

NOTES

STIRRUPS IN CAP MAY BE SHIFTED AS NECESSARY TO CLEAR DOWELS.

HOOKS ON V1 BARS MAY BE TURNED AS NECESSARY FOR PLACING REINFORCING STEEL.

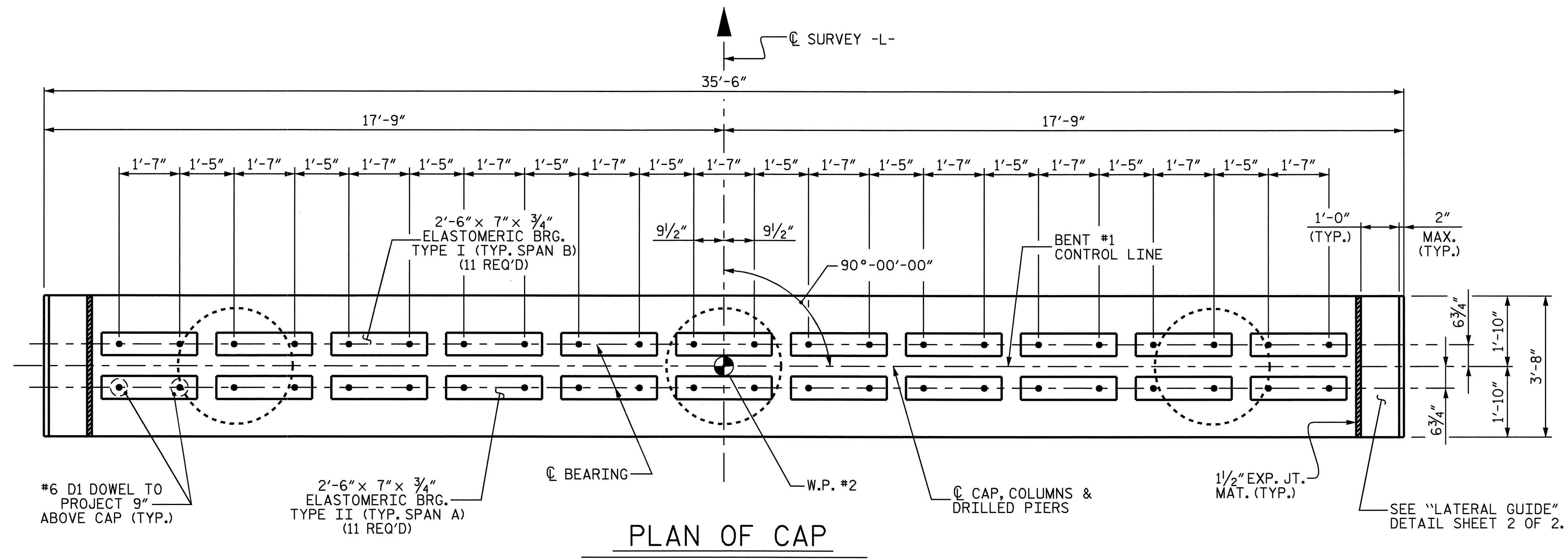
FOR DRILLED PIERS, SEE SPECIAL PROVISIONS.

THE LOCATION OF THE CONSTRUCTION JOINT IN THE DRILLED PIERS IS BASED ON AN APPROXIMATE GROUND LINE ELEVATION. IF THE CONSTRUCTION JOINT IS ABOVE THE ACTUAL GROUND ELEVATION, THE CONTRACTOR SHALL PLACE THE CONSTRUCTION JOINT 1 FT. BELOW THE GROUND LINE.

NO SEPARATE PAYMENT SHALL BE MADE FOR ANY ADDITIONAL STEEL REQUIRED IN THE CONSTRUCTION OF THE DRILLED PIER AS THIS IS CONSIDERED INCIDENTAL TO THE LINEAR FOOT PRICE FOR DRILLED PIER.

