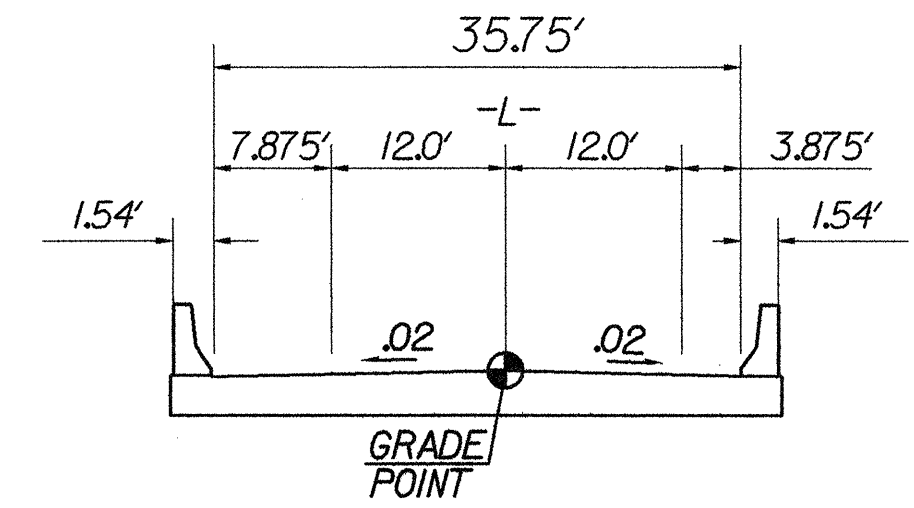
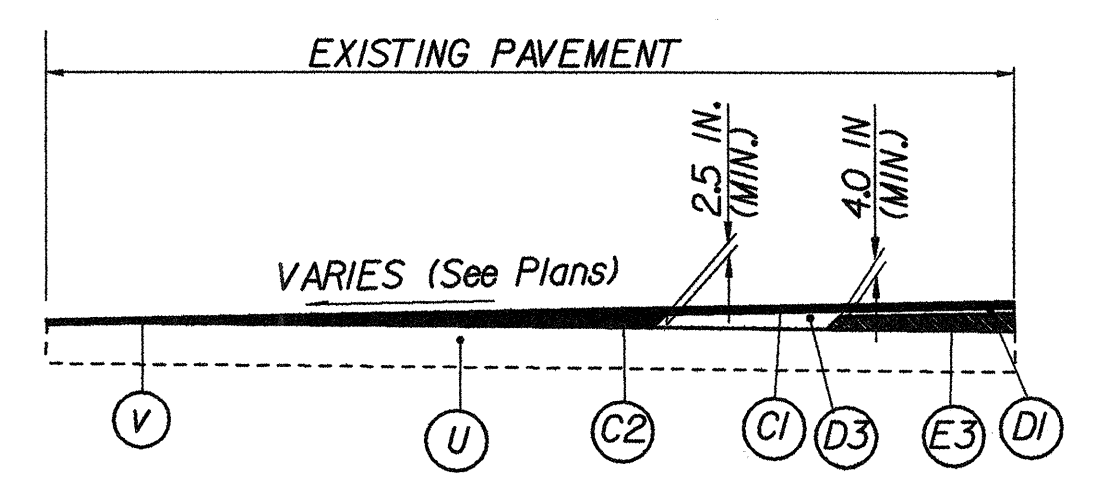


