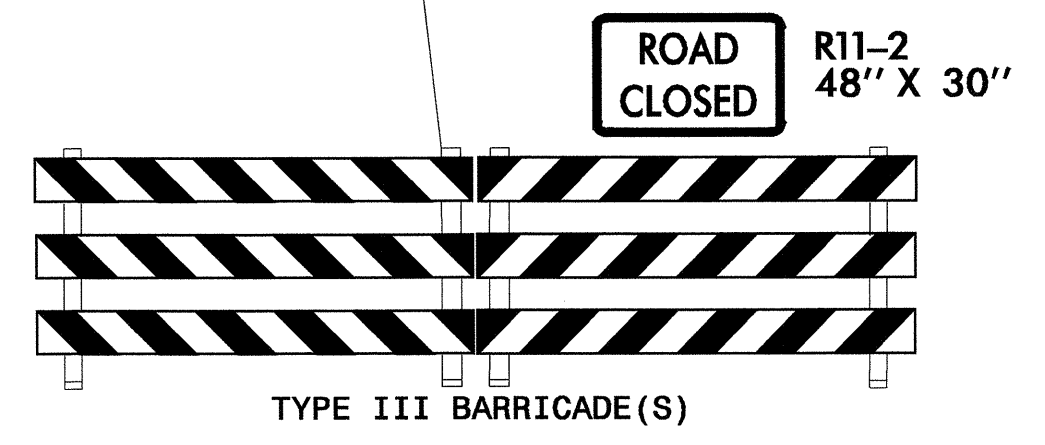
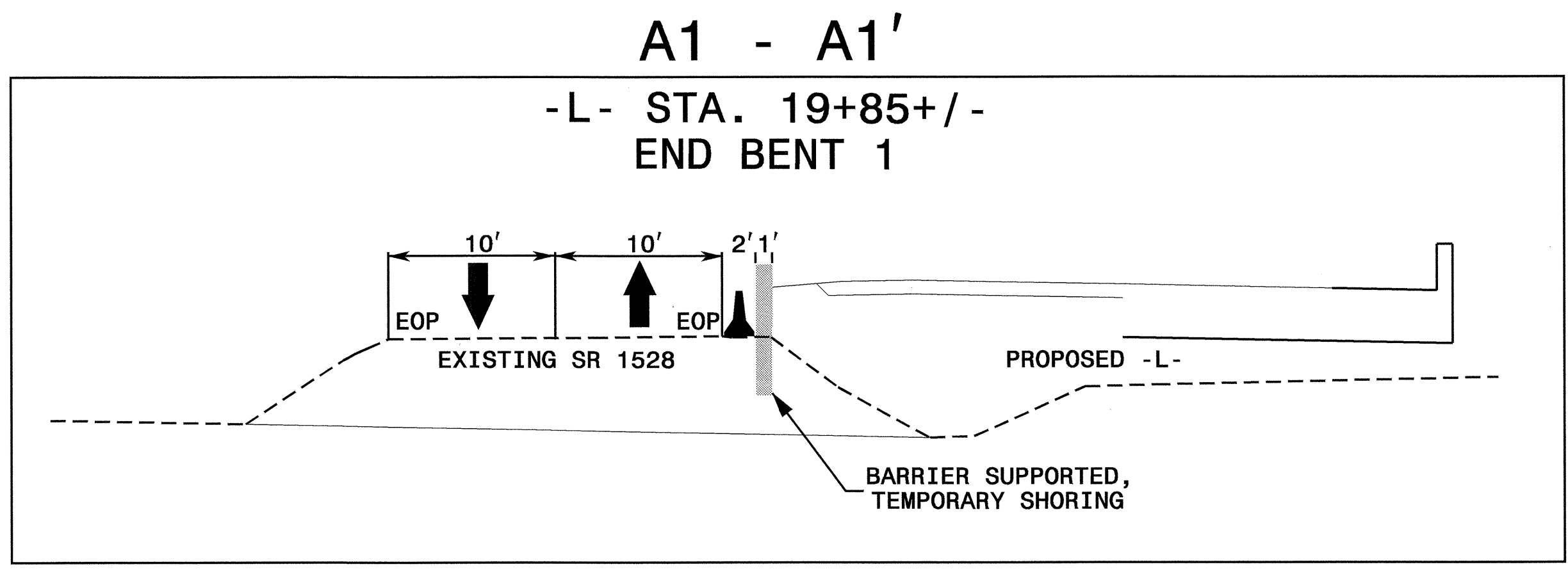
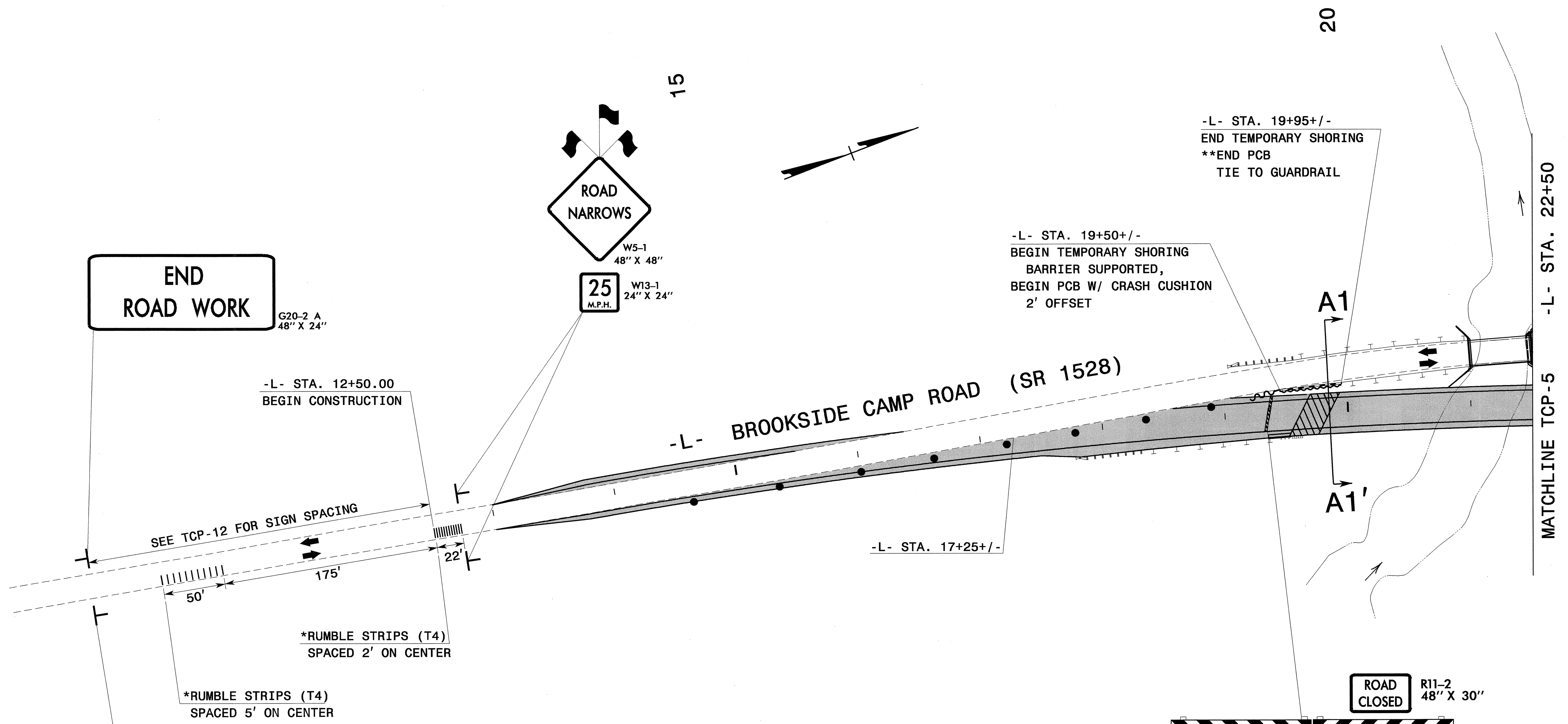


