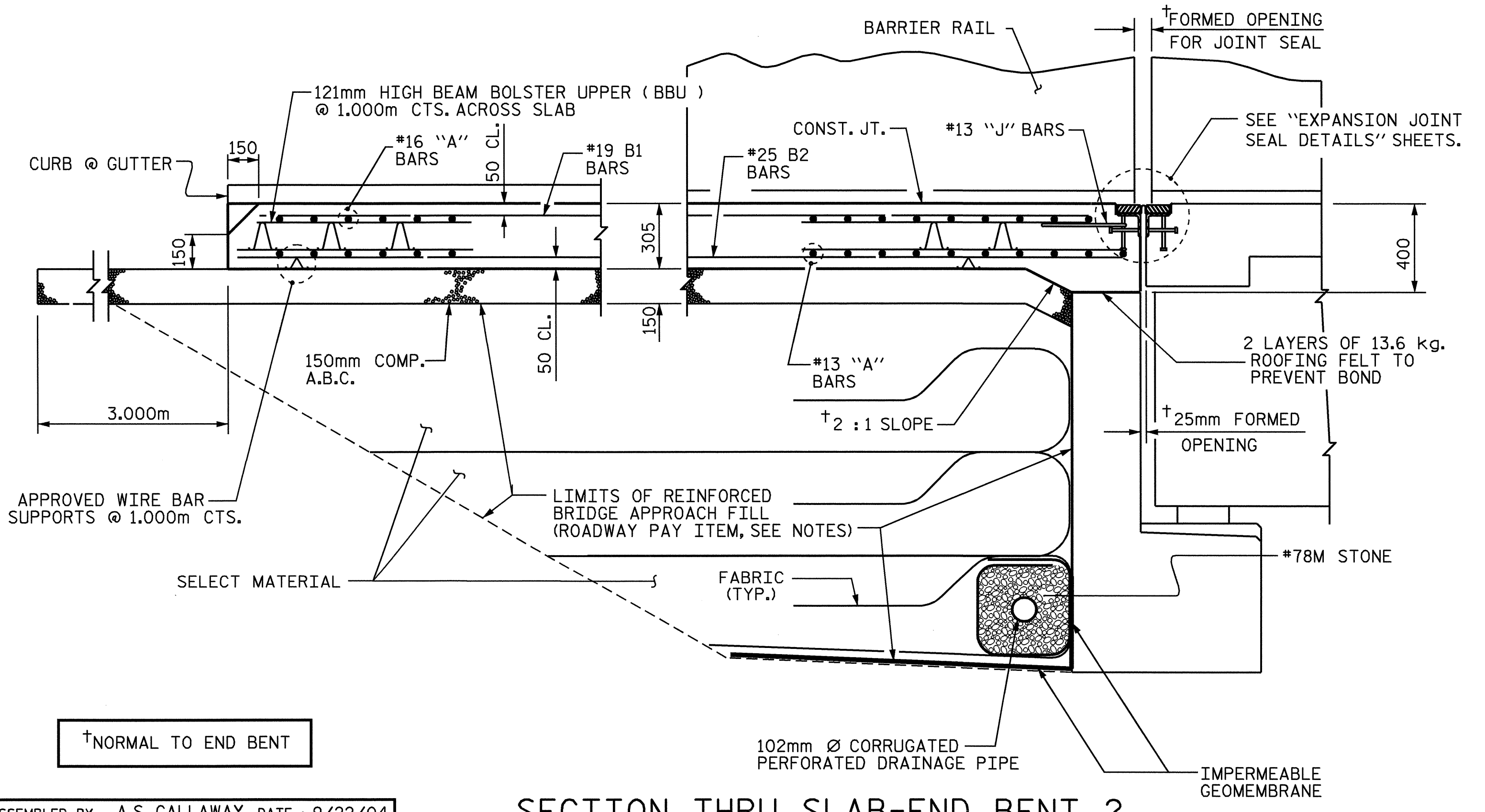


SECTION THRU SLAB-END BENT 1



SECTION THRU SLAB-END BENT 2

NOTES

THE COST OF THE BARRIER RAIL AND PARAPET/END POST ON THE APPROACH SLAB SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE BID FOR BRIDGE APPROACH SLABS.