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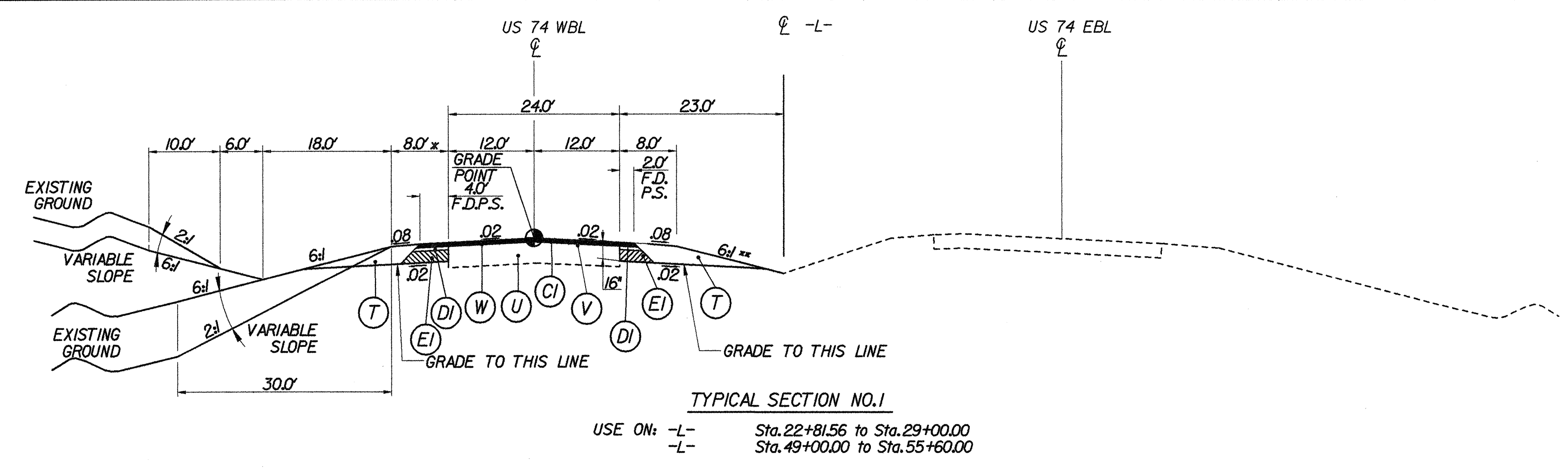
PROJECT REFERENCE NO. <b>B-4009</b>	SHEET NO. <b>2</b>
RW SHEET NO.	
ROADWAY DESIGN ENGINEER <i>[Signature]</i> 19026 10/10/05	PAVEMENT DESIGN ENGINEER <i>[Signature]</i> 22896 10/10/05
<small>333 Fayetteville Street, Suite 1400, Raleigh, N.C. 27601</small> <small>PAVEMENT MANAGEMENT UNIT</small> <small>1900 WALKER CENTER</small> <small>RALEIGH, NC 27601</small>	



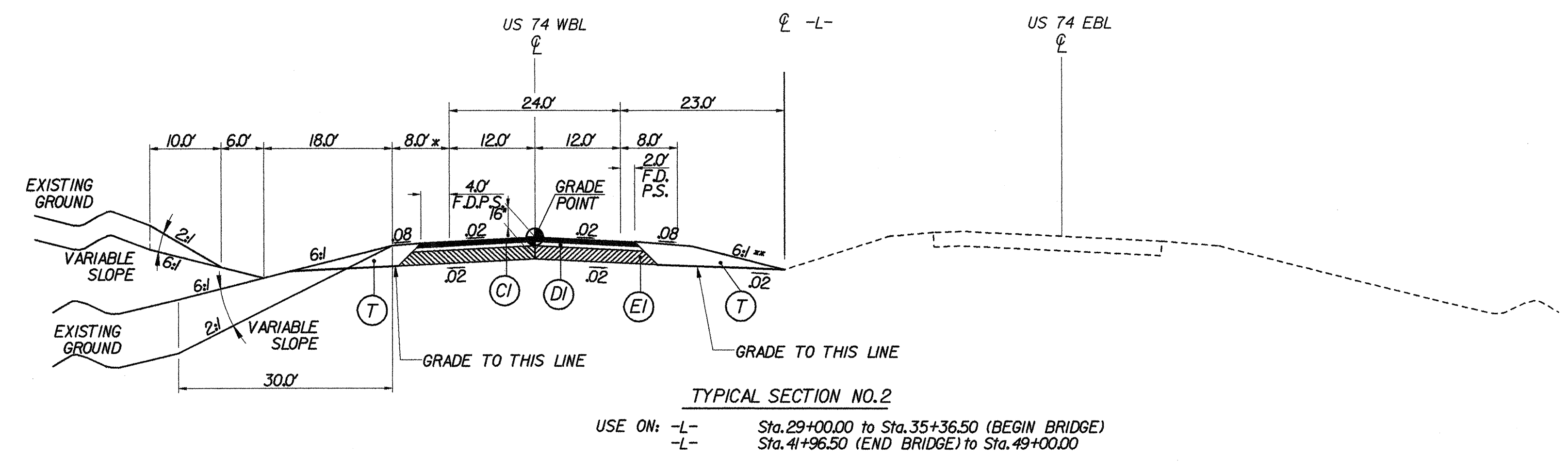
**TYPICAL SECTION ON STRUCTURE**  
 USE ON: -L- Sta. 35+65.50 +/- (BEGIN BRIDGE) to Sta. 41+96.50 +/- (END BRIDGE)



