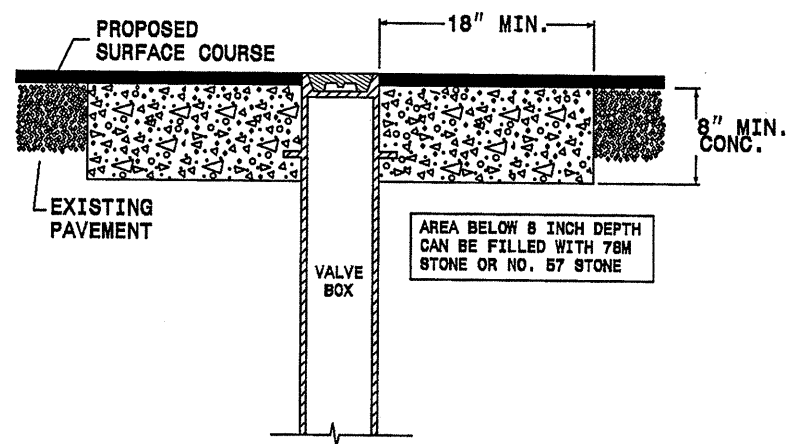


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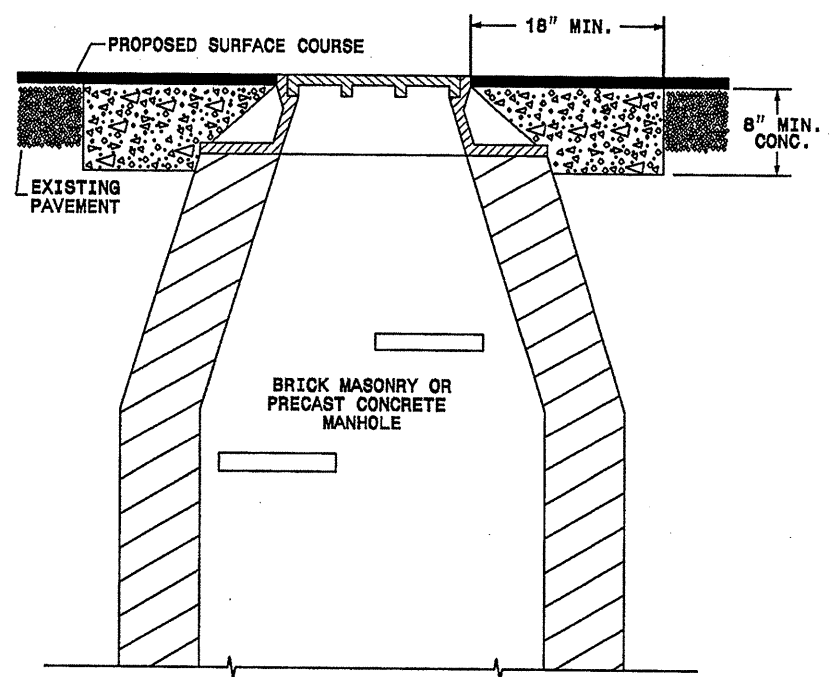
STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
N.C.	7CR.10171.5, ETC.	10	16

- 7CR.10171.5
- 7CR.10791.5
- 7CR.20171.5
- 7CR.20791.5
- 7CR.307915.5
- 7CR.307918.5
- 7CR.307933.5

**STANDARD CONCRETE ENCASEMENT FOR MANHOLE & VALVE CASTINGS IN PAVEMENT
DETAIL DRAWING NO. 858.01**

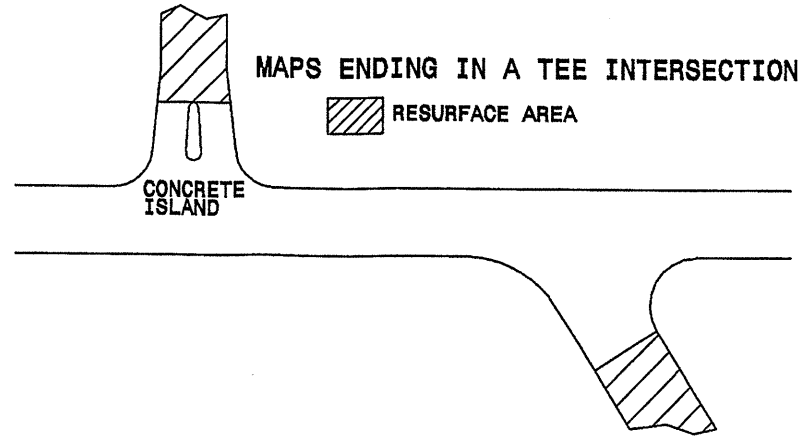


USE RAPID SET GROUT, MORTAR, OR CONCRETE CLASS B CONCRETE MAY BE USED WHEN ADJUSTMENTS ARE NOT IN THE TRAVEL LANE.

