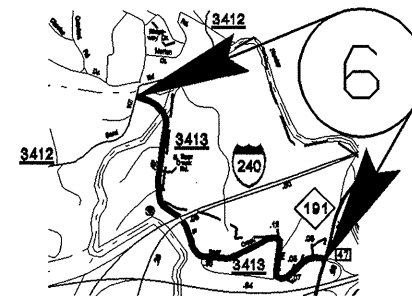
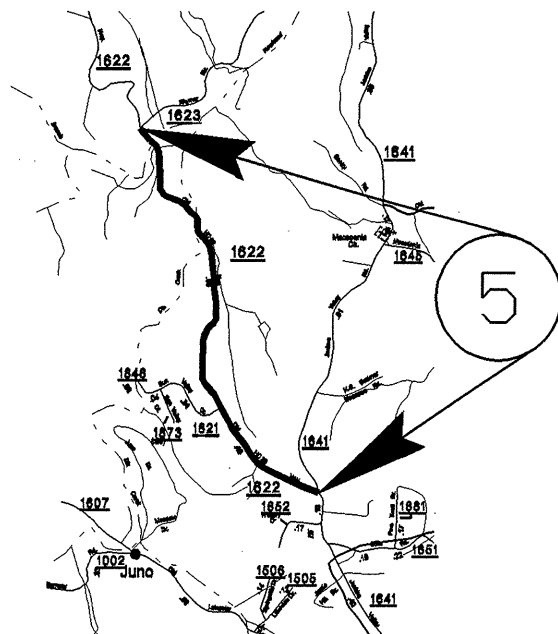
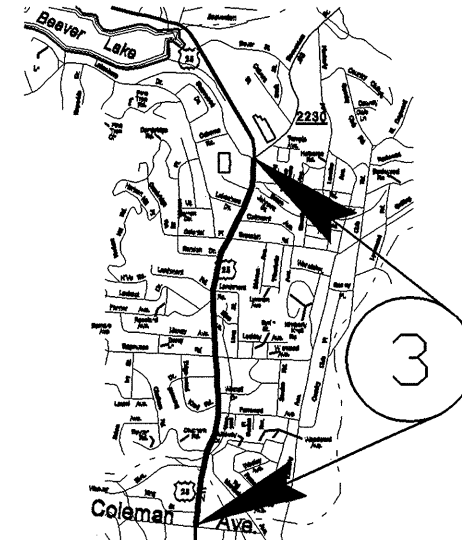
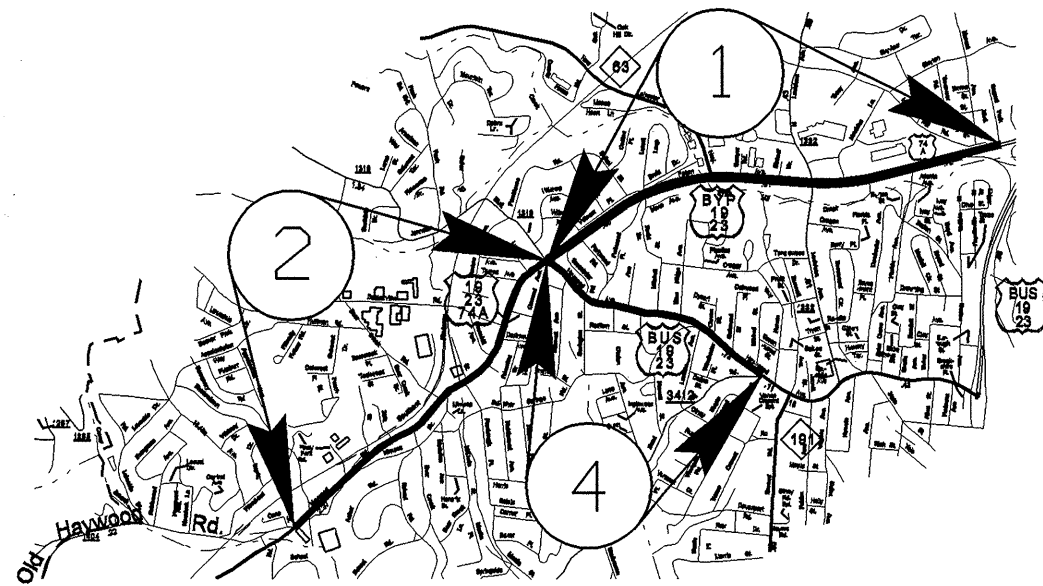
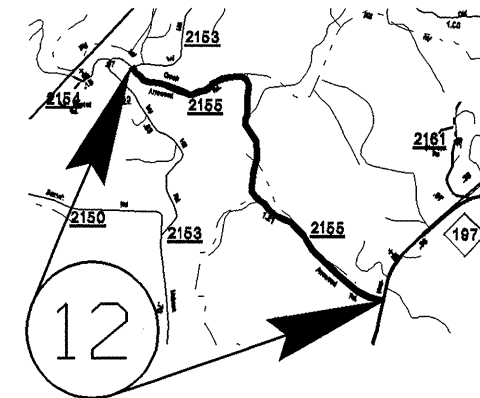
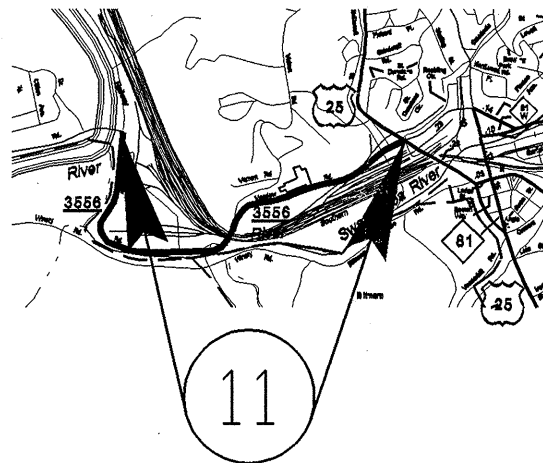
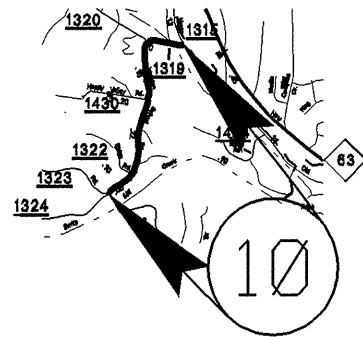
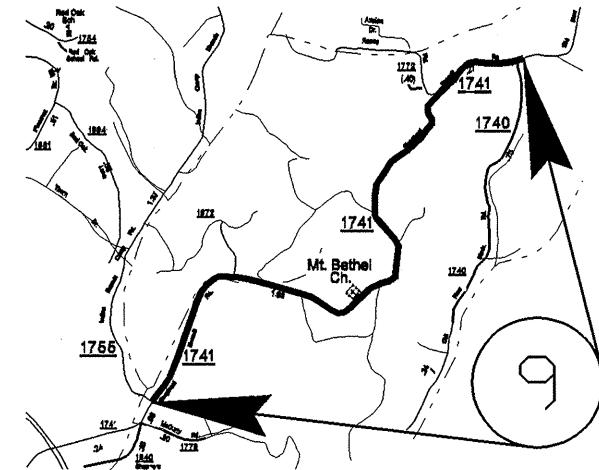
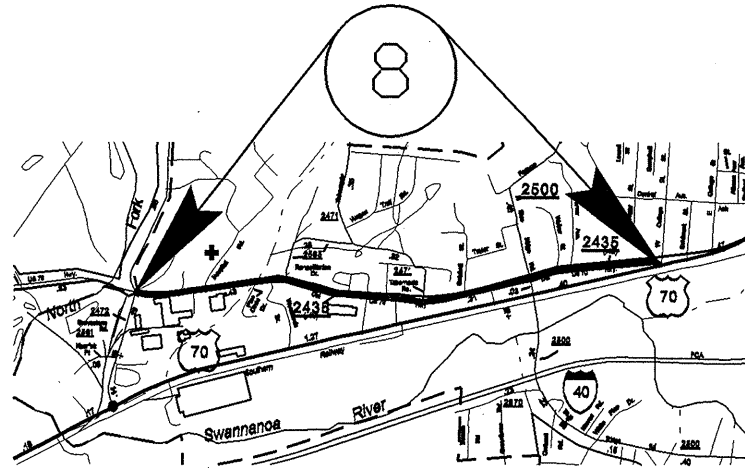
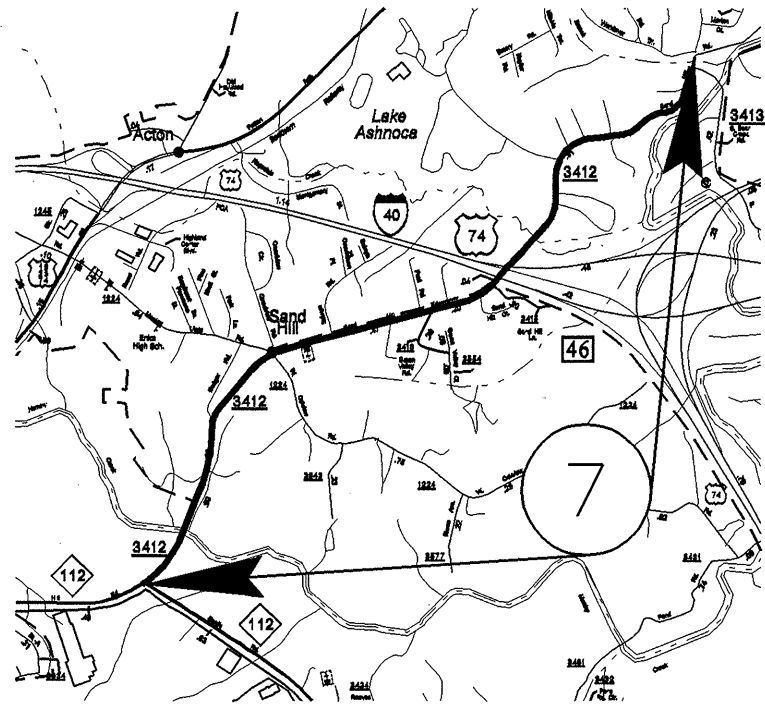