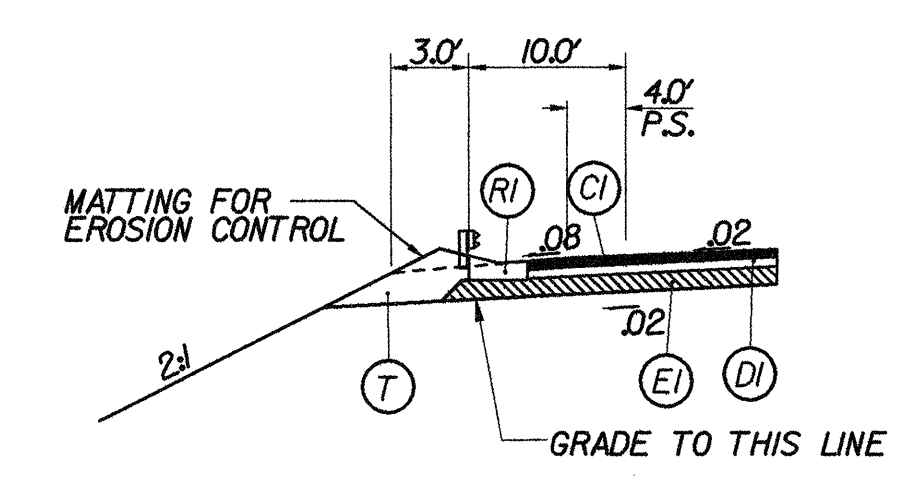
**WEDGING DETAIL**



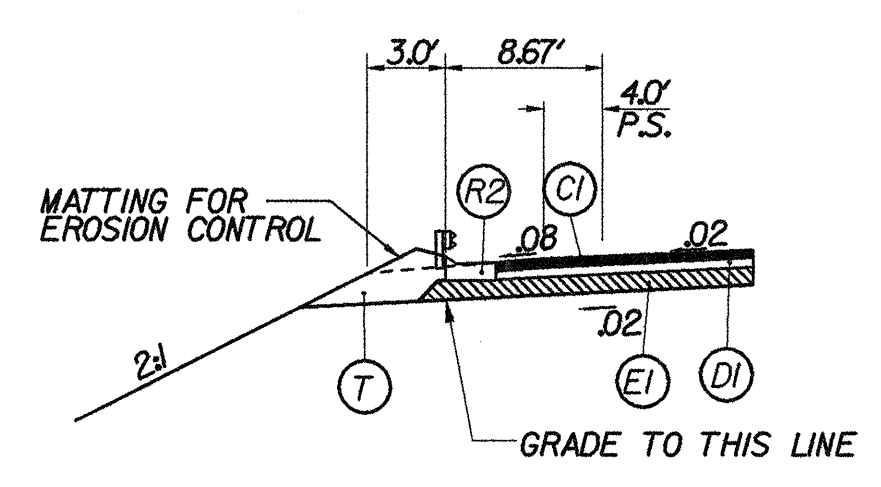
**TYPICAL SECTION NO. 1**  
 USE ON: -L- Sta. 22+81.56 to Sta. 29+00.00  
 -L- Sta. 49+00.00 to Sta. 55+60.00



