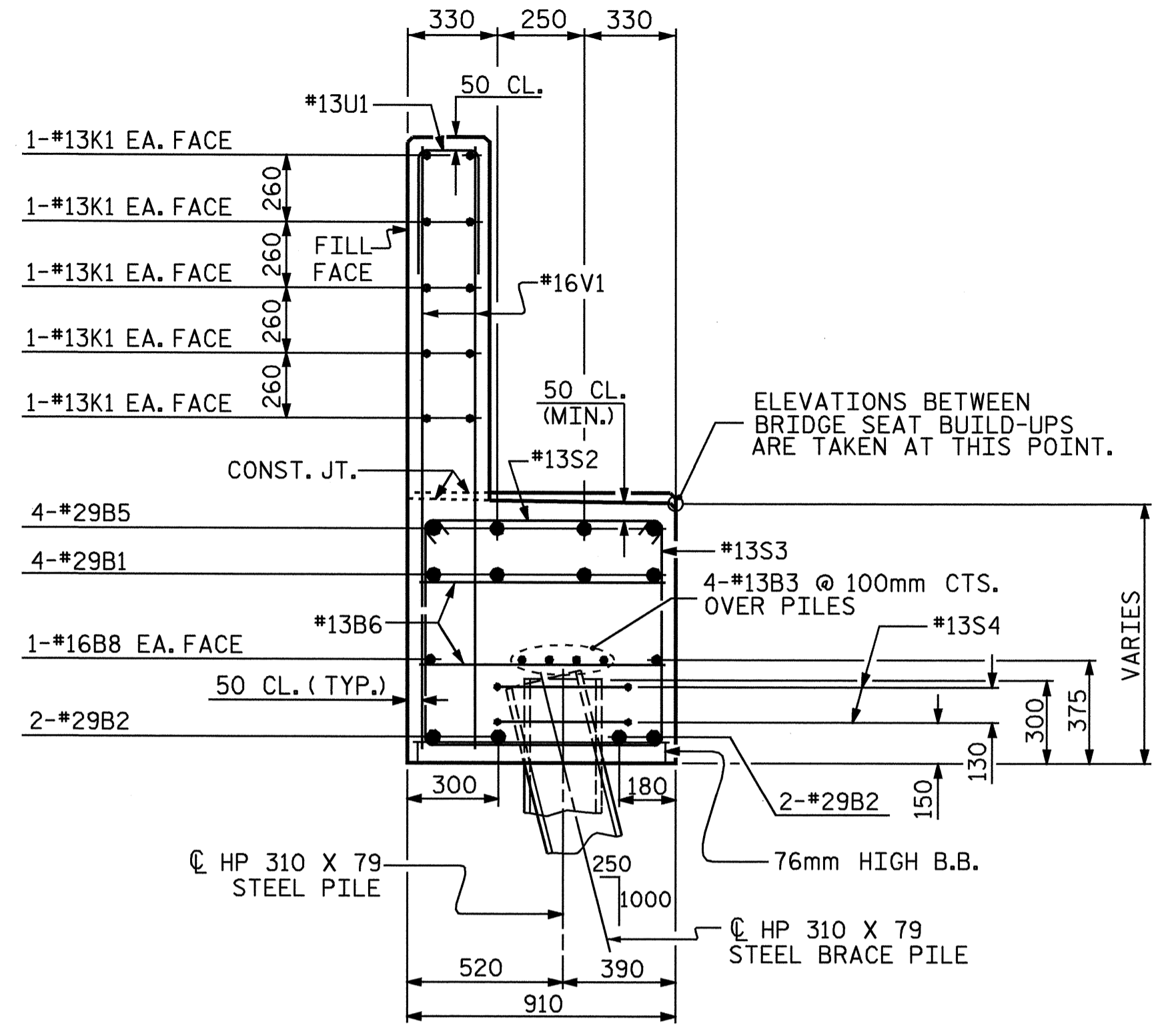