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13CR.10111.6 , 13CR.20111.4	1	



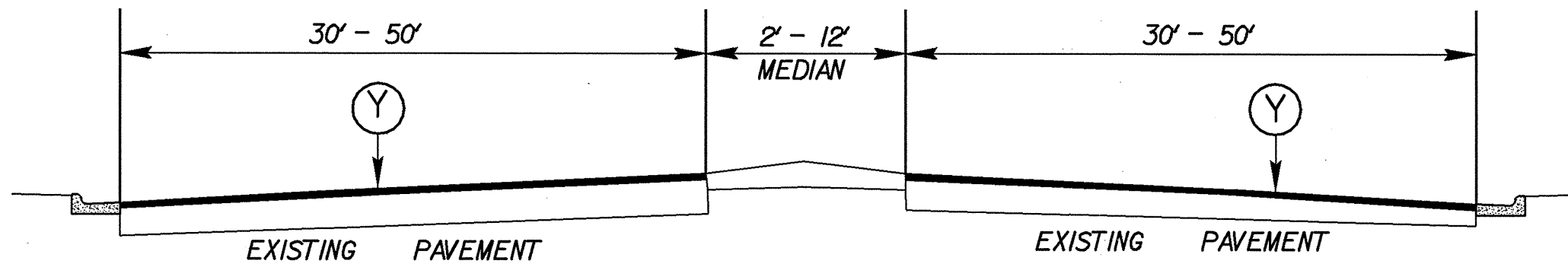
# BUNCOMBE COUNTY

PROJECT NO.	SHEET NO.	TOTAL SHEETS
13CR.10111.6 , 13CR.20111.4	2	



# BUNCOMBE COUNTY

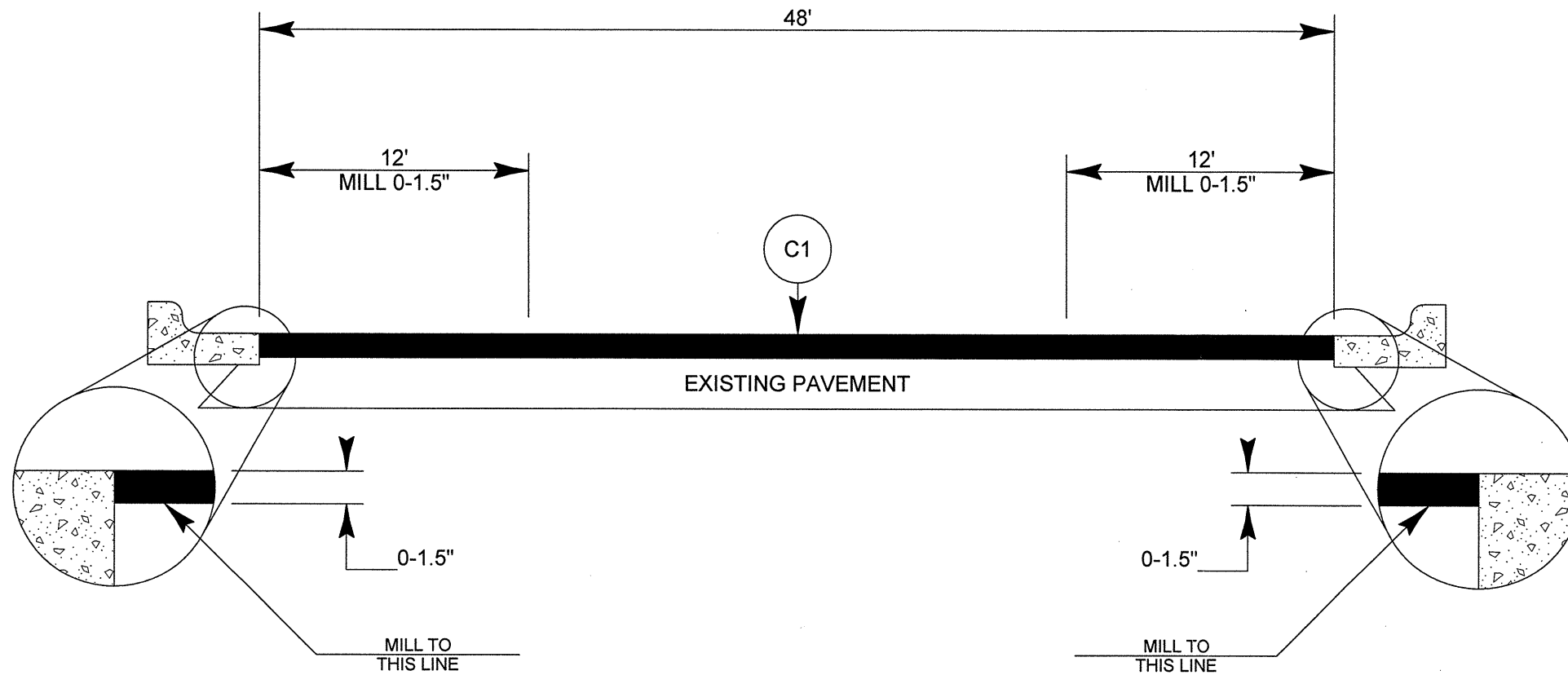
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13CR.10111.6 , 13CR.20111.4	3	