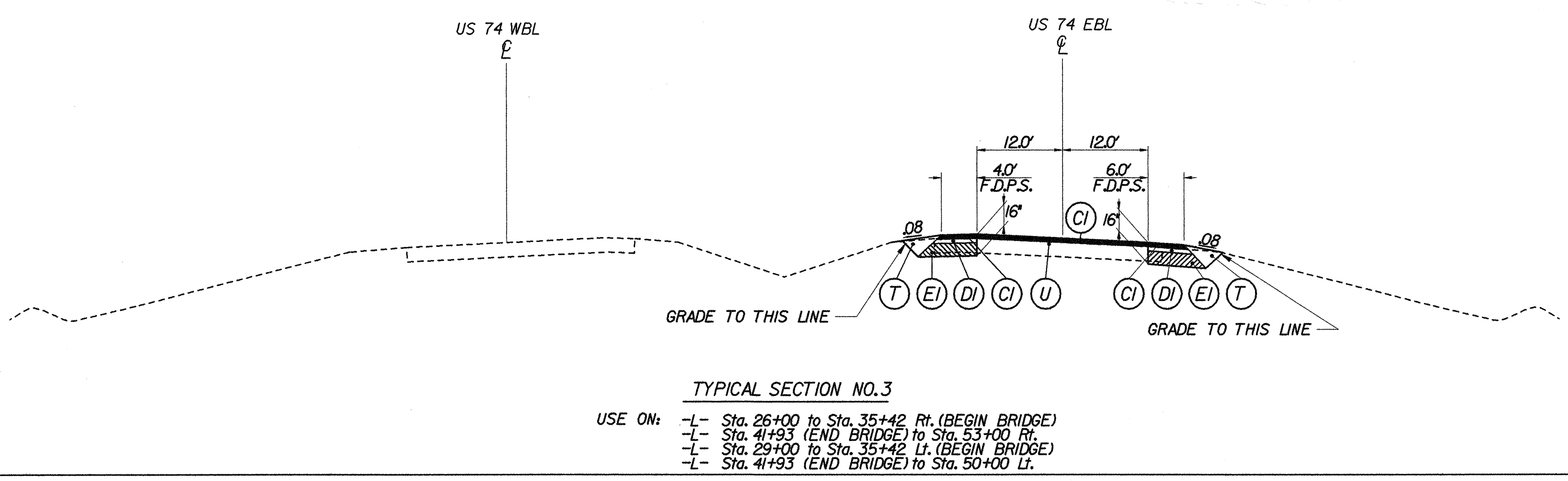
**TYPICAL SECTION NO. 2**  
 USE ON: -L- Sta. 29+00.00 to Sta. 35+36.50 (BEGIN BRIDGE)  
 -L- Sta. 41+96.50 (END BRIDGE) to Sta. 49+00.00



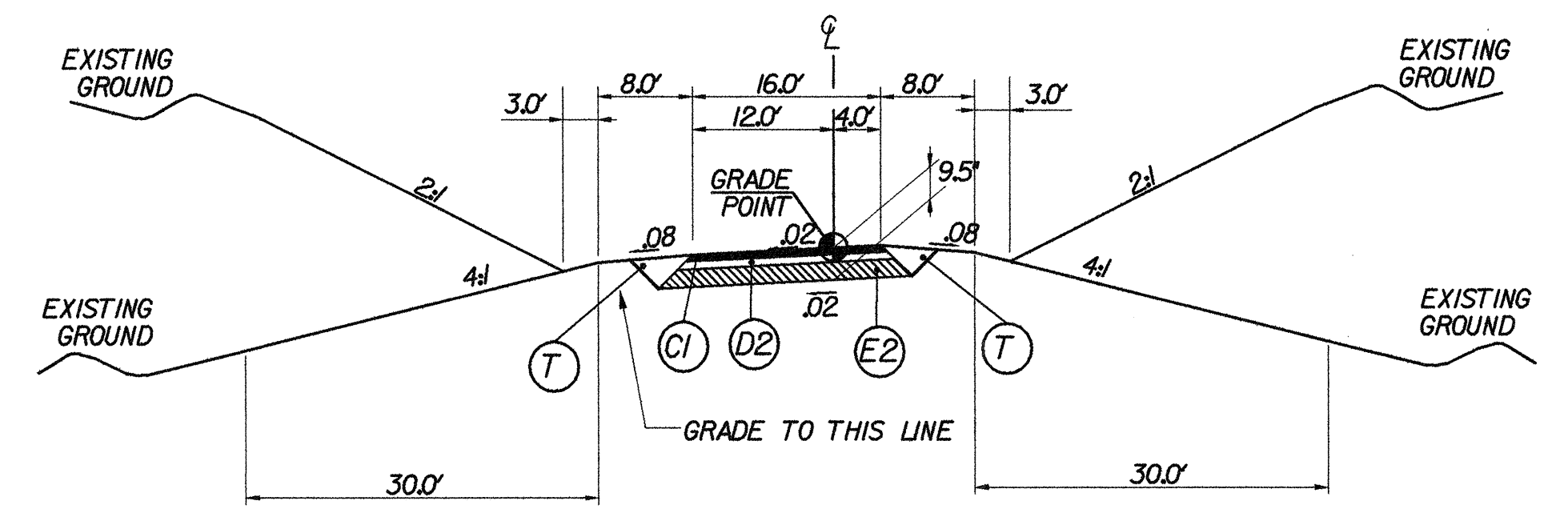
**TYPICAL SECTION NO. 2-A**  
 USE ON: -L- Sta. 45+50.00 to Sta. 52+00.00 LT.



