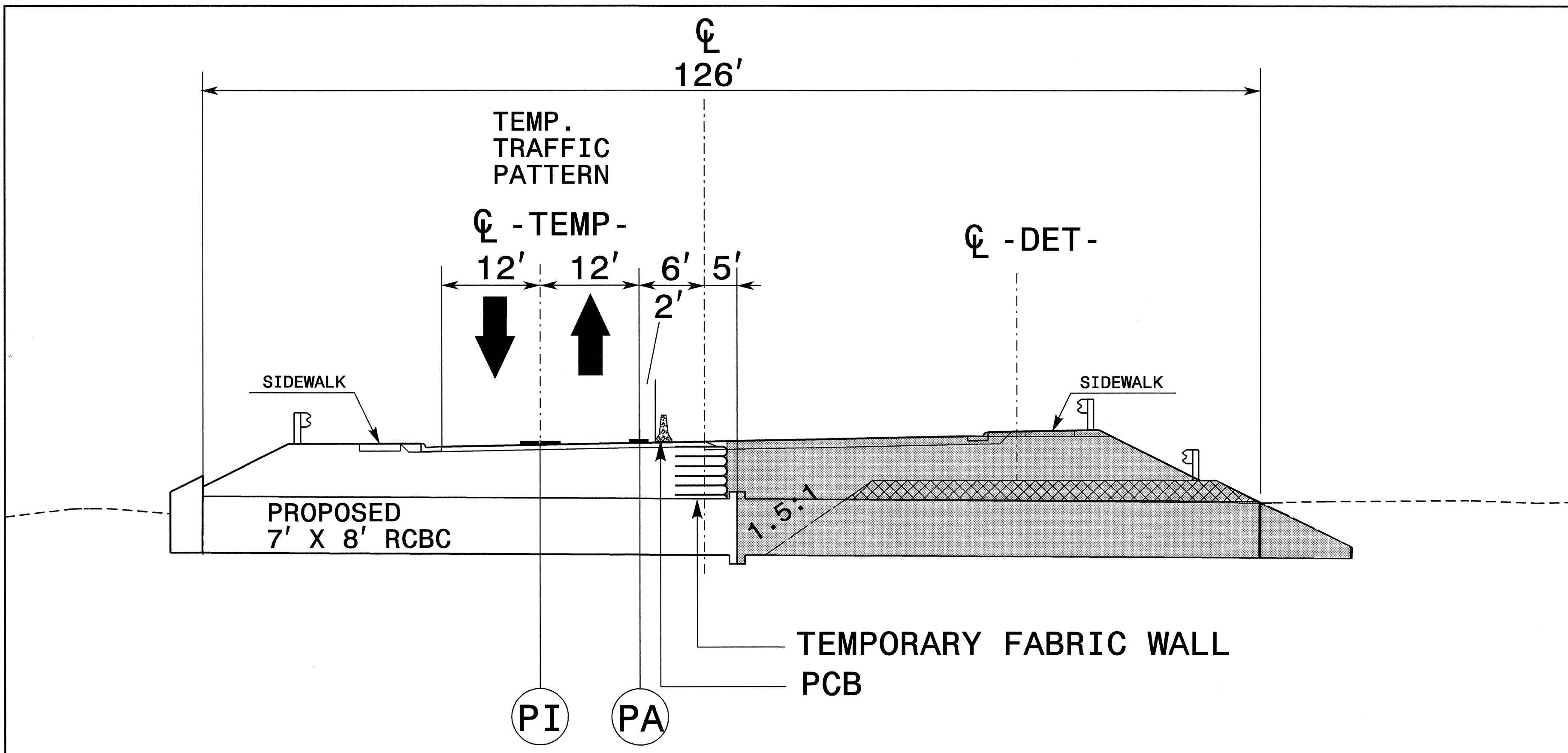
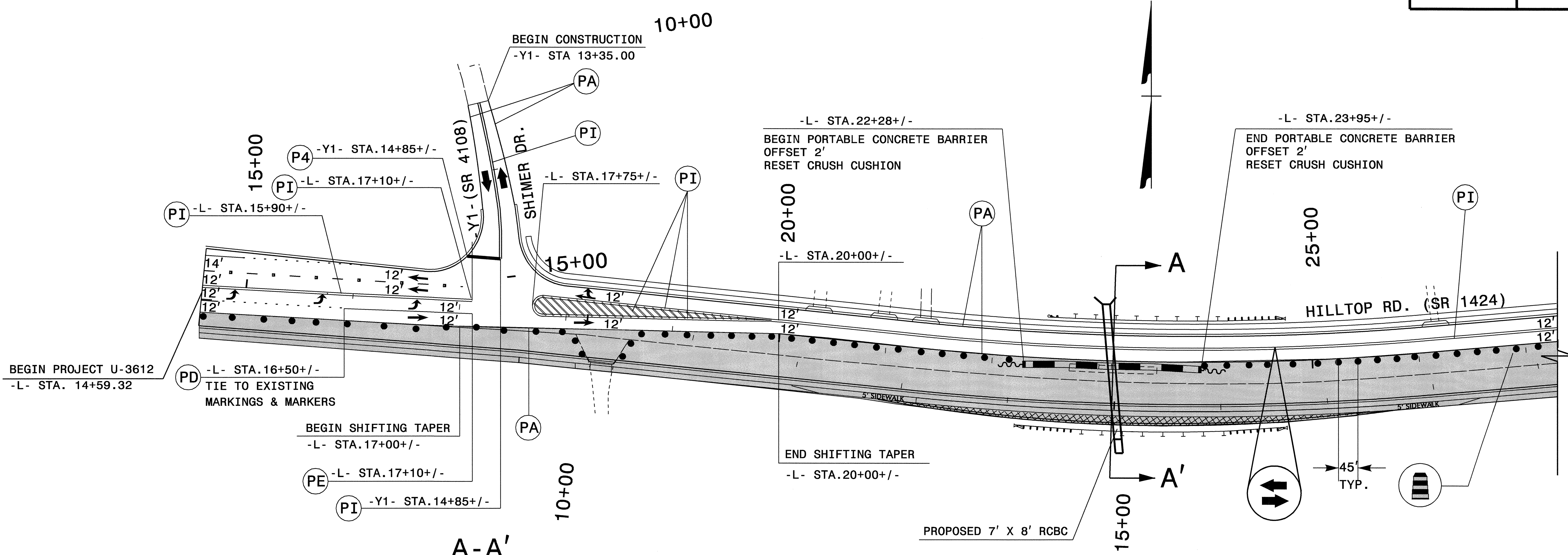
NOTE: USE THIS DETAIL AND RSD 1250.01 AND 1251.01 TO INSTALL TEMPORARY PAVEMENT MARKERS.