FOR DESIGN OF TEMPORARY SHORING AT END BENT 1, USE THE FOLLOWING SOIL PARAMETERS:

UNIT WEIGHT OF SOIL ABOVE WATER TABLE,  $\gamma = 110$  pcf  
 UNIT WEIGHT OF SOIL BELOW WATER TABLE,  $\gamma' = 50$  pcf  
 FRICTION ANGLE,  $\phi = 30^\circ$   
 COHESION,  $c = 0$  psf

STANDARD SHORING IS ALLOWED.

- NOTES**
- \* ALL RUMBLE STRIPS SHALL BE CENTERED IN THE LANE AND SHALL BE 2 FEET LESS THAN THE WIDTH OF THE TRAVEL LANE.
  - \* RUMBLE STRIPS SHALL BE PLACED USING 4", 240 MIL WHITE THERMOPLASTIC MARKING LINES.
  - \*\* SEE ROADWAY PROPOSAL FOR PCB/GUARDRAIL TRANSITION DETAIL DRAWING, PAY ITEMS, AND QUANTITIES.



APPROVED: <i>Joseph Isak</i> DATE: 3/8/05	<b>PHASE I DETAIL 1</b>									
SCALE: NONE	DATE: 3-8-05	<table border="1"> <thead> <tr> <th colspan="2">REVISIONS</th> </tr> <tr> <th>NO.</th> <th>DESCRIPTION</th> </tr> </thead> <tbody> <tr> <td> </td> <td> </td> </tr> <tr> <td> </td> <td> </td> </tr> </tbody> </table>	REVISIONS		NO.	DESCRIPTION				
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REVIEWED BY: BLW										

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