**TYPICAL SECTION NO. 2-B**  
 USE ON: -L- Sta. 34+83.00 to Sta. 35+16.00 LT & RT  
 -L- Sta. 42+16.50 to Sta. 42+46.33 LT & RT



**TYPICAL SECTION NO. 3**  
 USE ON: -L- Sta. 26+00 to Sta. 35+42 Rt. (BEGIN BRIDGE)  
 -L- Sta. 41+93 (END BRIDGE) to Sta. 53+00 Rt.  
 -L- Sta. 29+00 to Sta. 35+42 Lt. (BEGIN BRIDGE)  
 -L- Sta. 41+93 (END BRIDGE) to Sta. 50+00 Lt.



**TYPICAL SECTION NO. 4**  
 USE ON: CROSSOVER DETOUR 1 Sta. 25+60.13 to Sta. 30+67.82  
 CROSSOVER DETOUR 2 Sta. 46+52.12 to Sta. 52+27.00

**PAVEMENT SCHEDULE**

ITEM	DESCRIPTION	ITEM	DESCRIPTION	ITEM	DESCRIPTION	NOTES
(C1)	PROP. APPROX. 3 IN. ASPHALT SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 168 LBS/SY IN EACH OF TWO LIFTS.	(E2)	PROP. APPROX. 4 IN. ASPHALT BASE COURSE, TYPE B25.0C, AT AN AVERAGE RATE OF 456 LBS/SY.	(W)	WEDGING	* INCREASE SHOULDER WIDTH 5 FEET WHEN GUARDRAIL IS USED ** VARY SLOPES TO PROVIDE POSITIVE DRAINAGE (MAX 4%)
(C2)	PROP. VAR. DEPTH ASPHALT CONC. SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 112 LBS/SY IN LIFTS NOT LESS THAN 1.5 IN. NOR GREATER THAN 2 IN.	(E3)	PROP. VAR. DEPTH ASPHALT BASE COURSE, TYPE B25.0C, AT AN AVERAGE RATE OF 114 LBS/SY IN LIFTS NOT LESS THAN 3 IN. NOR GREATER THAN 5.5 IN.	(V)	INCIDENTAL MILLING	
(D1)	PROP. APPROX. 3 IN. ASPHALT INT. COURSE, TYPE I19.0C, AT AN AVERAGE RATE OF 342 LBS/SY.	(R1)	EXPRESSWAY GUTTER			
(D2)	PROP. APPROX. 2.5 IN. ASPHALT INT. COURSE, TYPE I19.0C, AT AN AVERAGE RATE OF 285 LBS/SY.	(R2)	SHOULDER BERM GUTTER			
(D3)	PROP. VAR. DEPTH ASPHALT CONC. INT. COURSE, TYPE I19.0C, AT AN AVERAGE RATE OF 114 LBS/SY IN LIFTS NOT LESS THAN 2.5 IN. NOR GREATER THAN 4 IN.	(T)	EARTH MATERIAL			
(E1)	PROP. APPROX. 10 IN. ASPHALT BASE COURSE, TYPE B25.0C, AT AN AVERAGE RATE OF 570 LBS/SY IN EACH OF TWO LIFTS.	(U)	EXISTING PAVEMENT			

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 TIME: 10:53 PM  
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REVISIONS