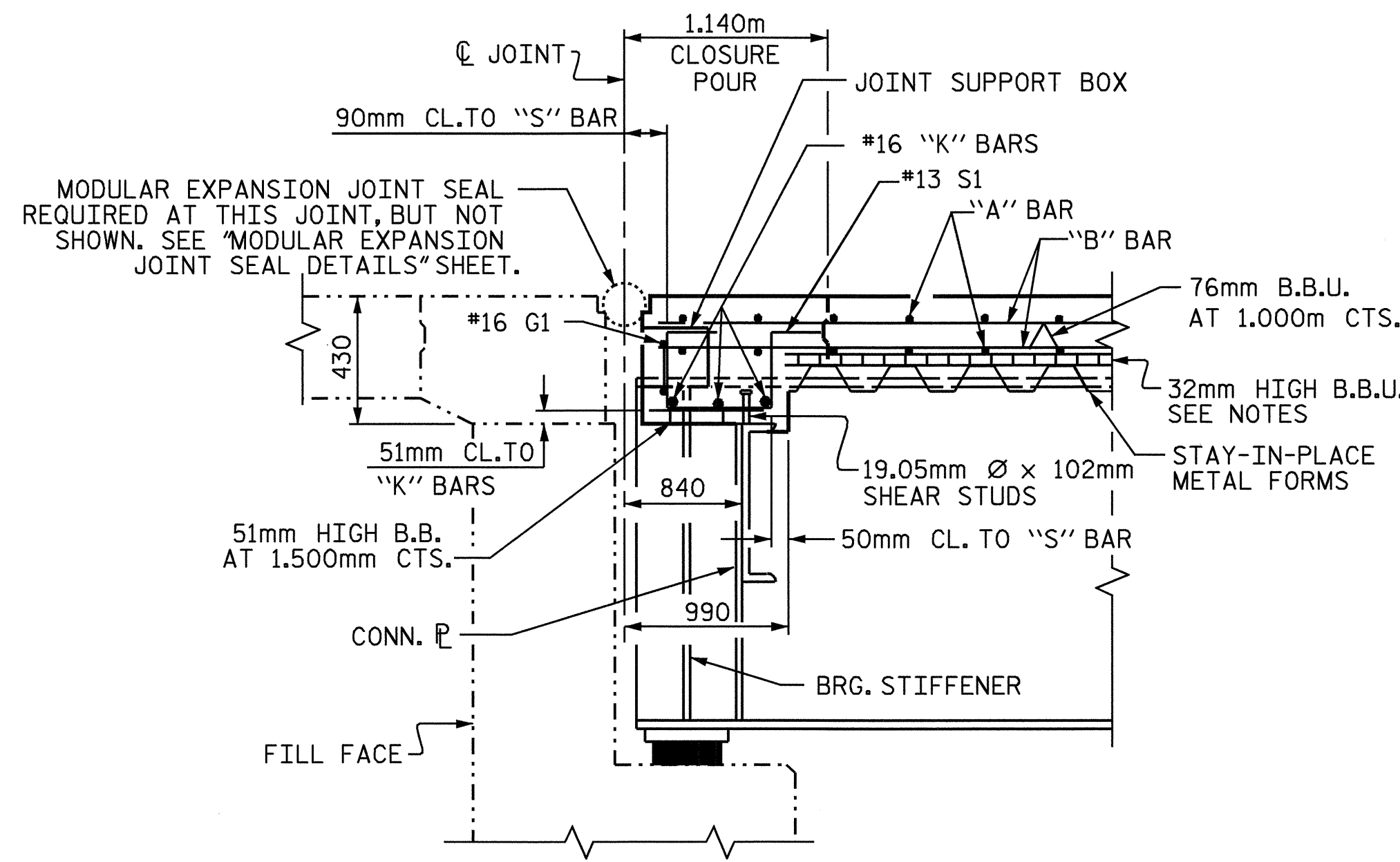


PART TYPICAL SECTION THRU END BENT 2 DIAPHRAGM

PART TYPICAL SECTION THRU END BENT 1 DIAPHRAGM

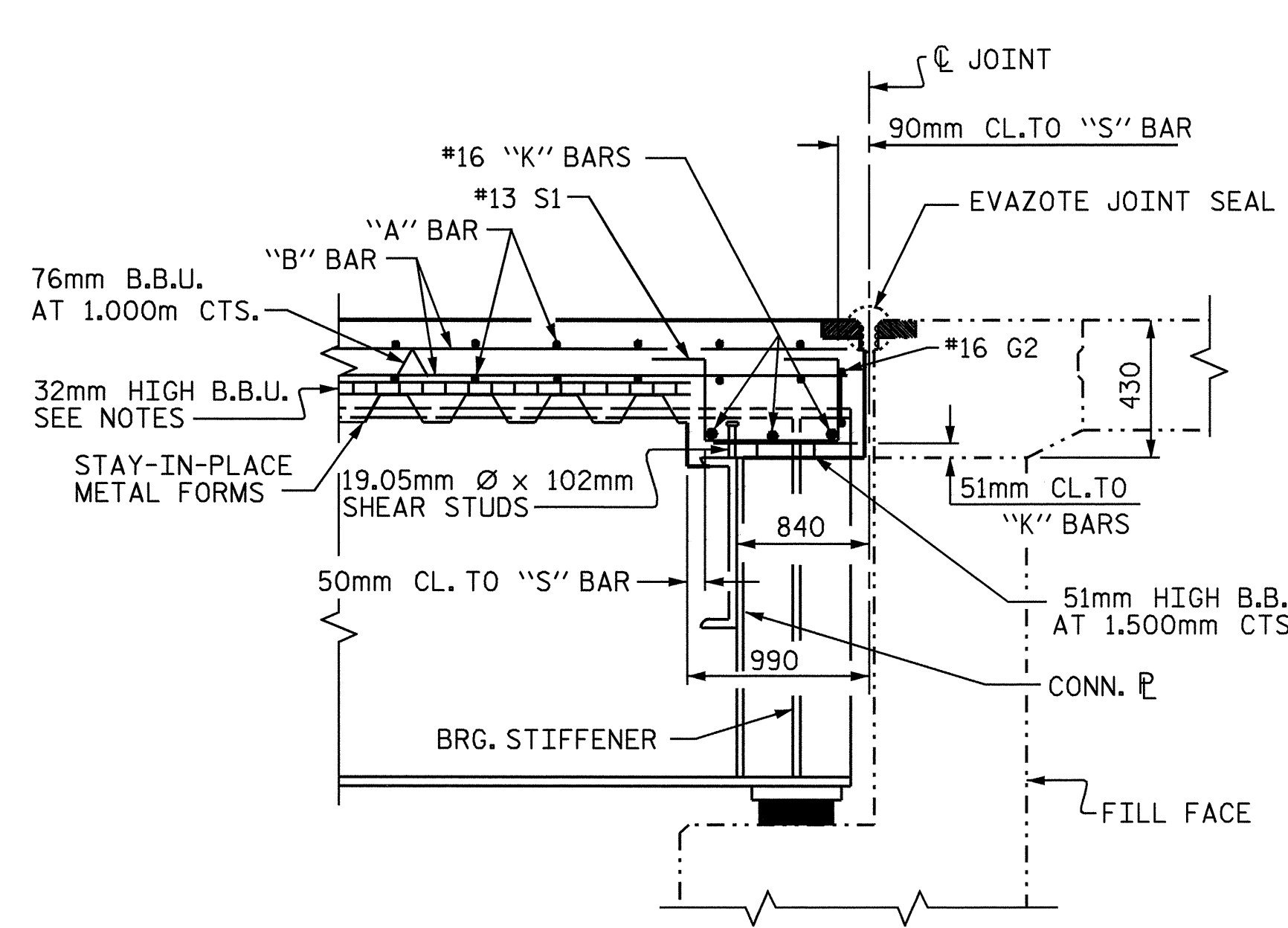
NOTES

- PROVIDE 32mm HIGH BEAM BOLSTERS UPPER AT 1.2m CTS. ATOP THE METAL STAY-IN-PLACE FORMS TO SUPPORT THE BOTTOM MAT OF 'A' BARS. WHEN USING REMOVABLE FORMS, PROVIDE CONTINUOUS HIGH CHAIRS FOR METAL DECK (C.H.C.M.) @ 1.2m CTS. WITH A HEIGHT TO SUPPORT THE BOTTOM MAT OF 'A' BARS A CLEAR DISTANCE OF 65mm ABOVE THE TOP OF THE REMOVABLE FORM.
- METAL STAY-IN-PLACE FORMS SHALL NOT BE WELDED TO BEAM OR GIRDER FLANGES IN THE ZONES REQUIRING CHАРY V-NOTCH TEST. SEE STRUCTURAL STEEL DETAIL SHEETS.