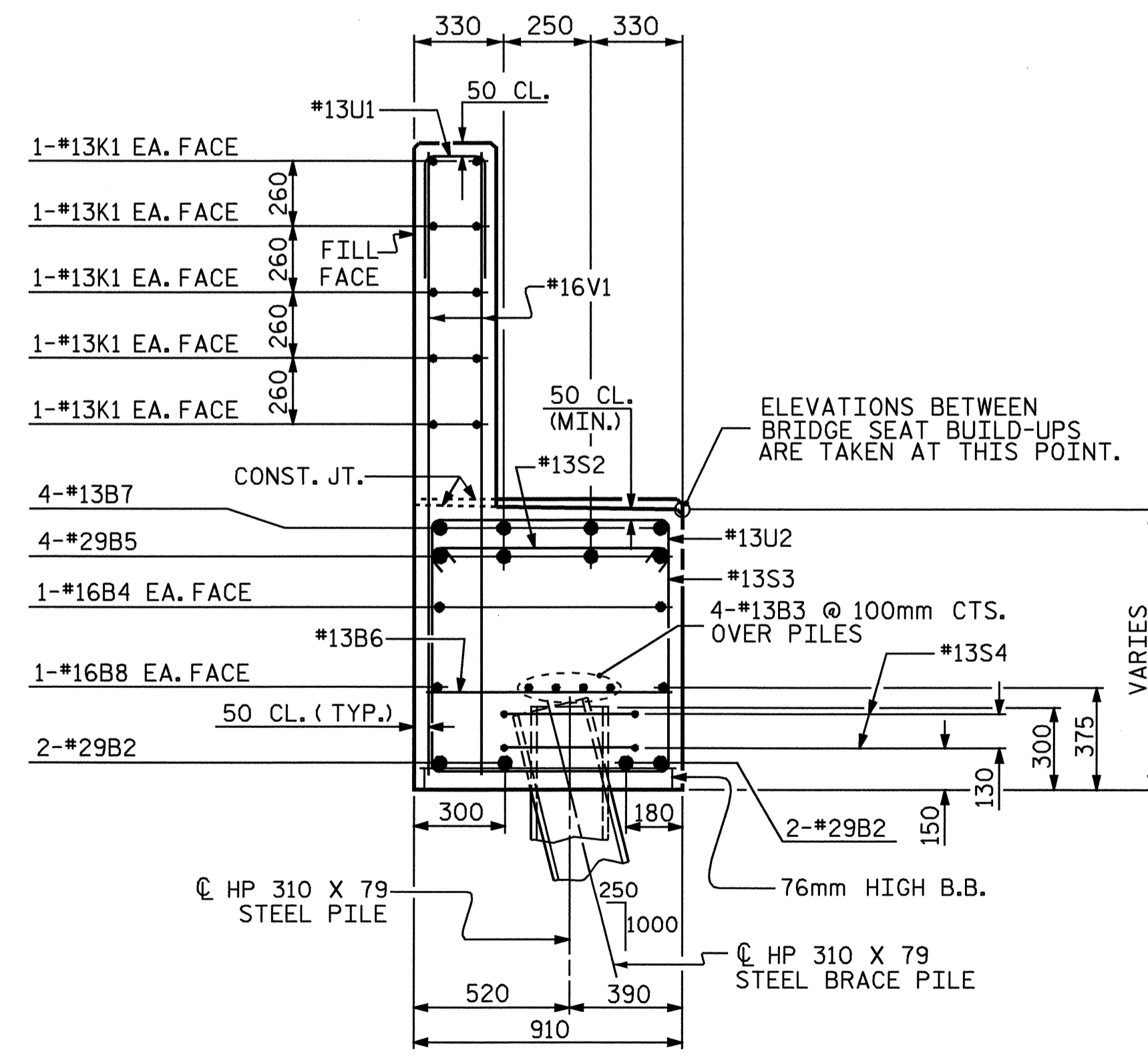