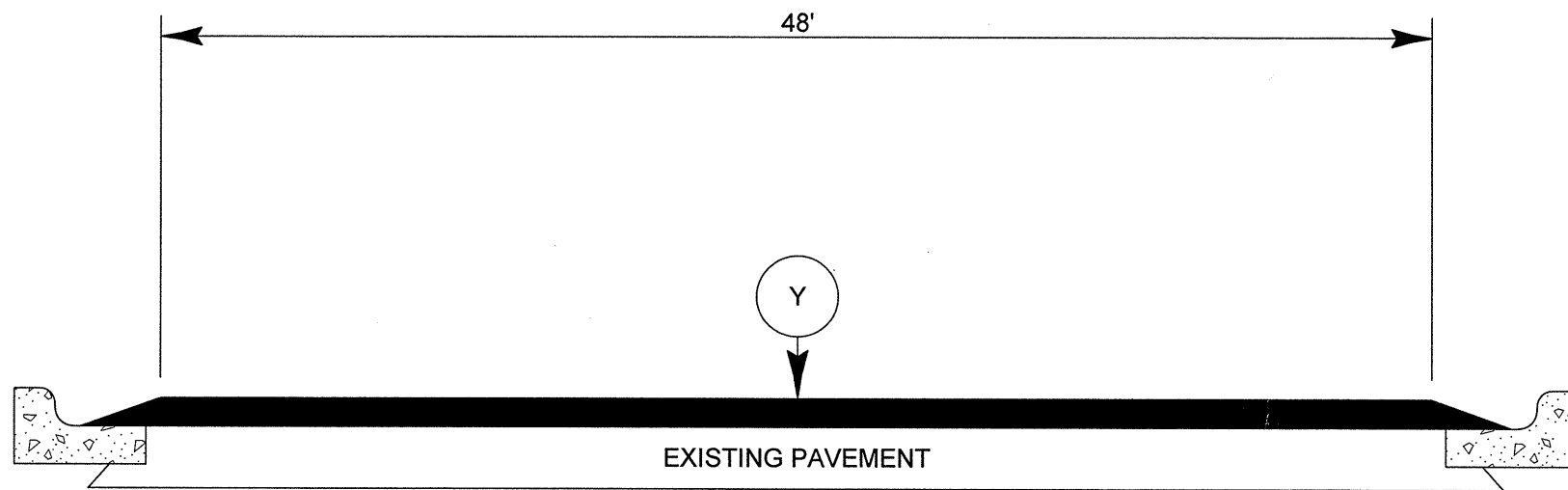
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PAVEMENT SCHEDULE	
C1	PROP. APPROX. 1.5" ACSC, TYPE S9.5B, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YARD
Y	PROP. APPROX. 5/8" ULTRATHIN HOT MIX ASPHALT, TYPE B, AT AN AVERAGE RATE OF 70 LBS. PER SQ YARD

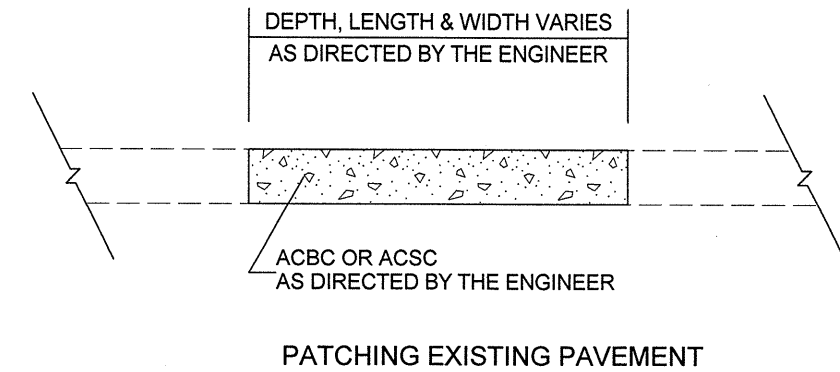
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13CR.10111.6, 13CR.20111.4	4	



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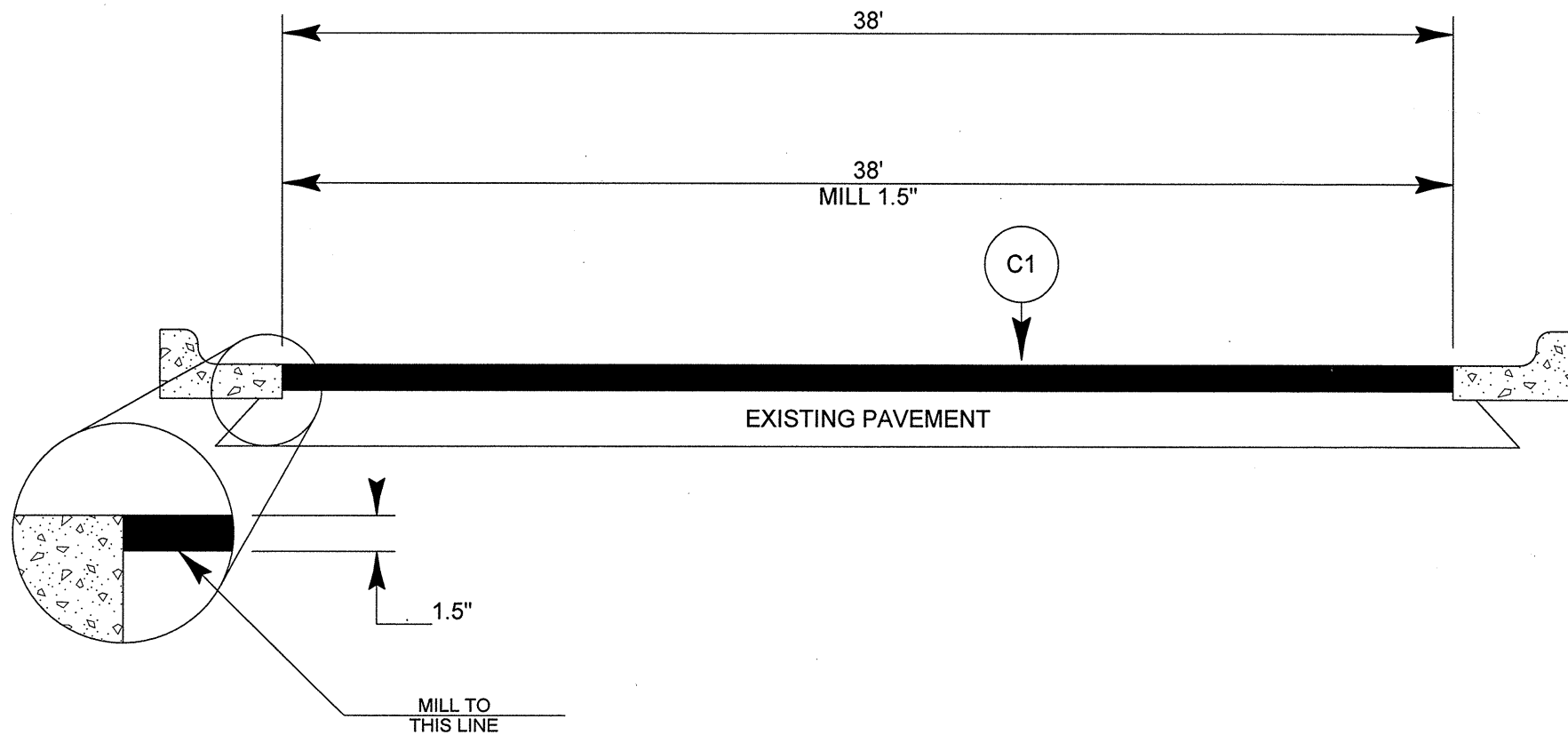


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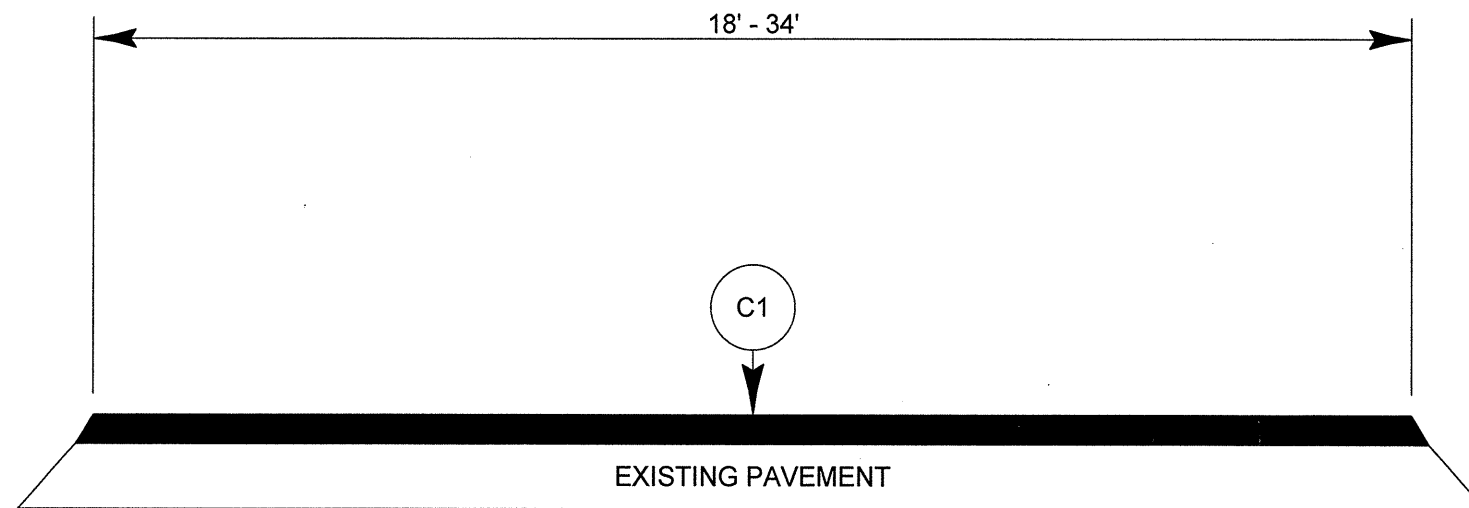