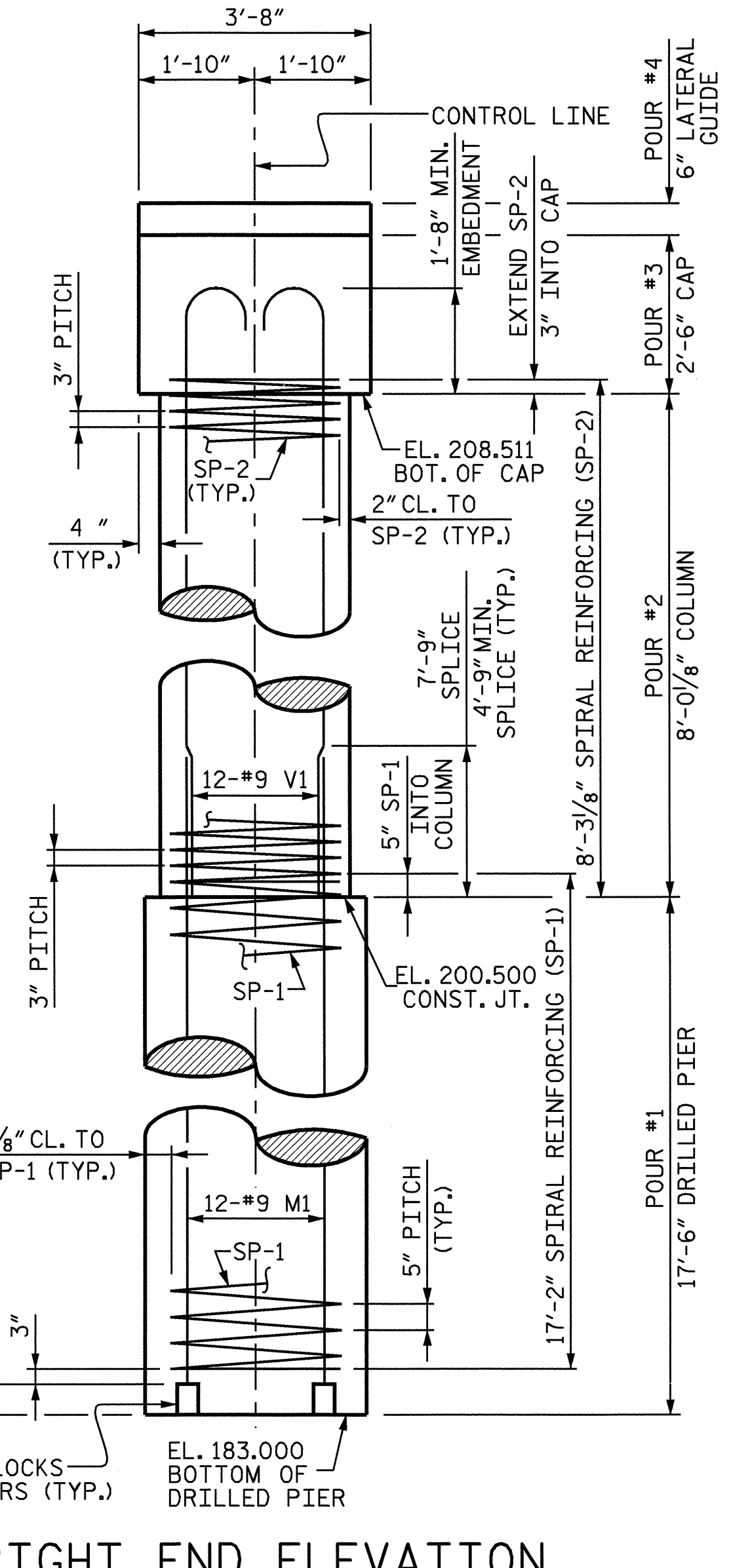
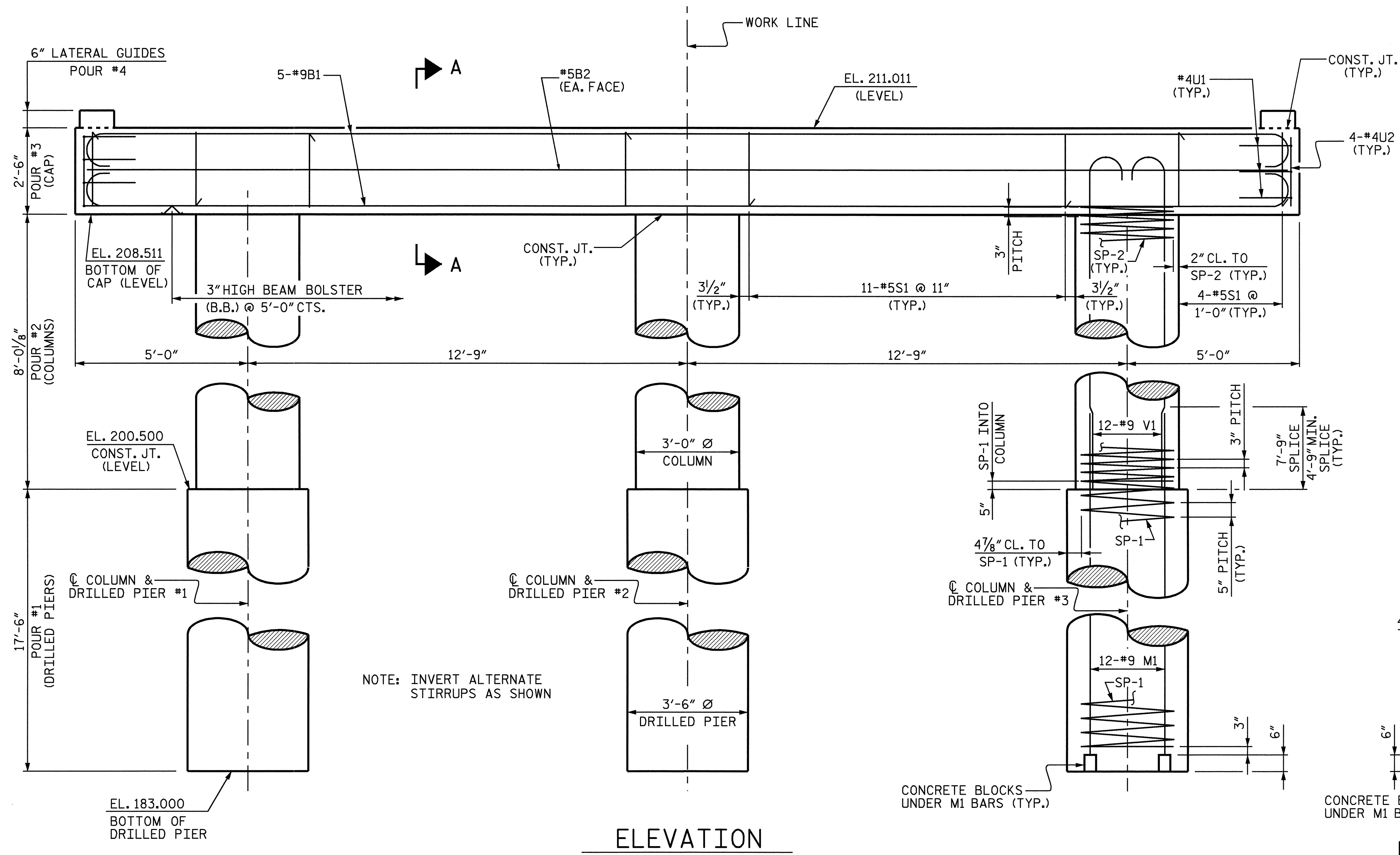
THE CONTRACTOR'S ATTENTION IS CALLED TO THE FACT THAT THE LONGITUDINAL REINFORCEMENT FOR THE DRILLED PIERS IS DETAILED WITH 3 FEET OF EXTRA LENGTH.