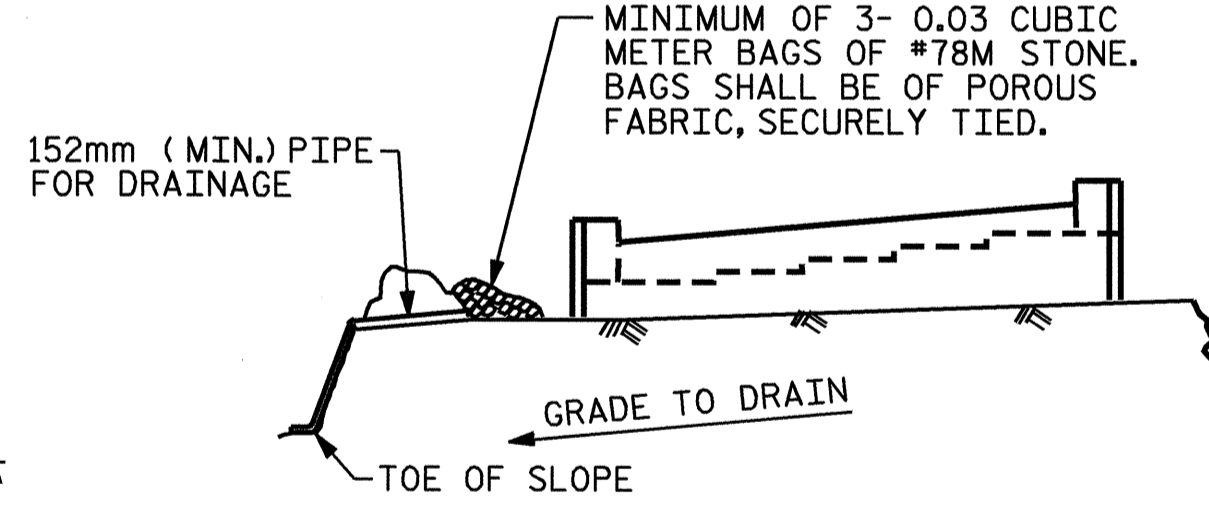
SECTION A-A



SECTION B-B



SECTION C-C



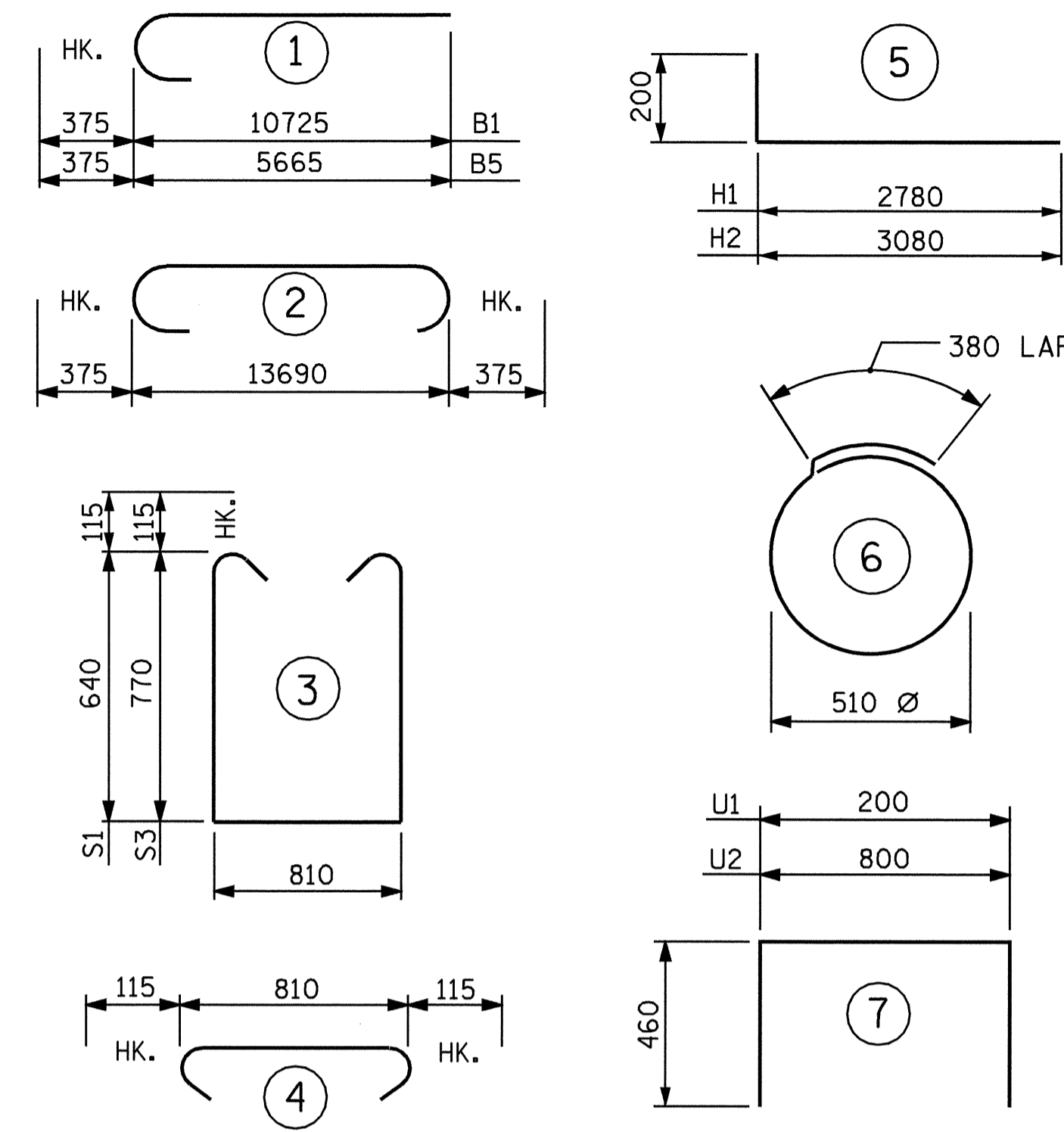
BAGGED STONE AND PIPE SHALL BE PLACED IMMEDIATELY AFTER COMPLETION OF END BENT EXCAVATION. PIPE MAY BE EITHER CONCRETE, CORRUGATED STEEL, CORRUGATED ALUMINUM ALLOY, OR CORRUGATED PLASTIC. PERFORATED PIPE WILL NOT BE ALLOWED.