PAVEMENT SCHEDULE	
C1	PROP. APPROX. 1.5" ACSC, TYPE S9.5B, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YARD
Y	PROP. APPROX. 5/8" ULTRATHIN HOT MIX ASPHALT, TYPE B, AT AN AVERAGE RATE OF 70 LBS. PER SQ. YARD

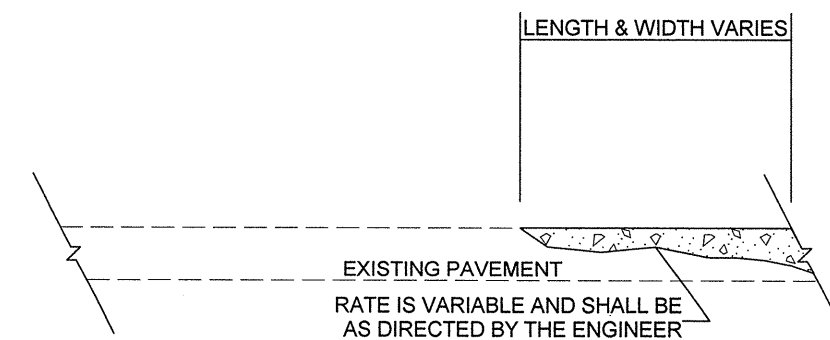
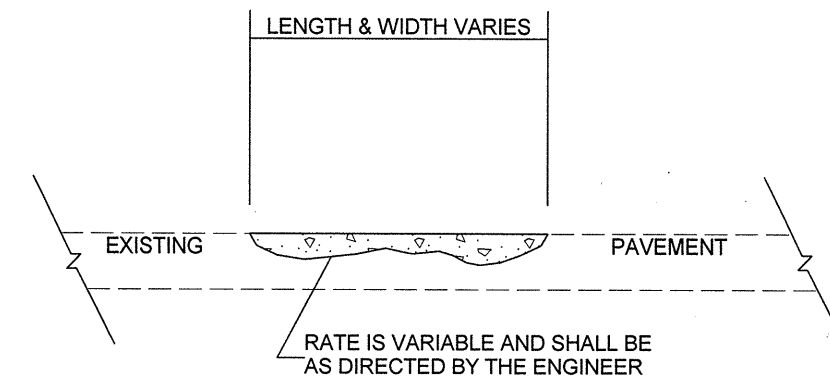
PROJECT NO. 13CR.10111.6, 13CR.20111.4	SHEET NO. 5	TOTAL SHEETS
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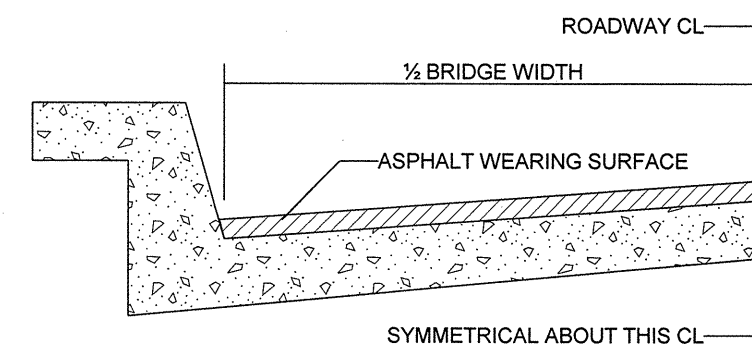
TYPICAL SECTION NO. 4



TYPICAL SECTION NO. 5



ASPHALT CONCRETE SURFACE COURSE (LEVELING COURSE)



BRIDGE HALF TYPICAL SECTION

FOR BRIDGES WITH FLOOR DRAINS, CARE SHALL BE EXERCISED IN PLACING THE WEARING SURFACE AROUND FLOOR DRAINS SO AS NOT TO HINDER EFFECTIVE DRAINAGE. ALL DRAINS SHALL BE LEFT OPEN

THE PROPOSED WEARING SURFACE SHALL VARY IN THICKNESS AS NECESSARY TO PROVIDE A SMOOTH RIDING SURFACE. A THICKNESS OF NOT LESS THAN 5/8" SHALL BE PROVIDED. THE MAXIMUM THICKNESS SHALL PREFERABLY BE 1 1/2" UNLESS IT IS IMPRACTICAL TO PROVIDE A SMOOTH RIDING SURFACE OTHERWISE.

**NOTES**

ALL UNPAVED ROADS TO BE RESURFACED 50' FROM EDGE OF PAVEMENT OF MAIN PROJECT.  
 ALL PAVED S.R. ROADS TO BE RESURFACED TO THE ENDS OF THE RADII, OR AS DIRECTED BY THE ENGINEER.  
 EDGES, PAVEMENT WIDENING, INTERSECTIONS AND BRIDGE FLARES ARE INCLUDED IN THE TABLE OF QUANTITIES.  
 SHOULDERS AND DITCHES ARE TO BE CONSTRUCTED BY OTHERS UNLESS OTHERWISE INDICATED.  
 BRIDGES ARE TO BE RESURFACED AT LOCATIONS AND TO DEPTH AS DIRECTED BY THE ENGINEER.

PROJECT NO.	SHEET NO.	TOTAL NO.
13CR.10111.6, 13CR.20111.4	6	

## SUMMARY OF QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LENGTH MI	WIDTH FT	INCIDENTAL STONE BASE TONS	1 1/2" MILLING SY	0" TO 1.5" MILLING SY	SURFACE COURSE, S9.5B TONS	LEVELING COURSE, SF9.5A TON	PG 64-22 PLANT MIX TONS	PATCHING EXISTING PAVEMENT TONS	ULTRATHIN HOT MIX ASPHALT, TYPE B TON	APPLICATION OF ULTRATHIN HOT MIX ASPHALT SY	PG 70-28 PLANT MIX TON	WHEELCHAIR RAMPS EA	DROP INLET EA	MANHOLES EA	METER OR VALVE BOX EA	
13CR.10111.6	Buncombe	1	US 19-23 BYPASS	CONCRETE PAVEMENT TO US 19-23 BUSINESS	1	1.34	84					30	2	100	2,313	66,035	134	5				
		2	US 19-23	US 19-23 BUSINESS TO OLD HAYWOOD RD	2	1.16	48			16333	3430		206	12				55	10	30	3	
		3	US 25	COLEMAN STREET TO SR 2230	3	1.1	48							50	1,085	30,976	63					
		4	US 19-23 BUSINESS	SR 3412 TO US 19-23 BYPASS	4	0.8	38			17835	1874		112	15								
<b>TOTAL FOR PROJ NO. 13CR.10111.6</b>						<b>4.4</b>				<b>17835</b>	<b>16333</b>	<b>5304</b>	<b>30</b>	<b>320</b>	<b>177</b>	<b>3,398</b>	<b>97,011</b>	<b>197</b>	<b>158</b>	<b>10</b>	<b>30</b>	<b>3</b>
13CR.20111.4	Buncombe	5	SR 1622	SR 1623 TO SR 1641	5	1.4	20	70			1866		112	45								
		6	SR 3413	NC 191 TO SR 3412	5	1.1	22	55			1924		115	100						13	1	
		7	SR 3412	NC 112 TO ASHEVILLE CITY LIMITS	5	2.5	22	60			3273		196	10						10	1	
		8	SR 2435	BLACK MT. CITY LIMITS TO US 70	5	1.75	20	85			2074		124	45						2	8	
		9	SR 1741	SR 1755 TO SR 1740	5	1.9	19	95			3015		181	400								
		10	SR 1319	SR 1315 TO SR 1324	5	0.6	18	40			700		42	10								
		11	SR 3556	US 25 TO SR 3556 (AMBOY)	5	1.5	34	75			2775		166	10						4	1	
		12	SR 2155	NC 197 TO SR 2153	5	1.2	19	60			1796		108	12								
<b>TOTAL FOR PROJ NO. 13CR.20111.4</b>						<b>11.95</b>		<b>540</b>	<b>0</b>	<b>0</b>	<b>17423</b>	<b>0</b>	<b>1044</b>	<b>632</b>						<b>29</b>	<b>11</b>	
<b>GRAND TOTAL</b>						<b>16.35</b>		<b>540</b>	<b>17835</b>	<b>16333</b>	<b>22727</b>	<b>30</b>	<b>1364</b>	<b>809</b>	<b>3,398</b>	<b>97,011</b>	<b>197</b>	<b>158</b>	<b>10</b>	<b>59</b>	<b>14</b>	