APPROVED: *Joseph Ishak* DATE: 5/10/06

SEAL 028380
 PROFESSIONAL ENGINEER
 JOSEPH ISHAK

STEP 4 OVERVIEW	
SCALE: NONE	REVISIONS
DATE: 04/27/06	
DWG. BY: AYL	
DESIGN BY: AYL	
REVIEWED BY: JI	

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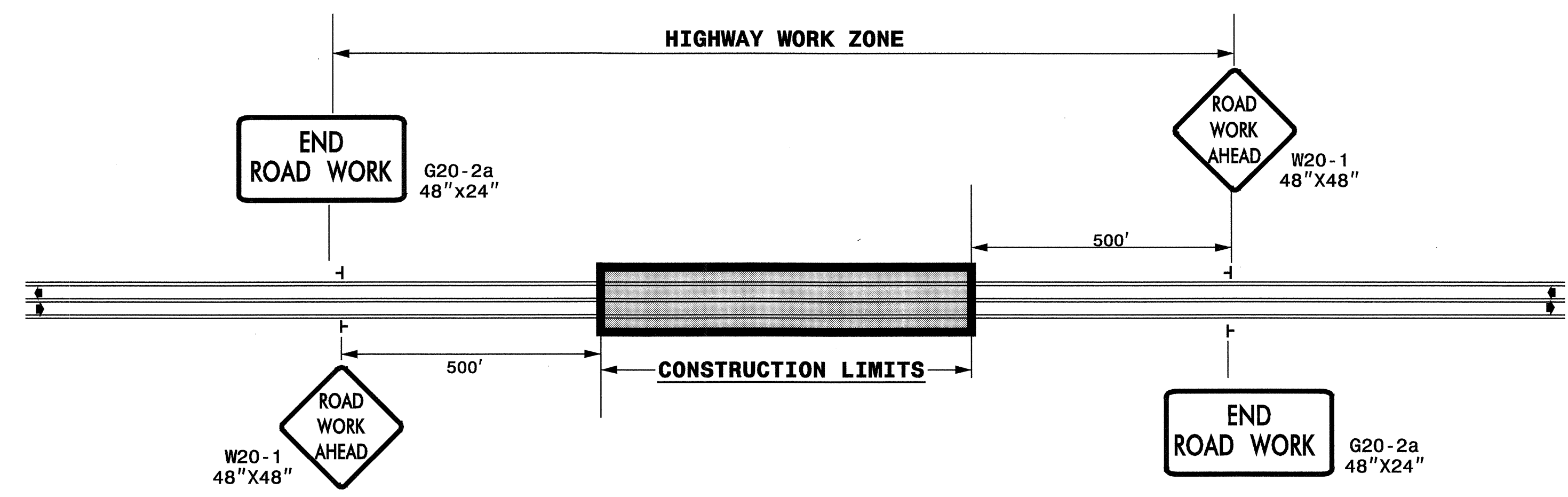