- PREVIOUSLY CAST CONCRETE IN A CONTINUOUS UNIT SHALL HAVE ATTAINED A MINIMUM COMPRESSIVE STRENGTH OF 20.7 MPa BEFORE ADDITIONAL CONCRETE IS CAST IN THE UNIT.
- BARRIER RAIL IN A CONTINUOUS UNIT SHALL NOT BE CAST UNTIL ALL SLAB CONCRETE IN THE UNIT HAS BEEN CAST AND HAS REACHED A MINIMUM COMPRESSIVE STRENGTH OF 20.7 MPa.
- STRUCTURAL STEEL ERECTION IN A CONTINUOUS UNIT SHALL BE COMPLETE BEFORE FALSEWORK OR FORMS ARE PLACED ON THE UNIT.
- THE CONTRACTOR MAY, WHEN NECESSARY, PROPOSE A SCHEME FOR AVOIDING INTERFERENCE BETWEEN METAL STAY-IN-PLACE FORM SUPPORTS OR FORMS AND GIRDER STIFFENERS OR CONNECTOR PLATES. THE PROPOSAL SHALL BE INDICATED, AS APPROPRIATE, ON EITHER THE STEEL WORKING DRAWINGS OR THE METAL STAY-IN-PLACE FORM WORKING DRAWINGS.
- USE SAND LIGHTWEIGHT CONCRETE IN THE DECK SLAB. SEE SPECIAL PROVISIONS FOR REINFORCED CONCRETE DECK SLAB (SAND LIGHTWEIGHT CONCRETE).
- CONCRETE SHALL BE EXCLUDED FROM THE METAL STAY-IN-PLACE FORM VALLEYS, SEE SPECIAL PROVISIONS FOR FABRICATED METAL STAY-IN-PLACE FORMS.
- SPECIAL SNOWPLOW PROTECTION IS REQUIRED AT END BENT 1. SEE SPECIAL PROVISION FOR MODULAR EXPANSION JOINT SEALS.
- THE 'B' BARS IN THE DECK SLAB AT END BENT 1 MAY BE CUT AS DIRECTED BY THE ENGINEER TO CLEAR THE MODULAR JOINT SUPPORT BOXES.