## THERMOPLASTIC AND PAINT QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	4685000000-E		4686000000-E		4697000000-E	4710000000-E	4721000000-E	4725000000-E			4810000000-E		4835000000-E	4840000000-N		
					4" X 90 M YELLOW THERMO LF	4" X 90 M WHITE THERMO LF	4" X 120 M YELLOW THERMO LF	4" X 120 M WHITE THERMO LF	8" X 120 M WHITE THERMO LF	24" X 120 M WHITE THERMO LF	THERMO MSG ONLY 120 M EA	THERMO LT ARROW 90 M EA	THERMO STR ARROW 90 M EA	THERMO STR & LT ARROW 90 M EA	THERMO RT ARROW 90 M EA	4" WHITE PAINT LF	4" YELLOW PAINT LF	24" WHITE PAINT LF	PAINT MSG SCHOOL EA	
13CR.10111.6	Buncombe	1	US 19-23 BYPASS	CONCRETE PAVEMENT TO US 19-23 BUSINESS	14,150	14,150		7,075	900	400	24	54								
		2	US 19-23	US 19-23 BUSINESS TO OLD HAYWOOD RD		12,250	12,250	3,062				3	11							
		3	US 25	COLEMAN STREET TO SR 2230		440	11,616	2,904	2,000		8	25								
		4	US 19-23 BUSINESS	SR 3412 TO US 19-23 BYPASS			8,448		100	300	3		3	3			100	12		
<b>TOTAL FOR PROJ NO. 13CR.10111.6</b>					<b>14,150</b>	<b>26,840</b>	<b>32,314</b>	<b>13,042</b>	<b>3,000</b>	<b>700</b>	<b>32</b>	<b>85</b>	<b>11</b>	<b>3</b>	<b>3</b>		<b>100</b>	<b>12</b>		
					<b>40,990</b>		<b>45,355</b>						<b>102</b>							
13CR.20111.4	Buncombe	5	SR 1622	SR 1623 TO SR 1641												30,128	30,128			
		6	SR 3413	NC 191 TO SR 3412												23,232	23,232			
		7	SR 3412	NC 112 TO ASHEVILLE CITY LIMITS												52,800	52,800			
		8	SR 2435	BLACK MT. CITY LIMITS TO US 70												36,960	36,960			
		9	SR 1741	SR 1755 TO SR 1740												40,128	40,128			
		10	SR 1319	SR 1315 TO SR 1324												12,672	12,672			
		11	SR 3556	US 25 TO SR 3556 (AMBOY)												31,680	31,680			
		12	SR 2155	NC 197 TO SR 2153												25,344	25,344			
<b>TOTAL FOR PROJ NO. 13CR.20111.4</b>																<b>252,944</b>	<b>252,944</b>			
																<b>505,888</b>				
<b>GRAND TOTAL</b>					<b>14,150</b>	<b>26,840</b>	<b>32,314</b>	<b>13,042</b>	<b>3,000</b>	<b>700</b>	<b>32</b>	<b>85</b>	<b>11</b>	<b>3</b>	<b>3</b>		<b>252,944</b>	<b>252,944</b>	<b>100</b>	<b>12</b>
					<b>40,990</b>		<b>45,355</b>						<b>102</b>			<b>505,888</b>				

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ENGLISH DETAIL DRAWING FOR  
**WHEELCHAIR RAMP AND EXISTING SIDEWALK  
WITH GRASS STRIP**  
CURB CUT

STATE OF NORTH CAROLINA  
DEPT. OF TRANSPORTATION  
DIVISION OF HIGHWAYS  
RALEIGH, N.C.

SHEET 1 OF 5  
**848D06**

**ISOMETRIC VIEW**

NOTE: 1. DETECTABLE WARNING DOMES SHALL COVER 9'-0" LENGTH AND FULL WIDTH OF THE RAMP. 2. FULL WIDTH CONTRAST VISIBILITY WITH ADJOINING SURFACE, EITHER LIGHT-ON-DARK, OR DARK-ON-LIGHT SEQUENCE COVERING THE ENTIRE RAMP.

**DETECTABLE WARNING DOMES**

BASE DIAMETER 0.45" TO 0.70"  
TOP DIAMETER OF NO. LESS THAN 45% OF THE BASE DIAMETER  
PRECAST CLASS "B" CONCRETE  
0.8" TO 1.5" TO 2.7"

**SECTION B-B**

RETROFITTED SIDEWALK  
WHEELCHAIR RAMP  
2" DETECTABLE WARNING DOMES  
9'-0" MAX. RAMP  
12:1

**SECTION A-A**

EXISTING SIDEWALK  
EXPANSION JOINT (SEE STD. 848.01)  
3'-4" MIN. CURB  
6'-0" SIDEWALK TRANSITION ON GRASS STRIP  
12:1  
VAR. LENGTH TO 12'-8" WITH VARIABLE SLOPE  
DETECTABLE WARNING DOMES  
TOP OF CURB  
4"

**PLAN VIEW (40" MIN. FLOOR WIDTH)**

EXPANSION JOINT (SEE STD. 848.01)  
EXISTING SIDEWALK  
11 GRASS STRIP  
12:1  
RETROFITTED SIDEWALK (IF NEEDED)  
PAY LIMITS  
EXPANSION JOINT (SEE STD. 848.01)  
EXISTING SIDEWALK  
NOT LESS THAN 2' FULL HEIGHT CURB BETWEEN RAMP  
THIS PORTION OF RAMP MUST FALL WITHIN CURB WALK LIMITS (SEE NOTE 10)  
PLAN VIEW (60" MIN. FLOOR WIDTH)  
DIAGONAL RAMP  
40" MIN. FLOOR WIDTH

ENGLISH DETAIL DRAWING FOR  
**WHEELCHAIR RAMP AND EXISTING SIDEWALK  
ADJACENT TO CURB**  
CURB CUT

STATE OF NORTH CAROLINA  
DEPT. OF TRANSPORTATION  
DIVISION OF HIGHWAYS  
RALEIGH, N.C.

SHEET 2 OF 5  
**848D06**

**ISOMETRIC VIEW**

NOTE: 1. DETECTABLE WARNING DOMES SHALL COVER 2'-0" LENGTH AND FULL WIDTH OF THE RAMP FLOOR AS BROWN ON LIGHT SURFACE. 2. OBTAIN TOP CONTRAST VISIBILITY WITH ADJOINING SURFACE, EITHER LIGHT-ON-DARK, OR DARK-ON-LIGHT SEQUENCE COVERING THE ENTIRE RAMP.