BAGGED STONE SHALL REMAIN IN PLACE UNTIL THE ENGINEER DIRECTS THAT IT BE REMOVED. THE CONTRACTOR SHALL REMOVE AND DISPOSE OF SILT ACCUMULATIONS AT BAGGED STONE WHEN SO DIRECTED BY THE ENGINEER. BAGS SHALL BE REMOVED AND REPLACED WHENEVER THE ENGINEER DETERMINES THAT THEY HAVE DETERIORATED AND LOST THEIR EFFECTIVENESS.

NO SEPARATE PAYMENT WILL BE MADE FOR THIS WORK AND THE ENTIRE COST OF THIS WORK SHALL BE INCLUDED IN THE UNIT CONTRACT PRICE BID FOR THE SEVERAL PAY ITEMS.

TEMPORARY DRAINAGE AT END BENT

BAR TYPES



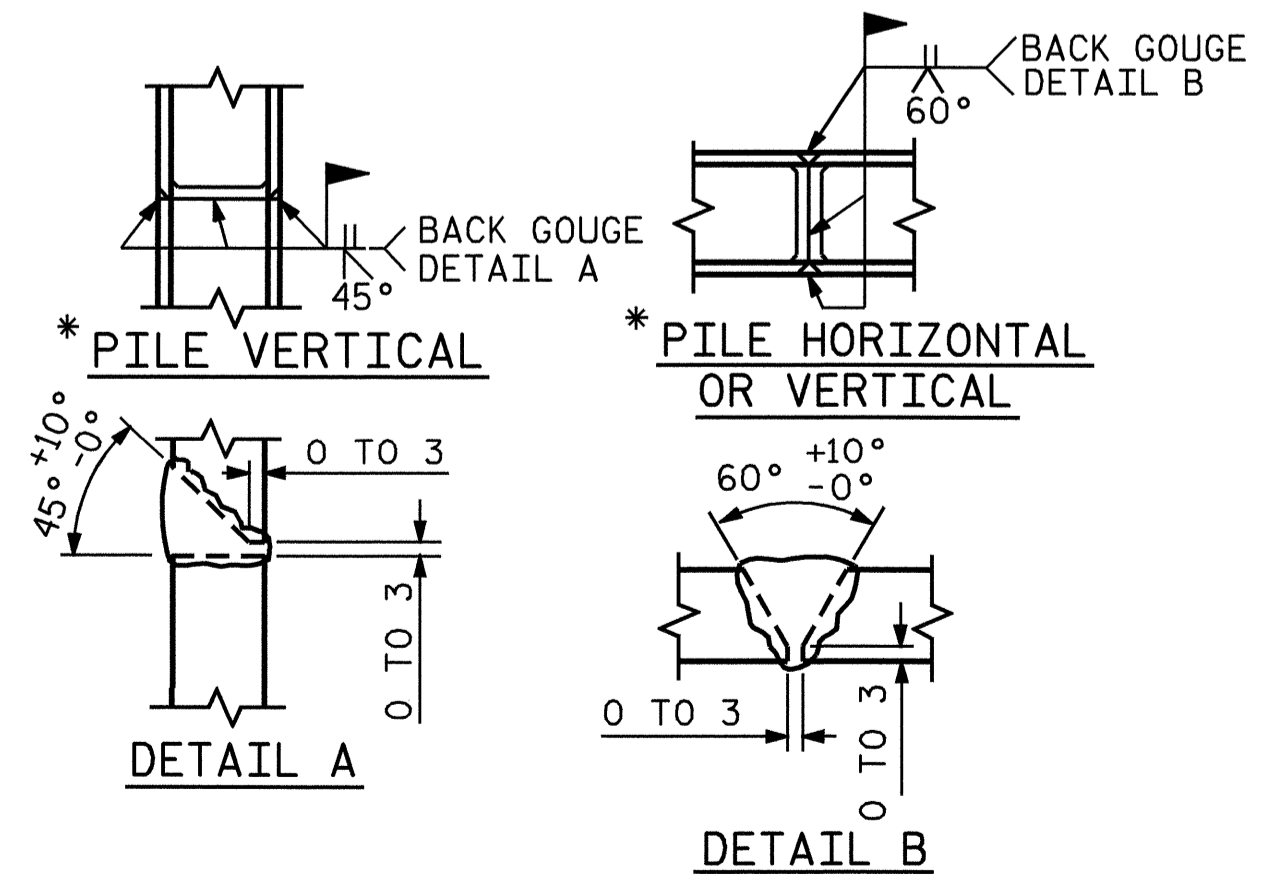
ALL BAR DIMENSIONS ARE OUT TO OUT.