APPROACH SLAB SHALL NOT BE CONSTRUCTED PRIOR TO COMPLETION OF THE BRIDGE DECK.

FOR REINFORCED BRIDGE APPROACH FILL INCLUDING FABRIC, IMPERMEABLE GEOMEMBRANE, 102mm Ø DRAINAGE PIPE, #78M STONE, AND SELECT MATERIAL, SEE ROADWAY PLANS.

TEMPORARY DRAINAGE AND TEMPORARY BERM AND SLOPE DRAINS WILL BE PAID FOR UNDER THE LUMP SUM PRICE FOR BRIDGE APPROACH SLAB.

AREA BETWEEN THE WINGWALL AND APPROACH SLAB SHALL BE GRADED TO DRAIN THE WATER AWAY FROM THE FILL FACE OF THE BRIDGE.

THE 150mm COMP. A.B.C. SHALL EXTEND 3m BEYOND THE END OF THE APPROACH SLAB AND 300mm OUTSIDE OF EACH EDGE OF THE SLAB.

THE CONTRACTOR MAY USE 100mm TYPE B-25.0B ASPHALT CONCRETE COURSE IN LIEU OF 150mm COMP. A.B.C. IF THIS OPTION IS USED, THE BASE COURSE SHALL EXTEND 300mm BEYOND THE END OF THE APPROACH SLAB AND THE WIDTH SHALL BE THE SAME AS THAT OF THE APPROACH SLAB.

THE CONTRACTOR MAY USE 125mm CLASS "A" CONCRETE BASE IN LIEU OF 150mm COMP. A.B.C. IF THIS OPTION IS USED, THE CONCRETE BASE SHALL EXTEND 300mm BEYOND THE END OF THE APPROACH SLAB AND THE WIDTH SHALL BE THE SAME AS THAT OF THE APPROACH SLAB. THE CONCRETE SHALL BE FINISHED TO A SMOOTH SURFACE AND A LAYER OF 13.6 kg. ROOFING FELT SHALL BE PLACED BETWEEN THE CONCRETE BASE AND THE APPROACH SLAB TO PREVENT BOND. THE APPROACH SLAB SHALL NOT BE CAST UNTIL THE CONCRETE BASE HAS REACHED AN AGE OF THREE CURING DAYS.

THE JOINT AT END BENT 1 SHALL BE SAWED PRIOR TO THE CASTING OF THE BARRIER RAIL, PARAPET AND END POST.