**DETECTABLE WARNING DOMES**

BASE DIAMETER 0.45" TO 0.70"  
TOP DIAMETER OF NO. LESS THAN 50% OF THE BASE DIAMETER  
PRECAST CLASS "B" CONCRETE  
0.8" TO 1.5" TO 2.7"

**SECTION B-B**

RETROFITTED WHEELCHAIR RAMP AND EXISTING SIDEWALK  
2" DETECTABLE WARNING DOMES  
DROP CURB (SEE NOTE 10)  
9'-0" MAX. RAMP  
12:1 MAX. RAMP  
12:1

**SECTION A-A**

EXISTING SIDEWALK  
EXPANSION JOINT (SEE STD. 848.01)  
3'-4" MIN. CURB  
6'-0" SIDEWALK TRANSITION ON GRASS STRIP  
12:1  
VAR. LENGTH TO 12'-8" WITH VARIABLE SLOPE  
DETECTABLE WARNING DOMES  
TOP OF CURB  
4"

**PLAN VIEW (40" MIN. FLOOR WIDTH)**

EXPANSION JOINT (SEE STD. 848.01)  
EXISTING SIDEWALK  
11 GRASS STRIP  
12:1  
RETROFITTED SIDEWALK (IF NEEDED)  
PAY LIMITS  
EXPANSION JOINT (SEE STD. 848.01)  
EXISTING SIDEWALK  
NOT LESS THAN 2' FULL HEIGHT CURB BETWEEN RAMP  
THIS PORTION OF RAMP MUST FALL WITHIN CURB WALK LIMITS (SEE NOTE 10)  
PLAN VIEW (60" MIN. FLOOR WIDTH)  
DIAGONAL RAMP  
40" MIN. FLOOR WIDTH

PROJECT SERVICES UNIT  
STANDARDS AND SPECIAL DESIGN  
Office 919-250-4128 FAX 919-250-4119

SEE PLATE FOR TITLE

ORIGINAL BY: DETAIL 848D06 DATE: \_\_\_\_\_  
 MODIFIED BY: E.E. WARD DATE: 09-06-05  
 CHECKED BY: \_\_\_\_\_ DATE: \_\_\_\_\_  
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 DEPT. OF TRANSPORTATION  
 DIVISION OF HIGHWAYS  
 RALEIGH, N.C.

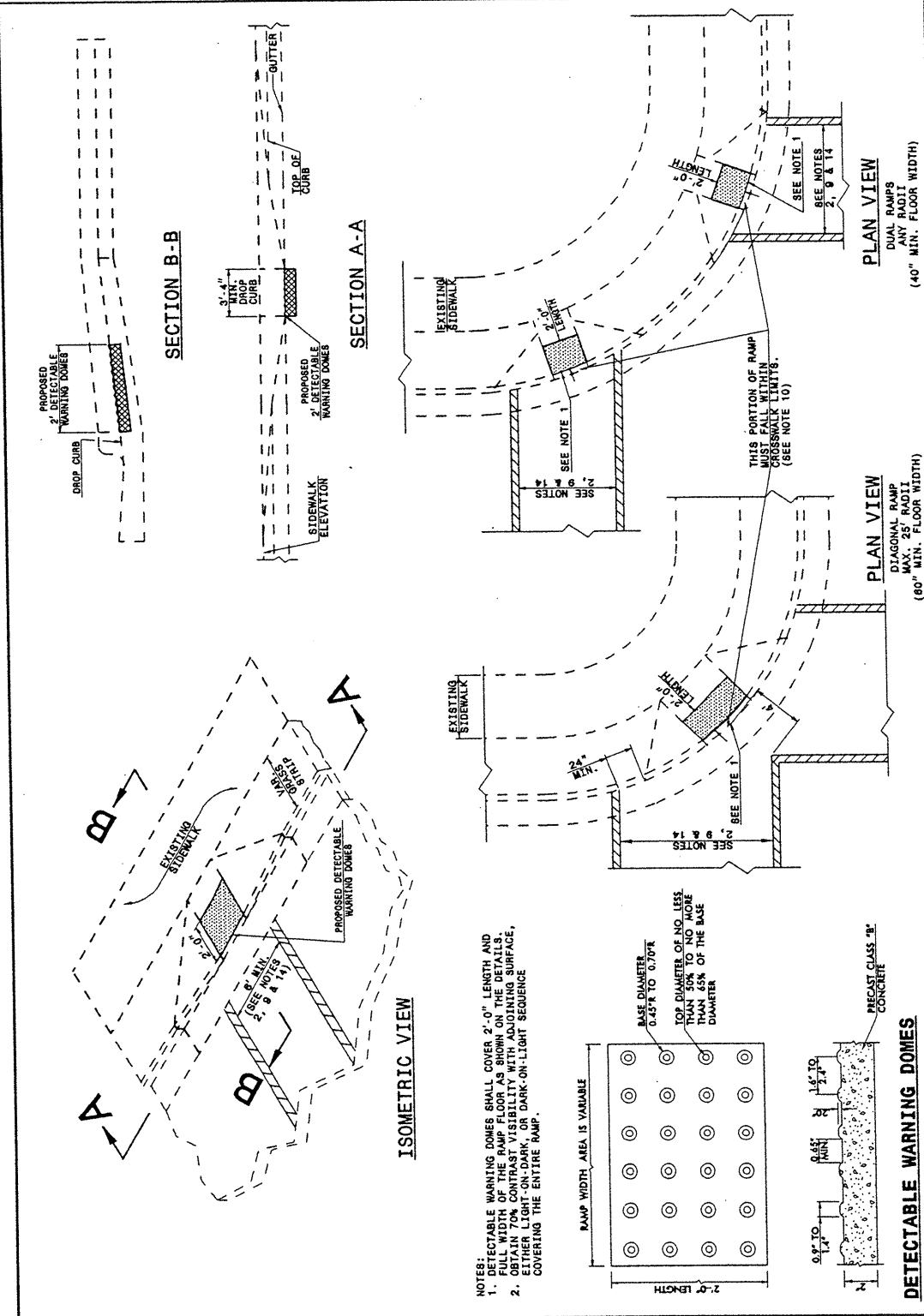
ENGLISH DETAIL DRAWING FOR  
**RETROFITTING DETECTABLE WARNING DOMES**  
 ONTO EXISTING WHEELCHAIR RAMP  
 CURB CUT

SHEET 3 OF 5  
**848D06**

STATE OF  
 NORTH CAROLINA  
 DEPT. OF TRANSPORTATION  
 DIVISION OF HIGHWAYS  
 RALEIGH, N.C.

ENGLISH DETAIL DRAWING FOR  
**RETROFITTING DETECTABLE WARNING DOMES**  
 ONTO EXISTING WHEELCHAIR RAMP  
 CURB CUT

SHEET 3 OF 5  
**848D06**



STATE OF  
 NORTH CAROLINA  
 DEPT. OF TRANSPORTATION  
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 RALEIGH, N.C.