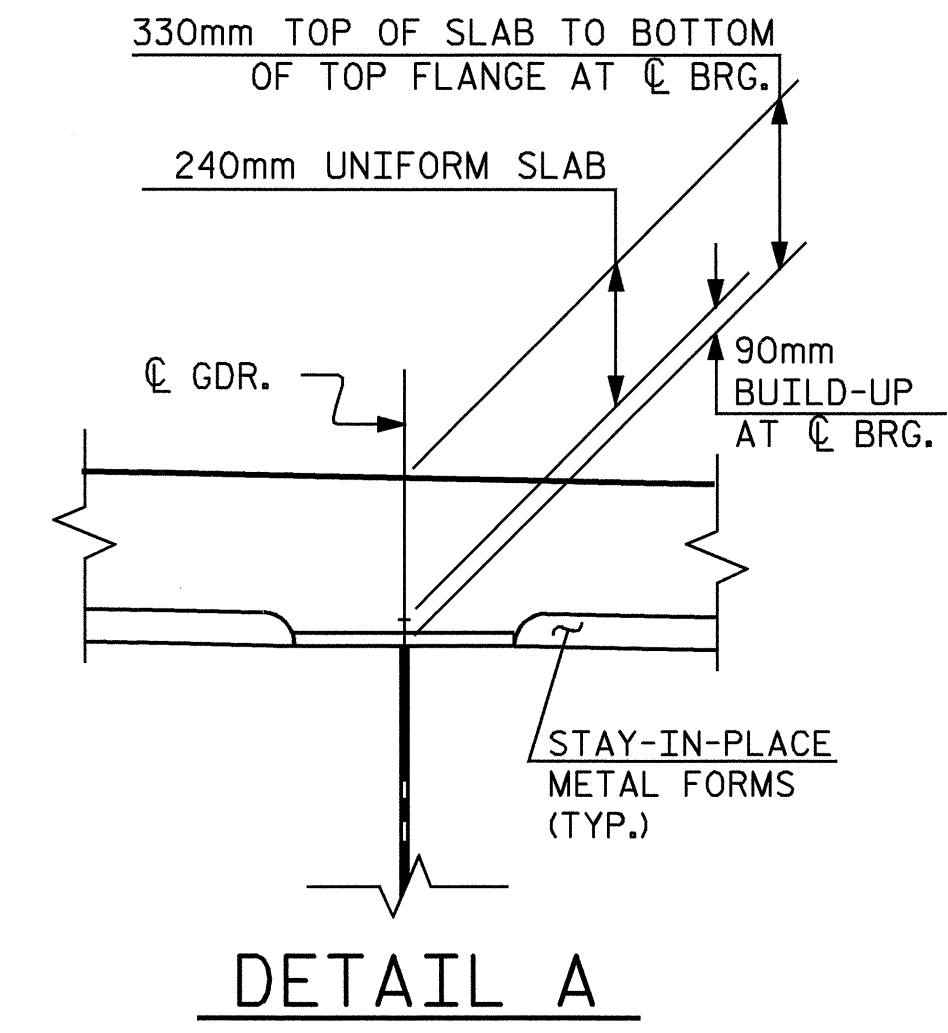
SECTION THRU END BENT 1

*16 G1 BAR MAY BE SHIFTED SLIGHTLY AS NECESSARY, TO CLEAR DIAPHRAGM AND REINFORCING STEEL.



SECTION THRU END BENT 2

*16 G2 BAR MAY BE SHIFTED SLIGHTLY AS NECESSARY, TO CLEAR DIAPHRAGM AND REINFORCING STEEL.

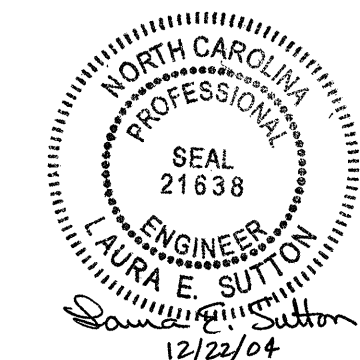


DETAIL A

PROJECT NO. R-0977A
 CHEROKEE COUNTY
 STATION: 16+02.300-LC1B-

SHEET 1 OF 2

STATE OF NORTH CAROLINA
 DEPARTMENT OF TRANSPORTATION
 RALEIGH
 SUPERSTRUCTURE
 TYPICAL SECTION



REVISIONS						SHEET NO.	
NO.	BY:	DATE:	NO.	BY:	DATE:	S-4	TOTAL SHEETS
1			3				230
2			4				

DRAWN BY: A.S. CALLAWAY DATE: 1/20/04
 CHECKED BY: B.N. BARODAWALA DATE: 6/17/04