NOTE: USE THIS DETAIL AND RSD 1250.01 AND 1251.01 TO INSTALL TEMPORARY PAVEMENT MARKERS.

APPROVED: <i>Joseph Ishak</i> DATE: 5/10/06	STEP 4 DETAIL	
SCALE: NONE		REVISIONS
DATE: 04/27/06		
DWG. BY: AYL		
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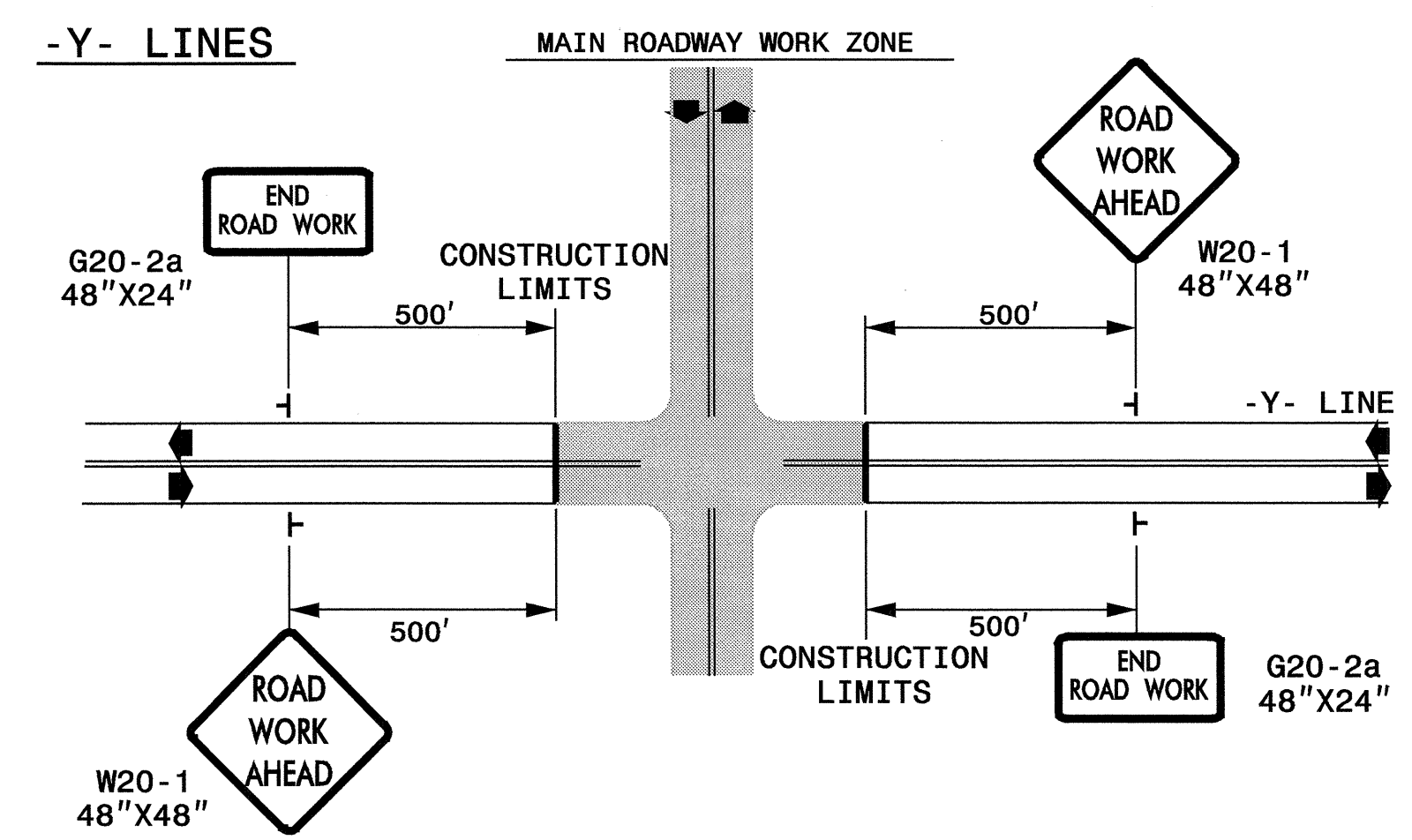
TWO-WAY UNDIVIDED & URBAN FREEWAYS (L-LINES)