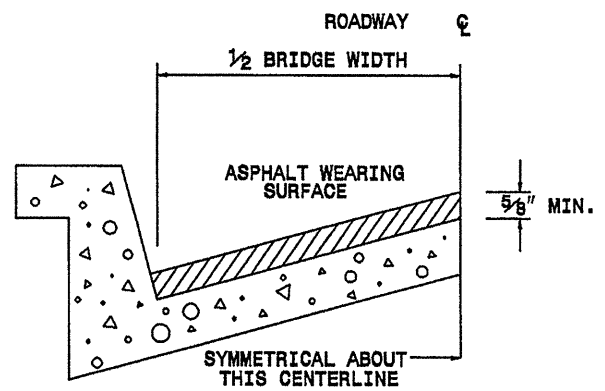
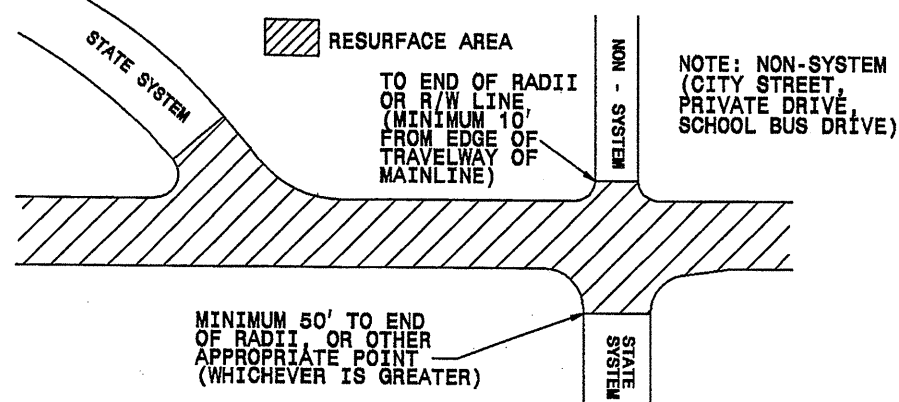


- NOTES:**
- MORTAR SHALL BE MIXED TO NCDOT SPECIFICATIONS.
 - ALL FAULTY EXISTING BRICKWORK TO BE REMOVED AND REPLACED WITH NEW BRICK MASONRY.
 - EXCAVATION FOR THE ADJUSTMENT SHALL BE SHEER CUT ON ALL SIDES.
 - RAPID SET GROUT, MORTAR, OR CONCRETE SHALL BE USED

**PAVING DETAIL 1
MAIN LINE IS NOT BEING RESURFACED**



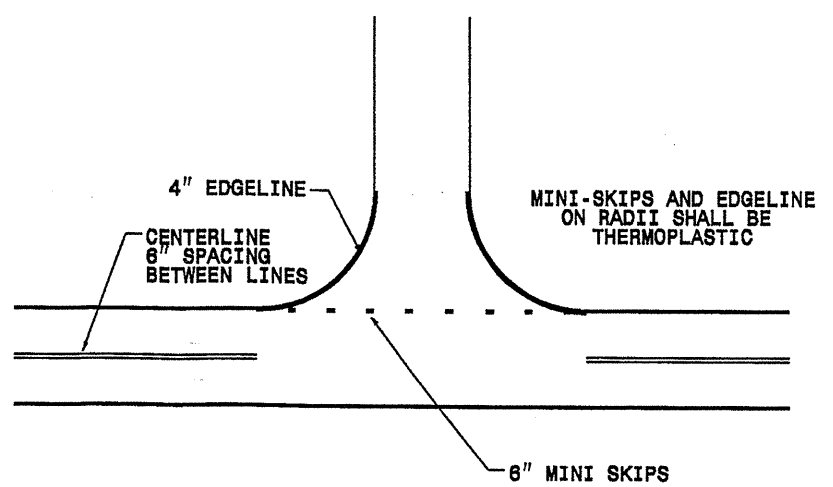
**PAVING DETAIL 2
MAIN LINE IS BEING RESURFACED**



BRIDGE HALF TYPICAL SECTION

FOR BRIDGES WITH FLOOR DRAINS, CARE SHALL BE EXERCISED IN PLACING THE WEARING SURFACE AROUND FLOOR DRAINS SO AS NOT TO HINDER EFFECTIVE DRAINAGE. ALL DRAINS SHALL BE LEFT OPEN. THE PROPOSED WEARING SURFACE SHALL VARY IN THICKNESS AS NECESSARY TO PROVIDE A SMOOTH RIDING SURFACE. A THICKNESS OF NOT LESS THAN 5/8" SHALL BE PROVIDED. THE MAXIMUM THICKNESS SHALL PREFERABLY BE 1-1/2" UNLESS IT IS IMPRACTICAL TO PROVIDE A SMOOTH RIDING SURFACE OTHERWISE.

**TO BE USED AT ALL
NON-SIGNALIZED INTERSECTIONS
(NOT TO SCALE)**



NOTE: MINI SKIPS SHALL BE PLACED ON A 10' CYCLE, CONTAINING AN 8' AND 2' SKIP, THE WIDTH OF THE SKIP SHALL BE 6".

15-SEP-2004 15:28 Resurfacing 2005\Typicals_Rockingham-2005.dgn