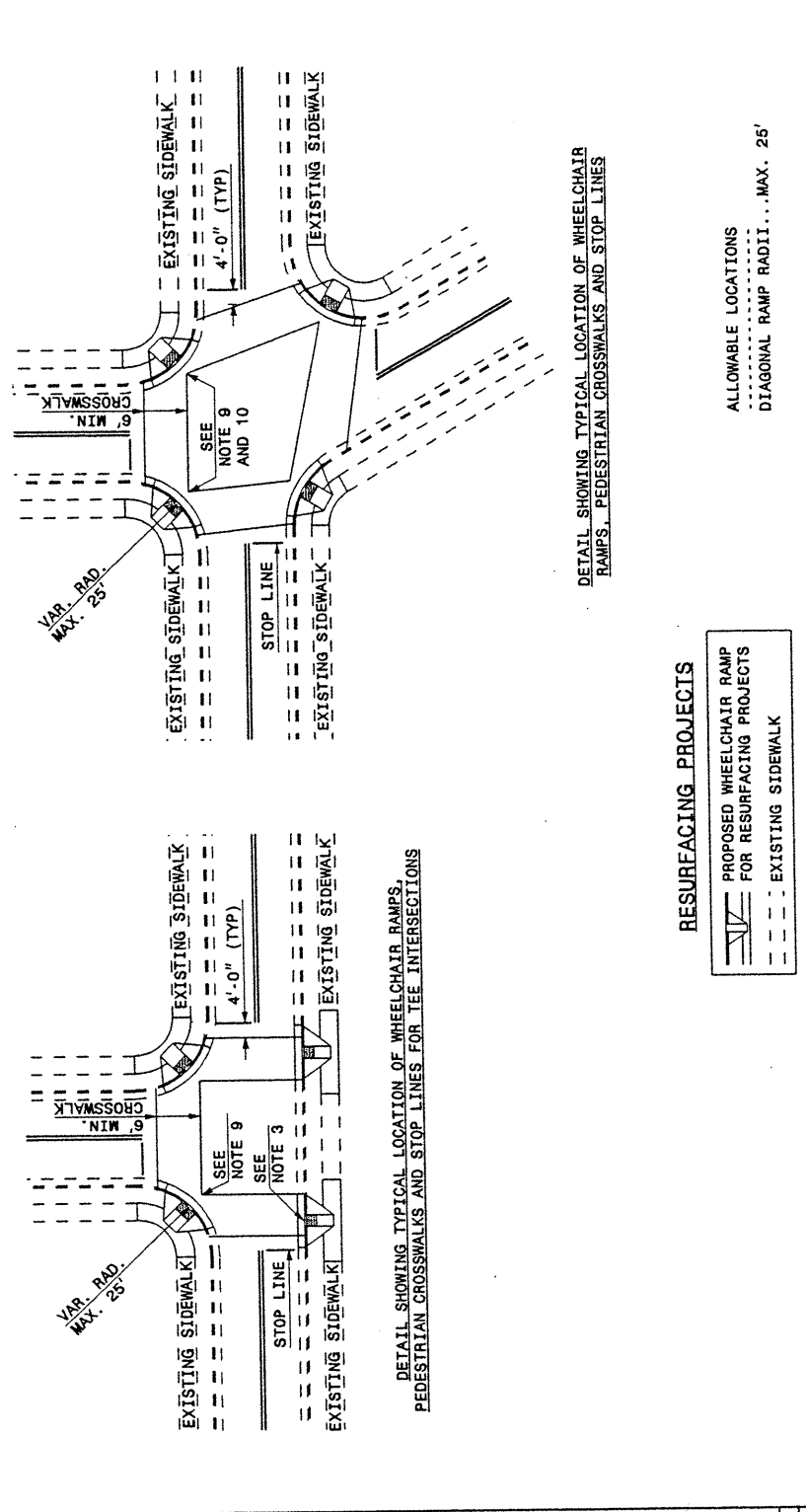
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**WHEELCHAIR RAMP**  
 CURB CUT

SHEET 4 OF 5  
**848D06**

STATE OF  
 NORTH CAROLINA  
 DEPT. OF TRANSPORTATION  
 DIVISION OF HIGHWAYS  
 RALEIGH, N.C.

ENGLISH DETAIL DRAWING FOR  
**WHEELCHAIR RAMP**  
 CURB CUT

SHEET 4 OF 5  
**848D06**



PROJECT SERVICES UNIT  
 STANDARDS AND SPECIAL DESIGN  
 Office 919-250-4128 FAX 919-250-4119

**SEE PLATE FOR TITLE**

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STATE OF  
NORTH CAROLINA  
DEPT. OF TRANSPORTATION  
DIVISION OF HIGHWAYS  
RALEIGH, N.C.

ENGLISH DETAIL DRAWING FOR  
**WHEELCHAIR RAMP AND EXISTING SIDEWALK**  
CURB CUT

SHEET 5 OF 5  
**848D06**

NOTES:

1. CONSTRUCT THE WALKING SURFACE WITH SLIP RESISTANCE AND A 70% CONTRASTING COLOR TO THE SIDEWALK.
2. CROSSWALK WIDTHS AND CONFIGURATION VARY, BUT MUST CONFORM TO TRAFFIC DESIGN STANDARDS.
3. NORTH CAROLINA GENERAL STATUTE 136-44.14 REQUIRES THAT ALL STREET CURBS BEING CONSTRUCTED OR RECONSTRUCTED FOR MAINTENANCE PROCEDURES, TRAFFIC OPERATIONS, REPAIRS, CORRECTION OF UTILITIES OR ALTERED FOR ANY REASON AFTER SEPTEMBER 1, 1973 SHALL PROVIDE WHEELCHAIR RAMPS FOR THE PHYSICALLY DISABLED AT ALL INTERSECTIONS WHERE BOTH CURB AND GUTTER AND SIDEWALKS ARE PROVIDED AND AT OTHER POINTS OF PEDESTRIAN FLOW.  
IN ADDITION, SECTION 228 OF THE 1973 FEDERAL AID HIGHWAY SAFETY ACT REQUIRES PROVISION OF CURB RAMPS ON ANY CURB CONSTRUCTION AFTER JULY 1, 1976 WHETHER A SIDEWALK IS PROPOSED INITIALLY OR IS PLANNED FOR A FUTURE DATE.  
THE AMERICANS WITH DISABILITIES ACT (ADA) OF 1990 EXTENDS TO INDIVIDUALS WITH DISABILITIES, COMPREHENSIVE CIVIL RIGHTS PROTECTIONS SIMILAR TO THOSE PROVIDED TO PERSONS ON THE BASIS OF RACE, SEX, NATIONAL ORIGIN AND RELIGION UNDER THE CIVIL RIGHTS ACT OF 1964. THESE CURB RAMPS HAVE BEEN DESIGNED TO COMPLY WITH THE CURRENT ADA STANDARDS.
4. PROVIDE WHEELCHAIR RAMPS AT LOCATIONS AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER. LOCATE WHEELCHAIR RAMPS AS DIRECTED BY THE ENGINEER WHERE EXISTING LIGHT POLES, FIRE HYDRANTS, DROP INLETS, ETC. AFFECT PLACEMENT. WHERE TWO RAMPS ARE INSTALLED PLACE NOT LESS THAN 2 FEET OF FULL HEIGHT CURB BETWEEN THE RAMPS. PLACE DUAL RAMPS AS NEAR PERPENDICULAR TO THE TRAVEL LANE BEING CROSSED AS POSSIBLE.
5. DO NOT EXCEED 0.08 (12:1) SLOPE ON THE WHEELCHAIR RAMP IN RELATIONSHIP TO THE GRADE OF THE STREET.
6. CONSTRUCT WHEELCHAIR RAMPS 40" (3'-4") OR GREATER FOR DUAL RAMPS AND 60" (5'-0") OR GREATER FOR DIAGONAL RAMPS.
7. USE CLASS "B" CONCRETE WITH A SIDEWALK FINISH IN ORDER TO OBTAIN A ROUGH NON-SKID TYPE SURFACE.
8. PLACE A 1/2" EXPANSION JOINT WHERE THE CONCRETE WHEELCHAIR RAMP JOINS THE CURB AND AS SHOWN ON STD. DWG. 848.01.
9. PLACE THE INSIDE PEDESTRIAN CROSSWALK LINES NO CLOSER IN THE INTERSECTION BY BISECTING THE INTERSECTION RADIUS, WITH ALLOWANCE OF A 4' CLEAR ZONE IN THE VEHICULAR TRAVELWAY WHEN ONE RAMP IS INSTALLED. (SEE NOTE 14)
10. COORDINATE THE CURB CUT AND THE PEDESTRIAN CROSSWALK LINES SO THE FLOOR OF THE WHEELCHAIR RAMP WILL FALL WITHIN THE PEDESTRIAN CROSSWALK LINES. PLACE DIAGONAL RAMPS WITH FLARED SIDES SO 24" OF FULL HEIGHT CURB FALLS WITHIN THE CROSSWALK MARKINGS ON EACH SIDE OF THE FLARES.
11. CONSTRUCT THE PEDESTRIAN CROSSWALK A MINIMUM OF 6 FEET. A CROSSWALK WIDTH OF 10 FEET OR GREATER IS DESIRABLE.
12. USE STOP LINES, NORMALLY PERPENDICULAR TO THE LANE LINES, WHERE IT IS IMPORTANT TO INDICATE THE POINT BEHIND WHICH VEHICLES ARE REQUIRED TO STOP IN COMPLIANCE WITH A TRAFFIC SIGNAL, STOP SIGN OR OTHER LEGAL REQUIREMENT. AN UNUSUAL APPROACH SKEW MAY REQUIRE THE PLACEMENT OF THE STOP LINE TO BE PARALLEL TO THE INTERSECTING ROADWAY.
13. TERMINATE PARKING A MINIMUM OF 20 FEET BACK OF PEDESTRIAN CROSSWALK.
14. PLACE ALL PAVEMENT MARKINGS IN ACCORDANCE WITH THE LATEST EDITION OF THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) PUBLISHED BY THE FEDERAL HIGHWAY ADMINISTRATION AND THE NORTH CAROLINA SUPPLEMENT TO THE MUTCD.