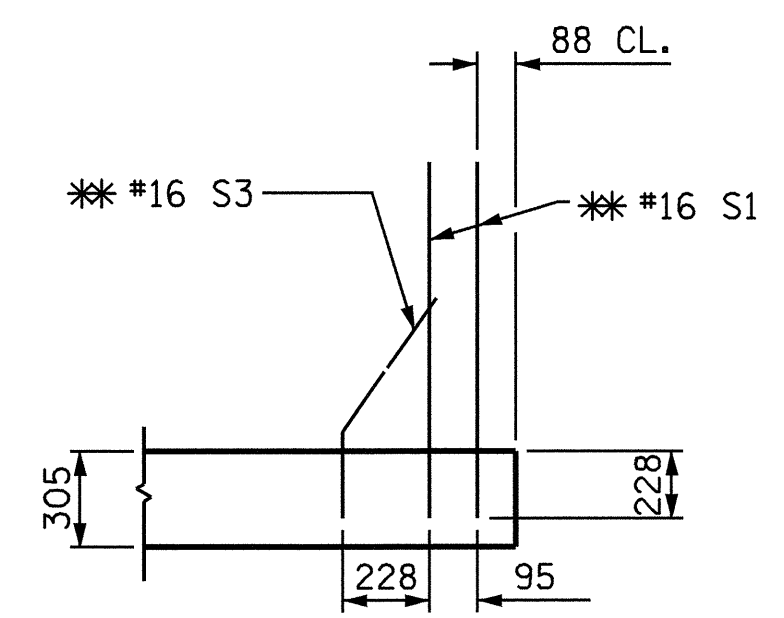
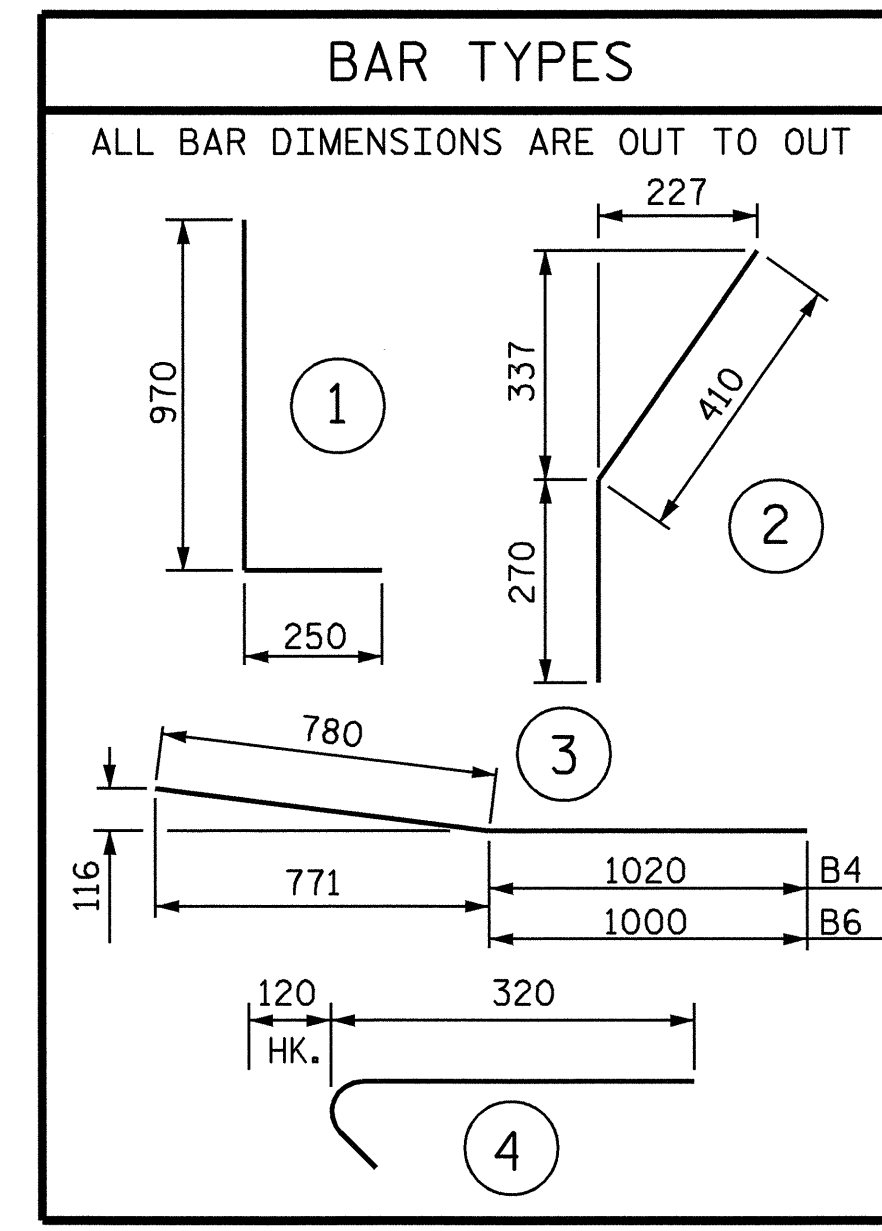
FOR EVAZOTE JOINT SEALS, SEE SPECIAL PROVISIONS.

THE NORMAL UNCOMPRESSED SEAL WIDTH OF THE EVAZOTE JOINT SEAL SHALL BE 87mm.

