

SITE 2

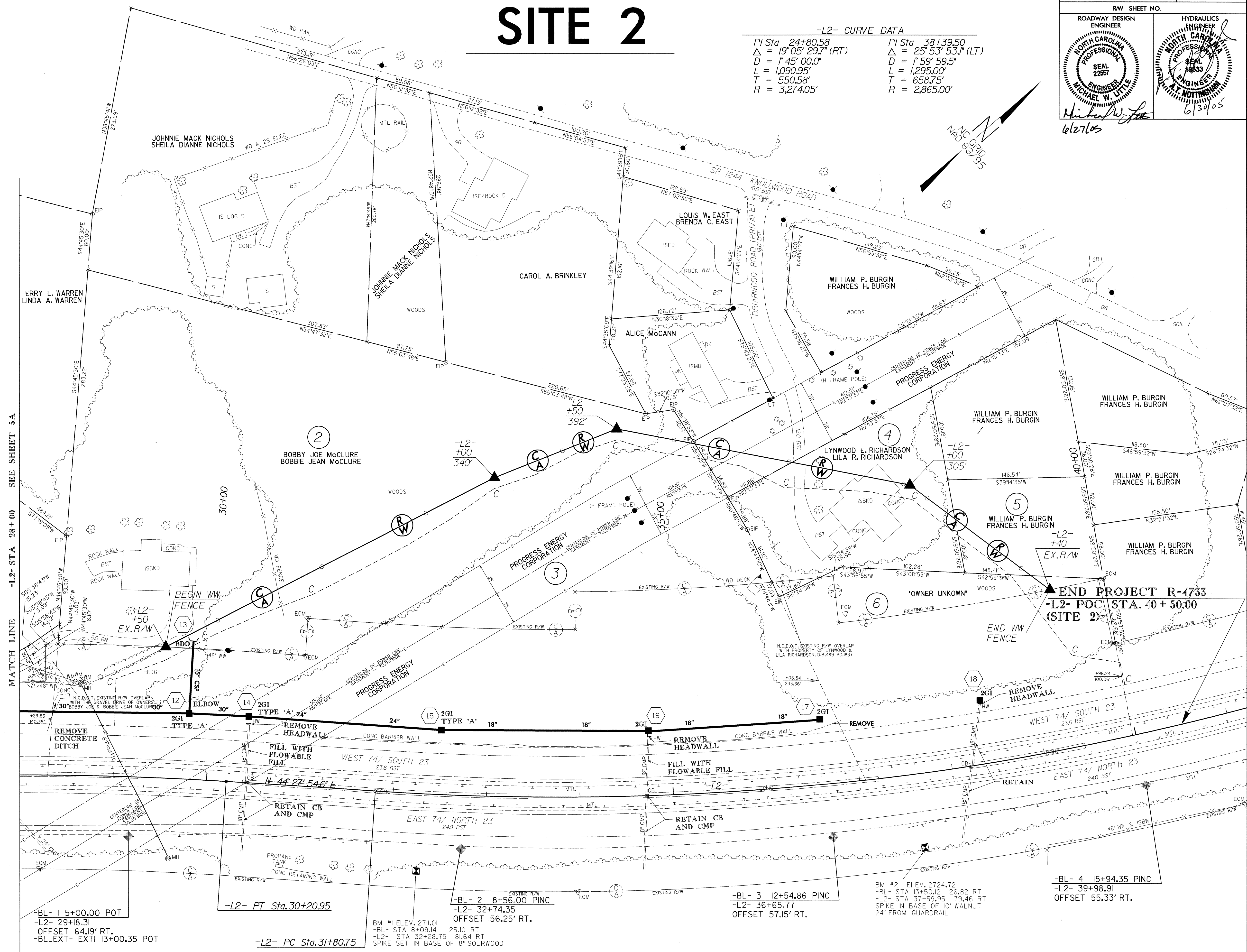
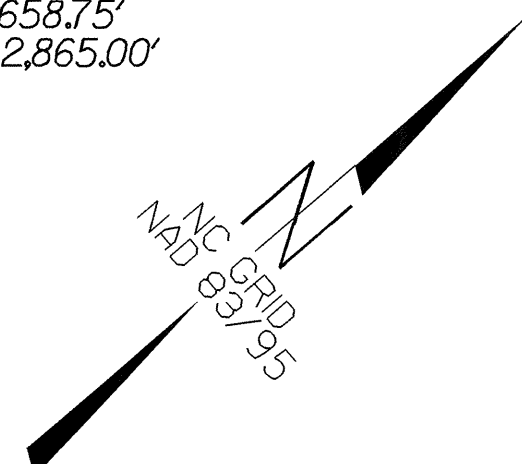
| | |
|----------------------------------------------|--------------------------------------|
| PROJECT REFERENCE NO. R-4733 | SHEET NO. 5 |
| RW SHEET NO. | |
| ROADWAY DESIGN ENGINEER MICHAEL W. LITTLE | HYDRAULICS ENGINEER T. MOTTINGHAM |

Seal: MICHAEL W. LITTLE, ENGINEER, SEAL 2257, NORTH CAROLINA PROFESSIONAL ENGINEERS, 6/27/05

Seal: T. MOTTINGHAM, ENGINEER, SEAL 1833, NORTH CAROLINA PROFESSIONAL ENGINEERS, 6/30/05

-L2- CURVE DATA

| | |
|---------------------------------------|---------------------------------------|
| PI Sta 24+80.58 | PI Sta 38+39.50 |
| $\Delta = 19^{\circ} 05' 29.7''$ (RT) | $\Delta = 25^{\circ} 53' 53.1''$ (LT) |
| D = 1'45' 00.0" | D = 1'59' 59.5" |
| L = 1,090.95' | L = 1,295.00' |
| T = 550.58' | T = 658.75' |
| R = 3,274.05' | R = 2,865.00' |



REVISIONS

03-JUN-2005 09:59
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 \$\$\$USERNAME\$\$\$

- BL- 1 5+00.00 POT
- L2- 29+18.31
- OFFSET 64.19' RT.
- BL-EXT- EXTI 13+00.35 POT
- L2- PT Sta.30+20.95
- L2- PC Sta.31+80.75
- BM #1 ELEV. 2711.01
- BL- STA 8+09.14 25.10 RT.
- L2- STA 32+28.75 81.64 RT
- SPIKE SET IN BASE OF 8' SOURWOOD
- BL- 2 8+56.00 PINC
- L2- 32+74.35
- OFFSET 56.25' RT.
- BL- 3 12+54.86 PINC
- L2- 36+65.77
- OFFSET 57.15' RT.
- BM #2 ELEV. 2724.72
- BL- STA 13+50.12 26.82 RT
- L2- STA 37+59.95 79.46 RT
- SPIKE IN BASE OF 10' WALNUT
- 24' FROM GUARDRAIL
- BL- 4 15+94.35 PINC
- L2- 39+98.91
- OFFSET 55.33' RT.