POSTED SPEED LIMIT (M.P.H.)	RECOMMENDED MINIMUM SIGN SPACING
≤ 50	500'
≥ 55	1000'

STATE OF NORTH CAROLINA
 DEPT. OF TRANSPORTATION
 DIVISION OF HIGHWAYS
 RALEIGH, N.C.

ROADWAYS INTERSECTING ALONG 2 WAY UNDIVIDED WORK ZONE (Y-LINES)



DETAIL DRAWING FOR
 TWO-WAY UNDIVIDED
 WORK ZONE WARNING SIGNS

GENERAL NOTES

- USE FLUORESCENT ORANGE SHEETING (TYPE VII OR HIGHER) ON ALL ADVANCED WORK ZONE SIGNS.
- DO NOT INSTALL ADVANCE WARNING SIGNS MORE THAN 3 DAYS PRIOR TO BEGINNING OF WORK.
- SIGNS SHOWN ARE REQUIRED FOR WORK ZONES THAT WILL REMAIN IN EFFECT OVERNIGHT. FOR SHORT-TERM DAILY MAINTENANCE TYPE OPERATIONS, THIS SIGNING APPLICATION IS OPTIONAL; MAY USE ONLY APPLICABLE ROADWAY STANDARD DRAWINGS INSTEAD. HOWEVER, IF THIS SIGNING APPLICATION IS USED, SIGNS MAY BE PORTABLE MOUNTED.
- ALL SIGN SPACING DIMENSIONS ARE APPROXIMATE, FIELD ADJUST AS NECESSARY OR AS DIRECTED.
- USE 3LB STEEL U-CHANNEL POST OR 4" X 4" WOOD POST FOR ALL WORK ZONE SIGNS. 3LB STEEL U-CHANNEL POSTS MUST MEET THE REQUIREMENTS OF STANDARD SPECIFICATION SECTION 1094-1(B). MAY BE GALVANIZED STEEL, OR MAY BE PAINTED GREEN BY THE POST MANUFACTURER. SQUARE STEEL TUBING POSTS HAVING EQUIVALENT STRENGTH OF THE 3 LB STEEL U-CHANNEL POST ARE ALSO ACCEPTABLE FOR USE. ERECT SIGNS PER ROADWAY STANDARD DRAWING 1110.01. PAYMENT FOR WOOD POSTS, 3LB STEEL U-CHANNEL AND SQUARE STEEL TUBING POSTS WITH SIGNS WILL BE MADE ACCORDING TO STANDARD SPECIFICATION "WORK ZONE SIGNS" SECTION 1110.
- WHEN NECESSARY, USE SPLICING IN ACCORDANCE WITH ROADWAY STANDARD DRAWING NO. 1110.01. REMOVE ENTIRE POST WHEN REMOVING SIGNS WITH SPLICED POSTS.
- DO NOT BACK BRACE SIGN SUPPORTS.

LEGEND

┆ STATIONARY SIGN

◄ DIRECTION OF TRAFFIC FLOW

SHEET 1 OF 1

APPROVED: <i>Joseph Estak</i> DATE: 5/19/06	DETAIL DRAWING FOR TWO-WAY UNDIVIDED AND URBAN FREEWAYS ADVANCED WORK ZONE WARNING SIGNS	
	SCALE: NONE	REVISIONS
	DATE: 6/21/05	7-98 10/01
	DWG. BY: AYL	10-98 03/04
	DESIGN BY: AYL	01/01
REVIEWED BY: BLW		

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