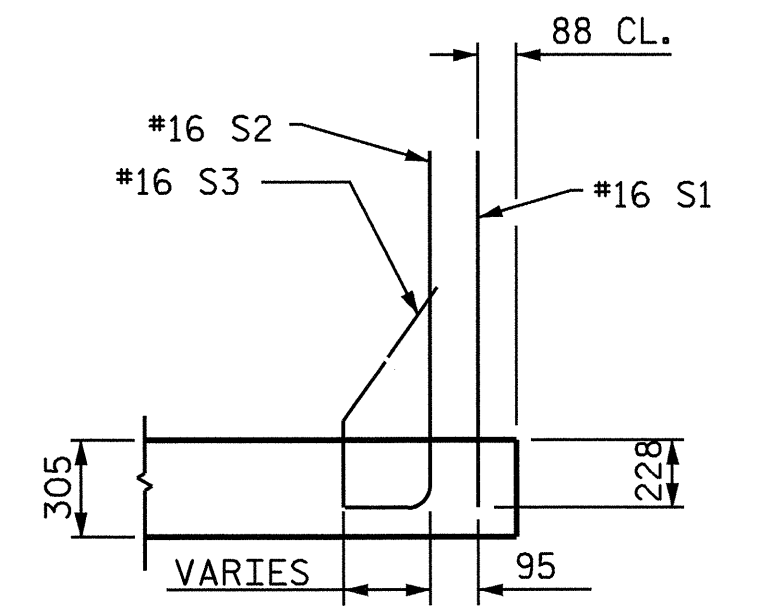
FOR EXPANSION JOINT SEALS, SEE SPECIAL PROVISIONS.

BILL OF MATERIAL

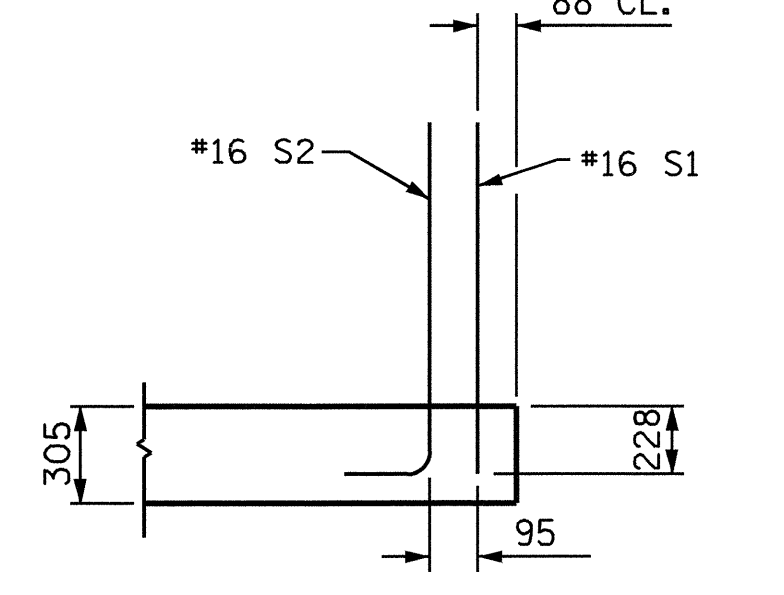
| FOR TWO APPROACH SLABS | | | | | |
|----------------------------------|-----|------|------|--------|------------|
| BAR | NO. | SIZE | TYPE | LENGTH | WEIGHT |
| *A1 | 25 | #16 | STR | 14180 | 550 |
| *A2 | 17 | #16 | STR | 14260 | 376 |
| *A3 | 7 | #16 | STR | 14500 | 158 |
| A4 | 84 | #13 | STR | 7420 | 620 |
| A5 | 16 | #13 | STR | 7540 | 120 |
| *A7 | 25 | #16 | STR | 14880 | 577 |
| *A8 | 17 | #16 | STR | 14960 | 395 |
| *A9 | 7 | #16 | STR | 15200 | 165 |
| A10 | 84 | #13 | STR | 7760 | 648 |
| A11 | 16 | #13 | STR | 7880 | 125 |
| *B1 | 162 | #19 | STR | 7200 | 2606 |
| B2 | 162 | #25 | STR | 7500 | 4828 |
| *B3 | 7 | #16 | STR | 3460 | 38 |
| *B4 | 1 | #16 | 3 | 1800 | 3 |
| *B5 | 7 | #16 | STR | 3400 | 37 |
| *B6 | 1 | #16 | 3 | 1780 | 3 |
| *B7 | 8 | #16 | STR | 1060 | 14 |
| *B8 | 8 | #16 | STR | 1240 | 16 |
| *E1 | 4 | #22 | STR | 780 | 10 |
| *E2 | 4 | #22 | STR | 920 | 12 |
| *E3 | 4 | #22 | STR | 1060 | 12 |
| *E4 | 4 | #22 | STR | 1220 | 14 |
| *E5 | 4 | #22 | STR | 1320 | 16 |
| *F1 | 2 | #19 | STR | 580 | 2 |
| *F2 | 2 | #19 | STR | 740 | 4 |
| *F3 | 2 | #19 | STR | 900 | 4 |
| *F4 | 2 | #19 | STR | 1080 | 4 |
| *F5 | 4 | #19 | STR | 980 | 8 |
| *J1 | 52 | #13 | 4 | 440 | 23 |
| *S1 | 54 | #16 | STR | 980 | 82 |
| *S2 | 46 | #16 | 1 | 1220 | 87 |
| *S3 | 20 | #16 | 2 | 680 | 22 |
| *S4 | 16 | #16 | STR | 920 | 22 |
| REINFORCING STEEL | | | | | 6341 kg. |
| * EPOXY COATED REINFORCING STEEL | | | | | 5260 kg. |
| CLASS AA CONCRETE BREAKDOWN | | | | | |
| POUR 1 SLAB & CURB | | | | | 57.8 C. M. |
| POUR 2 RAIL & PARAPET | | | | | 2.6 C. M. |
| CLASS AA CONCRETE | | | | | 60.4 C. M. |