FOR PERMANENT STEEL CASING, SEE SPECIAL PROVISION FOR DRILLED PIERS.



SPAN B

SPAN A

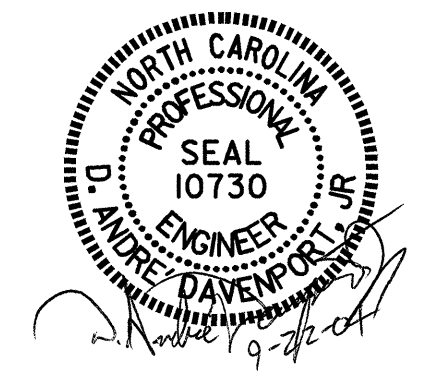


ELEVATION

RIGHT END ELEVATION

NOTE: INVERT ALTERNATE STIRRUPS AS SHOWN

ALL COLUMNS AND DRILLED PIERS ARE IDENTICAL



PROJECT NO. B-4515
FRANKLIN COUNTY
 STATION: 15+37.00 -L-
 SHEET 1 OF 2

STATE OF NORTH CAROLINA DEPARTMENT OF TRANSPORTATION RALEIGH					
SUBSTRUCTURE BENT #1					
REVISIONS					
NO.	BY:	DATE:	NO.	BY:	DATE:
1			3		
2			4		
					SHEET NO. S - 14
					TOTAL SHEETS 24

DRAWN BY: S. P. LAM DATE: 06-25-04
 CHECKED BY: H. T. BARBOUR DATE: 07-04