STATE OF  
NORTH CAROLINA  
DEPT. OF TRANSPORTATION  
DIVISION OF HIGHWAYS  
RALEIGH, N.C.

ENGLISH DETAIL DRAWING FOR  
**WHEELCHAIR RAMP AND EXISTING SIDEWALK**  
CURB CUT

SHEET 5 OF 5  
**848D06**

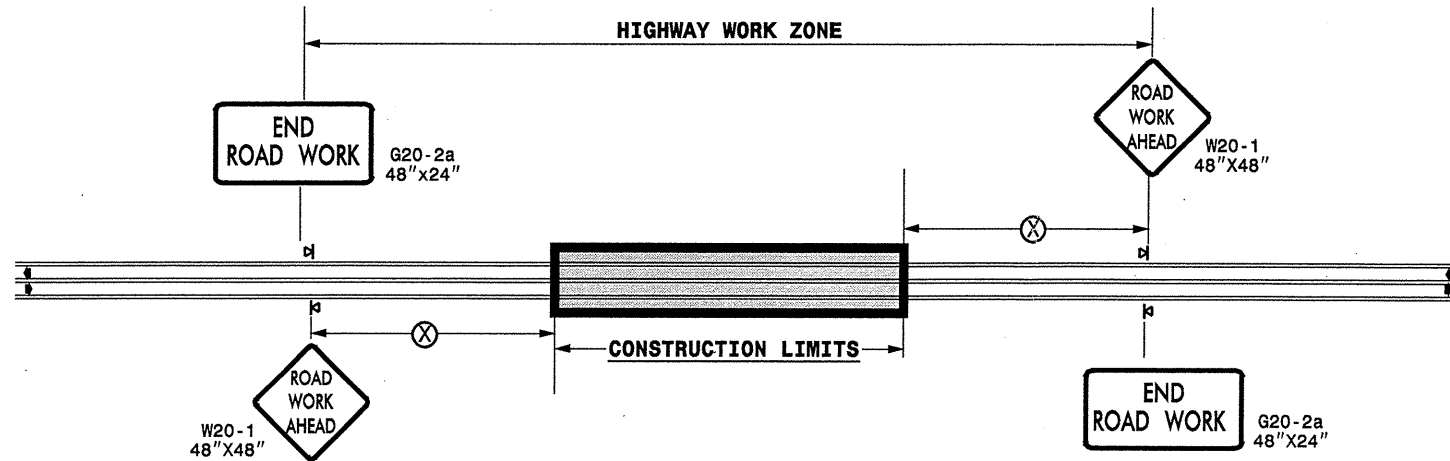
PROJECT SERVICES UNIT  
STANDARDS AND SPECIAL DESIGN  
Office 919-250-4128 FAX 919-250-4119

**SEE PLATE FOR TITLE**

ORIGINAL BY: DETAIL 848D05 DATE: \_\_\_\_\_  
MODIFIED BY: E.E. WARD DATE: 09-06-05  
CHECKED BY: \_\_\_\_\_ DATE: \_\_\_\_\_  
FILE SPEC.: /usr/stds/02/detail/english/84806/848d06.dgn

PROJECT REFERENCE NO. 13 CR. 2011.6  
SHEET NO. 9  
13 CR. 2011.4

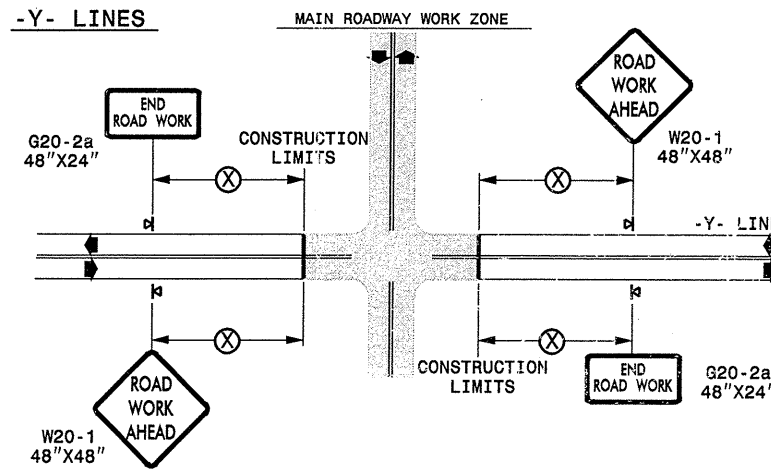
**TWO-WAY UNDIVIDED \*\* (L-LINES)**



POSTED SPEED LIMIT (M.P.H.)	RECOMMENDED MINIMUM SIGN SPACING
≤ 50	500'
≥ 55	1000'

STATE OF NORTH CAROLINA  
DEPT. OF TRANSPORTATION  
DIVISION OF HIGHWAYS  
RALEIGH, N.C.

**ROADWAYS INTERSECTING ALONG 2 WAY UNDIVIDED WORK ZONE (Y-LINES)**



DETAIL DRAWING  
FOR TWO-WAY UNDIVIDED  
WORK ZONE WARNING SIGNS

**GENERAL NOTES**

- USE FLUORESCENT ORANGE SHEETING (TYPE VII OR HIGHER) ON ALL ADVANCED WORK ZONE SIGNS.
- DO NOT INSTALL ADVANCE WARNING SIGNS MORE THAN 3 DAYS PRIOR TO BEGINNING OF WORK.
- ALL SIGN SPACING DIMENSIONS ARE APPROXIMATE, FIELD ADJUST AS NECESSARY OR AS DIRECTED.
- USE PORTABLE WORK ZONE SIGNS ONLY WITH PORTABLE WORK ZONE SIGN STANDS SPECIFICALLY DESIGNED FOR ONE ANOTHER. PORTABLE WORK ZONE SIGNS MAY BE ROLL UP OR APPROVED COMPOSITE.
- PROVIDE PORTABLE WORK ZONE SIGN STANDS, PORTABLE SIGNS AND SIGN SHEETING WHICH ARE LISTED ON THE NORTH CAROLINA DEPARTMENT OF TRANSPORTATION'S APPROVED PRODUCT LIST OR ACCEPTED AS TRAFFIC QUALIFIED BY THE TRAFFIC CONTROL UNIT.
- \*\* TWO-WAY UNDIVIDED ADVANCE WARNING SIGN CONFIGURATION MAY BE USED ON URBAN MULTI-LANE FACILITIES WHERE CONDITIONS LIMIT THE USE OF DUAL MOUNTED SIGNS AS DETERMINED BY THE ENGINEER.

**LEGEND**

◀ PORTABLE SIGN

➔ DIRECTION OF TRAFFIC FLOW

SHEET 1 OF 1

APPROVED: _____	DATE: _____
SEAL	
DETAIL DRAWING FOR TWO-WAY UNDIVIDED ADVANCED WORK ZONE WARNING SIGNS	
SCALE: NONE	REVISIONS
DATE: _____	7-98 10/01
DWG. BY: _____	10-98 03/04
DESIGN BY: _____	01/01 11/04
REVIEWED BY: _____	DATE FILE

25-SEP-2006 16:29 R:\D:\DESIGN\GROUPS-WZTCCC\design\group4\common4\resurfacing\resurfacing2006\div13\3cr10111.6+cbuncombe\13cr10111.6-2wayundivurbfrwys\july2006.dgn  
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