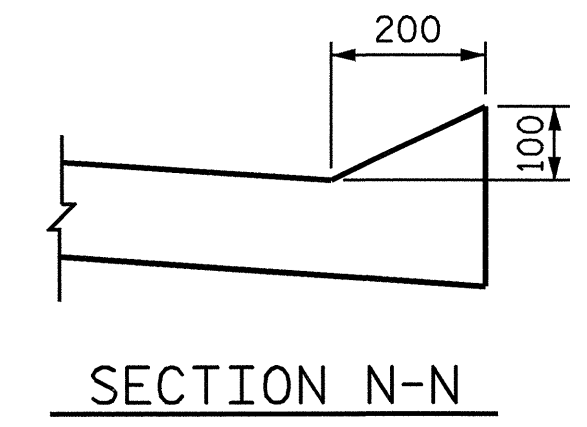
SECTION K-K
* ADHESIVELY ANCHORED



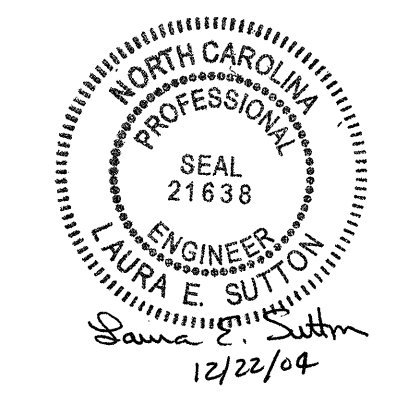
SECTION L-L



SECTION M-M



SECTION N-N



PROJECT NO. R-0977A
CHEROKEE COUNTY
 STATION: 41+19.524-LREV-

SHEET 2 OF 6

| STATE OF NORTH CAROLINA DEPARTMENT OF TRANSPORTATION RALEIGH STANDARD | | | | SHEET NO. |
|--|-----|-------|-----|--------------|
| BRIDGE APPROACH SLAB FOR FLEXIBLE PAVEMENT WITH BARRIER RAIL | | | | S-167 |
| REVISIONS | | | | TOTAL SHEETS |
| NO. | BY: | DATE: | NO. | 230 |
| 1 | | | 3 | |
| 2 | | | 4 | |

ASSEMBLED BY : A.S. CALLAWAY DATE : 9/22/04
 CHECKED BY : P.C. BREWER DATE : 11/2/04
 DRAWN BY : RWW 8/01
 CHECKED BY : LES 8/01
 ADDED 12/01
 REV. 5/7/03R RWW/JTE