BILL OF MATERIAL

END BENT #2						
BAR	NUM	SZ	TYP	LENGTH	WEIGHT	
B1	4	29	1	11100	225	
B2	4	29	2	14440	292	
B3	8	13	STR	7220	57	
B4	2	16	STR	3940	12	
B5	4	29	1	6040	122	
B6	15	13	STR	800	12	
B7	8	13	STR	900	7	
B8	2	16	STR	13700	43	
H1	20	16	5	2980	92	
H2	22	16	5	3280	112	
K1	20	13	STR	7220	144	
K2	8	16	STR	880	11	
S1	47	13	3	2320	108	
S2	80	13	4	1040	83	
S3	33	13	3	2580	85	
S4	14	13	6	1980	28	
U1	40	13	7	1120	45	
U2	6	13	7	1720	10	
V1	80	16	STR	1860	231	
V2	28	16	STR	2220	96	
V3	30	16	STR	2440	114	
REINFORCING STEEL					KG.	1,929
CLASS A CONCRETE BREAKDOWN						
POUR #1 (CAP & LOWER WINGS)					12.2	m ³
POUR #2 (BACKWALL & UPPER WINGS)					8.0	m ³
TOTAL					20.2	m ³
HP 310 X 79 STEEL PILES						
NO.	7	METERS			85.4	

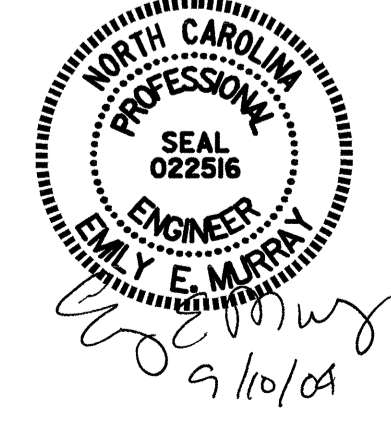
DRAWN BY: M. M. PARSONS DATE: 2/10/04
 CHECKED BY: C. MILLER DATE: 3-04

03-SEP-2004 14:24
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 dorutcher



PILE SPLICE DETAILS

* POSITION OF PILE DURING WELDING.



PROJECT NO. R-2206C
 LINCOLN-CATAWBA COUNTY
 STATION: 256+91.000 -L-

SHEET 3 OF 3

STATE OF NORTH CAROLINA
 DEPARTMENT OF TRANSPORTATION
 RALEIGH
 SUBSTRUCTURE
 END BENT #2
 LEFT LANE

REVISIONS				SHEET NO.	
NO.	BY:	DATE:	NO.	BY:	DATE:
1			3		
2			4		
TOTAL SHEETS					374